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Appendix B

Terrestrial Natural Features Memo



Technical Memorandum

Terrestrial Assessment

Date: Feburary 2023Feburary 2023 **Project No.:** 300053425.0000

Project Name: Oxford Road 19 Class Environmental Assessment

Client Name: Oxford County

Submitted To: Avid Banihashemi

Submitted By: Sarah Yoshida

Reviewed By: Kevin Butt, B.Sc. (Env.) Rest Cert, TRAQ, ISA Certified Arborist & Terrestrial Ecologist

1.0 Introduction

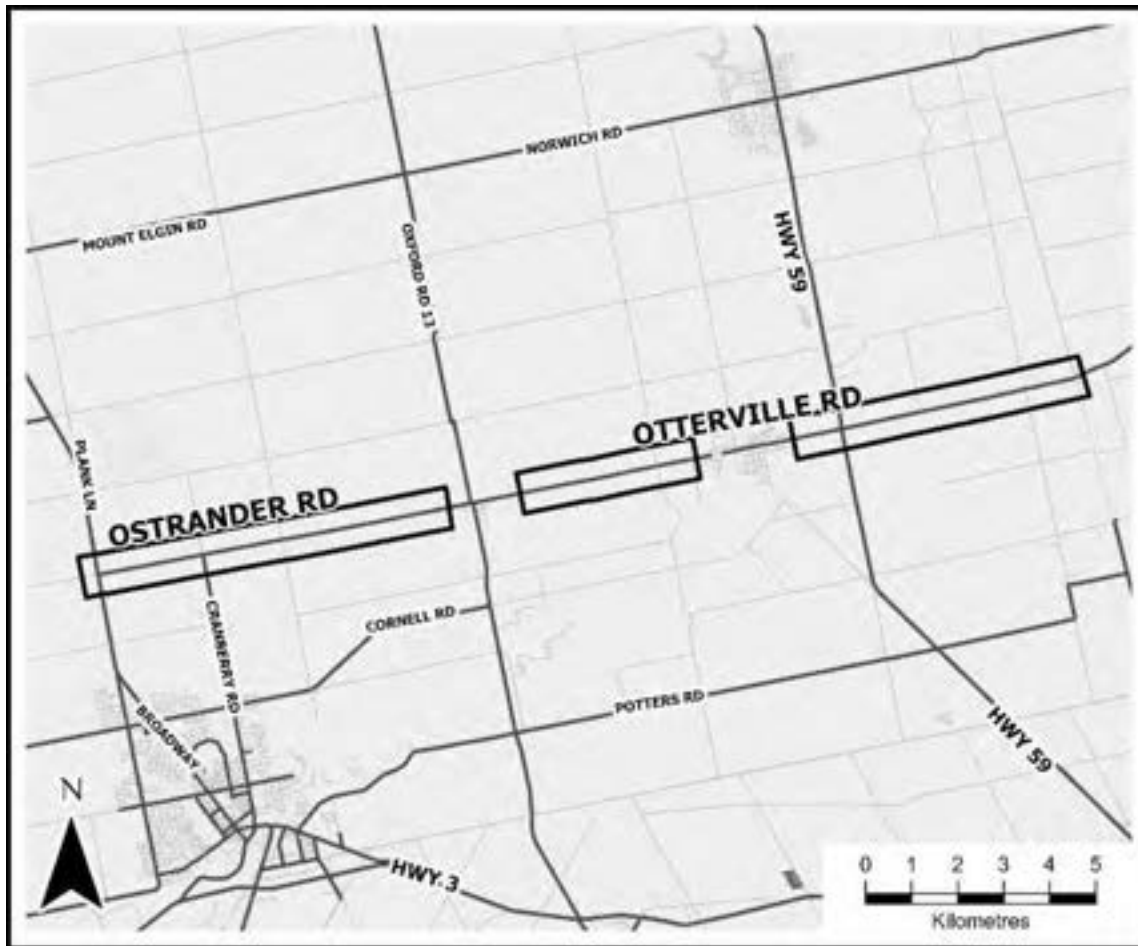
R.J. Burnside & Associates (Burnside) was retained by the County of Oxford (the County) to undertake a Municipal Class Environmental Assessment (MCEA) to assess deficiencies associated with Oxford Road 19 between Highway 19 and the Norfolk County boundary and recommend improvements.

Following completion of Oxford County's 2019 Transportation Master Plan, the County has identified the need to improve Oxford Road 19 to support the safe. Accordingly, the County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider

improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond efficient movement of people and goods.

In support of the Environmental Study Report (ESR), Burnside's Terrestrial Ecologist undertook field surveys to characterize the terrestrial habitats within 120 m of the existing right-of-way (ROW) (the Study Area) that may be impacted by road improvements (Figure 1). The following memo describes the terrestrial habitat present adjacent to the ROW and the surrounding areas.

Figure 1: Study Area



2.0 Background Information Review

The following sources of background information were reviewed to identify ecological features which may be impacted by the proposed works:

- Aerial photography;
- MNR Natural Heritage Information Centre (NHIC) database for significant species and designated natural features within 120 m of the subject lands;
- Draft 2016 Oxford Natural Heritage Systems Study (ONHSS);
- The Ontario Reptile and Amphibian Atlas (ORAA);
- The Ontario Breeding Bird Atlas (OBBA); and
- Ontario Ministry of Agriculture, Food and Rural Affairs (OMAFRA), AgMaps mapping (2021).

3.0 Existing Conditions

3.1 Methodology

Field Surveys were conducted on June 10, 2022. During the field investigations, information was collected about the plant species encountered at the Study Area. Vegetation communities were characterized using the Ecological Land Classification (ELC) system at the ecosite level for the Study Area using protocols outlined in Lee *et al.* (1998)¹. Specific ecosites (based on the 2008 ELC approximations²) were determined based on collected data that best represent each distinct ecological unit.

Surveys were conducted primarily from the ROW for portions of the Study Area not in public ownership. Searches were also conducted for wildlife habitat features.

3.2 Environmental Land Classification

A total of 31 ecosites were documented within the Study Area. A variety of vegetation communities including terrestrial, aquatic, wetland, agricultural, and constructed ecosites were observed (Photos 1 – 7).

¹ Lee, H.T, W.D. Bakowsky, J.L. Riley, J. Bowles, M. Puddister, P. Uhlig, S. McMurray. 1998. Ecological Land Classification for Southern Ontario: First Approximation and its Application. Ontario Ministry of Natural Resources, Southcentral Region, Science Development and Transfer Branch. Technical Manual ELC-005.

² Lee, H.T. 2008. Southern Ontario Ecological Land Classification: Vegetation Type List. Ontario Ministry of Natural Resources, London, Ontario.



Photo 1: MAMM1-2 (Cattail Graminoid Meadow Marsh) ecosite.



Photo 2: Example of the MEGM3-5 (Smooth Brome Graminoid Meadow Type) and TAGM5 (hedgerow) ecosites present throughout the corridor.



Photo 3: Example of the THDM2-1 (Sumac Deciduous Shrub Thicket Type) ecosite



Photo 4: The WODM4-7 (Moist-Fresh Black Walnut Deciduous Forest type) and SAF_1-3 (Duckweed Floating-leaved Shallow Aquatic Type) ecosites



Photo 5: FODM7-7 (Fresh - Moist Manitoba Maple Lowland Deciduous Forest Type) adjacent to the MEGM3-5 and OAO (Open Water) ecosites



Photo 6: Example of the FOD8-3 (Fresh - Moist Cottonwood Deciduous Forest Type) ecosite



Photo 7: Representative example of the FODM5-1 (Dry – Fresh Sugar Maple Deciduous Forest Type) ecosite.

A description of the ecosites, along with the dominant and associated species of each ecosite are provided in Attachment A. Delineations of the vegetation communities are illustrated on Figure A-1 in Appendix A to the main MCEA report.

One rare vegetation communities (as identified in the SWH Technical Guide), Moist-Fresh Black Walnut Deciduous Forest types (FOD7-4) were found within the Study Area. It is unlikely that this ecosite is naturally occurring. Instead, it is likely that the composition similarity is a consequence of cultural influence such as historical clearing activities and shoreline disturbances. This interpretation is further supported by the ecosite's proximity to the Otter Creek Golf Club and the village of Otterville.

3.3 Incidental Wildlife Observations

Incidental wildlife sightings were limited to the Study Area and were documented during field investigations to provide a general characterization of the habitat functions of the Study Area. Incidental observations were those recorded during targeted surveys for other aquatic or terrestrial investigations. Examples include tracks, carcasses, live sightings, etc.

A summary of incidental wildlife observations can be found in Table 1 below:

Table 1. Summary of Incidental Wildlife Observations

Common Name	Scientific Name	Location / ELC community	Comments
Tree Swallow	<i>Tachycineta bicolor</i>	MAMM1-2	Observed nesting within standing snag

Common Name	Scientific Name	Location / ELC community	Comments
Barn Swallow	<i>Hirundo rustica</i>	OAG	Fly-over.
Cliff Swallow	<i>Petrochelidon pyrrhonota</i>	OAO	Observed nesting on bridge over Spittler Creek
Red-winged Blackbird	<i>Agelaius phoeniceus</i>	MEG / MAMM ecosites	Frequently observed breeding throughout the Study Area
Eastern Kingbird	<i>Charadrius vociferus</i>	THDM2-1	Fly-over
Killdeer	<i>Tyrannus tyrannus</i>	THDM2-1	Fly-over
Yellow Warbler	<i>Setophaga petechia</i>	FODM7-4	Overheard calling
Baltimore Oriole	<i>Icterus galbula</i>	FODM7-4	Overheard calling
American Goldfinch	<i>Spinus tristis</i>	FODM7-4	Overheard calling
Turkey Vulture	<i>Cathartes aura</i>	MEGM3-5	Fly-over
Green Frog	<i>Lithobates clamitans</i>	SAF_1-3	

4.0 Natural Heritage Features

4.1 Provincially Significant Wetlands (PSW)

One PSW, the Otterville Complex BOC 7, is present within the Study Area. The Otterville Complex BOC 7 is found in association with Otter Creek. A small portion of the wetland extends into the Study Area just west of Otterville.

4.2 Significant Woodlands

Currently, mapping of Significant Woodlands has not been included within the County of Oxford's OP. Instead, the identification of significant woodlands is identified through the development process.

In line with the PPS, the OP specifies that Significant Woodlands are identified based on the following characteristics:

- The size of the feature;
- The occurrence of other significant features;
- The provision of important ecological functions such as biodiversity, linkage, buffering, or water quality;
- The composition, age, or site quality results in a feature which is uncommon to the County; and
- Woodland economic and social values.

Criteria for Significant Woodlands are outlined within the Draft Oxford County Natural Heritage Study (ONHSS). The criteria for significance include:

- Located within or touching a Significant Valleyland;
- Located within or touching a provincial or regional Life Science ANSI;
- Located within 30 m of an open watercourse;
- Are woodlands vegetation group ≥ 4 ha;
- Are located within 100 m of a ≥ 4 ha Woodland Vegetation Group; and,
- Are classified as Groundwater Dependent Wetlands and Ecosystems.

Based on the above criteria, the following ELC communities, as shown in Attachment B, are, from west to east, considered to be significant woodlands:

- The Dry-Fresh Sugar Maple Deciduous Forest (FODM5-1) along Spitler Creek;
- The Dry-Fresh Sugar Maple Deciduous Forest (FODM5-1) west of Zenda Line; and,
- The Fresh-Moist Cottonwood Deciduous Forest (FODM8-3) along Spitler Creek between Springford and Otterville.

Minor encroachment into Significant Woodlands may occur as a result of the proposed road improvements.

4.3 Significant Valleylands

Significant Valleylands are found in association with the Big Otter Creek which flows through the community of Otterville. Impacts to this feature is not anticipated.

4.4 Areas of Natural Scientific Interest

There are no Areas of Natural and Scientific Interest (ANSI) identified within the Study Area. ANSIs are not present within the Study Area.

4.5 Significant Habitat of Species at Risk

Two (2) species at risk (SAR), Barn Swallow (*Hirundo rustica*; SC) and Butternut (*Juglans cinerea*; END) were incidentally observed during field studies. The locations of SAR occurrences can be found on Figure A-1 in Appendix A to the main MCEA report.

Barn Swallows were observed foraging within agricultural fields. Barn Swallows often nest within old, open barn structures. No suitable structures were observed, although some may be set back from the road and were not clearly visible. No barns are expected to be disturbed or removed for the road improvements. Barn Swallows can also nest in culverts or on bridges. No nests were present on bridges or culverts at the time of field investigations. Consideration should be provided for this species during the detailed design phase as this species is known to nest within bridges and culverts. This species was recently downlisted to Special Concern and

no longer receives habitat protection under the *Endangered Species Act* although nests of this species still receives protection under the *Migratory Birds Convention Act*.

A single Butternut was observed along the margin of ecosite FOD7-4 on the south side of the ROW. Additional surveys including a Butternut Health Assessment may be required if it is anticipated that the specimen will be impacted by road improvements.

Additional species at risk could be present which were unobserved at the time of the site visit. To identify potential Habitat of Endangered and Threatened Species, a review of aerial photography, the Ontario Reptile and Amphibian Atlas, Ontario Breeding Bird Atlas and Natural Heritage Information Centre on-line databases was conducted. Based on the conditions observed within the study area, it is anticipated that the thicket, forest, wetland, and open water ecosites may support terrestrial SAR species. Results are presented in Table 2. A more detailed summary is provided in Attachment B.

Table 2 Habitats for Species at Risk in the Study Area

Species	Designation under the Endangered Species Act	Potential Habitat
Endangered and Threatened Species		
American Chestnut (<i>Castanea dentata</i>)	Endangered	Woodlands. Not observed from roadside but could be present in woodlands away from roadside on private properties.
Red-headed Woodpecker (<i>Melanerpes erythrocephalus</i>)	Endangered	Woodlands along Spitler Creek and the Tributary of the Big Otter Creek.
Little Brown Myotis (<i>Myotis lucifugus</i>)	Endangered	Woodlands along Spitler Creek and the Tributary of the Big Otter Creek. May also use individual trees outside of woodlands for maternal roosting.
Northern Myotis (<i>Myotis septentrionalis</i>)	Endangered	Woodlands along Spitler Creek and the Tributary of the Big Otter Creek. May also use individual trees outside of woodlands for maternal roosting.
Tri-colored Bat (<i>Perimyotis subflavus</i>)	Endangered	Woodlands along Spitler Creek and the Tributary of the Big Otter Creek. May also use individual trees outside of woodlands for maternal roosting.
Blanding's Turtle (<i>Emydoidea blandingii</i>)	Endangered	Suitable overwintering habitat may be present in the open water pond within the

Species	Designation under the Endangered Species Act	Potential Habitat
		woodland along Spitler Creek between Springford and Otterville. Suitable overwintering habitat may also be present in in a small pond just west of the golf course and Otterville along the Big Otter Creek Tributary. Turtles may use watercourses and upland forests areas adjacent to wetlands as movement corridors.
Special Concern Species		
Crooked-stem aster (<i>Symphyotrichum prenanthoides</i>)	Special Concern	Woodlands.
Canada Warbler (<i>Cardellina canadensis</i>)	Special Concern	Small woodland (FODM7-7) north of Spitler Creek and narrow woodlands along tributary of the Big Otter Creek west of the golf course. Both are moist lowland forests with dense understory layers.
Eastern Wood-pewee (<i>Contopus virens</i>)	Special Concern	Woodlands.
Golden-winged Warbler (<i>Vermivora chrysoptera</i>)	Special Concern	Shrub thicket communities along Spitler Creek.
Wood Thrush (<i>Hylocichla mustelina</i>)	Special Concern	Woodlands.
Snapping Turtle (<i>Chelydra serpentina</i>)	Special Concern	Suitable overwintering habitat may be present in the open water pond within the woodland along Spitler Creek between Springford and Otterville. Suitable overwintering habitat may also be present in in a small pond just west of the golf course and Otterville along the Big Otter Creek Tributary. Turtles may use watercourses as a movement corridor.

4.6 Significant Wildlife Habitat (SWH)

The Natural Heritage Reference Manual (MNR, 2010), Significant Wildlife Habitat Technical Guide (MNR, 2000) and Ecoregion 7E Criteria Schedules list a number of different types of habitat that are considered significant. A screening of potential habitats in the Study Area relative to

SWH criteria is provided in Attachment C. In summary the following types of habitats may be present:

- Bat Maternity Colonies;
- Turtle Wintering Areas;
- Amphibian Breeding Habitat (Woodland); and
- Habitat for Special Concern and Rare Wildlife.

In addition, a rare vegetation community was identified. This is a Moist-Fresh Black Walnut Deciduous Forest types (FOD7-4) located in proximity to the Otter Creek Golf Club. As noted in Section 3.2, it is unlikely that this ecosite is naturally occurring and therefore, it is not considered to be significant.

The other habitats identified are characterized as “Candidate Significant Wildlife Habitat.” Detailed wildlife surveys were not carried out and thus the use of these habitats by significant populations or species of wildlife are not known. For the purpose of this study, these habitats are assumed to be significant.

A description of each candidate habitat is provided below.

Bat Maternity Colonies (BMC)

All woodlands within Ontario have the potential to provide suitable habitat for bat maternity colonies for Ontario's 8 bats species. Characteristics of forest lands that best support bat maternity colonies include mature to old-growth forest stands with >10 /ha large diameter (>25 cm dbh) snag trees.

Within the Study Area, the woodlands have potential to support this type of habitat. Bats are most vulnerable to disturbance within the maternal roosting season from April 1 to September 31 of each year.

Turtle Wintering Areas

Turtles overwinter in the mucky bottom of deep pools or slow-moving streams. There is a large pond beside Spittler Creek in a woodland west of Otterville. Additional habitat may be present in Mill Pond in Otterville but this is outside of the Study Area. Overwintering turtles are vulnerable to disturbance during hibernation. They may also migrate through the Study Area along various watercourse corridors.

Amphibian Breeding Habitat (Woodland)

The Amphibian Breeding Habitat (Woodland) type of habitat is described as a wetland, pond, or woodland pool (including vernal pools) >500 within or adjacent (within 120 m) to a woodland (no minimum size). Breeding pools within the woodland or the shortest distance from forest habitat are more significant because they are more likely to be used due to reduced risk to

migrating amphibians. Woodlands with permanent water or those with hydroperiods extending into mid-July are most likely to be used by breeding amphibians.

No vernal pools were observed within the first 15 m of the margins of the forested communities and any features present further in the interior of the properties are unlikely to be impacted by any proposed road improvements. The large pond noted as a possible overwintering habitat for turtles could also provide habitat for breeding amphibians.

This type of habitat is vulnerable to changes in water levels and sedimentation from construction.

5.0 Conclusions

Overall, the majority of ecological communities present within the Study Area are commonly occurring, low-sensitivity features. A small number of sensitive features, including a small portion of the Otterville Provincially Significant Wetland, and sensitive species, including one endangered Butternut, have been identified. The woodlands along Spitler Creek and Big Otter Creek represent the most significant habitats in the Study Area. These woodlands may provide habitat for rare bat species, breeding amphibians, turtles and birds.

Potential impacts to these features will be assessed in the Environmental Study Report for the Oxford Road Corridor Improvement Schedule C Municipal Class Environmental Assessment.

R.J. Burnside & Associates Limited

Sarah Yoshida
Ecologist
SY:js

Enclosure(s) Attachment A – ELC Summary List
 Attachment B – SAR Screening Table
 Attachment C – SWH Screening Table for Ecoregion 7E

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Appendix C

Aquatic Habitat Memo



Technical Memorandum

Aquatic Assessment

Date: February 13, 2023 **Project No.:** 300053425.0000

Project Name: Oxford Road 19 Class EA

Client Name: Oxford County

Submitted To: Avid Banihasemi

Submitted By: Joseph Bernardi H.B.Sc, Aquatic Ecologist

Reviewed By: Chris Pfohl, CET, EP, CAN-CISEC, Sr. Aquatic Ecologist

1.0 Background

The County of Oxford has contracted R.J Burnside & Associates Limited, to conduct an Environmental Assessment for the widening of Otterville Rd (Also known as Oxford Rd 19 or Ostrander Rd) between Norfolk County boundary, intersection of Oxford Road 59, Otterville, Springford, and Ostrander. On May 12, 2022, Burnside's Aquatic Ecologists traveled to the Study Area along Oxford Rd 19 and assessed water crossings along the corridor that may be affected by the road improvements. Burnside's Aquatic Ecologists identified six locations along Oxford Rd 19 that may be affected by the road improvements (Table 1 and Figure 1). The following memo describes the aquatic habitat within the watercourses and surrounding area.

Table 1. Locations of Watercourse Crossings

Site Location	Coordinates
CR1 (3 km West of Browns Corner along Oxford Rd 19)	42.924287, -80.618157
CR2 (4.8 km West of Browns Corner along Oxford Rd 19, Spittler Creek)	42.921160, -80.639036
CR3 (12 km West of Browns Corner along Otterville Rd, Spittler Creek)	42.908870, -80.726018
CR4 (13.5 km West of Browns Corner Along Ostrander Rd, Spittler Creek)	42.906233, -80.744772
CR5 (14.3 km West of Browns Corner Along Ostrander Rd, Unnamed Tributary of Spittler Creek)	42.904963, -80.753869

Site Location	Coordinates
CR6 (8 km West of Browns Corner Along Otterville Rd, Unnamed Tributary of Plumb Creek)	42.915617, -80.677689



Figure 1 Listed Crossings along Oxford Road 19.

2.0 Background Information Review

Burnside's Aquatic Ecology staff reviewed the following sources of background information to determine ecological constraints which may impact the proposed works:

- Aerial Imagery (2021);
- Ministry of Northern Development, Mines, Natural Resources and Forestry (NDMNRF); Aquatic Resources Area (ARA) mapping (2017);
- Department of Fisheries and Oceans (DFO) Species at Risk (SAR) mapping (2022); and
- Natural Heritage Information Centre (NHIC) mapping (2022)

The primary watercourse along Oxford Road 19 is not listed on MNDMNRF ARA mapping, however, Unnamed Tributaries Plumb Creek which have cold-water thermal regimes and cross the roadway in two areas. Furthermore, the primary watercourse is listed on water resource mapping as Splitter Creek, a tributary of Otter Creek, and has an unknown thermal regime.

According to the DFO SARA mapping, there are no species at risk found within the work area. If there is the potential for in water works to occur along Oxford Road 19, the proposed timing window for this project would be from June 15 to September 30.

The NHIC mapping states that significant woodland is only found around the second crossing along Oxford Road 19.

3.0 Aquatic Habitat Assessment

3.1 Existing Conditions

Existing conditions were observed from the right-of-way of the study area on May 12 2022. Water conditions were mainly clear with excellent visibility and weather conditions were sunny with a high of 29 °C.

Crossing CR1:

The first site along Otterville Rd (CR1) was two ponds found on the north and south side of the roadway that were connected by an Unnamed Tributary of Otter Creek (Photo 1 and 3). No turtles were observed within the ponds however, fish activity was noted. The southern pond had evidence of ground water seepage due to iron staining along the western bank where large quantities of Skunk Cabbage (*Symplocarpus foetidus*) were present (Photo 2). Furthermore, Duck Weed (*Lemnoideae*) was found throughout both ponds and signifies lentic type conditions suitable for aquatic macrophytes. Gabion baskets were also noted on the south bank of the north pond and provide bank protection next to the road shoulder.



Photo 2 Looking west down at the north pond.



Photo 3 Looking south, down at iron staining in the south pond.



Photo 4 Looking south at the southern pond.

Crossing CR2:

The second site is an 18 m long Clear Span Bridge which crosses Spittler creek, a tributary of Otter Creek. The upstream reach of the watercourse flows northwest to south through a well shaded woodland before a small clearing 20 meters upstream of the bridge. The morphology of the upstream reach is comprised of 100% flats with an approximate wetted depth of 0.7 m- 1 m and an approximate wetted width of 7 m – 9 m. The flat of the upstream reach had moderate current and deep channel with a dominate substrate of gravel followed by sand and cobble (Photo 4).

The banks of the watercourse showed evidence of erosion with some sections sloughing into the watercourse. There was no in stream or overhanging woody debris found within the immediate section of the reach, however the woodland provided overhanging vegetation and moderate shading.

The downstream reach of the watercourse meanders south through a well shaded woodland. The morphology consisted of approximately 60% flat (mean wetted width of 7 m and a mean wetted depth of 0.4 m – 0.5 m), 20% riffle (mean wetted width of 7 m – 8 m and a mean wetted depth of 0.05 m – 0.10 m) and 20% pool (mean wetted width of 9 m and a mean wetted depth of 1.2 m). The primary substrates throughout the downstream reach were gravel, cobble, and sand. Cobble and gravel were primarily found within the riffle and flat, while sand was found throughout the entire length of the run and especially within the pool.

The banks of the downstream reach of the watercourse were stable with minimal undercut banks along the left bank. Woody debris was observed to be scattered throughout the reach, the majority of which overhanging into the watercourse. There was evidence ground water entering the watercourse as iron staining was observed in multiple locations along the right bank. Algae was observed on rocks suggesting that the watercourse experiences a large influx of nutrients.



Photo 5 Looking north upstream from the inlet of the bridge



Photo 6 Looking west at the left bank of the watercourse



Photo 7 Looking south at the downstream reach of the watercourse beneath the bridge



Photo 8 Looking north at the bridge downstream of the road

Crossing CR3:

The third site along Oxford Rd 19, is a clear span bridge which also crosses Spittler Creek. Upstream from the inlet of the bridge the watercourse flows north to south through a minimally shaded woodland. The morphology of the reach was comprised of approximately 100% flats with a mean wetted depth of 0.2 m – 0.3 m and a mean wetted width of 5.8 m. The primary substrates of the upstream reach are cobble, gravel, and sand.

The banks of the watercourse were moderately unstable with evident signs of erosion a limited stabilization is provided by densely vegetated grasses along the bank. Large riparian trees were found along the west bank, however, provided little to no shading throughout the reach (Photo 8,9 and 10). There was no instream vegetation, and a tile drainage system was observed along the left bank feeding into the watercourse. Algae was noted on the rocks indicating potential nutrient influx from agricultural runoff (Photo 9).

Downstream from the bridge, the watercourse dissects a small woodlot which is directly adjacent to agricultural fields. The morphology of this reach is comprised of 100% runs with a mean wetted depth of 0.1 m – 0.2 m and primary substrates comprised of sand, cobble and gravel. Sand and cobble were the two most dominant substrates as they were both found throughout the entire length of the run. Gravel was found in patches breaking up sections of cobble.

The banks of the watercourse were stable with minimal signs of erosion, as they were densely packed with grasses and large riparian trees. Furthermore, the banks were not as steep when compared to the upstream reach which has an effect on potential for erosion. Large riparian trees were found as the watercourse entered the woodlot, however provided adequate shading to the overall reach (Photo 8). No aquatic vegetation was observed upstream or downstream of the bridge. A log jam was observed towards the end of the reach, and spawning habitat for cyprinids was noted underneath the clear span bridge based on observed spawning activity of Chub species.



Photo 9 Looking north, downstream from the inlet of the bridge.

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Photo 10 Looking at abundant algae on instream rocks within the upstream reach.



Photo 11 Looking south upstream from the inlet of the bridge.



Photo 12 Tile outlet found on the east bank of the upstream reach.



Photo 13 Looking southwest, upstream of the inlet of the bridge

Crossing CR4:

The fourth site along Oxford Rd 19 was another clear span bridge which also crosses Spittler Creek. The upstream reach comprised of two morphological traits, the first being a flat comprising an approximate 90% of the upstream reach and the second being a run comprising approximately 10% of the upstream reach. The flat had a mean wetted depth of 0.5 m – 0.6 m, a mean wetted width of 5 m – 6 m, and dominant substrates consisting of sand and gravel. The run had a mean wetted depth of 0.2 m – 0.3 m, a mean wetted width of 2.5 m – 2.7 m, and a dominant substrate of rip rap which was found at the mouth of the inlet of the culvert and extending a few meters upstream.

The banks of the upstream reach were stable with minimal signs of erosion, rip rap was placed along the banks in the immediate vicinity of the bridge to prevent erosion of the bed and banks. This reach is slightly shaded by riparian vegetation found along the east bank of the watercourse. There was no instream vegetation and minimal organic debris found throughout the flat.

Similar to the previous two crossings, algae were observed suggesting a large influx of nutrients into the watercourse. An unnamed Tributary of Spittler Creek was observed flowing from the west into the upstream reach.

The downstream reach of the watercourse flowed adjacent to the front yard of a nearby property owner and an agricultural field. The morphology of the downstream reach was 100% flats with

a mean wetted depth of 0.4 m – 0.6 m and a mean wetted width of 2.8 m. The primary substrate of the downstream reach was sand and cobble with rip rap found directly at the outlet of the bridge.

The banks of the watercourse had significant evidence of erosion especially along the right bank where rip rap from the bridge did not extend to more than a few meters downstream (Photo 14). There was little shading throughout the downstream reach of the watercourse, as there was the lack of riparian vegetation along both banks of the downstream reach. There was no instream and limited overhanging vegetation.



Photo 14 Looking northwest, upstream from the inlet of the bridge



Photo 15 Looking downstream at the algae found in abundance along the rocks



Photo 16 Looking southeast, downstream from the outlet of the bridge



Photo 17 Looking at the right bank of watercourse and bank erosion

Crossing CR5:

The fifth crossing along Oxford Rd 19 was a culvert found on an unnamed tributary of Spittler Creek. The watercourse flows from southwest to northeast through the culvert, upstream it flows through the front yard of a nearby rural property while downstream it dissects two agricultural fields (Photos 17, 18, 19, 20, 21).



Photo 18 Looking northeast at the outlet of the culvert viewed from the ditch of the roadway



Photo 19 Looking down at concrete debris found in the ditch of the roadway



Photo 19 Standing in the downstream reach looking down at the outlet of the culvert.



Photo 20 Looking southwest at the downstream reach from the ditch of the roadway.



Photo 21 Looking down at the inlet of the culvert from the ditch of the roadway.

Crossing CR6:

The sixth and final crossing that Burnside's Aquatic ecologists visited on Otterville Road was another unnamed tributary of Spittler Creek. The water crossing was made up of two concrete culverts that were protected by rip rap and the watercourse appeared to be fed by tile drainage from nearby fields. Downstream of the outlet of the culvert, the water course dissects a wetland while heading south away from the roadway (Photos 22, 23, 24, 25).



Photo 22 Looking south downstream from the outlet of the culvert



Photo 23 Looking down at the outlets of the culverts.



Photo 24 Looking down at the inlets of the culverts.



Photo 25 Looking down at the two tile outlets into the watercourse.

4.0 Fish Habitat

On May 12, 2022, Burnsides Aquatic Ecologists visited six watercourse crossings within the study area along Oxford Rd 19 in Oxford County. Of the six crossings visited that day, all crossings have the potential to support fish habitat. Each crossing was either found on

Spittler Creek or was an unnamed tributary of Spittler Creek. This creek is a tributary of Otter Creek with an unknown thermal regime and is listed on MNRF water resource mapping. According to DFO SAR mapping there are no aquatic species at risk found within the region of Spittler Creek.

Fish habitat as defined under the Fisheries Act was noted at all sites with significant fish habitat (i.e. spawning habitat) observed at CR 2, CR3, and CR4. Each crossing had adequate, flow / depth, riparian life, and substrate providing suitable fish habitat. While visiting these sites, large schools of bait fish were noted throughout both the upstream and downstream reaches of the crossings. Furthermore, spawning Chub species were observed in the downstream reach of CR2 and underneath the clear span bridge in CR3.

Fish were not observed at crossings five and six however, this does not denounce the fact that these two crossings still have the potential to provide fish habitat. At CR5, the watercourse had adequate structure and depth to support cyprinid species. Downstream from the culvert where riparian vegetation was abundant, the watercourse widened and increased in depth potentially providing suitable fish habitat.

Upstream of CR6 there is no fish habitat as there are only two tile drainage features feeding this watercourse. However, downstream from the culverts an open channel exists and provides the potential for fish habitat based on its connectivity to Plumb Creek which is a tributary of Spittler Creek.

5.0 Conclusion

Oxford Rd 19 is found to be suitable fish habitat defined under the *Fisheries Act*. The mitigation measures implicated throughout the construction, and the proposed design for the road widening will not cause the death of any fish or the Harmful Alteration, Disruption or Destruction (HADD) fish habitat during the fish salvage and construction process.

R.J. Burnside & Associates Limited

Joseph Bernardi, H.BS.c, CAN-CISEC-IT
Aquatic Ecologist
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Enclosure(s) MTO Protocol Fish Habitat Assessment Forms.
Map of Crossings

cc: Chris Pfohl, Sr. Aquatic Ecologist, R.J. Burnside & Associates Limited (enc.) (Via: Email)

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Appendix D

Stage 1 Archaeological Assessment

Stage 1 Archaeological Assessment Oxford Road 19 Corridor Improvements (Lots 1-8, Concessions 8-9, Former Dereham Township and Lots 2-28, Concession 8-9, and the Gore, Former Norwich Township, County of Oxford) South-West Oxford and Norwich Townships, County of Oxford

Original Report

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Executive Summary

Archaeological Services Inc. was contracted by R.J. Burnside & Associates Limited to conduct a Stage 1 Archaeological Assessment (Background Research and Property Inspection) as part of the Oxford Road 19 Corridor Improvements project in the County of Oxford. This project involves proposed road improvements to Oxford Road 19 from Highway 19 to the Norfolk County boundary at Windham Road 19. Stage 1 scope involves a 25-metre buffer from the centreline of Oxford Road 19 between Highway 19 and Windham Road 19.

The Stage 1 background study determined 27 previously registered archaeological sites are located within one kilometre of the Study Area. Two sites (AfHe-21, AfHe-22) are located within the Study Area, and six sites (AfHe-19, AfHe-20, AfHe-23, AfHe-24, AfHe-25, AfHe-27) are located within 50 metres. The Pettman Cemetery (AfHe-26) is approximately 100 metres north of the Study Area.

Springford Community Cemetery and Pine Street Burial Ground are located within the Study Area and should be avoided. The property inspection determined that parts of the Study Area exhibit archaeological potential and will require archaeological assessment.

The preferred design concepts indicate that the proposed grading limits impact some areas of archaeological potential beyond the existing right-of-way, as well as lands within the existing right-of-way near AfHe-26 the Pettman Cemetery site and the Springford Community Cemetery.

A summary of the recommendations is below:

- 1 Parts of the Study Area and preferred design concepts exhibit archaeological potential. These lands require Stage 2 archaeological assessment by test pit/pedestrian survey at five metre intervals, where appropriate. Stage 2 is required prior to any proposed construction activities on these lands;
- 2 AfHe-19, AfHe-20, AfHe-21 and AfHe-22 AfHe-23, AfHe-24, AfHe-25, and AfHe-27 are previously registered sites within 50 metres of the Study Area



identified in 1981 by Foster (1982-15). Due to the passage of time and paucity of mapping associated with the former archaeological assessment, these sites must be reassessed during any Stage 2 survey to determine cultural heritage value or interest as per the 2011 S & G.

- 3 AfHe-26 Pettman Cemetery was identified in 1981 by Foster (1982-15) and is known from local knowledge to be a historical Black community burying ground possibly located within 50 metres of the Study Area and preferred design concepts. The cemetery boundaries and number of burials remains unknown but is in close proximity to the preferred design concepts. Further archaeological assessment is required.
- 4 Springford Community Cemetery is within the Study Area and Preferred Design Concept. If future works are unable to avoid the legal boundaries of the cemetery and archaeological fieldwork is required, detailed strategies should be formulated once the impacts are understood.
- 5 Pine Street Burial Ground is within the Study Area. The cemetery property must be avoided by any proposed construction. Any construction impacts within the cemetery's legal boundaries will require a Stage 3 Cemetery Investigation to be conducted to confirm the presence of burial shafts. While the preferred design concepts do not propose any impacts to this area, additional conditions would apply for future work.



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1.0 Project Context

Archaeological Services Inc. (ASI) was contracted by R.J. Burnside & Associates Limited to conduct a Stage 1 Archaeological Assessment (Background Research and Property Inspection) as part of the Oxford Road 19 Corridor Improvements project in the County of Oxford. This project involves proposed road improvements to Oxford Road 19 from Highway 19 to the Norfolk County boundary at Windham Road 19.

Stage 1 scope involves a 25-metre buffer from the centreline of Oxford Road 19 between Highway 19 and Windham Road 19 (Figure 1). Preferred design concepts are presented in Appendix B.

All activities carried out during this assessment were completed in accordance with the *Ontario Heritage Act* (Ontario Heritage Act, R.S.O. c. O.18, 1990, as amended in 2019) and the 2011 *Standards and Guidelines for Consultant Archaeologists* (S & G), administered by the Ministry of Citizenship and Multiculturalism (MCM 2011).

1.1 Development Context

All work has been undertaken as required by the *Environmental Assessment Act*, RSO (Environmental Assessment Act, R.S.O., 1990 as amended 2020) and regulations made under the Act, and are therefore subject to all associated legislation. This project is being conducted in accordance with the Municipal Engineers' Association document *Municipal Class Environmental Assessment* (Municipal Class Environmental Assessment, 2000, as amended 2015).

Authorization to carry out the activities necessary for the completion of the Stage 1 archaeological assessment and property inspection was granted by R.J. Burnside & Associates Limited on December 1, 2021.

1.1.1 Treaties and Traditional Territories

The Study Area is within Treaty 3, the Between the Lakes Purchase. Following the 1764 Niagara Peace Treaty and the follow-up treaties with Pontiac, the



English colonial government considered the Mississaugas to be their allies since they had accepted the Covenant Chain. The English administrators followed the terms of the Royal Proclamation and insured that no settlements were made in the hunting grounds that had been reserved for their use (Johnston, 1964; Lytwyn, 2005). In 1784, under the terms of the “Between the Lakes Purchase” signed by Sir Frederick Haldimand and the Mississaugas, the Crown acquired over one million acres of land in-part spanning westward from near modern day Niagara-on-the-Lake along the south shore of Lake Ontario to modern day Burlington (Aboriginal Affairs and Northern Development Canada, 2016).

1.2 Historical Context

1.2.1 Indigenous Land Use and Settlement

Southern Ontario has been occupied by human populations since the retreat of the Laurentide glacier approximately 13,000 years before present (B.P.) (Ferris, 2013). Populations at this time would have been highly mobile, inhabiting a boreal-parkland similar to the modern sub-arctic. By approximately 10,000 B.P., the environment had progressively warmed (Edwards & Fritz, 1988) and populations now occupied less extensive territories (Ellis & Deller, 1990).

Between approximately 10,000-5,500 B.P., the Great Lakes basins experienced low-water levels, and many sites which would have been located on those former shorelines are now submerged. This period produces the earliest evidence of heavy wood working tools, an indication of greater investment of labour in felling trees for fuel, to build shelter, and watercraft production. These activities suggest prolonged seasonal residency at occupation sites. Polished stone and native copper implements were being produced by approximately 8,000 B.P.; the latter was acquired from the north shore of Lake Superior, evidence of extensive exchange networks throughout the Great Lakes region. The earliest evidence for cemeteries dates to approximately 4,500-3,000 B.P. and is indicative of increased social organization, investment of labour into social infrastructure, and the establishment of socially prescribed territories (Brown, 1995, p. 13; Ellis et al., 1990, 2009).



Between 3,000-2,500 B.P., populations continued to practice residential mobility and to harvest seasonally available resources, including spawning fish. The Woodland period begins around 2,500 B.P. and exchange and interaction networks broaden at this time (Spence et al., 1990, pp. 136, 138) and by approximately 2,000 B.P., evidence exists for small community camps, focusing on the seasonal harvesting of resources (Spence et al., 1990, pp. 155, 164). By 1,500 B.P. there is macro botanical evidence for maize in southern Ontario, and it is thought that maize only supplemented people's diet. There is earlier phytolith evidence for maize in central New York State by 2,300 B.P. - it is likely that once similar analyses are conducted on Ontario ceramic vessels of the same period, the same evidence will be found (Birch & Williamson, 2013, pp. 13–15). As is evident in detailed Anishinaabek ethnographies, winter was a period during which some families would depart from the larger group as it was easier to sustain smaller populations (Rogers, 1962). It is generally understood that these populations were Algonquian-speakers during these millennia of settlement and land use.

From the beginning of the Late Woodland period at approximately 1,000 B.P., lifeways became more similar to that described in early historical documents. Between approximately 1000-1300 Common Era (C.E.), the communal site is replaced by the village focused on horticulture. Seasonal disintegration of the community for the exploitation of a wider territory and more varied resource base was still practised (Williamson, 1990, p. 317). By 1300-1450 C.E., this episodic community disintegration was no longer practised and populations now communally occupied sites throughout the year (Dodd et al., 1990, p. 343). By the mid-sixteenth century these small villages had coalesced into larger communities (Birch et al., 2021). Through this process, the socio-political organization of the First Nations, as described historically by the French and English explorers who first visited southern Ontario, was developed.

By 1600 C.E., the Huron-Wendat communities within Simcoe County had formed the Confederation of Nations encountered by the first European explorers and missionaries. Samuel de Champlain in 1615 reported that a group of Iroquoian-speaking people situated between the Haudenosaunee and the Huron-Wendat were at peace and remained "la nation neutre". Like the Huron-Wendat, Petun,



and Haudenosaunee, the Neutral or Attawandaron people were settled village agriculturalists. In the 1640s, the Attawandaron and the Huron-Wendat (and their Algonquian allies such as the Nipissing and Odawa) were decimated by epidemics and ultimately dispersed by the Haudenosaunee. Shortly afterwards, the Haudenosaunee established a series of settlements at strategic locations along the trade routes inland from the north shore of Lake Ontario. By the 1690s however, the Anishinaabeg were the only communities with a permanent presence in southern Ontario. From the beginning of the eighteenth century to the assertion of British sovereignty in 1763, there was no interruption to Anishinaabeg control and use of southern Ontario.

1.2.2 Post-Contact Settlement

Historically, the Study Area is located in the Former Dereham Township, County of Oxford in Lots 1-8 & Concessions 8-9, and Former Norwich Township, County of Oxford in Lots 2-28 & Concessions 8-9, and the Gore.

The S & G stipulates that areas of early Euro-Canadian settlement (pioneer homesteads, isolated cabins, farmstead complexes), early wharf or dock complexes, pioneer churches, and early cemeteries are considered to have archaeological potential. Early historical transportation routes (trails, passes, roads, railways, portage routes), properties listed on a municipal register or designated under the Ontario Heritage Act or a federal, provincial, or municipal historic landmark or site are also considered to have archaeological potential.

For the Euro-Canadian period, the majority of early nineteenth century farmsteads (i.e., those that are arguably the most potentially significant resources and whose locations are rarely recorded on nineteenth century maps) are likely to be located in proximity to water. The development of the network of concession roads and railroads through the course of the nineteenth century frequently influenced the siting of farmsteads and businesses. Accordingly, undisturbed lands within 100 metres of an early settlement road are also considered to have potential for the presence of Euro-Canadian archaeological sites.



The first Europeans to arrive in the area were transient merchants and traders from France and England, who followed Indigenous pathways and set up trading posts at strategic locations along the well-traveled river routes. All of these occupations occurred at sites that afforded both natural landfalls and convenient access, by means of the various waterways and overland trails, into the hinterlands. Early transportation routes followed existing Indigenous trails, both along the lakeshore and adjacent to various creeks and rivers (ASI 2006).

Black History in Oxford County

The institution of slavery existed in Canada and after the end of the American War of Independence (1783) Loyalists who left America that were also enslavers were incentivised by the 1790 *Imperial Act* to bring their slaves duty-free to Canada, resulting in around 3000 enslaved Black people being brought across the border, between 500 and 700 of which were in Upper Canada (Henry, 2022). In 1793, the *Act to Limit Slavery in Upper Canada* was passed to restrict slaves being brought into Canada, though it did not prevent sales of slaves between provinces or to America. Then in 1807 Britain passed the *Abolition of the Slave Trade Act* in an attempt to suppress slave trading throughout its empire. In 1834 the *Slavery Abolition Act* took effect, abolishing slavery in the British colonies of the Caribbean, South America, and North America, emancipating over 780,000 enslaved persons (Rennalls, 2021). The abolition of slavery in the United States came in 1863 with the Emancipation Proclamation and the 1865 Thirteenth Amendment to the United States Constitution.

The *Fugitive Slave Laws* (1793, 1850) passed by the United States Congress criminalized the escape from bondage in America, resulting in many formerly enslaved people – and some Black people who may never have been enslaved – being captured and returned to enslavers in America. The Underground Railroad was one of the systems of Black resistance, which was organized with and for freedom seekers in the United States, assisting in their escape typically to either northern states or to Canada and was started in the early nineteenth-century. The Underground Railroad was commonly assisted by some members of the Society of Friends (otherwise known as Quakers) (Pettigrew, 2006). In Oxford County, Ingersoll was a terminus for the Underground Railroad with one of the



largest populations of freedom seekers settling there; however, many other Black communities formed throughout the county such as Otterville (Pettigrew, 2006; Rennalls, 2001).

It is reported that as early as 1829, free Black people had settled in Norwich Township (Pettigrew, 2006, p. 36). Frederick Stover, a Quaker and land agent, assisted in settling many free Black people from Cincinnati and New York State who had the means to purchase property (Rennalls, 2001). The first registered Black landowner in Norwich Township was Samuel Jones in 1833. By 1842, the School Section Number 18 school had been established along what is now Middletown Line in the north half of Lot 15, Concession 7 near the largest group of Black settlers (Pettigrew, 2006). The land the school sat on was property owned by another Black settler, Charles Joiner who also ran a sawmill. The school disappeared in the 1850s when Norwich Township divided into North and South Norwich (Rennalls, 2001). In 1853, it was reported that 26 Black families lived in Norwich Township (Pettigrew, 2006, p. 45). Dereham Township had 101 Black settlers, the overwhelming majority of the 123 total Black settlers in all of Oxford County at this time (Shenston, 1852). In 1861 there were 51 Black people counted in the census for Dereham Township, however the communities did not seem to stay in those areas and through to 1901 the Black population remained around only 25 (Pettigrew, 2006, pp. 105–106).

Otterville was another important Black community. Land had been purchased in 1856 and five years later an African Methodist Episcopal Church and cemetery were built, and due to the Fugitive Slave Act, changed its name to the British Methodist Episcopal Church (Pettigrew, 2006, p. 47). Large “bush meetings” or camp meetings were held at this church and many people, both Black and White would attend. These meetings would last for several days and those in attendance would travel far to be there. Although the church no longer stands and many of the gravestones are gone, a plaque and cairn now commemorate the location (Rennalls, 2001). The property of the former church and the cemetery is located approximately 620 metres north of Oxford Road 19 on the west side of Church Street/Pick Line. Black families also attended a variety of churches and other congregations in the area.



During the 1880's the number of Black settlers began to decline as the supply of white pine was depleted and the number of lumber mills in operation dwindled. The Black population went from 165 for the original area of Norwich Township in 1861 to only 27 in 1901. As agriculture on the land became more difficult in the early-twentieth century and made more challenging by the Great Depression, much of the Black population (along with many other groups of people) moved on from the smaller communities to larger towns and cities such as Ingersoll, Hamilton, and Toronto (Oxford County Archives, 2020; Pettigrew, 2006).

Dereham Township

Dereham Township was first formed as part of the County of Norfolk in 1792 but was switched to Oxford County in 1798. Parts of the township were first surveyed in 1799, and then another portion in 1810, with lots and concessions being finished in 1821. A re-survey was conducted in 1822 or 1832 (Shenston, 1852). When George Tillson constructed an iron forge and began the settlement of what is now Tillsonburg in 1825, there were 16 settlers in the entire township. However, the township grew steadily and by 1830, there were 329 residents. That number grew to 776 by 1840. That number jumped significantly over the following two decades. In 1850, the township – which consisted of 67,200 acres – had a population of 2,839 residents and that grew to 3,644 by 1852 (Shenston, 1852).

Most of the township's residents were involved in agriculture in the nineteenth century, with wheat and oats being the primary crops. A mix of sheep, horses, cows, and pigs were also found scattered throughout the township. Some industry was also taking place. By 1852, the township had 12 sawmills, and lumber was sent down the Otter Creek for export, especially to the United States. There were also two operational grist mills, a carding and fulling mill, and a tannery in the township at this time (Shenston, 1852). Dereham soon became famous for cheese production and is claimed to be the birthplace of commercial cheese making in Canada, exemplified by the township showing a giant block of cheese weighing 7,300 pounds at the New York State Fair of 1866.



The most prominent roadway in the mid-nineteenth century was the Ingersoll and Port Burwell Plank and Gravel road, which ran the length of the whole township (Shenston, 1852).

The Township was annexed into part of the Township of South-West Oxford and the Town of Tillsonburg in 1975 (Mika & Mika, 1977, pp. 543–544).

Ostrander

Ostrander is a hamlet at the intersection of Ostrander Road and Plank Line. The 1857 Tremaine's map shows an H. Ostrander owned Lot 7, Concession 9, located in the southeast corner of what would become the hamlet of Ostrander (Tremaine, 1857). The 1876 Illustrated Atlas of the County of Oxford depicts the community of Ostrander, and H. B. Ostrander as owner of Lot 7, Concession 9 (Walker and Miles, 1876). The 1891 Canada Census has a record of a Henry Ostrander in Dereham Township (Library and Archives Canada, 1891, p. 13).

St. Charles Anglican Church and Cemetery

St. Charles Anglican church is located at 224570 Ostrander Road in Tillsonburg. The church was built in 1844, founded by and named after Dr. Charles James Stewart. Land for the church, and later a cemetery, was donated by local farmers. The St. Charles Anglican Cemetery was added in 1857, around rear of the church building. The cemetery includes a "Potters Field" for indigent, unknown, or unclaimed people is located on the east side of the cemetery (County of Oxford, 2006; Groundspeak Inc., 2022).

Norwich Township

Norwich Township was originally part of Norfolk County in the territorial division made by Governor Simcoe in 1792. It was attached to the County of Oxford in 1798. The first survey was conducted by a Mr. Hamley in 1809. It was not until 1821 that the gore of land on the east side of Norwich became part of Norwich Township, though that area was not surveyed until 1835 (Shenston, 1852). Settlement was started by Quakers Peter Lossing and Peter De Long, immigrants from New York state, circa 1810 (Stagg, 1987). Farming was the dominant



occupation in the nineteenth century. Yet, as early as 1818, the township had one store, one grist mill, and three sawmills. By that same year, the Religious Society of Friends, or Quakers, had established two meeting houses and there was also a school. Several sawmills were operational along the township's principal river, Otter Creek, by the 1820s. The township's population rose from 699 in 1820 to 1,215 by 1830. It continued to grow steadily thereafter, reaching 2,419 by 1840 and 4,483 by 1850 (Shenston, 1852). In 1855, the township was divided into North and South halves (Norwich & District Historical Society, 2013).

Agriculture remained the dominant industry in the nineteenth century. By the 1850s, over 25,000 acres of land were under cultivation. Yet other forms of industry were emerging in this decade as well. Most notably, there were twelve sawmills which were capable of sawing over four million feet of lumber annually (Shenston, 1852). The leading village in the township during the first half of the nineteenth century was Otterville, which was settled as early as 1807 when grants were given for the erection of mills. In 1852, Otterville was described as a "beautiful, clean, and flourishing village" with a population of 250. Other communities in the township during the nineteenth century included Norwichville (the town of Norwich), Erbtown, Springford, Cornell, and Hawtrey (Shenston, 1852).

Agriculture continued to dominate the area in the twentieth century, with tobacco being the dominant cash crop. The present boundaries of Norwich Township were established in 1975 when the former village of Norwich amalgamated with the townships of East Oxford, North Norwich, and South Norwich (Township of Norwich, 2019).

Springford

The Study Area borders the village of Springford. The earliest residents in the Springford area settled in the early nineteenth century and include Joseph Spitler, John Philips, and John Fox. The village of Springford's first settler was the Loyalist Josiah Gilbert, and other early residents included Squire Ebenezer Healy, Amos Scott, Amos Palmer, and Willet Post. Healy was the first town warden, taking up the position in 1830. These early residents were largely self-sufficient,



having to travel to Otterville for the nearest mill and store. By the mid-nineteenth century, though, Springford began to develop community infrastructure and a commercial base, including sawmills, a blacksmith shop, an inn, and a store. By 1857, the community had a wagon shop, Baptist and Congregationalist Churches, a school, stores, post office, a hotel, tannery, a cabinet-making shop, a tailoring business, and a shoemaker in addition to multiple residences. During the latter half of the nineteenth century, especially following the arrival of the Brantford, Norwich, and Port Burwell Railway through the community in 1875, other industries and/or cultural buildings were added, including lumbering, a cheese factory, shoemaking, cooperage, hotels, a Methodist Church, a Temperance Hall, a train station, a doctor, a tin smithing business, a brick and tile operation, all of which added to the village's prosperity. However, by the 1880s and 1890s, the village was in decline, with factories further away making goods more cheaply than the local factories (Springford Women's Institute, 2000; Walker and Miles, 1876).

While the community was smaller, there were significant developments in the twentieth century. A Women's Institute was formed in 1903 and Mrs. F.W. Vardon was elected the first president. A Town Hall had been erected in the late nineteenth century, and the Women's Institute took ownership of the it in 1928 for their various functions. While the former Town Hall ultimately closed in 1988, a new community hall was built later that same year, and on its grounds was a plaque dedicated to The Establishment of Free Rural Mail Delivery, which was instigated by the community's most famous citizen, George Wilcox. A water system was installed in 1924. Over time, the Springford Area Athletic Club was formed, an Agricultural Society also was created, and a baseball park was built in 1969. The railway through Springford stopped providing passenger service in 1954, and the line was discontinued altogether in 1980 (Springford Women's Institute, 2000).

Springford United Church

The land on which the Springford United Church now sits, near the southeast corner of Main Street and West Street South was first owned by Dyer Wilcox, one of the first settlers in Springford. He donated a parcel with the intention of



having a church, a yard, and a parsonage. A cemetery was also added later on. The Congregationalists were the first to occupy the church, built at an unknown date, and it was later used by the Methodists. The larger community of Springford was built around the intersection where the church property was located (Springford Women's Institute, 2000). In 1894, the church was moved, raised, bricked, and remodeled by and for the Methodist congregation. This is the extant church at Main Street and West Street South. The church's builder was Stephen Pratt, and the official dedication ceremony was 8 December 1894.

Springford Community Cemetery

This cemetery was originally called the Fox's Burial Ground, after landowner Levi Fox. Stone markers date to 1826, however burials were known to take place prior to this date (Find a Grave, 2022). Burials from the Springford United Church cemetery were exhumed and reburied at Springford Community Cemetery in 1894 to accommodate moving the church to its extant location. A monument located at the cemetery is dedicated to the Springford United Church (formerly Congregationalist and Methodist), acknowledging those who served the church from 1826 to 2004. West of the cemetery access road is the newest section, with burials dating to the 2000s. Directly east of the access road are burials with grave markers dating the burials to the 1990s and early 2000s. The oldest marked burials are in the easternmost portion of the cemetery near the treeline. In this early section of cemetery, there broken grave marker bases and grave markers which have been placed against trees.

Erbtown

The Study Area passes through the small community of Erbtown, which was founded around 1850 just west of Otterville. The settlement started when Abraham Erb and his four brothers came to operate a sawmill and woolen mill. According to the 1857 Tremaine's map, Sam Erb owned Lots 14-15, Concession 9, and part of Lot 14, Concession 8 (Tremaine, 1857). Jos Erb owned Lot 16, Concession 9. Samuel Erb had emigrated from Pennsylvania to Upper Canada at some point between his birth in 1792 and his marriage to Mary Lewis of Wentworth Country in 1816. The 1876 Illustrated Atlas of the County of Oxford (Walker and Miles, 1876) shows Erbtown to the west of Otterville, on parts of



Lots 13-14, Concession 9, and part of Lot 14, Concession 8. An Abraham Erb owned part of Lot 14, Concessions 8-9. He was married to Mariah Quickfall and they engaged in farming (Lythgoe, 2022). A Jared Erb owned part of Lot 15, Concession 9. The Erb family donated land for an Episcopal Methodist church and cemetery prior to 1861, and Abraham built the church (County of Oxford, 2018). However, besides the church, the community was not well-established enough to warrant a school, post office, or any other cultural institution by the 1870s, and was not commented upon in the 1876 Illustrated Atlas (Walker and Miles, 1876).

Erbtown Cemetery

Erbtown Cemetery is on land donated by the Erb family between 1855 and 1861 for an Episcopal Methodist Church and Cemetery, in the Settlement of Erbtown. A letter from the Ontario Ministry of Consumer and Commercial Relations on October 31, 1980 (see Appendix A Figure 63), references work done for Erbtown Cemetery. By this time the grave markers had been gathered into a V shaped monument and the cemetery name added over a gate. Approval was given to plant trees south of the cemetery, which at the time was an agricultural field.

Pine Street Burying Ground

Pine Street Burial Ground is a Quaker burying ground for many of the earliest settlers around Otterville. The burying grounds is located in the property parcel east of the Pine Street Friend's Meeting House, now the Woodlawn Adult Community Centre. The Pine Street Friend's Meeting House was first a log church, built in 1819. It was replaced in 1849 by a frame building with porches. In 1980, a proposal to move the grave markers into a V shaped monument was approved by the Ontario Ministry of Consumer and Commercial Relations (see Appendix A Figure 63). The Cemetery Name was added above the gate in 1980 and repairs to the wrought iron fence had been completed.

Otterville

Between the middle portion of the Study Area and the eastern-most portion of the Study Area is the village of Otterville. Settlement of Otterville began in 1907



on Big Otter Creek when a government grant and machinery was provided to John Earls and Paul Avery for the erection of grist and sawmills (Mika & Mika, 1983, p. 148). The first store and tavern were built in 1816. A tannery and shoe and harness building were started in 1833. A post office opened in 1837. A foundry was built and saw- and woolen mills erected during the 1850s. Twelve steam and fourteen water mills, the A. Parsons Carriage Works, the John Furlong Shingle and Cooperage Factory, and the J.J. Warner Match Factory began during the 1850s. A private bank was opened in 1879, followed by a Trader's Bank within the post office building. St. John's Anglican Church was built in 1854, the Otterville New Connexion Methodist Church was built in 1862 with renovations completed in 1918, and the Baptist Church was built in 1865. The first school was built in the 1830s. A two-storey school was built in 1857, followed by a continuation school in 1924. A continuation and public school were constructed in 1947 which became a central public school in 1947. Otterville became part of Norwich Township on January 1, 1975 (Mika & Mika, 1983, p. 148).

Tillsonburg, Lake Erie and Pacific Railway

Intersecting with the western-most portion of the Study Area is the Ontario Southland Railway. Originally the Tillsonburg, Lake Erie and Pacific Railroad, the line was constructed beginning in 1895 to connect Port Burwell on Lake Erie with the Grand Trunk Railway Mainline in Ingersoll, approximately 33 miles to the north. The Canadian Pacific Railroad (C.P.) leased the line beginning in 1904 and ceased operations in 1987. In 1998 the line was purchased by the Ontario Southland Railway (O.S.R.) which continues to operate the railroad (Kennedy, 2019).

Brantford, Norwich and Port Burwell Railway

Transecting the Study Area at Springford is the alignment of the former Brantford, Norwich and Port Burwell Railway. The Norfolk Railway incorporated in 1869 with the intention to build a line from Lake Erie to the Great Western Railway (G.W.R) in Paris, Ontario, but these plans stalled, and construction never commenced. In 1874, the company, now The Brantford, Norfolk and Port Burwell Railway (B.N.P.B.R.) was authorized to build from Lake Erie, through Tillsonburg, to connect to the G.W.R. in Brantford. Construction began two



years later, and though intended to reach the great lakes, due to mismanagement and delays the company struggled to finish the 34 miles of track between Brantford and Tillsonburg. Passenger service began, depositing passengers at a station in West Brantford while the company waited on the completion of a bridge over the Grand River. On January 1, 1877, The B.N.P.B.R. was leased to the Canada Southern Railway (C.S.R.), owned by the wealthy and powerful Vanderbilt family. Around this time, the Grand River bridge was completed allowing passengers to disembark, despite the lack of a station, at Market Street in downtown Brantford. However, just four months following the agreement, the company's board were convinced to revoke the lease with the C.S.R. by the G.W.R. who agreed to lease the line in perpetuity and to allow trains to travel an additional mile on their tracks to the Colborne Street Station. In leasing the B.N.P.B.R., the G.W.R. created a loop line from Brantford to their Canada Air Line Railway at Tillsonburg Junction. In 1883 the G.W.R. amalgamated with the Grand Trunk Railway which operated the line until its purchase by the Canadian National Railway (C.N.R.) in 1923. The C.N.R. abandoned the line in sections from 1978-1988 leaving only a small 1-mile spur in operation (Brantford Public Library, n.d.; D. N. M. Smith, n.d.; J. Smith, 2017).

Port Dover and Lake Huron Railway

Transecting the Study Area is the former alignment of the Port Dover and Lake Huron Railway (P.D.L.H.R). The P.D.L.H.R was incorporated in 1872 to build a rail line from Port Dover to Stratford. To save costs, they were granted permission to acquire the roadbeds and holdings of the former Woodstock and Lake Erie Railway and Harbour Company whose own efforts to build in the region had been halted when an enquiry into their business affairs found evidence of reckless spending, bad faith investments, and bribery. The P.D.L.H.R line from Port Dover to Woodstock opened in 1875 with a further extension to Stratford the following year. The railway company purchased Port Dover Harbour from the federal government in 1877 and in that same year, amalgamated with the Stratford and Huron Railway to form the Port Dover and Lake Huron Railway. In 1881, they amalgamated with the Stratford and Huron Railway and the Georgian Bay and Lake Erie Railway to form a new subsidiary of the Grand Trunk Railway (G.T.R.) called the Grand Trunk, Georgian Bay and Lake Erie Railway which was



eventually completely absorbed into the G.T.R. in 1893 (Cooper, 2017a; Delamere, n.d.). In 1923, in an effort to nationalize the railway system, the federal government acquired the G.T.R. following the acquisition of several other major rail companies, creating the Canadian National Railway System (Cooper, 2017b).

1.2.3 Map Review

The 1857 Tremaine's Map of the County of Oxford (Tremaine, 1857), 1876 Illustrated Historical Atlas of the County of Oxford (Walker and Miles, 1876), 1909 topographic map Tillsonburg Sheet (Department of Militia and Defence, 1909), and the 1999 National Topographic System Tillsonburg Sheet (Department of Energy, Mines and Resources, 1999) were examined to determine the presence of historic features within the Study Area during the nineteenth and twentieth centuries (Figure 2 to Figure 8).

The 1857 Tremaine's Map (Figure 2) shows County Road 19, Plank Line, Cranberry Line/Tillson Avenue, Middletown Line, Highway 59, and Base Line are historically surveyed road allowances. The community of Springford with a small grid of roads to the south and a post office is shown between the western and middle sections of the Study Area. The larger community of Otterville is shown to be built up east of Otter Creek, between the middle and eastern sections of the Study Area. A steam saw mill is depicted at the northwest corner of Oxford Road 19 and Highway 59, and a second at the southeast corner of Oxford Road 19 and Base Line. Spittler Creek intersects the Study Area at six points.

The 1876 Illustrated Historical Atlas (Figure 3) shows the community of Ostrander at the west end of the Study Area. A sawmill is depicted north of Oxford Road 19 and east of Spittler Creek between Plank Line and Cranberry Line. By this time, the Brantford, Norwich & Port Burwell Railway and the Port Dover & Lake Huron Railway have been constructed. The Brantford, Norwich & Port Burwell Railway is at the east end of the western most section of the Study Area, west of Spring ford. The Port Dover & Lake Huron Railway intersects the west end of the eastern most section of the Study Area east of Otterville. The small community of Erbtown now exists west of Otterville. A church is depicted



south of Oxford Road 19, between Cranberry Line and Zelda Line. A schoolhouse is depicted south of Oxford Road 19 and west of Base Line.

The 1909 topographic map Tillsonburg Sheet (Figure 5 to Figure 7) shows that Brantford, Norwich & Port Burwell Railway and the Port Dover & Lake Huron Railway have become part of the Grand Trunk Railway. A third line, the Canadian Pacific Railway, intersects the Study Area east of Plank Line. A railway station is located along the line just north of Oxford Road 19. A church, post office, and sawmill are shown at the intersection of Plank Line and Oxford Road 19. Bridges carry Oxford Road 19 over Spittler Creek at eight points. Part of Oxford Road 19 curves north around Spittler Creek before returning to a linear alignment. The church depicted south of Oxford Road 19 between Cranberry Line and Zelda Line is now shown to have a cemetery, behind the church farther south from the Study Area. West of Middletown Line is a branch of Spittler Creek, and directly to its east is a cemetery. Another cemetery is located between Middletown Line and Church Street, south of Oxford Road 19. A railway station is now located along the Grand Trunk Railway line that is east of Otterville. A school is located south of Oxford Road 19 to the west of Base Line. Approximately 61 unlabelled structures are depicted along Oxford Road 19.

The 1999 National Topographic System Tillsonburg Sheet (Figure 8) shows growth in the size of the communities of Ostrander, Springford, and Otterville and increased presence of structures along Oxford Road 19.

1.2.4 Aerial and Orthoimagery Review

Historical aerial imagery of the 1954 aerial imagery (Hunting Survey Corporation Limited, 1954) indicates the Study Area is along a road corridor bound by agricultural fields with some treed areas and rural communities (Figure 9 to Figure 12). At points Spittler Creek and Otter Creek cross under the roadway.

A review of available Google satellite imagery shows:

- Widening of driveway at 224275 Ostrander Road between 2013 and 2016 (Image 59 to Image 60)
- Demolition of house at 224276 Ostrander Road in 2016 (Image 60)



- Earthmoving activities at the northwest corner of Oxford Road and Ontario Southland Railway in 2016 (Image 61 to Image 62)
- Parking lot created between 2009-2013 at the southwest corner of Ostrander Road and Cranberry Line (Image 63 to Image 64)

2.0 Property Inspection

2.1 Field Methods

A Stage 1 property inspection must adhere to the S & G, Section 1.2, Standards 1-6, which are discussed below. The entire property and its periphery must be inspected. The inspection may be either systematic or random. Coverage must be sufficient to identify the presence or absence of any features of archaeological potential. The inspection must be conducted when weather conditions permit good visibility of land features. Natural landforms and watercourses are to be confirmed if previously identified. Additional features such as elevated topography, relic water channels, glacial shorelines, well-drained soils within heavy soils and slightly elevated areas within low and wet areas should be identified and documented, if present. Features affecting assessment strategies should be identified and documented such as woodlots, bogs or other permanently wet areas, areas of steeper grade than indicated on topographic mapping, areas of overgrown vegetation, areas of heavy soil, and recent land disturbance such as grading, fill deposits and vegetation clearing. The inspection should also identify and document structures and built features that will affect assessment strategies, such as heritage structures or landscapes, cairns, monuments or plaques, and cemeteries.

The Stage 1 archaeological assessment property inspection was conducted under the field direction of Jessica Lytle (P1066) of ASI, on April 26, 2022, in order to gain first-hand knowledge of the geography, topography, and current conditions and to evaluate and map archaeological potential of the Study Area. It was a systematic visual inspection from publicly accessible lands/public right-of-ways only and did not include excavation or collection of archaeological resources. Fieldwork was conducted when weather conditions were deemed clear with good visibility (overcast and seven degrees Celsius), per S & G Section



1.2., Standard 2. Field observations are compiled onto the existing conditions of the Study Area in Section 8.0 (Figure 16 to Figure 55) and associated photographs are presented in Section 7.0 (Image 1 to Image 57).

2.2 Archaeological Context

This section provides background research pertaining to previous archaeological fieldwork conducted within and in the vicinity of the Study Area, its environmental characteristics (including drainage, soils or surficial geology and topography, etc.), and current land use and field conditions. Three sources of information were consulted to provide information about previous archaeological research: the site record forms for registered sites available online from the MCM through “Ontario’s Past Portal”; published and unpublished documentary sources; and the files of ASI.

2.2.1 Geography

In addition to the known archaeological sites, the state of the natural environment is a helpful indicator of archaeological potential. Accordingly, a description of the physiography and soils are briefly discussed for the Study Area.

The S & G stipulates that primary water sources (lakes, rivers, streams, creeks, etc.), secondary water sources (intermittent streams and creeks, springs, marshes, swamps, etc.), ancient water sources (glacial lake shorelines indicated by the presence of raised sand or gravel beach ridges, relic river or stream channels indicated by clear dip or swale in the topography, shorelines of drained lakes or marshes, cobble beaches, etc.), as well as accessible or inaccessible shorelines (high bluffs, swamp or marsh fields by the edge of a lake, sandbars stretching into marsh, etc.) are characteristics that indicate archaeological potential.

Water has been identified as the major determinant of site selection and the presence of potable water is the single most important resource necessary for any extended human occupation or settlement. Since water sources have remained relatively stable in Ontario since 5,000 BP (Karrow & Warner, 1990, p.



Figure 2.16), proximity to water can be regarded as a useful index for the evaluation of archaeological site potential. Indeed, distance from water has been one of the most commonly used variables for predictive modeling of site location.

Other geographic characteristics that can indicate archaeological potential include elevated topography (eskers, drumlins, large knolls, and plateaux), pockets of well-drained sandy soil, especially near areas of heavy soil or rocky ground, distinctive land formations that might have been special or spiritual places, such as waterfalls, rock outcrops, caverns, mounds, and promontories and their bases. There may be physical indicators of their use, such as burials, structures, offerings, rock paintings or carvings. Resource areas, including; food or medicinal plants (migratory routes, spawning areas) are also considered characteristics that indicate archaeological potential (S & G, Section 1.3.1).

The Study Area is situated within the spillways and undrumlinized till plains of the Mount Elgin Ridges physiographic region, the sand plains of the Norfolk Sand Plain physiographic region, and the till moraines of the Horseshoe Moraines physiographic region of southern Ontario.

The Study Area is within a spillway of the Mount Elgin Ridges physiographic region of southern Ontario. The region is made up of a series of ridges and vales between the Thames Valley and the Norfolk sand plain, and covers the south-eastern part of Middlesex, southern Oxford, and part of Elgin and Brant Counties. The ridges are well drained moraines of calcareous clay or silty clay till, whereas the vales are within glacial spillways of alluvium with imperfect to poor drainage in the hollows. The region has many kettle lakes, such as Mud Lake, Walker Pond, and Whittaker Lake, as well as large swamps, with waterways draining into the Thames River or Lake Erie (L. J. Chapman & Putnam, 1984, pp. 144–146).

The Norfolk Sand Plain physiographic region is a wedge-shaped feature that extends from the Lake Erie shoreline and tapers northward to a point in Brantford on the Grand River (Chapman and Putnam 1984:153-154). The region encompasses an area of 3,134 square kilometres and consists of sands and silts that were deposited as a delta in glacial Lakes Whittlesey and Warren. A massive



discharge of meltwater from the Grand River area entered the lakes between the ice front and the moraines to the northwest, building the delta from west to east as the glacier withdrew, thus covering most of the area west of the Galt Moraine with sand. In the vicinity of the subject property, glaciolacustrine deep water sediments belonging to mainly glacial Lake Warren and younger deposits and consisting of stratified to varved silt and clay, minor sand, are overlain by veneer of sand (Zone 10) (Cowan 1972: Map 2240).

The Horseshoe Moraine physiographic region of southern Ontario forms the core of a horse-shoe shaped area flanking the upland that lies to the west of the highest part of the Niagara cuesta (Chapman and Putnam 1984:127-129). The southwestern limb of the region, located in the southern part of Huron County, has a fairly simple landscape consisting of morainic ridges composed of pale brown, hard calcareous, fine-textured till, with a moderate degree of stoniness. Huron clay loam is the most representative soil type, and it occurs widely throughout the region.

Figure 13 depicts surficial geology for the Study Area. The surficial mapping demonstrates that the Study Area is underlain by clay to silt-textured till derived from glaciolacustrine deposits or shale, fine-textured glaciolacustrine deposits of silt and clay, minor sand and gravel, massive to well laminated, modern alluvial deposits of clay, silt, sand, gravel, and organic remains, older alluvial deposits of clay, silt, sand, gravel and organic remains, and coarse-textured glaciolacustrine deposits of sand, gravel, minor silt and clay. Big Otter Creek between Church Street and New Street is bound by a fluvial terrace to each side. A minor moraine intersects the Study Area west of Zenda Line.

Soils within the Study Area (Figure 14) consist of: Huron clay loam, Fox loamy sand, and Honeywood silt loam, grey-brown podzolics with good drainage; and Embro silt loam and Brady sandy loam, grey-brown podzolics with imperfect drainage (Ontario Geological Survey, 2010).

Spittler Creek intersects the Study Area west of James Street, west of Middletown Line, and at five points between Plank Line and West Street. Spittler Creek joins Big Otter Creek south of Otterville and drains an area of 116 kilometres squared. Spittler Creek is a subwatershed of Big Otter Creek. The Big



Otter Creek watershed drains 712 square kilometres on the north shore of Lake Erie in southwestern Ontario. This watershed is covered by a substantial agricultural land base, approximately 74 percent. The remainder is 14 percent treed land, seven percent wetland, and five percent urban land (Grand River Conservation Authority, 2011).

2.2.2 Previously Registered Archaeological Sites

In Ontario, information concerning archaeological sites is stored in the Ontario Archaeological Sites Database (O.A.S.D.) maintained by the MCM. This database contains archaeological sites registered within the Borden system. Under the Borden system, Canada has been divided into grid blocks based on latitude and longitude. A Borden block is approximately 13 kilometres east to west, and approximately 18.5 kilometres north to south. Each Borden block is referenced by a four-letter designator, and sites within a block are numbered sequentially as they are found. The Study Area under review is located in Borden blocks *AfHe* and *AfHd*.

According to the O.A.S.D., 27 previously registered archaeological sites are located within one kilometre of the Study Area (MCM 2022). Two sites (*AfHe-21*, *AfHe-22*) are located within the Study Area. Six sites (*AfHe-19*, *AfHe-20*, *AfHe-23*, *AfHe-24*, *AfHe-25*, *AfHe-27*) are located within 50 metres. A summary of the sites is provided below in Table 1. Sites shown in bold are within the Study Area, sites shown in italics are within 50 metres.

Table 1: Registered Sites within One Kilometre of the Study Area

Borden number	Site Name	Temporal/ Cultural Affiliation	Site type	Researcher
<i>AfHd-9</i>	Davis	Woodland, Late	Hamlet	Fox 1970



Borden number	Site Name	Temporal/ Cultural Affiliation	Site type	Researcher
AfHd-10	Oatman- Wardel	Woodland, Late	Village	Fox 1986
AfHd-11	Sackrider – Jauneika	Woodland, Late; Euro- Canadian	Settlement	Fox 1986
AfHd-15	Not Applicable	Pre-Contact Indigenous	Findspot	ASI 1989
AfHd-16	Not Applicable	Woodland, Early	Campsite	ASI 1989
AfHd-17	Shelton	Woodland	Campsite	Poulton 1996
AfHd-20	Not Applicable	Pre-Contact Indigenous	Campsite	ASI 1989
AfHd-21	Not Applicable	Pre-Contact Indigenous	Campsite	ASI 1989
AfHd-22	Not Applicable	Euro- Canadian, Pre-Contact Indigenous	Campsite, dump	ASI 1989
AfHd-23	Not Applicable	Archaic, Late	Findspot	ASI 1989



Borden number	Site Name	Temporal/ Cultural Affiliation	Site type	Researcher
AfHd-24	Not Applicable	Pre-Contact Indigenous	Campsite	ASI 1989
AfHd-27	Not Applicable	Pre-Contact Indigenous	Findspot	ASI 1989
AfHd-28	Not Applicable	Woodland, Middle; Woodland, Late	Hamlet	ASI 1989
AfHd-31	Not Applicable	Woodland	Hamlet	ASI 1989
AfHd-36	Dewatcher 1	Woodland, Late	Scatter	AMICK Consultants Limited 2006, 2018
AfHd-37	Dewatcher 2	Pre-Contact Indigenous	Scatter	AMICK Consultants Limited 2006
AfHd-38	Dewatcher 3	Pre-Contact Indigenous	Scatter	AMICK Consultants Limited 2006
AfHe-19	Silver Thorns	Euro- Canadian	House	Foster 1982



Borden number	Site Name	Temporal/ Cultural Affiliation	Site type	Researcher
<i>AfHe-20</i>	<i>County Works</i>	<i>Euro-Canadian</i>	<i>House</i>	<i>Foster 1982</i>
AfHe-21	Garden	Euro-Canadian	Midden	Foster 1982
AfHe-22	Wilson	Euro-Canadian	Midden	Foster 1982
<i>AfHe-23</i>	<i>St. Charles</i>	<i>Euro-Canadian</i>	<i>Stable</i>	<i>Foster 1982</i>
<i>AfHe-24</i>	<i>Cranberry</i>	<i>Euro-Canadian</i>	<i>Midden</i>	<i>Foster 1982</i>
<i>AfHe-25</i>	<i>Pettman</i>	<i>Euro-Canadian</i>	<i>Homestead</i>	<i>Foster 1982</i>
AfHe-26	Pettman's Cemetery	Euro-Canadian	Cemetery	Foster 1982
<i>AfHe-27</i>	<i>Ostrander</i>	<i>Euro-Canadian</i>	<i>Midden</i>	<i>Foster 1982</i>
AfHe-28	Pettigrew	Iroquoian, Middle Woodland, Late	Village	Trent University 1986



An early archaeological report, *A Cultural Resource Appraisal for the Oxford County Road 19 Corridor Extension: Springford to Ostrander* (Foster, 1982), details an archaeological assessment within the current Study Area. The assessment identified nine sites. Sites AfHe-19, AfHe-20, AfHe-23, AfHe-24, AfHe-25, and AfHe-27 are within 50 metres of the current Study Area. Sites AfHe-21 and AfHe-22 are within the current Study Area (see *Supplementary Documentation*). This 1982 report recommended these sites not require further archaeological assessment due to being Euro-Canadian sites that were not considered significant or unique, however this is not consistent with the 2011 S & G. ASI recommends Stage 2 survey to relocate these sites to ensure survey methods and recommendations are compliant with the S & G. The ninth site, the Pettman Cemetery (AfHe-26), is approximately 100 metres north of the Study Area according to the O.A.S.D. It was outside the project area, and identified after a local named Mrs. M. Pettman, provided knowledge of a cemetery approximately 50 metres north of County Road 19 and along a fence line. The report noted that due to the undefined boundaries it may extend into the project area.

The Pettigrew Site (AfHe-28) is a Middle Iroquoian Late Woodland Village within one kilometre of the Study Area. Surface survey and collection occurred in 1986, locating approximately six middens scattered over one hectare knoll top.

2.2.3 Previous Archaeological Assessments

According to the background research, one previous report details fieldwork within 50 metres of the Study Area.

(Foster, 1982) A Cultural Resource Appraisal for the Oxford County Road 19 Corridor Extension: Springford to Ostrander [1982-15]

The project area overlaps the current Study Area between Highway 19 in Ostrander and County Road 13 in Springford. Test pit survey was conducted within woodlots at 10 metre intervals. Pedestrian survey was conducted in ploughed and crop remnant fields, at an unknown interval. Eight sites were encountered during survey: AfHe-19, AfHe-20, AfHe-21, AfHe-22, AfHe-23, AfHe-24, AfHe-25, and AfHe-27. These sites were determined to be nineteenth



century historic sites (middens, domestic structures, and a church stable) which were not recommended for further assessment due to not being considered unique or significant. A ninth site was detailed in this report. A local, Mrs. M. Pettman, provided knowledge of a cemetery approximately 50 metres north of County Road 19 and along a fence line. The grave markers had been removed and few fragments of marble and limestone markers remained. While this cemetery was outside the project area, the report noted that due to the undefined boundaries it may extend into the impact zone. The cemetery was named Pettman Cemetery and given the Borden Number AfHe-26.

The results mapping provided in the Foster report divides the project area into segments based on the seven analysis categories: pasture, swamp, ploughed, crop remnants, erosion, woodlot, and lawn, with another map showing the locations of archaeological sites identified during the survey (see *Supplementary Documentation*). The report describes what generally occurred in each category of ground conditions. While the report is a helpful resource for understanding the archaeological potential of the Study Area, ASI recommends that these areas should be resurveyed during Stage 2 to ensure compliance with the S & G.

2.3 Current Land Use and Field Conditions

The Study Area includes three sections of Oxford Road 19:

- Plank Line to the decommissioned rail corridor west of Springford;
- From approximately 800 metres east of Water Street in Springford to approximately 140 metres west of Cherry Street in Otterville; and
- From approximately 60 metres east of York Street in Otterville to Windham Road 19.

The Study Area excludes the main centres of the communities of Springford and Otterville. Oxford Road 19 is also known as Ostrander Road between Plank Line and Zenda Line, and as Otterville Road between Zenda Line and Windham Road 19. Oxford Road 19 is a two-lane road for west- and eastbound traffic. The road lacks curbs. The Study Area is bound by agricultural fields and houses are spaced far apart, usually set back from the roadway. The Ontario Southland Railway is



extant in the western portion of the Study Area. A decommissioned line of the Grand Trunk Railway is west of Springford Community Park.

Right-of-ways within the western section are ditched with culverts under laneways in parts (Images 1, 3-4, 12, 19-20). Line markers for a natural gas pipeline are within the north and south right-of-ways (Images 6, 14, 18).

Within the middle section, the road curves slightly south below the Springford Community Cemetery, which is on a raised hill (Images 28, 31). Erbtown Cemetery (Image 38) and Pine Street Burying Ground (Image 42) property parcels are within the eastern end of this section, west of where Big Otter Creek intersects (Images 38, 41).

Within the eastern section, utilities can be seen in the right-of-ways (Images 42-43, 45-48, 50, 53). Culverts are also seen in the right-of-way (Image 53, Image 56). A transformer station is at the northwestern corner of Oxford Road 19 and Base Line (Image 54-Image 55).

3.0 Analysis and Conclusions

The historical and archaeological contexts have been analyzed to help determine the archaeological potential of the Study Area. Results of the analysis of the Study Area property inspection and background research are presented in Section 3.1.

3.1 Analysis of Archaeological Potential

The S & G, Section 1.3.1, lists criteria that are indicative of archaeological potential. The Study Area meets the following criteria indicative of archaeological potential:

- Previously identified archaeological sites in the Study Area (See Table 1);
- Water sources: primary, secondary, or past water source (Spittler Creek, Big Otter Creek);
- Early historic transportation routes (Brantford Norwich & Port Burwel Railway, Port Dover & Lake Huron Railway, County Road 19, Plank Line,



Cranberry Line/Tillson Avenue, Zenda Line, Middletown Line, Highway 59, Base Line);

- Proximity to early settlements (Ostrander, Springford, Springford Community Cemetery, Erbtown, Erbtown Cemetery, Pine Street Burial Ground, Otterville);
- Proximity to early Black settlements (Norwich and Dereham Townships, Otterville, African Methodist Episcopal Church and Cemetery, Pettman Cemetery); and
- Well-drained soils (Huron clay loam, Fox loamy sand, Honeywood silt loam)

According to the S & G, Section 1.4 Standard 1e, no areas within a property containing locations listed or designated by a municipality can be recommended for exemption from further assessment unless the area can be documented as disturbed. The Municipal Heritage Register was consulted and two properties within the Study Area is Listed or Designated under the Ontario Heritage Act:

- 225422 Main Street West (Figure 41)
 - Designated under Part IV of the Ontario Heritage Act (By-law 18-90 and 84-77)
 - The property is located on the south side of Otterville Road (Oxford Road 19) and west of James Street. The property contains an octangular residence, two cemeteries, and former railway station. The South Norwich Historical Society operates a museum on the property and the Woodlawn Adult Community Centre is also on the property. The two cemeteries on the property are the Pine Street Burying Ground and the Erbtown Cemetery. The known heritage attributes include the octagonal house constructed in 1861 by Thomas and the former Port Dover and Lake Huron Railway Station which is considered to be symbol of nineteenth-century transportation and communication for a small town and its architecture is an example of a village railroad. The potential heritage attributes include the Pine Street Burying Ground and the Erbtown Cemetery.
- 225947 Otterville Road (Figure 49)
 - Designated under Part IV of the Ontario Heritage Act (By-law 57-88)



- The farmscape is located on the south side of Otterville Road between Base Line and Highway 59 and is the site of the former Innisfree Farm. Known heritage attributes include the residence, a former conference centre, tower silo, outbuildings, mature trees, and wood lots. Known heritage attributes also include an Ontario Heritage Trust Plaque commemorating the birthplace of historian Harold Innis (Ontario Heritage Trust, 2018). The conference centre was built in 1971 by Innis College’s Harold Innis Foundation, who purchased Innisfree Farm from Sam Innis in 1986. The conference centre was built in the Modern style and operated as a library for the works of Harold Innis, an educational and meeting space, and as a rural retreat for students of Innis College until the farm was sold in 1988 (J. Chapman, 2018).

These criteria are indicative of potential for the identification of archaeological resources, depending on soil conditions and the degree to which soils have been subject to deep disturbance.

The property inspection determined that parts of the Study Area exhibit archaeological potential. These areas will require Stage 2 archaeological assessment prior to any construction activities. This includes areas that contain registered sites AfHe-21 and AfHe-22 – that require Stage 2 to determine cultural heritage value or interest in compliance with the 2011 S & G. According to the S & G Section 2.1.1, pedestrian survey is required in actively or recently cultivated fields (Images 8, 12-19, 22-23, 33-34, 43, 48-52, 54-55; Figures 15-54: areas highlighted in orange). According to the S & G Section 2.1.2, test pit survey is required on terrain where ploughing is not viable, such as wooded areas, properties where existing landscaping or infrastructure would be damaged, overgrown farmland with heavy brush or rocky pasture, and narrow linear corridors up to 10 metres wide (Images 3, 6-. 9-13, 16-17. 20-22, 31-33, 35, 42-47, 49-50, 54-56; Figures 15-54: areas highlighted in green).

A part of the Study Area is located within low lying wet areas, and according to the S & G Section 2.1 do not retain potential (Images 9, 39, 40, 55; Figures 20,



38-39, 40, 54: areas highlighted in light blue). These areas do not require further survey.

The remainder of the Study Area has been subjected to deep soil disturbance events. This includes the construction of Oxford Road 19, Plank Line, Cranberry Line, Zenda Line, Middletown Line, Highway 59, Csont Line, Base Line, Windham Line, and Ontario Southland Railway. It includes the construction and subsequent removal of the rail lines west of Springford Community Park and east of York Street in Otterville, the construction of the original bridge over Splitter Creek, its removal, and construction of the present-day bridge to the south, the culverting and channelization of Big Otter Creek below the Oxford Road 19 right-of-way, and the construction of laneways and building footprints. According to the S & G Section 1.3.2 these areas do not retain archaeological potential (Images 1-23, 25, 28, 31-35, 42-56; Figures 15-54: areas highlighted in yellow) and do not require further survey.

3.1.1 Cemetery Analysis

There are four registered cemeteries within 20 metres of the Study Area: St. Charles Anglican Cemetery, Springford Community Cemetery, Erbtown Cemetery, and Pine Street Burial Grounds. See *Supplementary Documentation* for a record of communications involving cemeteries.

The Pettman Cemetery (AfHe-26) is a registered archaeological site within the vicinity of the Study Area and according to the O.A.S.D. and background research is a historical unregistered unmarked Black community cemetery.

St. Charles Anglican Cemetery

St. Charles Anglican Cemetery is a registered inactive cemetery at 224570 Ostrander Road in Tillsonburg, according to the Bereavement Authority of Ontario. The cemetery was established in 1857 south of St. Charles Anglican Church. The 1962 plan of the St. Charles Anglican Cemetery provided by the Bereavement Authority of Ontario shows the cemetery is set back behind the St. Charles Anglican Church (see Appendix A Figure 56). Visual inspection determined there is no fence surrounding the cemetery limits, and all grave



markers to be located south of the church. The rear of the church is 30 metres south of the Study Area (Image 12; Figure 23, Figure 56). All project impacts will avoid the legal cemetery boundaries.

There is low potential for unmarked graves within the Study Area. No Stage 3 Cemetery Investigation is required for the Study Area.

Springford Community Cemetery

The Springford Community Cemetery is an active cemetery located at 225229 Otterville Road in Otterville (Figure 24, Figure 36, Figure 58, Figure 59), within the Study Area. All project impacts must avoid the legal cemetery boundaries which are well defined (Images 24-30; Figure 36: area outlined in purple).

A white chain fence marks the southern limits of the western portion of the cemetery. Mature pines border the southern limits of the eastern portion of the cemetery. The eastern limits are marked by post and wire fencing before a steep gorge. The westernmost portion of land west of the gravel access road was added to the cemetery in the 1980s. Older grave markers are within the eastern portion of the Study Area, with the newest grave markers in the new western portion. Cemetery plans dating to 1991 and 1992 were provided by the Bereavement Authority of Ontario (See Appendix A Figure 57 to Figure 59) which have the cemetery divided into three sections: the narrow westmost strip of the new addition, the portion next to it labelled “Zone 1”, and the eastmost portion labelled “Zone 2”.

During the property inspection, Jessica Lytle (P1066) met with Gary Smith, caretaker of the Springford Community Cemetery. According to Gary, Oxford Road 19 originally was in a straight alignment adjacent the cemetery and was realigned around 1968 to the slight jog to the south that is the road’s present location. This occurred shortly after the construction of the present bridge at Spittler Creek, to the south of the original bridge. The realignment included a cut out of the landscape for a less severe slope down to the bridge. During this, Gary noted that human remains were encountered in the slope, which were pushed back into place. As such, there is potential for unmarked graves along the outside edge of the historical boundaries of the cemetery within the road



cut of the present right-of-way. Construction monitoring is required within the north side Oxford Road 19 right-of-way adjacent the earlier sections of the cemetery (Zone 1, Zone 2). This monitoring program should be designed in consideration of project impacts.

If future works are unable to avoid the legal boundaries of the cemetery and archaeological fieldwork is required, detailed strategies should be formulated once the impacts are understood. At a minimum the proponent must discuss this work with the BAO to ensure it addresses regulations under the Funeral, Burial and Cremation Services Act, 2002 and any invasive Stage 2-4 archaeological fieldwork will require a Cemetery Investigation Authorization.

Erbtown Cemetery

Erbtown Cemetery (Figure 40, Figure 41) is an inactive cemetery located within the eastmost portion of Lot 14, Concession 9 in Norwich Township. Erbtown Cemetery is approximately 65 metres south of the Study Area (Images 36-37; Figure 41: area outlined in purple). The 1975 Plan of Survey provided by the Bereavement Authority of Ontario shows the property limits labelled “abandoned free Methodist Cemetery Taken Over by the Township of Norwich.” The cemetery limits and burial plots are not demonstrated.

Erbtown Cemetery is set back from the south side of Otterville Road, and the grave markers have been gathered and placed in a wide “V” shaped monument. The cemetery is fenced by metal poles linked by horizontal chains along the north, west, and east sides. There is an opening on north side under the Erbtown Cemetery signage to enter the cemetery, with hardware for a gate. The south side of the cemetery is bounded by trees.

A letter from the Ontario Ministry of Consumer and Commercial Relations on October 31, 1980 (see Appendix A Figure 63) indicates approval for the trees along the south end to be planted. The provided photograph shows that by this time the grave markers were gathering into a V shaped monument, and the south end was bound by an agricultural field (Figure 61). The photograph depicts the cemetery name over a gate, and the cemetery does not appear to be fenced at that time. The letter references the completion of erecting the



cemetery name over the gate. The 1975 Plan of Survey indicates the land Erbtown Cemetery is on, part of Lot 14, Concession 9, to be abandoned Free Methodist Cemetery taken over by the Township of Norwich (Figure 60).

There is low potential for unmarked graves within the Study Area. No Stage 3 Cemetery Investigation is required for the Study Area.

Pine Street Burial Grounds

The Pine Street Burial Grounds (Figure 41) are located within the western half of Lot 13, Concession 9 in Norwich Township, within the Study Area. All project impacts must avoid the cemetery lands (Images 39-40; Figure 40-Figure 41: area outlined in purple). The 1975 Plan of Survey provided by the Bereavement Authority of Ontario shows the property limits labelled “Trustees of the Society of Friends.” The cemetery limits and burial plots are not shown.

The grave markers of the Pine Street Burial Grounds have been gathered and placed in a wide “V” shaped monument. The Pine Street Burial Grounds are enclosed by a short black wrought iron fence with a metal swinging gate centred at the point of the monument. The northern limit, marked by fencing, is parallel to and 10 metres from the southern Otterville Road edge.

A letter from the Ontario Ministry of Consumer and Commercial Relations on October 31, 1980 (see Appendix A Figure 63), indicates approval for the Pine Street Burial Grounds grave markers to be gathered and placed in the proposed arranging of monuments. A photograph was also provided of the cemetery’s conditions at the time, showing grave markers spread out behind the gate and wrought iron fencing (Figure 62). The letter notes completion of erecting the cemetery name over the gate. It also mentions repairs to the wrought iron fence, indicating the fencing was not an addition from 1980.

The 1975 Plan of Survey indicates the land the Pine Street Burial Grounds is on, part of Lot 13, Concession 9, belongs to Trustees of the Society of Friends (Figure 60).



Since the location of burials is unclear, there is potential for unmarked graves within the cemetery property in the Study Area. If future works are unable to avoid the legal boundaries of the cemetery and archaeological fieldwork is required, detailed Stage 3 cemetery investigation strategies should be formulated once the impacts are understood. At a minimum the proponent must discuss this work with the BAO to ensure it addresses regulations under the Funeral, Burial and Cremation Services Act, 2002 and any invasive Stage 2-4 archaeological fieldwork will require a Cemetery Investigation Authorization.

Pettman Cemetery

The Pettman Cemetery (AfHe-26) is registered as an archaeological site but is not a registered cemetery. The O.A.S.D. places the site approximately 100 metres north of the Study Area (see *Supplementary Documentation*) west of Cranberry Line. It was recorded in Gary Foster's 1982 survey of Oxford County Road 19 between Springford and Ostrander, and a Mrs. M. Pettman provided the cemetery location prior to that assessment. Foster found four marble corner grave marker fragments and one limestone corner grave marker fragment were located concentrated in an area approximately 50 metres north of County Road 19 along a fence line. Locals also suggested to Foster that the cemetery was larger at one time and the grave markers had been removed for farming activities.

During the property inspection, Jessica Lytle (P1066) met with Charles Pettman, a local property owner who grew up in the area, to discuss the Pettman Cemetery. Charles is also the manager of the St. Charles Anglican Cemetery. The area is currently an active agricultural field, and no evidence of grave markers or soil inclusions were observed at the time of the property inspection. Although the site was named Pettman Cemetery in the 1982 archaeological survey, Charles Pettman indicated there was no personal connection to the burials outside of knowledge of the cemetery's existence. According to Charles, this cemetery was a historical Black community burying ground approximately 15 metres by 15 metres in size located west of a small former road, which he says was removed when Cranberry Line was constructed to its east around 1944. It was in the 1940s that Charles remembers the grave markers and white fencing



were removed by a local farmer. Charles did not recall the burials being exhumed and moved. This report could not find any historical mapping indicating evidence of such a road. Charles mentioned that his friend (owner of the land/farm with the cemetery) remembers seeing additional stones on the south side of the road in Lot 5, Concession 9, but no evidence exists at present.

A Stage 3 Cemetery Investigation should be conducted within the Study Area on the north and south sides of the road near the reported cemetery/site, in order to determine the presence of any unmarked graves (Figure 19: areas hatched in black). Stage 3 cemetery investigation would be tailored to the project impacts but will likely entail complete mechanical topsoil removal under the supervision of a licensed archaeologist within the preferred design concepts. The exposed subsoil would then be shovel-shined and examined for the presence of burial shafts.

Lands within the Study Area on the north and south side of the road near the reported cemetery location should be reassessed during the Stage 2 survey based on the findings of the Foster survey, and the approximate location of the Pettman Cemetery site should be noted during any future work in this area.

3.2 Conclusions

The Stage 1 background study determined 27 previously registered archaeological sites are located within one kilometre of the Study Area. Two sites (AfHe-21, AfHe-22) are located within the Study Area, and six sites (AfHe-19, AfHe-20, AfHe-23, AfHe-24, AfHe-25, AfHe-27) are located within 50 metres. Springford Community Cemetery and Pine Street Burial Ground are located within 20 metres of the Study Area and the legal cemetery boundaries must be avoided by project designs. Stage 3 cemetery investigation is recommended at both cemeteries. The Pettman Cemetery (AfHe-26) is approximately 100 metres north of the Study Area; however the limits are unknown and based on local knowledge of the cemetery, lands within the Study Area north and south of the road require Stage 3 cemetery investigation.



The property inspection determined that parts of the Study Area exhibit archaeological potential and will require archaeological assessment (Figure 16-Figure 55: areas highlighted in orange and green).

The preferred design concepts (see Appendix B) indicate that the proposed grading limits impact some areas of archaeological potential beyond the existing right-of-way, as well as lands within the existing right-of-way near AfHe-26 the Pettman Cemetery site (Appendix B Plan 2) and the Springford Community Cemetery (Appendix B Plan 7).

4.0 Recommendations

The following recommendations are made:

- 1 Parts of the Study Area and preferred design concepts exhibit archaeological potential. These lands require Stage 2 archaeological assessment by test pit/pedestrian survey at five metre intervals, where appropriate (Figure 16-Figure 55: areas highlighted in orange and green). Stage 2 is required prior to any proposed construction activities on these lands;
- 2 AfHe-19, AfHe-20, AfHe-21 and AfHe-22 AfHe-23, AfHe-24, AfHe-25, and AfHe-27 are previously registered sites within 50 metres of the Study Area identified in 1981 by Foster (1982-15). Due to the passage of time and paucity of mapping associated with the former archaeological assessment, these sites must be reassessed during any Stage 2 survey to determine cultural heritage value or interest as per the 2011 S & G.
- 3 AfHe-26 Pettman Cemetery was identified in 1981 by Foster (1982-15) and is known from local knowledge to be a historical Black community burying ground possibly located within 50 metres of the Study Area and preferred design concepts. The cemetery boundaries and number of burials remains unknown but is in close proximity to the preferred design concepts. Further archaeological assessment is required:



- a. Lands within the Study Area should be assessed/reassessed by Stage 2 pedestrian survey on the north and south side of the roads for near-surface cultural material;
 - b. Stage 3 cemetery investigation specifically tailored to the project impacts would be developed by a licensed archaeologist. Mechanical topsoil removal should be conducted across the entire area to be impacted since there are no known cemetery boundaries. The exposed subsoil will then be shovel-shined and thoroughly examined for the presence of burial shafts.
 - c. If any burials are documented during the course of construction monitoring, the police or coroner and the Register of Burial Sites at the Ministry of Government and Consumer Services must be contacted as per the Cemeteries Act, R.S.O. 1990 c. C.4 and the Funeral, Burial and Cremation Services Act, 2002.
- 4 Springford Community Cemetery is within the Study Area and Preferred Design Concept (Figure 36: area outlined in purple).
- a. If future works are unable to avoid the legal boundaries of the cemetery and archaeological fieldwork is required, detailed strategies should be formulated once the impacts are understood. At a minimum the proponent must discuss this work with the BAO to ensure it addresses regulations under the Funeral, Burial and Cremation Services Act, 2002 and any invasive Stage 2-4 archaeological fieldwork will require a Cemetery Investigation Authorization.
 - b. Despite the cemetery boundaries being well documented, there is potential for burials to extend into the historical road alignment. Any construction impacts within the current road right-of-way will require a Stage 3 Cemetery Investigation to be conducted to confirm the presence of burial shafts outside the known limits of this cemeteries.



- c. Prior to any Stage 3 cemetery investigation or archaeological monitoring, consultation with the MCM, the Bereavement Authority of Ontario, and a Cemetery Investigation Authorization issued by the Bereavement Authority of Ontario¹ will be required prior to any “invasive” (Stages 2-4) fieldwork.
 - d. Due to the existing road cut and field conditions from the past road improvements, mechanical topsoil removal is not feasible. Archaeological construction monitoring is recommended (Figure 36: areas hatched pink). A licensed archaeologist will be present on-site during impacts within the 10-metre monitoring buffers to monitor for the presence of burial shafts outside the known limits of these cemeteries. The licensed archaeologists shall have the authority to halt soil-disturbing activity within the 10-metre monitoring buffer should they perceive a need to examine exposed deposits.
 - e. If any burials are documented during the course of construction monitoring, the police or coroner and the Register of Burial Sites at the Ministry of Government and Consumer Services must be contacted as per the Cemeteries Act, R.S.O. 1990 c. C.4 and the Funeral, Burial and Cremation Services Act, 2002.
- 5 Pine Street Burial Ground is within the Study Area (Figure 41: area outlined in purple). The cemetery property must be avoided by any proposed construction. Any construction impacts within the cemetery’s legal boundaries will require a Stage 3 Cemetery Investigation to be conducted to confirm the presence of burial shafts. While the preferred design concepts do not propose any impacts to this area, the following conditions would apply for future work:

¹ An Investigation Authorization is required whenever archaeological investigations are contemplated to verify and/or determine the boundaries of a cemetery or any similar situation where the records, maps and plans of the cemetery cannot confirm the existence and exact locations of burials within that cemetery.



- a. Lands within the Study Area should be assessed by Stage 2 test pit survey;
 - b. Once specific project impacts are understood, Stage 3 cemetery investigation specifically tailored to the project impacts should be developed by a licensed archaeologist and may include archaeological mechanical topsoil removal, observed by a licensed archaeologist;
 - c. Prior to any Stage 3 cemetery investigation or archaeological monitoring, consultation with the MCM, the Bereavement Authority of Ontario, and a Cemetery Investigation Authorization issued by the Bereavement Authority of Ontario will be required prior to any “invasive” (Stages 2-4) fieldwork.
 - d. If any burials are documented during the course of construction, the police or coroner and the Register of Burial Sites at the Ministry of Government and Consumer Services must be contacted as per the Cemeteries Act, R.S.O. 1990 c. C.4 and the Funeral, Burial and Cremation Services Act, 2002.
- 6 The remainder of the Study Area does not retain archaeological potential on account of deep and extensive land disturbance or low and wet conditions. These lands do not require further archaeological assessment; and,
- 7 Should the proposed work extend beyond the current Study Area, further archaeological assessment should be conducted to determine the archaeological potential of the surrounding lands.

NOTWITHSTANDING the results and recommendations presented in this study, ASI notes that no archaeological assessment, no matter how thorough or carefully completed, can necessarily predict, account for, or identify every form of isolated or deeply buried archaeological deposit. In the event that archaeological remains are found during subsequent construction activities, the consultant archaeologist, approval authority, and the Archaeology Programs



Unit of the Ministry of Citizenship and Multiculturalism should be immediately notified.

The above recommendations are subject to Ministry approval, and it is an offence to alter any archaeological site without Ministry of Citizenship and Multiculturalism concurrence. No grading or other activities that may result in the destruction or disturbance of any archaeological sites are permitted until notice of MCM approval has been received.

5.0 Legislation Compliance Advice

ASI advises compliance with the following legislation:

- This report is submitted to the Ministry of Citizenship and Multiculturalism as a condition of licensing in accordance with Part VI of the *Ontario Heritage Act*, RSO 2005, c 0.18. The report is reviewed to ensure that it complies with the standards and guidelines that are issued by the Minister, and that the archaeological field work and report recommendations ensure the conservation, preservation, and protection of the cultural heritage of Ontario. When all matters relating to archaeological sites within the project area of a development proposal have been addressed to the satisfaction of the Ministry of Citizenship and Multiculturalism, a letter will be issued by the Ministry stating that there are no further concerns with regards to alterations to archaeological sites by the proposed development.
- It is an offence under Sections 48 and 69 of the *Ontario Heritage Act* for any party other than a licensed archaeologist to make any alteration to a known archaeological site or to remove any artifact or other physical evidence of past human use or activity from the site, until such time as a licensed archaeologist has completed archaeological field work on the site, submitted a report to the Minister stating that the site has no further cultural heritage value or interest, and the report has been filed in the Ontario Public Register of Archaeology Reports referred to in Section 65.1 of the *Ontario Heritage Act*.



- Should previously undocumented archaeological resources be discovered, they may be a new archaeological site and therefore subject to Section 48 (1) of the *Ontario Heritage Act*. The proponent or person discovering the archaeological resources must cease alteration of the site immediately and engage a licensed consultant archaeologist to carry out archaeological fieldwork, in compliance with sec. 48 (1) of the *Ontario Heritage Act*.
- The *Funeral, Burial and Cremation Services Act, 2002, S.O. 2002, c.33*, requires that any person discovering or having knowledge of a burial site shall immediately notify the police or coroner. It is recommended that the Registrar of Cemeteries at the Ministry of Consumer Services is also immediately notified.
- Archaeological sites recommended for further archaeological field work or protection remain subject to Section 48(1) of the *Ontario Heritage Act* and may not be altered, nor may artifacts be removed from them, except by a person holding an archaeological license.

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7.0 Images

7.1 Field Photography



Image 1 Area is disturbed, no potential



Image 2 Area is disturbed, no potential



Image 3 Area beyond disturbed right-of-way requires Stage 2 survey



Image 4 Area is disturbed, no potential



Image 5 Area is disturbed, no potential



Image 6 Area beyond disturbed right-of-way requires Stage 2 survey



Image 7 Area beyond disturbed right-of-way requires Stage 2 survey



Image 8 Area beyond disturbed right-of-way requires Stage 2 survey



Image 9 Area beyond disturbed right-of-way requires Stage 2 survey



Image 10 Area beyond disturbed right-of-way and low and wet area requires Stage 2 survey



Image 11 Area beyond disturbed right-of-way requires Stage 2 survey



Image 12 St. Charles Anglican Church and Cemetery requires avoidance. Areas beyond disturbed path require Stage 2 survey



Image 13 Area beyond disturbed right-of-way requires Stage 2 survey



Image 14 Area beyond disturbed right-of-way requires Stage 2 survey



Image 15 Area beyond disturbed right-of-way requires Stage 2 survey



Image 16 Area beyond disturbed right-of-way requires Stage 2 survey



Image 17 Area beyond disturbed right-of-way requires Stage 2 survey



Image 18 Area beyond disturbed right-of-way requires Stage 2 survey



Image 19 Area beyond disturbed right-of-way requires Stage 2 survey





Image 20 Area beyond disturbed right-of-way requires Stage 2 survey



Image 21 Area beyond disturbed right-of-way requires Stage 2 survey



Image 22 Area beyond disturbed right-of-way requires Stage 2 survey



Image 23 Area beyond disturbed right-of-way requires Stage 2 survey



Image 24 Area beyond disturbed right-of-way requires Stage 2 survey



Image 25 Area beyond disturbed right-of-way requires Stage 2 survey



Image 26 Springford Community Cemetery requires avoidance



Image 27 Area beyond disturbed road cut requires construction monitoring



Image 28 Springford Community Cemetery requires avoidance



Image 29 Springford Community Cemetery requires avoidance



Image 30 Area beyond disturbed road cut requires construction monitoring



Image 31 Springford Community Cemetery requires avoidance



Image 32 Springford Community Cemetery requires avoidance



Image 33 Area beyond disturbed road cut requires construction monitoring



Image 34 Area beyond disturbed right-of-way requires Stage 2 survey



Image 35 Area beyond disturbed right-of-way requires Stage 2 survey



Image 36 Area beyond disturbed right-of-way requires Stage 2 survey



Image 37 Area beyond disturbed right-of-way requires Stage 2 survey



Image 38 Erbtown Cemetery parcel requires avoidance



Image 39 Erbtown Cemetery property requires avoidance



Image 40 Low and wet area, poorly drained section of Big Otter Creek where it has been culverted below Oxford Road, no potential.



Image 41 Pine Street Burial Grounds property requires avoidance



Image 42 Pine Street Burial Grounds property requires avoidance



Image 43 Swampy, low and wet area, poorly drained section of Big Otter Creek where it has been culverted at Oxford Road, no potential.





Image 44 Area beyond disturbed right-of-way requires Stage 2 survey



Image 45 Area beyond disturbed right-of-way requires Stage 2 survey



Image 46 Area beyond disturbed right-of-way requires Stage 2 survey



Image 47 Area beyond disturbed right-of-way requires Stage 2 survey



Image 48 Area beyond disturbed right-of-way requires Stage 2 survey



Image 49 Area beyond disturbed right-of-way requires Stage 2 survey





Image 50 Area beyond disturbed right-of-way requires Stage 2 survey



Image 51 Area beyond disturbed right-of-way requires Stage 2 survey



Image 52 Area beyond disturbed right-of-way requires Stage 2 survey



Image 53 Area beyond disturbed right-of-way requires Stage 2 survey



Image 54 Area beyond disturbed right-of-way and transformer station requires Stage 2 survey.



Image 55 Area is disturbed, no potential



Image 56 Area beyond disturbed right-of-way requires Stage 2 survey



Image 57 Area beyond disturbed right-of-way is low and wet, no potential





Image 58 Area beyond disturbed right-of-way requires Stage 2 survey

7.2 Historical Imagery



Image 59 224275 Ostrander Road 2013



Image 60 224275 Ostrander Road 2016



Image 61 Southland Railway in 2013



Image 62 Southland Railway in 2016



Image 63 Southwest corner of Ostrander Road and Cranberry Line 2006



Image 64 Southwest corner of Ostrander Road and Cranberry Line 2013

8.0 Maps

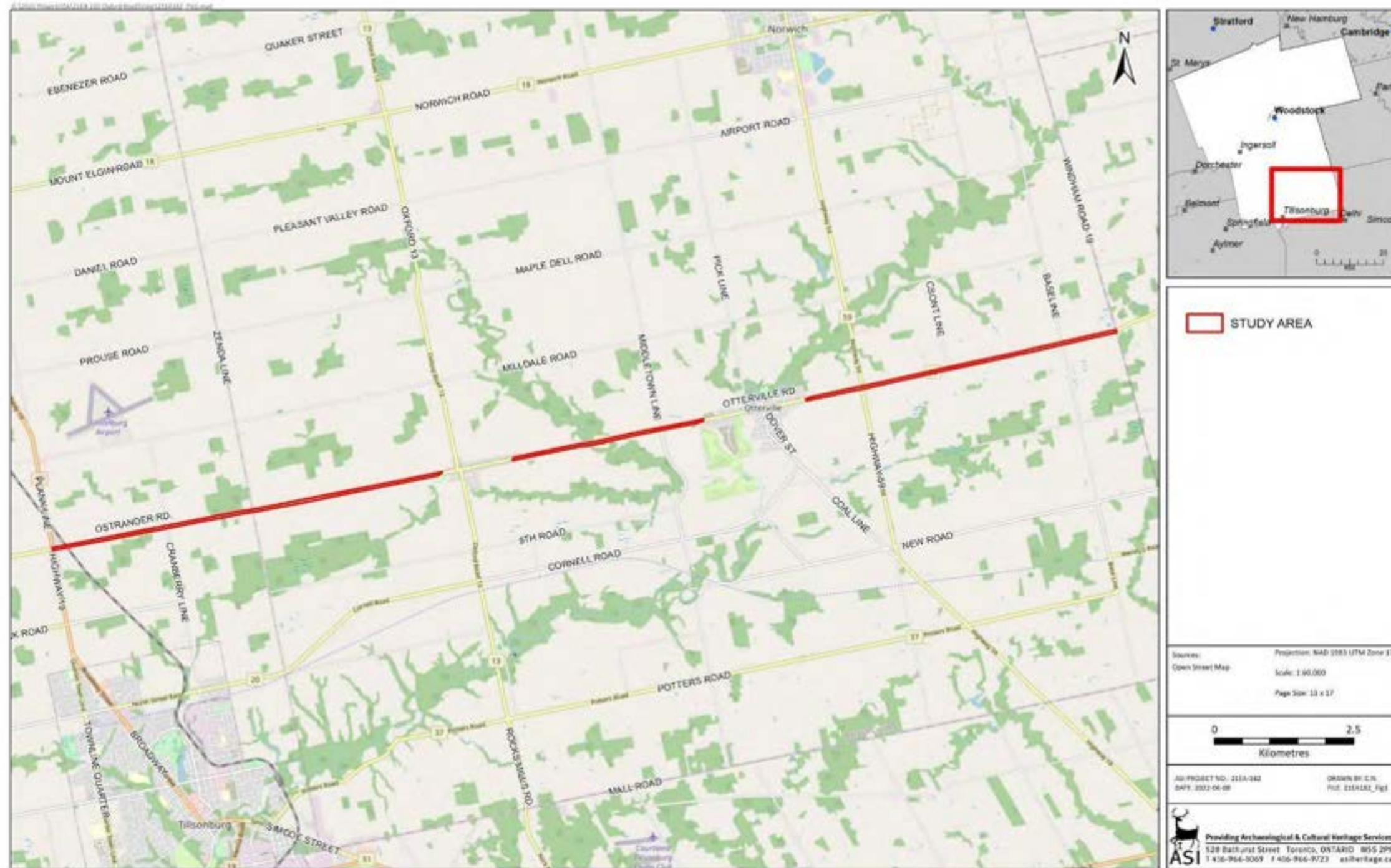


Figure 1 Oxford Road 19 Corridor Improvements Study Area

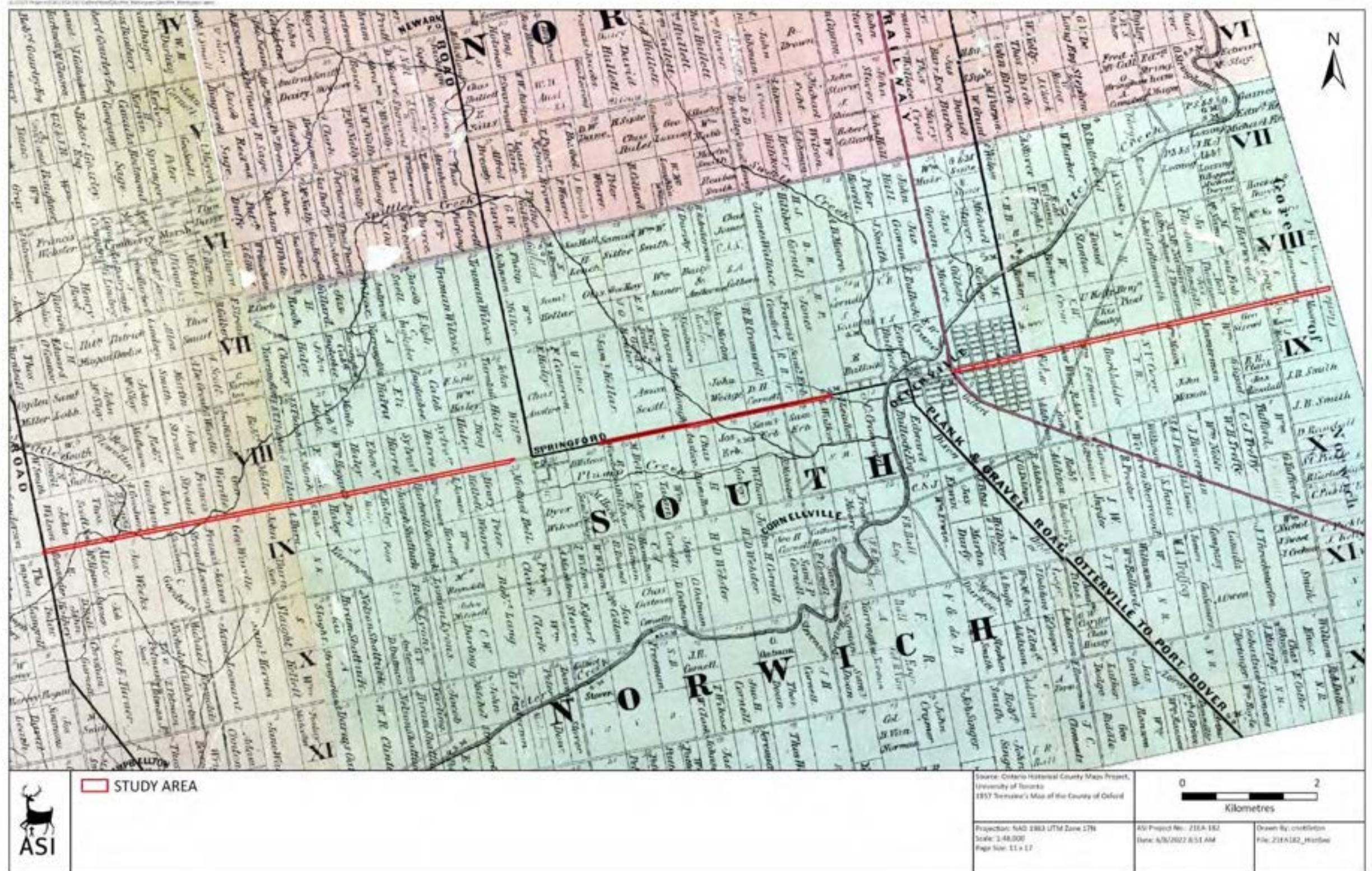


Figure 2 Study Area (Approximate Location) Overlaid on the 1857 Tremaine's Map of the County of Oxford



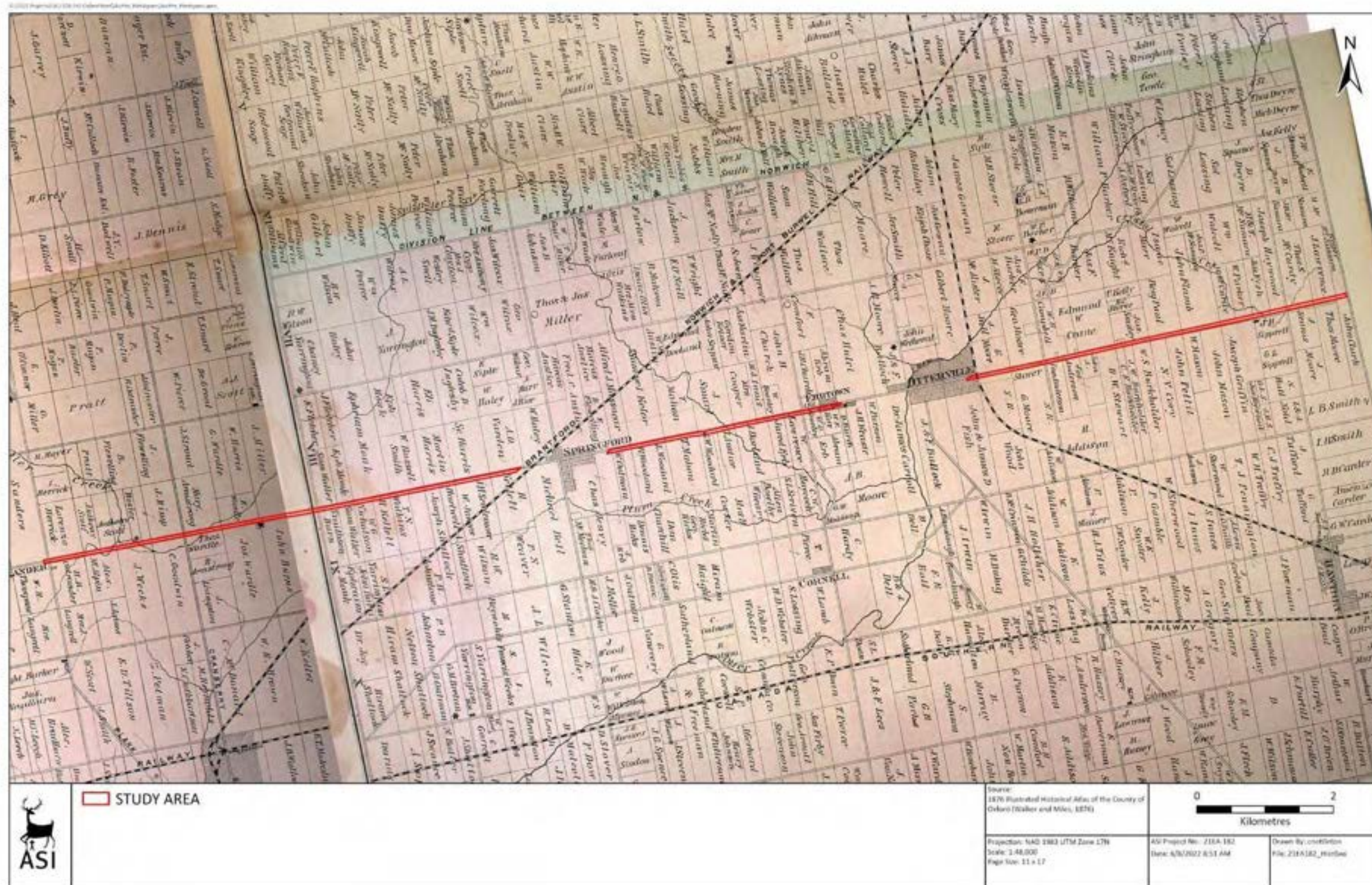


Figure 3 Study Area (Approximate Location) Overlaid on the 1876 Illustrated Historical Atlas of the County of Oxford

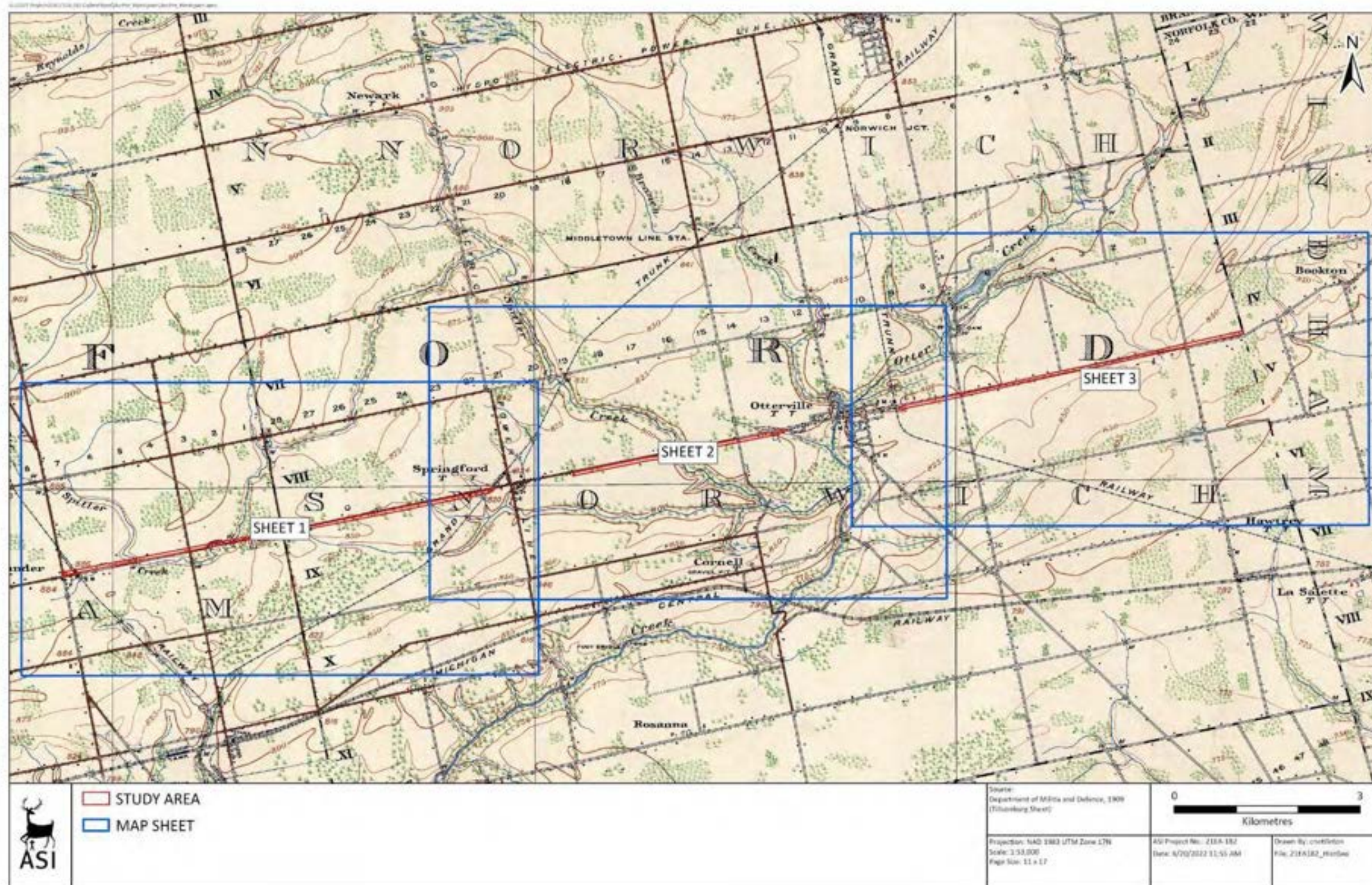


Figure 4 Study Area (Approximate Location) Overlaid on the 1909 Topographic Map Tillsonburg Sheet (Key Map)

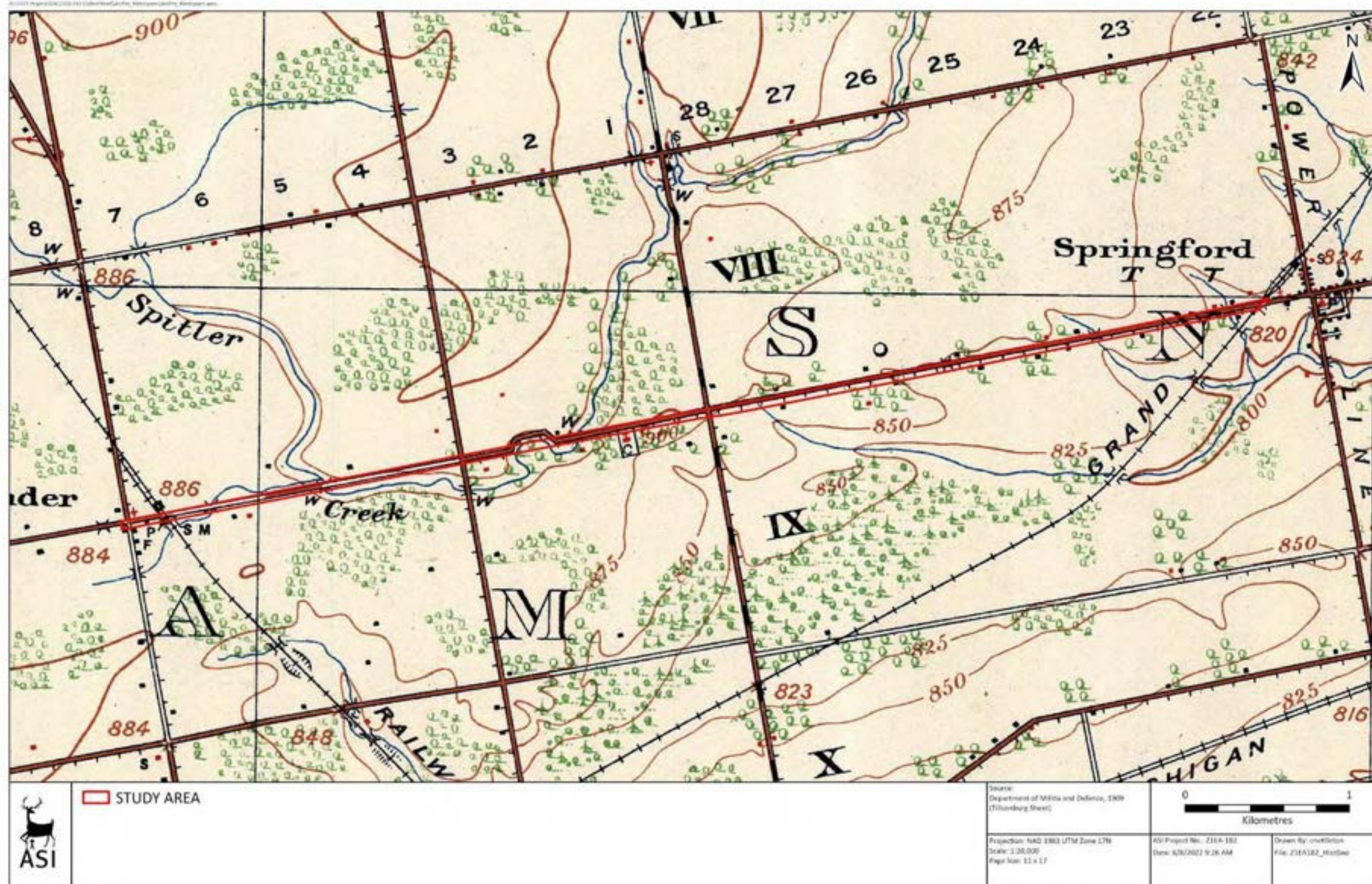


Figure 5 Study Area (Approximate Location) Overlaid on the 1909 Topographic Map Tillsonburg Sheet (Sheet 1)

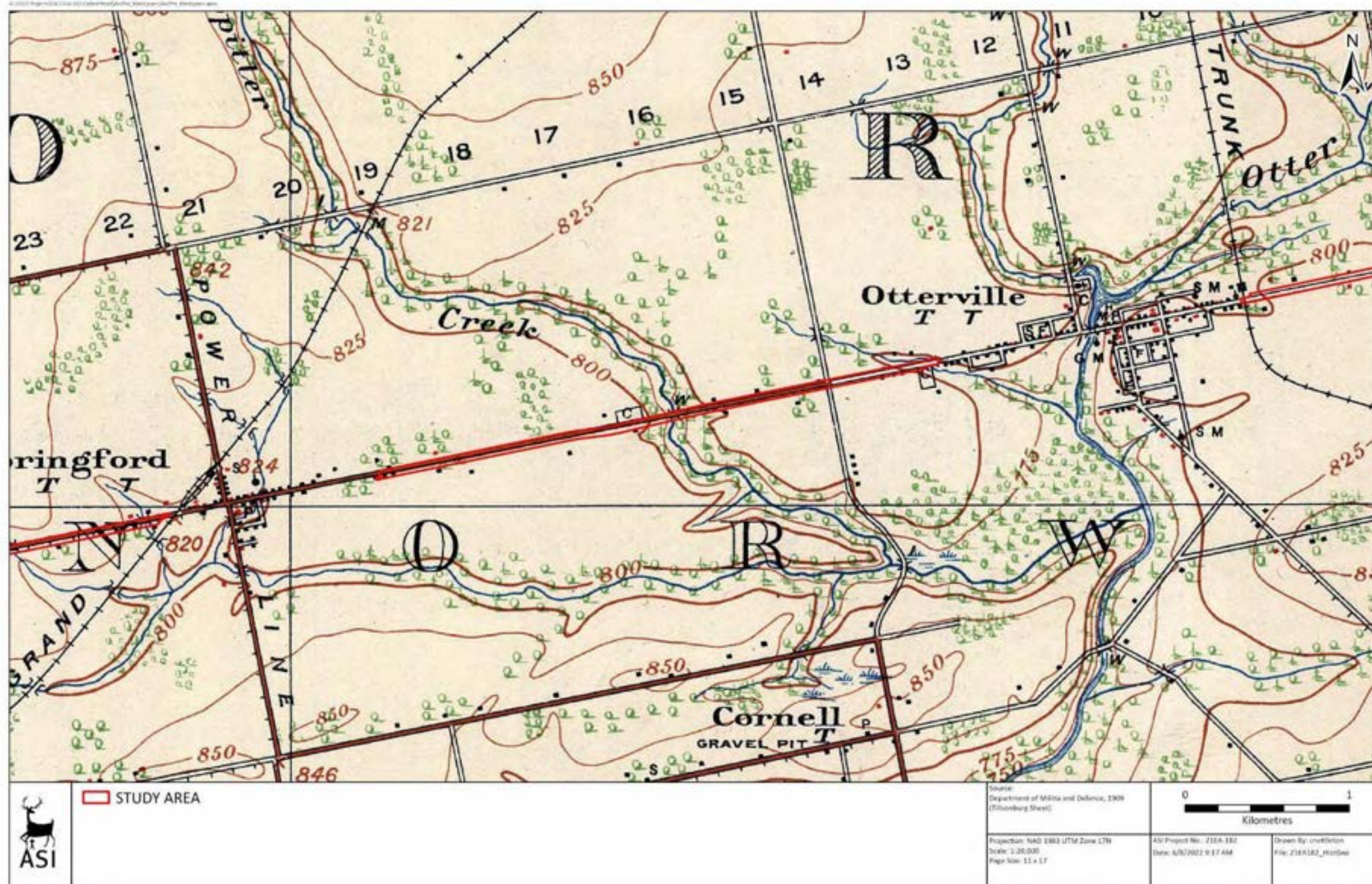


Figure 6 Study Area (Approximate Location) Overlaid on the 1909 Topographic Map Tillsonburg Sheet (Sheet 2)

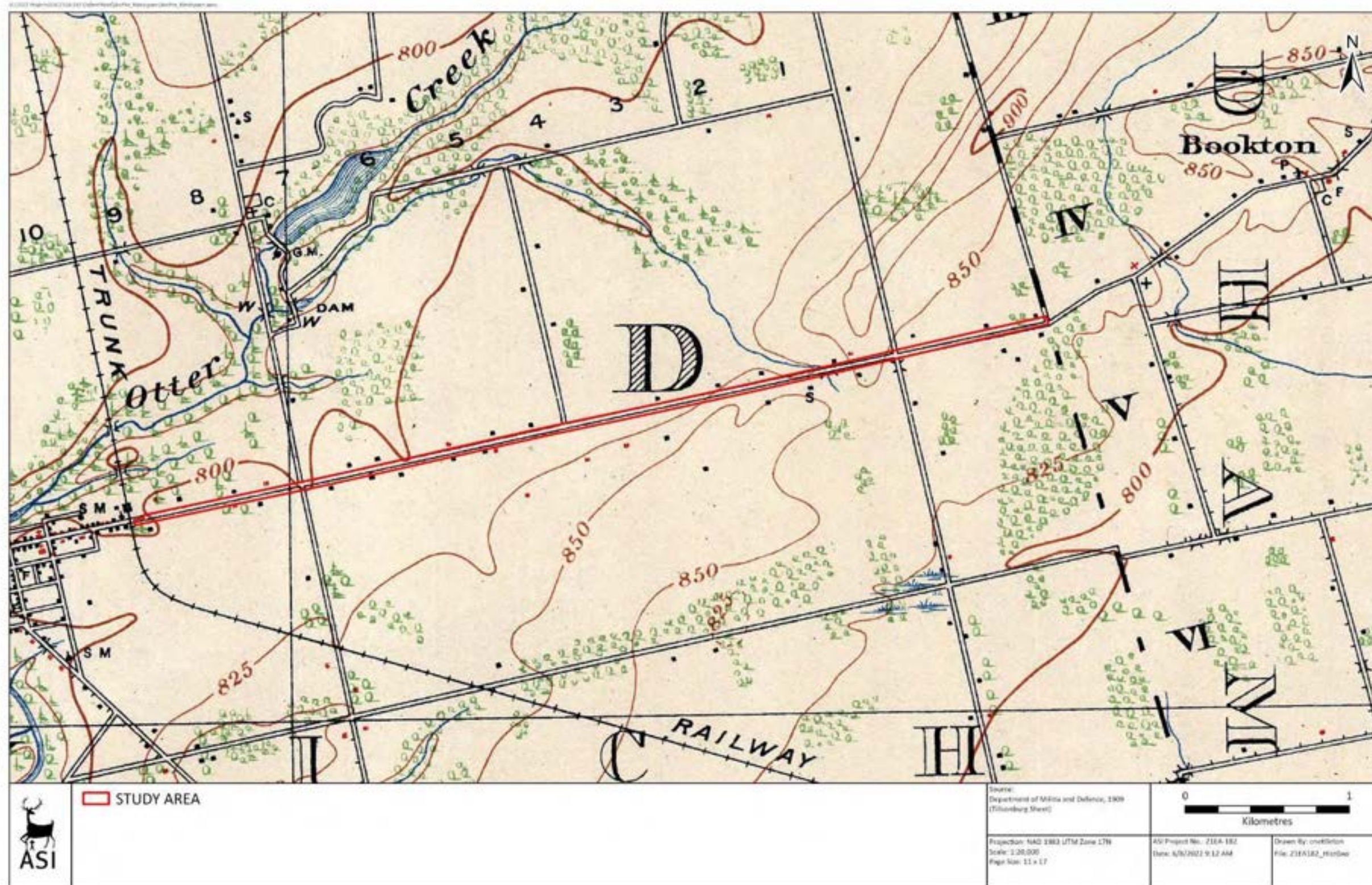


Figure 7 Study Area (Approximate Location) Overlaid on the 1909 Topographic Map Tillsonburg Sheet (Sheet 3)

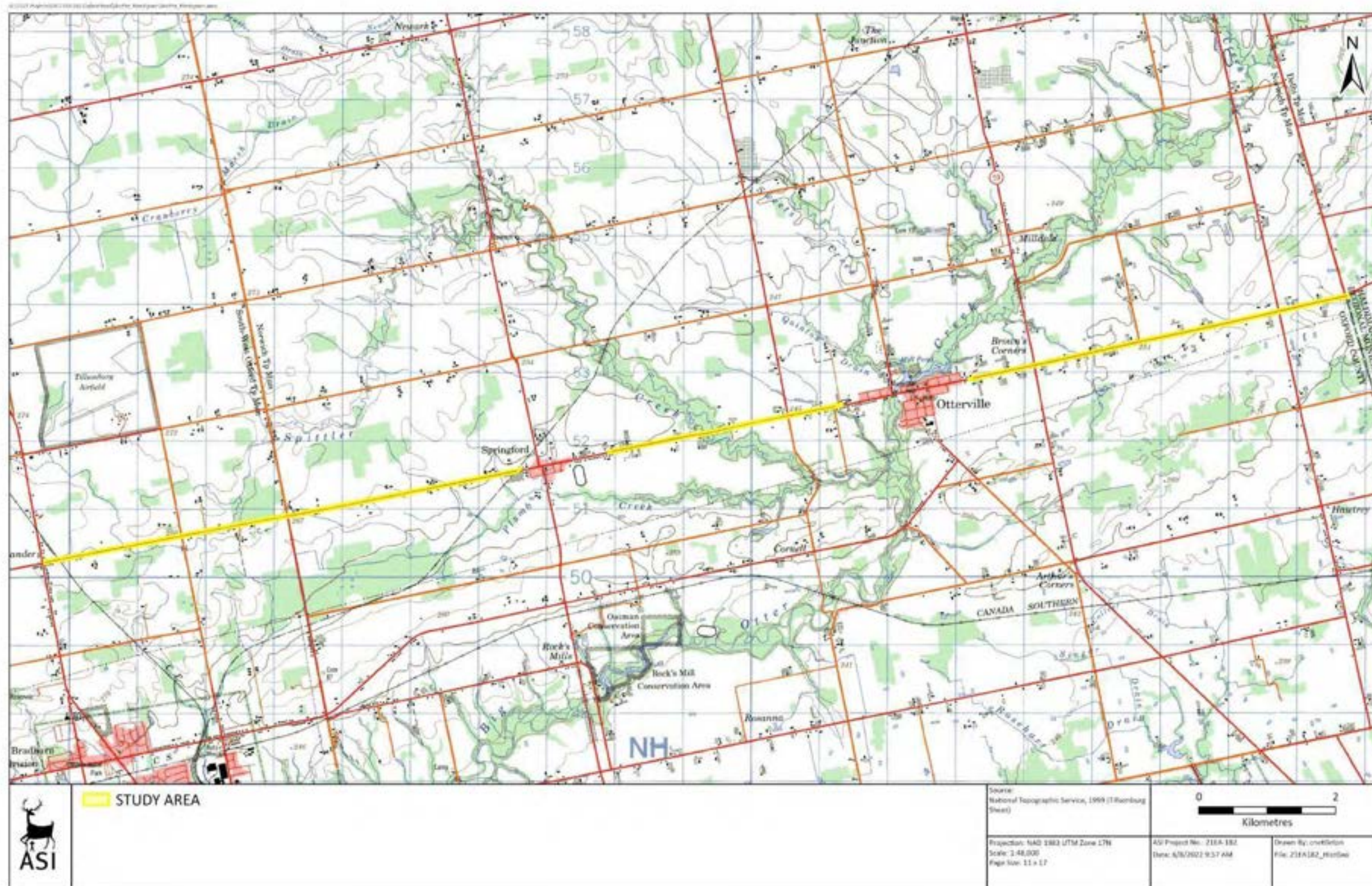


Figure 8 Study Area (Approximate Location) Overlaid on the 1999 National Topographic Series Tillsonburg Sheet

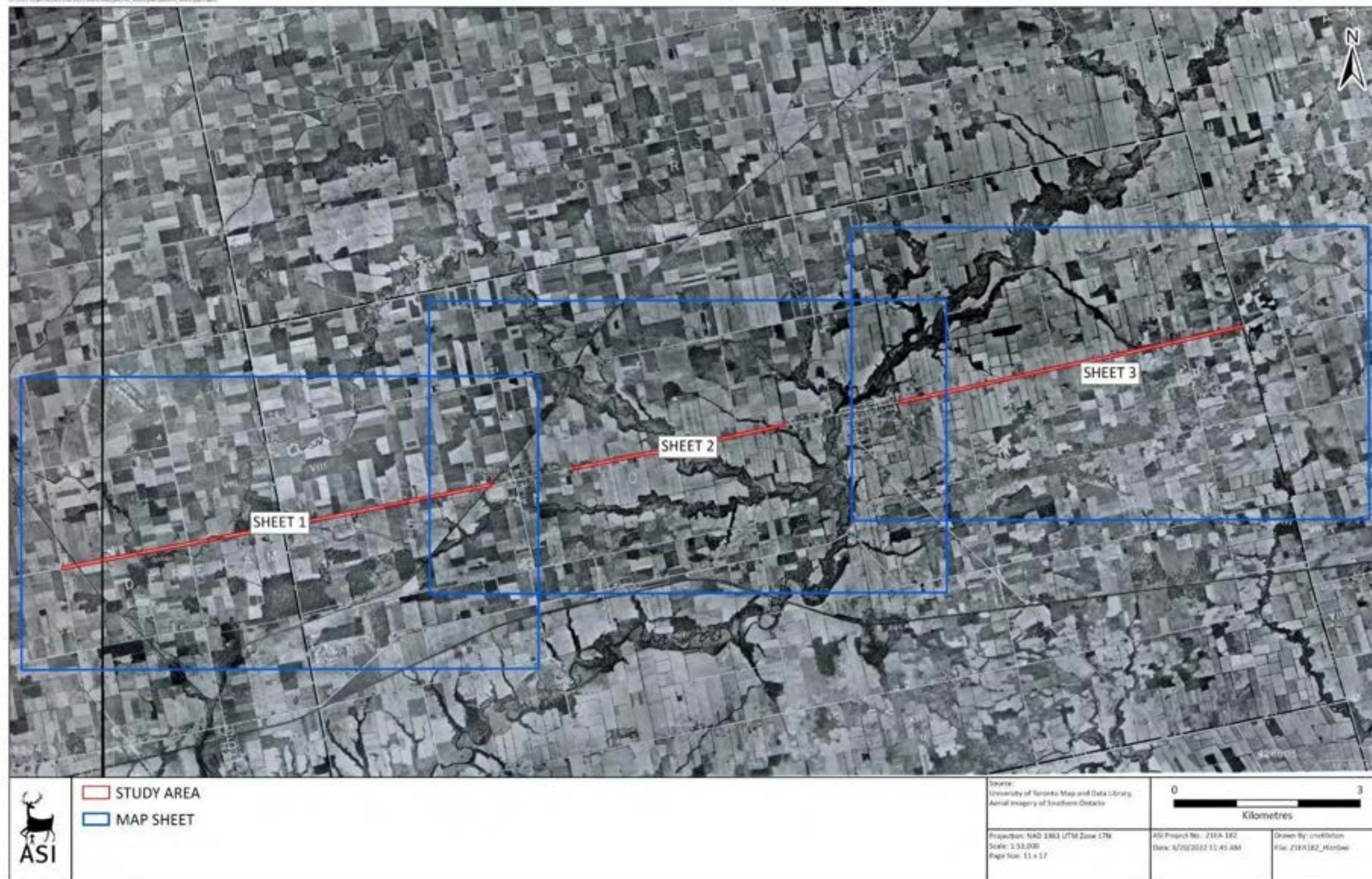


Figure 9 Study Area (Approximate Location) Overlaid on the 1954 Aerial Photography (Sheet 2)



Figure 10 Study Area (Approximate Location) Overlaid on the 1954 Aerial Photography (Sheet 1)



Figure 11 Study Area (Approximate Location) Overlaid on the 1954 Aerial Photography (Sheet 2)



Figure 12 Study Area (Approximate Location) Overlaid on the 1954 Aerial Photography (Sheet 3)

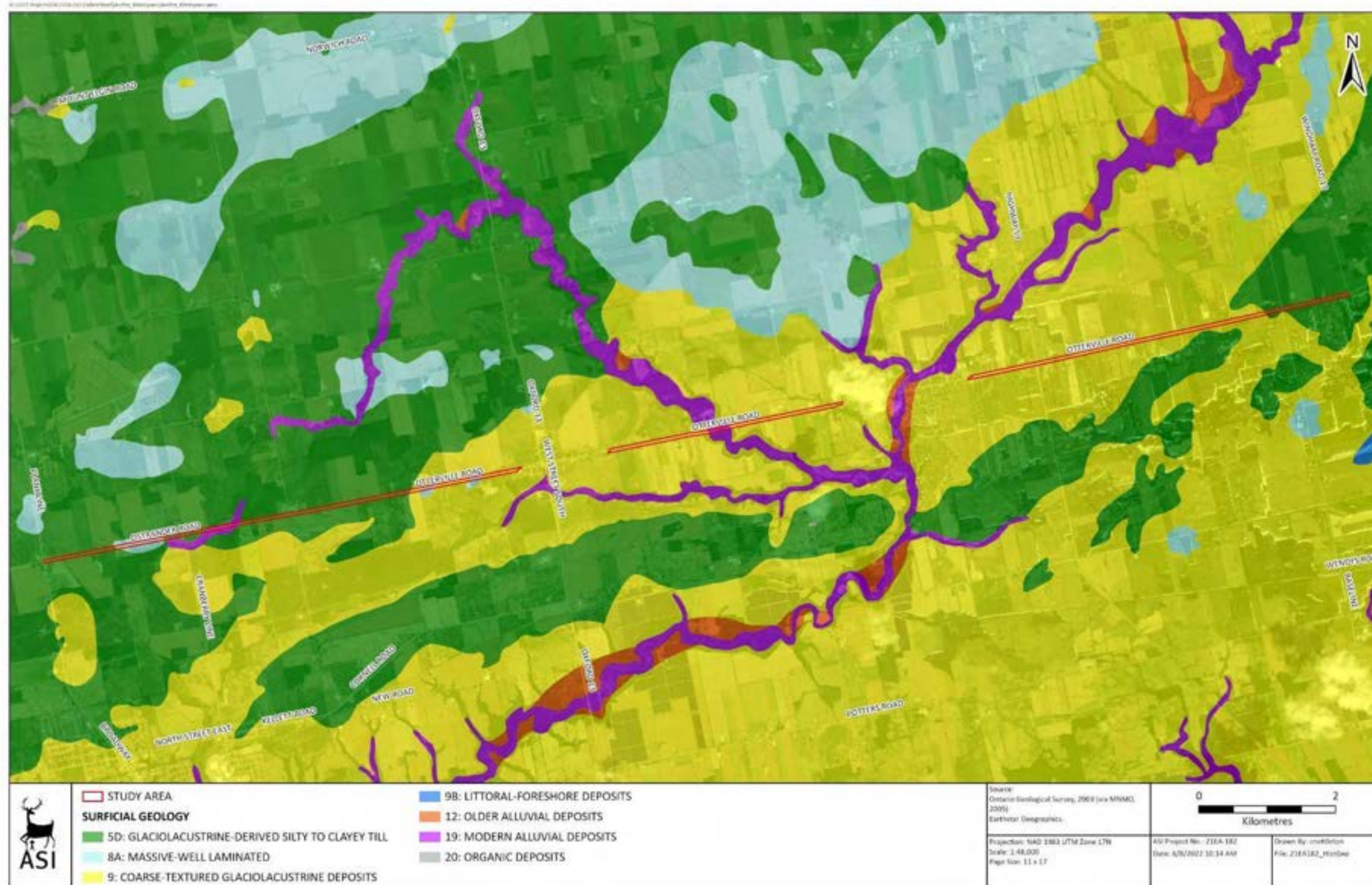


Figure 13 Study Area -Surficial Geology

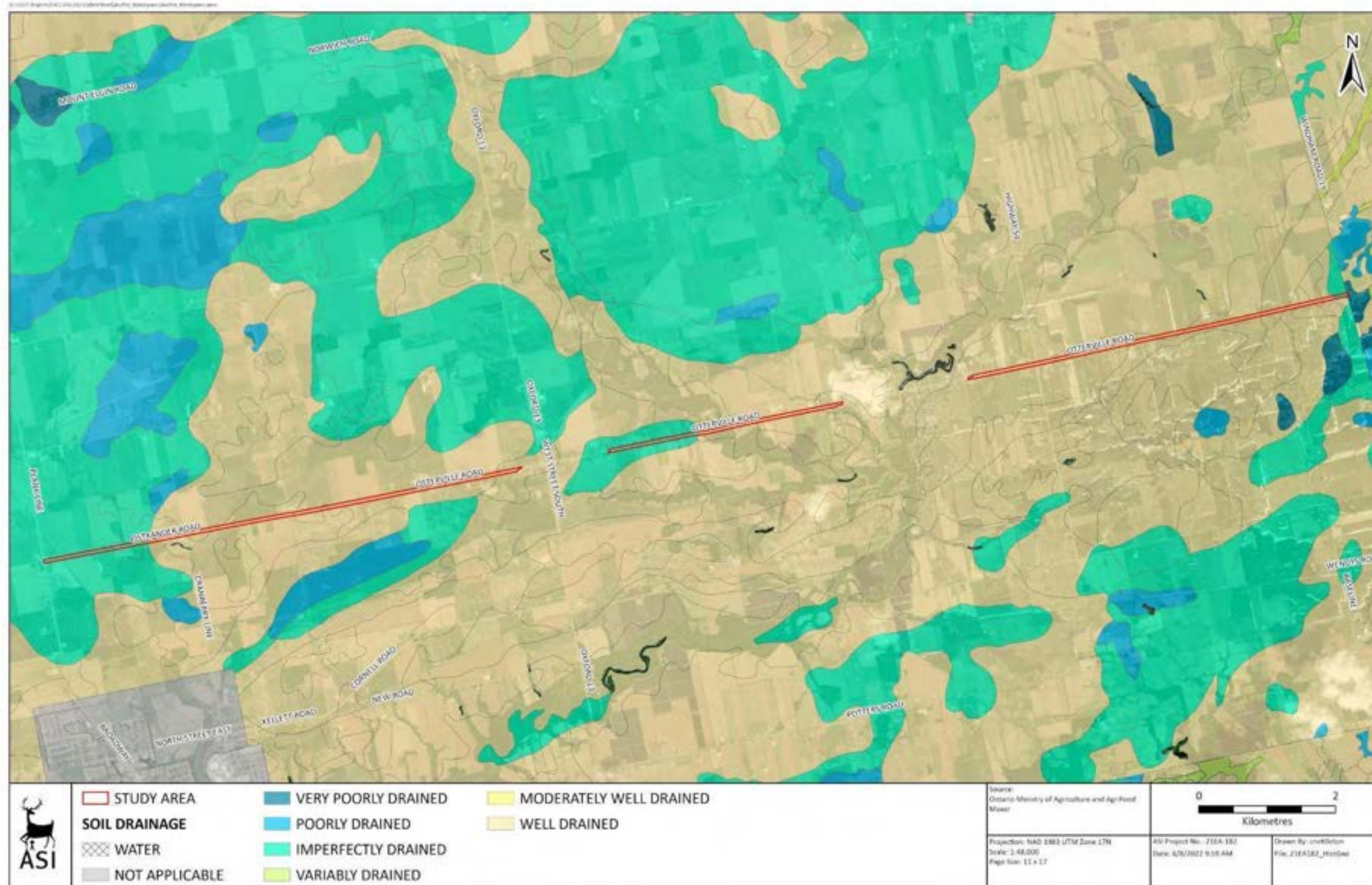


Figure 14 Study Area -Soil Drainage

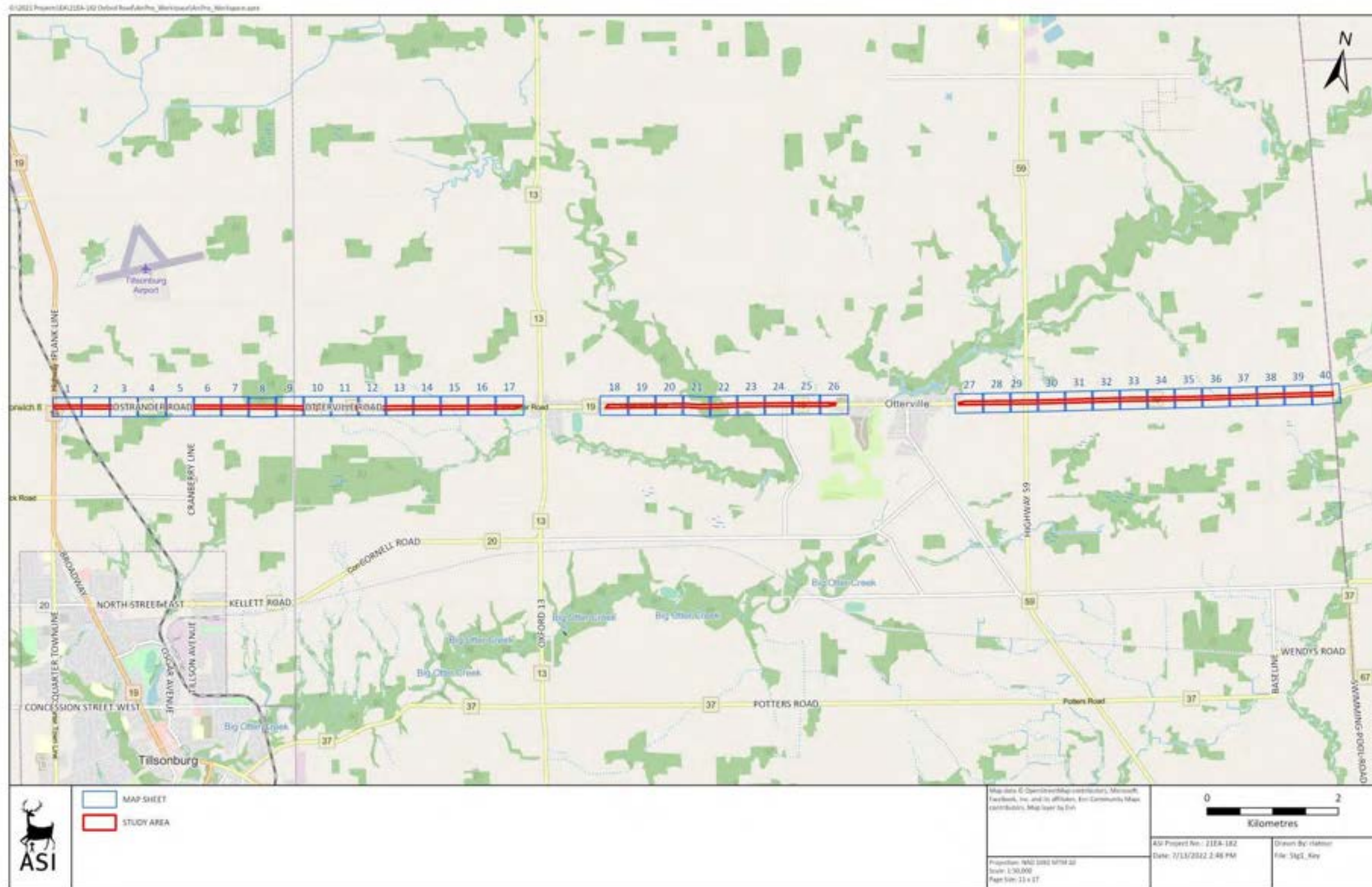


Figure 15 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Key Map)



Figure 16 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 1)



Figure 17 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 2)



Figure 18 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 3)





Figure 19 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 4)





Figure 20 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 5)



Figure 21 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 6)





Figure 22 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 7)





Figure 23 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 8)



Figure 24 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 9)





Figure 25 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 10)





Figure 26 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 11)





Figure 27 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 12)





Figure 28 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 13)





Figure 29 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 14)



Figure 30 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 15)





Figure 31 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 16)



Figure 32 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 17)



Figure 33 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 18)



Figure 34 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 19)



Figure 35 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 20)



Figure 36 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 21)



Figure 37 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 22)





Figure 38 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 23)





Figure 39 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 24)



Figure 40 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 25)

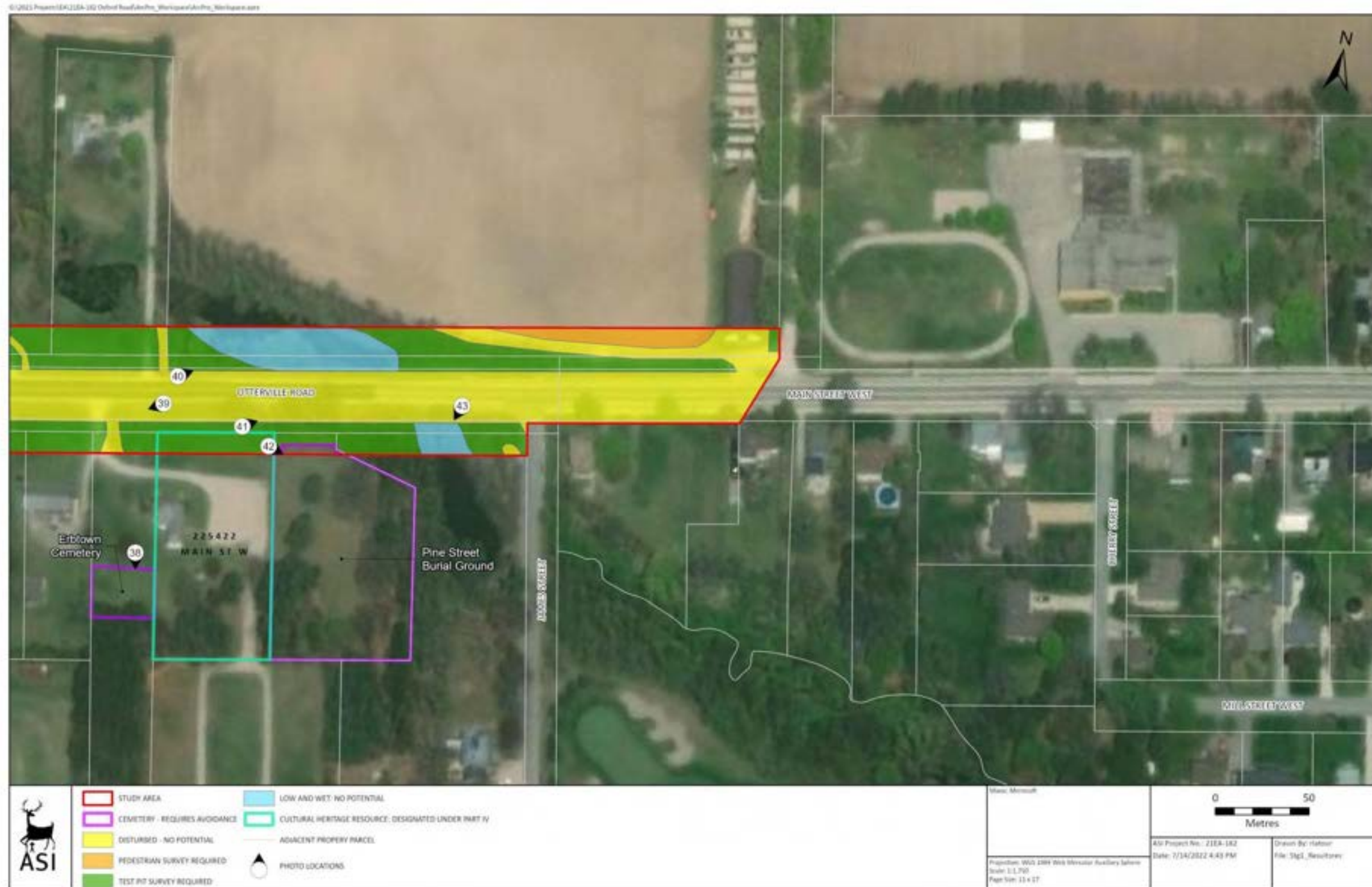


Figure 41 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 26)

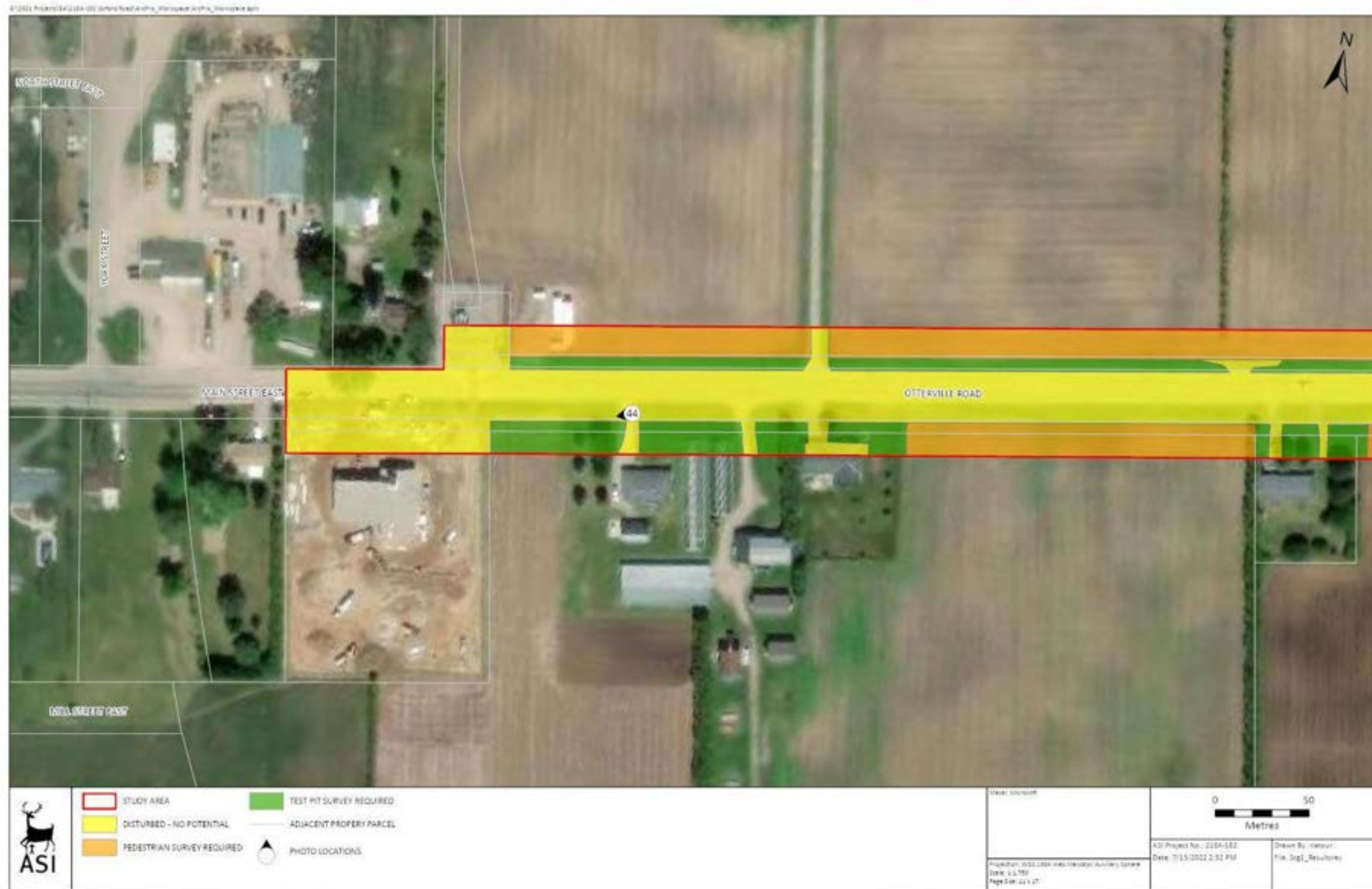


Figure 42 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 27)



Figure 43 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 28)



Figure 44 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 29)



Figure 45 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 30)



Figure 46 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 31)



Figure 47 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 32)



Figure 48 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 33)





Figure 49 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 34)





Figure 50 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 35)





Figure 51 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 36)





Figure 52 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 37)





Figure 53 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 38)





Figure 54 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 39)





Figure 55 Oxford Road 19 Corridor Improvements – Results of Stage 1 (Sheet 40)

Appendix A: Cemetery Documentation

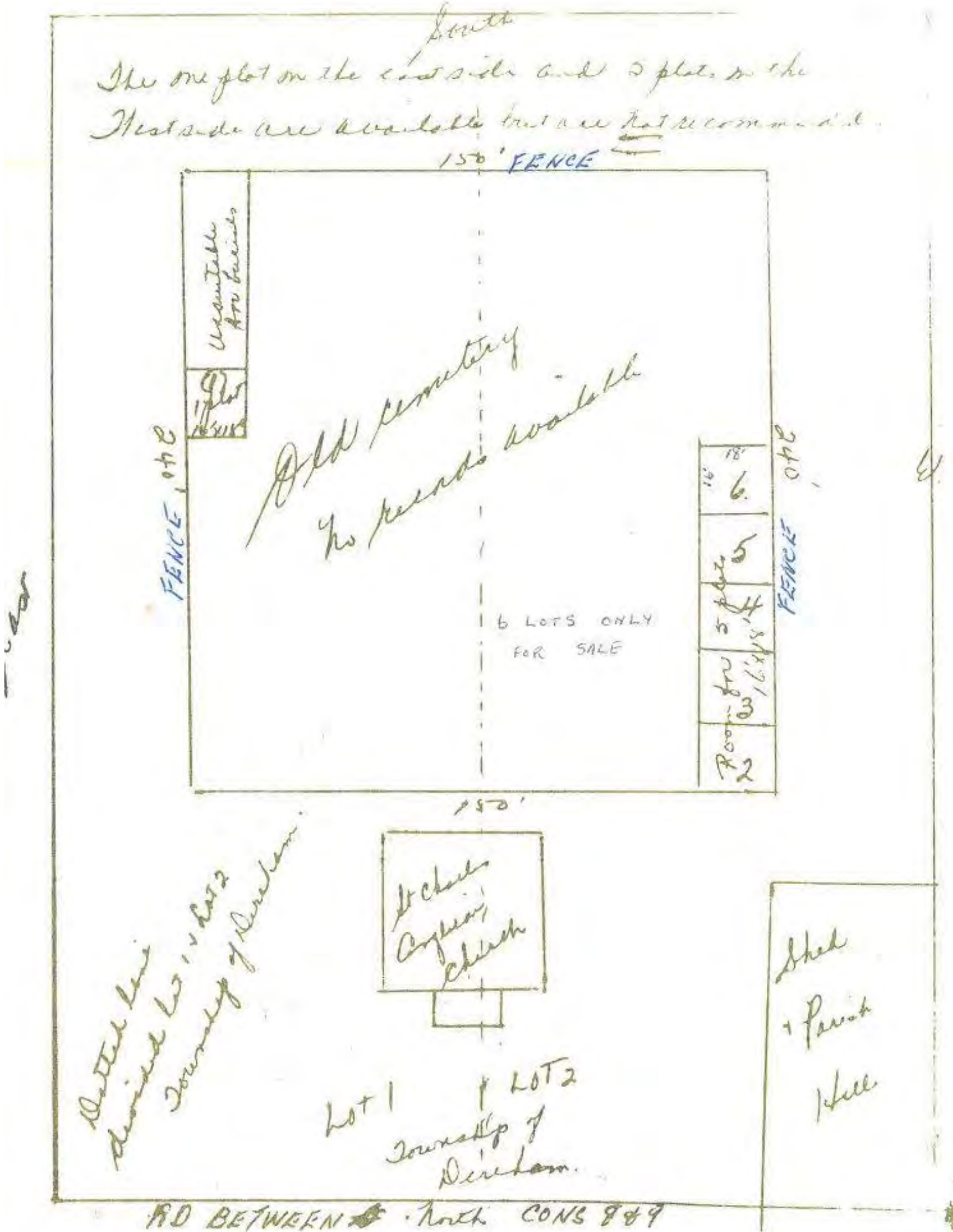


Figure 56 St. Charles Anglican Cemetery Map

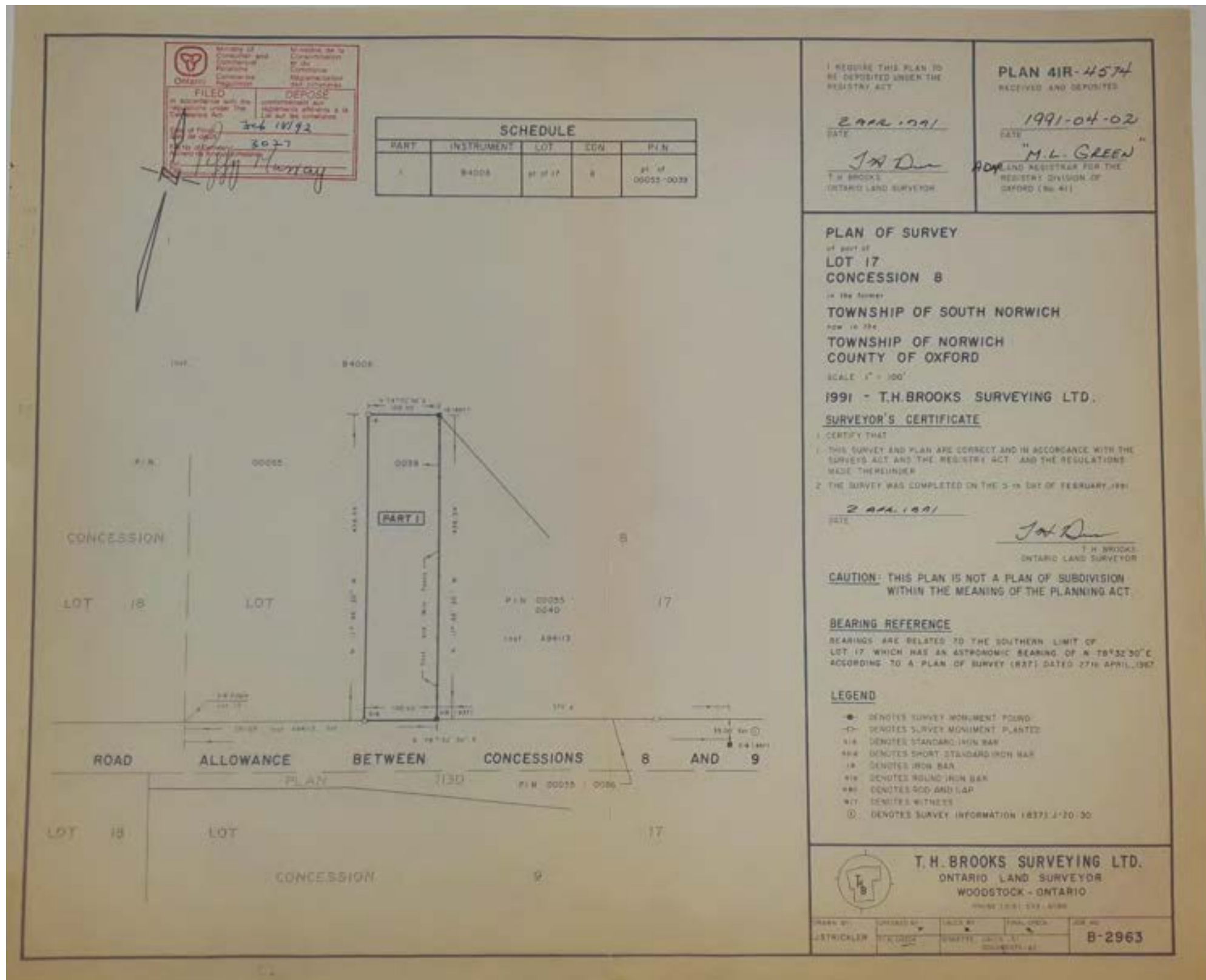


Figure 57 Springford Cemetery New Addition

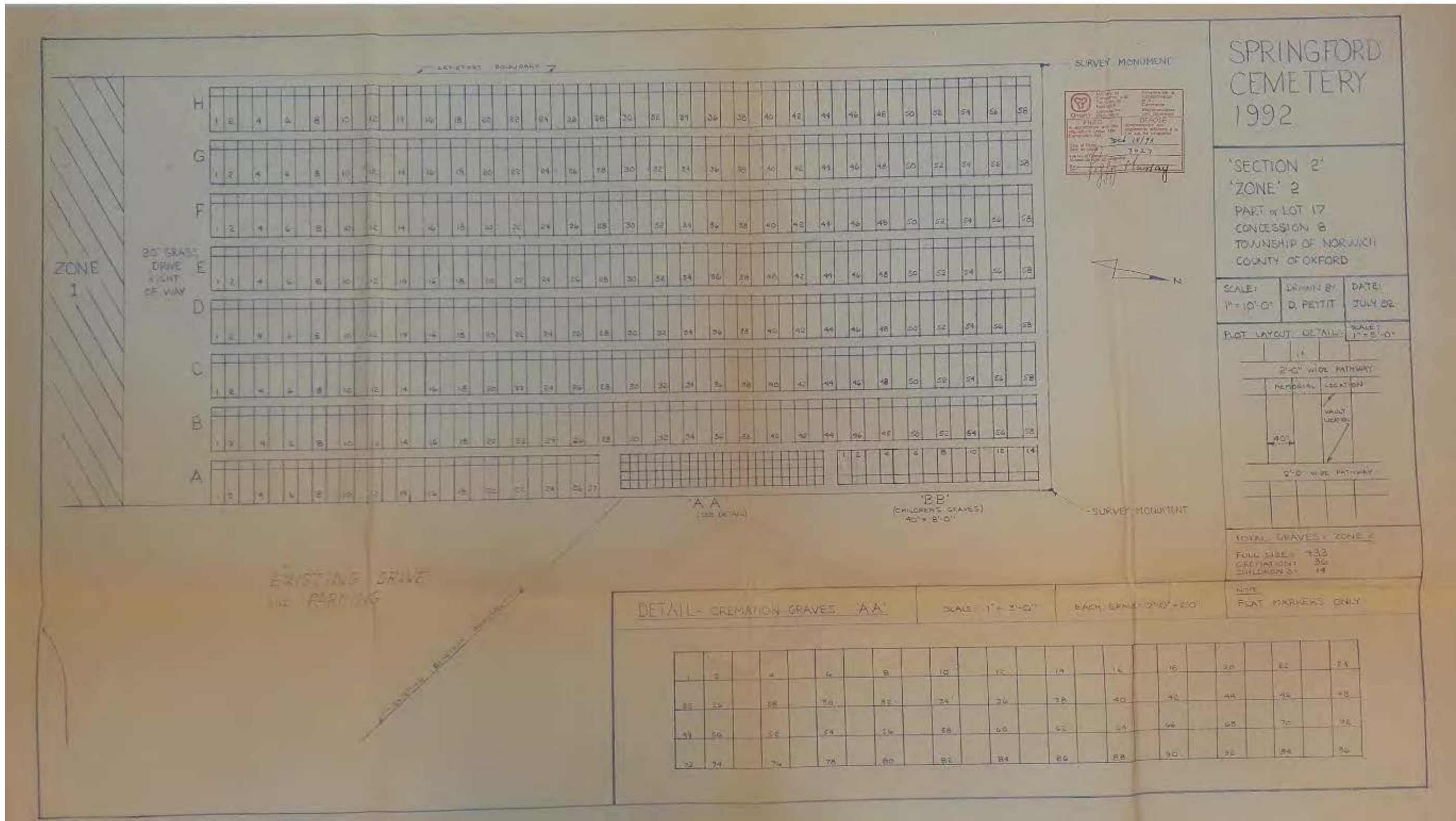


Figure 58 Springford Cemetery 1992 Zone 1

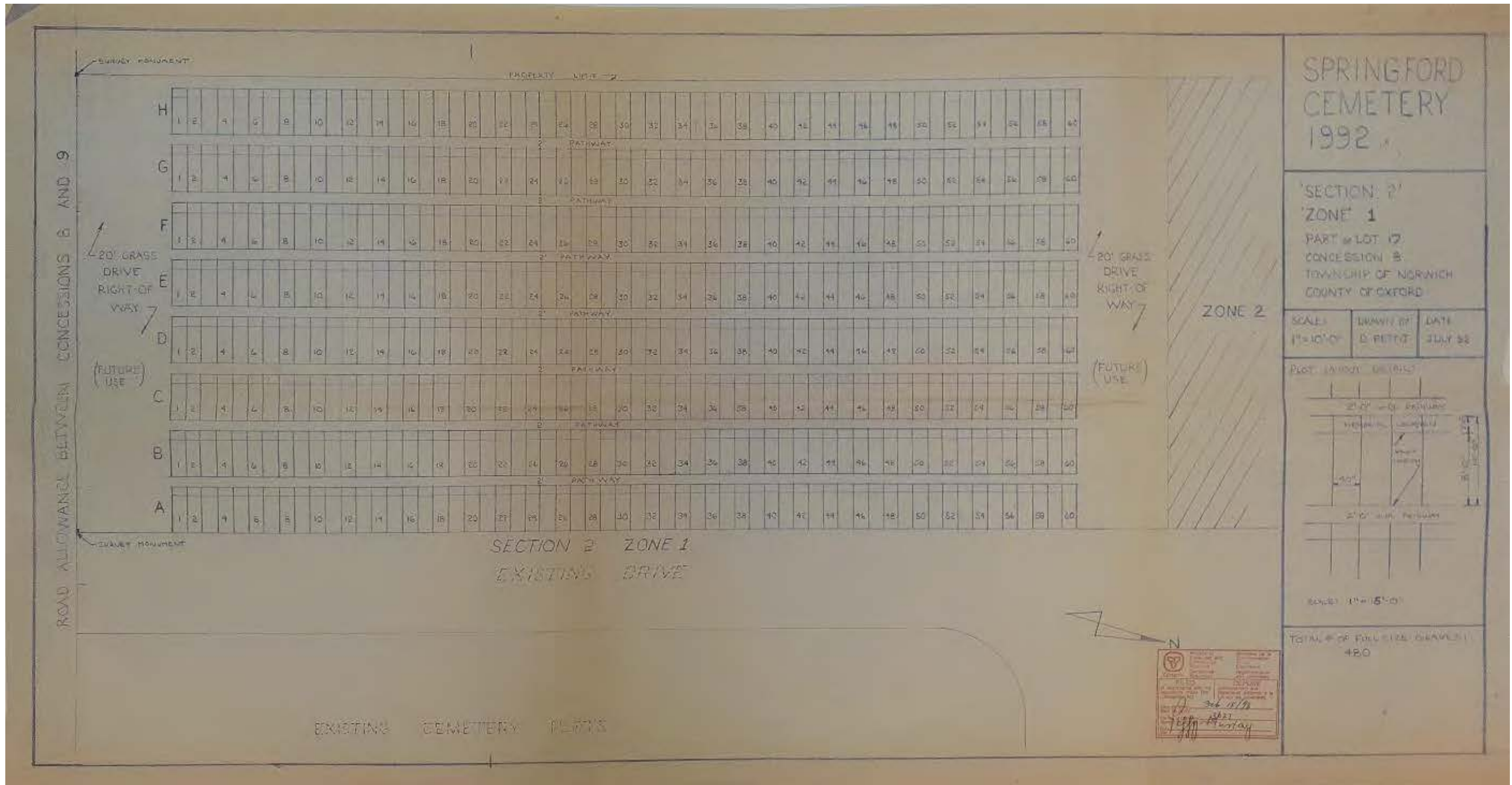


Figure 59 Springford Cemetery 1992 Zone 2

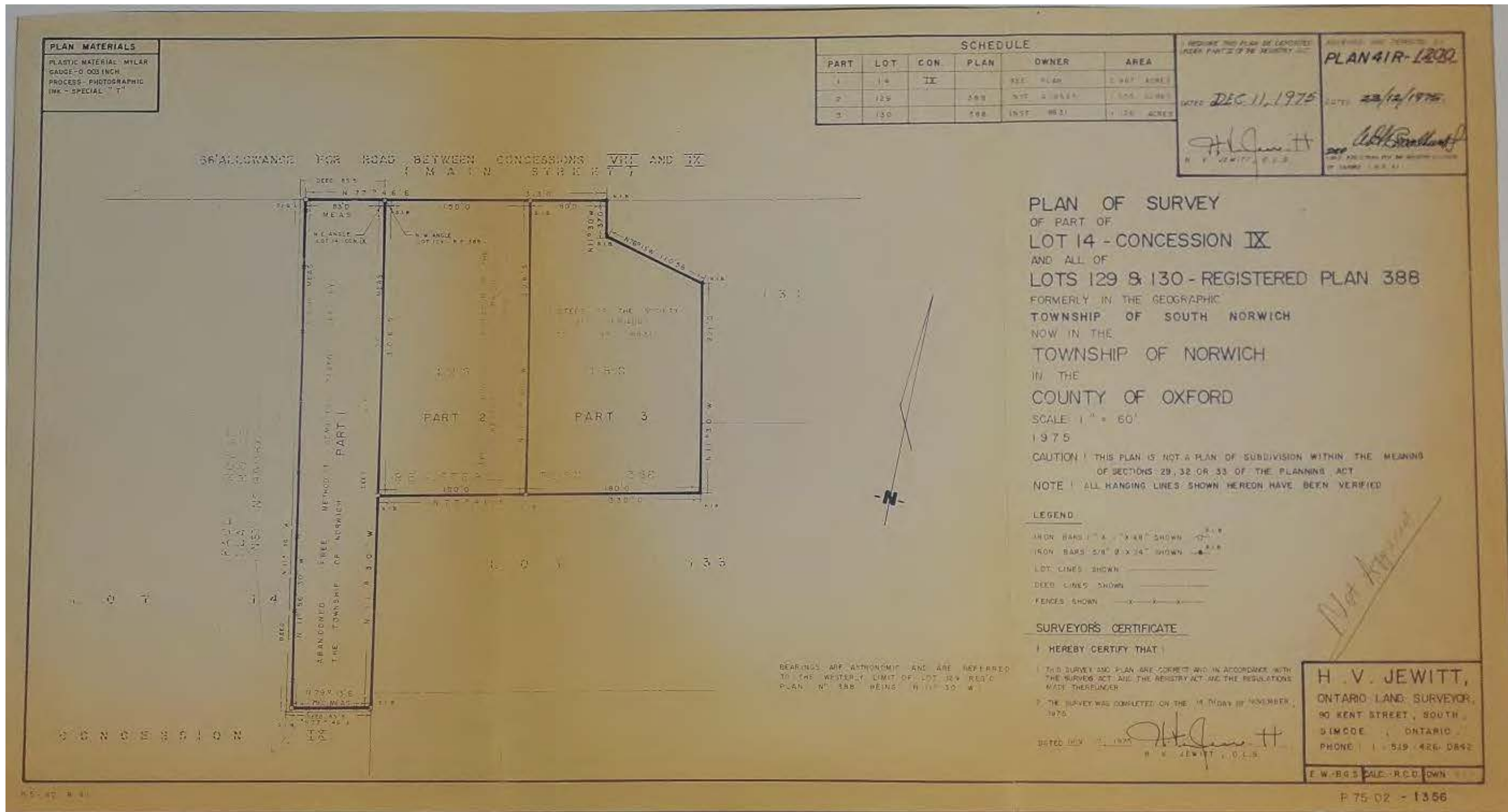


Figure 60 Erbtown Cemetery and Pine Street Burying Grounds



Figure 61 Erbtown Cemetery Polaroid from 1980



Figure 62 Pine Street Burial Ground Polaroid from 1980



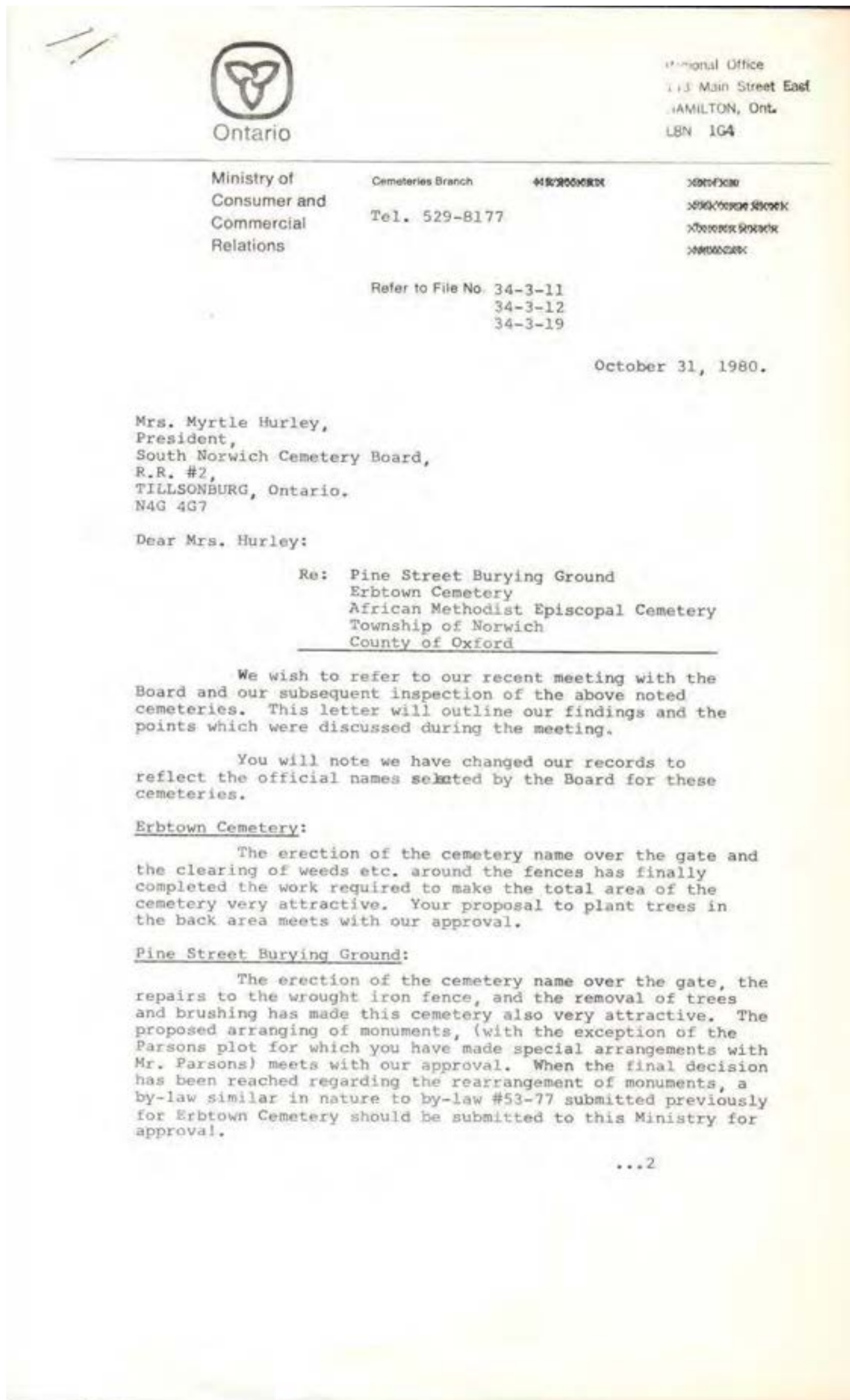
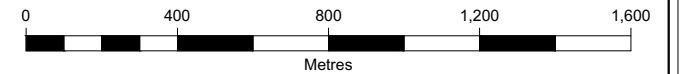
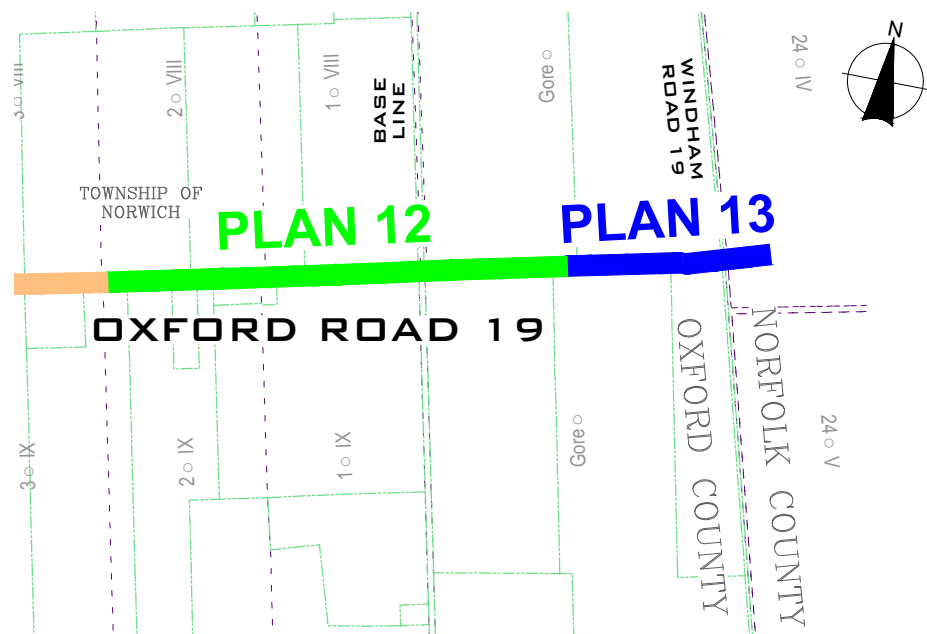
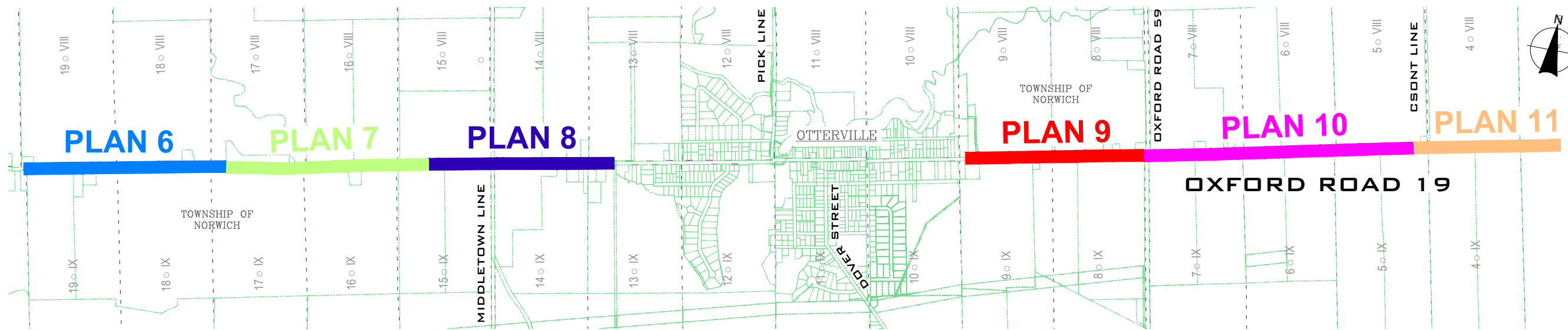
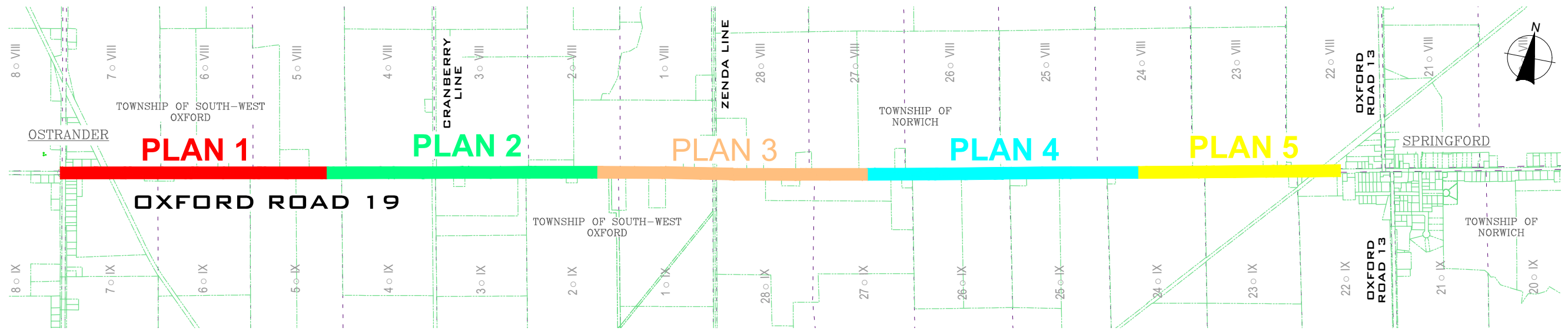


Figure 63 Ontario Ministry of Consumer and Commercial Relations Letter

Appendix B: Preferred Design Concepts





Client
COUNTY OF OXFORD

Figure Title
**OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
PROPERTY KEY MAP**

Drawn MS	Checked AB	Date 22/12/06
Scale 1:25,000	Project No. 300053425.0000	

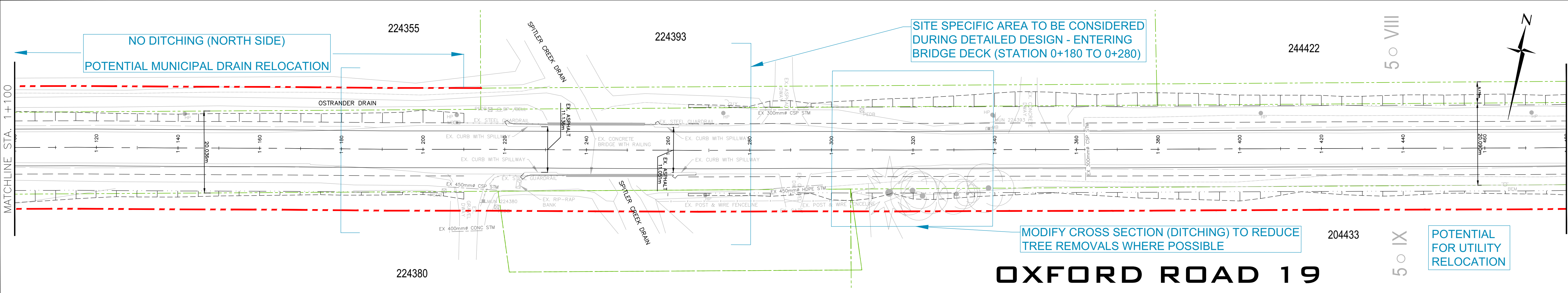
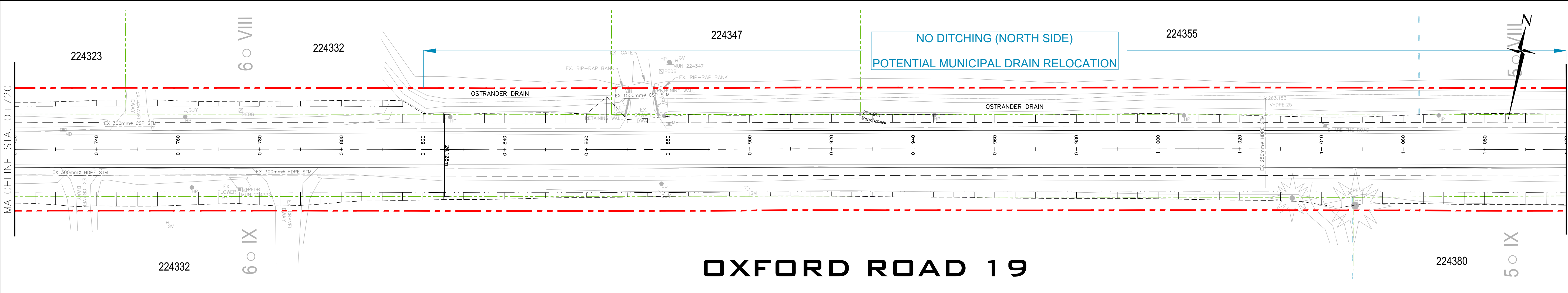
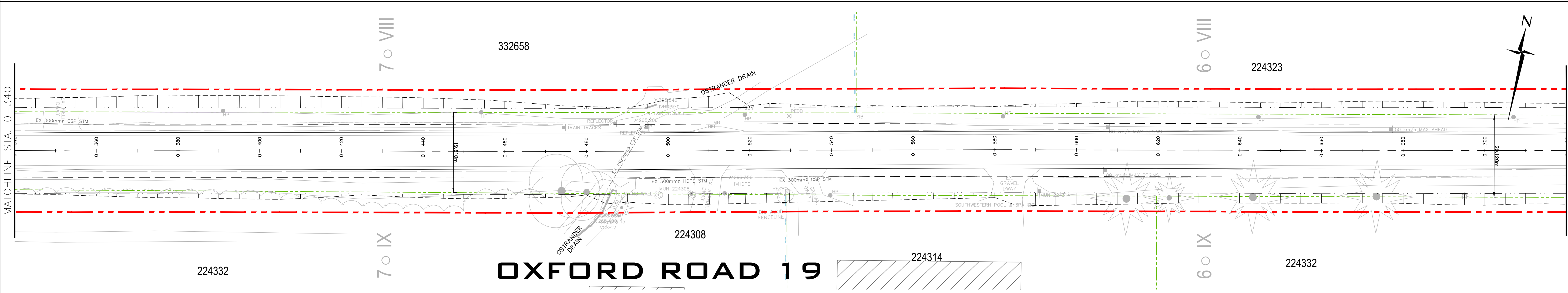
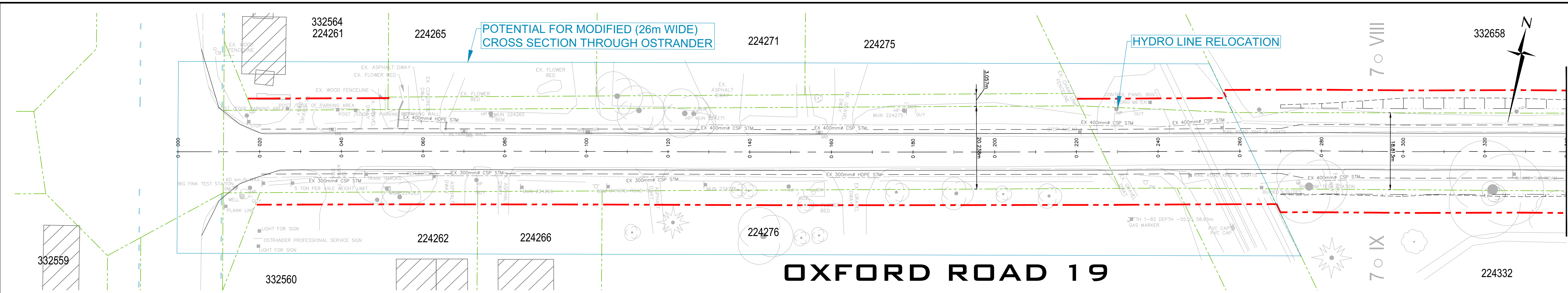
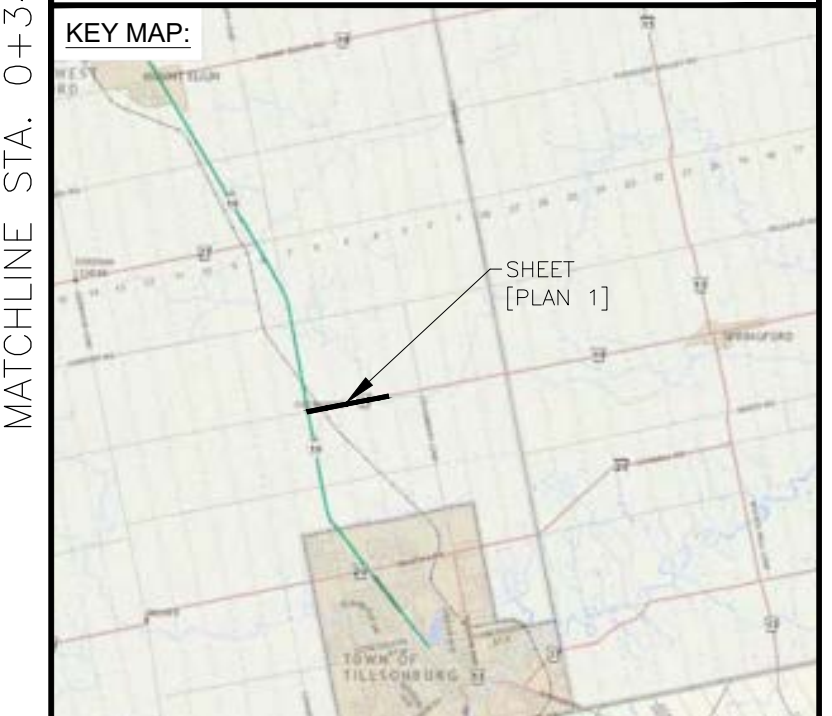
Figure No.
FIG

LEGEND

EXISTING PROPERTY LINE

EXISTING LOT/CONCESSION LINE

IX ← CONCESSION
9 ← LOT



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■ CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	● CO
Pr. Ditchline	---	Prop. Curb Stop	● CS
Pr. Edge of Pavement	---	Prop. Ditch Inlet CB	■ DICB
Ex. Property Line	---	Prop. Double CB	■ DCB
Ex. Centreline of Ditch	---	Prop. Fire Hydrant	● SAMH
Ex. Sanitary Forcemain	---	Prop. Sanitary Manhole	● SAMH
Ex. Storm Sewer	---	Prop. Storm Manhole	● STMH
Ex. U/G Telephone	T	Prop. Water Valve	●
Ex. U/G Cable TV	C	Remove & Dispose	⊗
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	HW		
Ex. Watermain	---		
Ex. Catchbasin	□ XCB		
Ex. Clean Out	○ CO		
Ex. Curb Stop	○ CS		
Ex. Ditch Inlet CB	□ DICB		
Ex. Double Catchbasin	□ DCB		
Ex. Fire Hydrant	○ SAMH		
Ex. Sanitary Manhole	○ SAMH		
Ex. Storm Manhole	○ STMH		
Ex. Water Valve	○		
Ex. Cut Cross	⊗		
Ex. Sign	⊗		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		

ISSUED FOR PCCH#2	3	DEC 6, 2022	HC
ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:		ELEVATION=m	
BENCHMARK #			

AREA MUNICIPALITY:
Township of South-West Oxford
312915 Dufferin Line
Mt. Elgin, Ontario, N0J 1N0
Telephone (519) 877-2702
Fax (519) 485-2932
Web www.swox.org

CONSULTANT:
BURNSIDE
R.J. Burnside & Associates Limited
35 Perry Street
Woodstock, Ontario, N4S 3C4
Telephone (519) 271-5111
Fax (519) 271-5750
Web www.rjburnside.com

DESIGN BY: HC	STAMP:
DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 0+000 TO 1+480

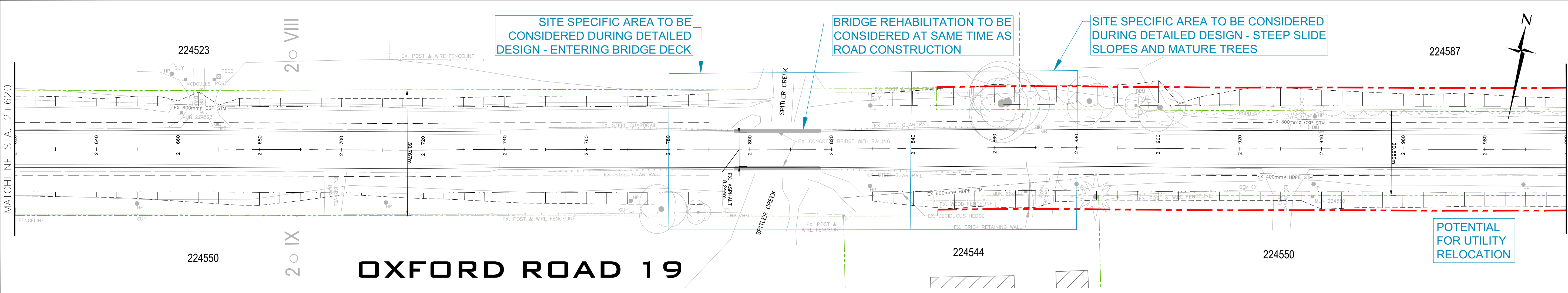
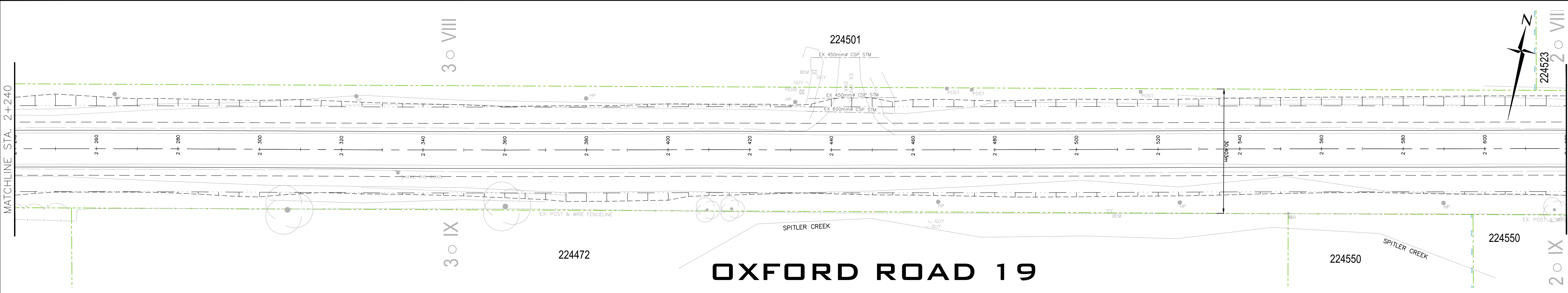
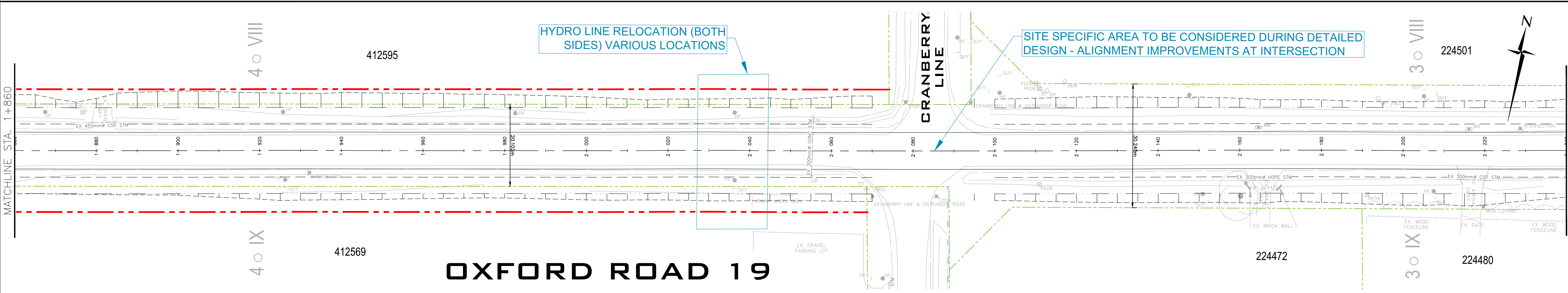
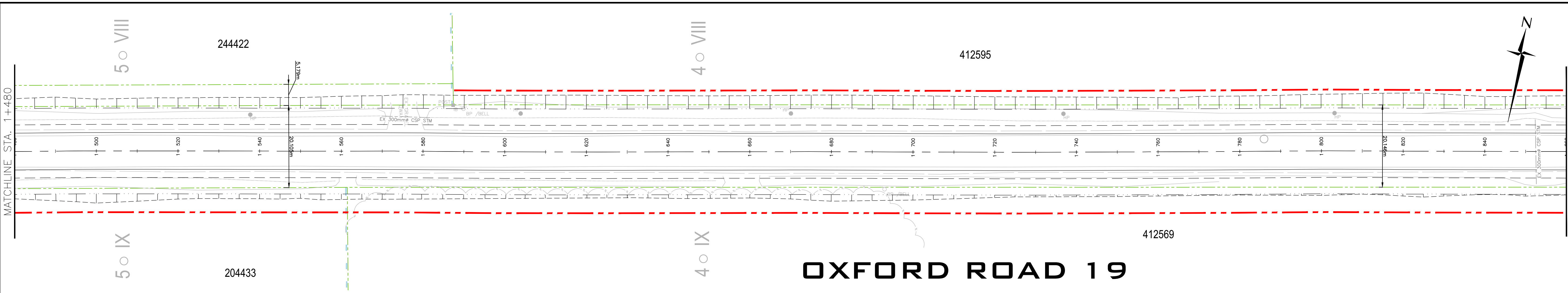
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CONTRACT NO:
DWG NO: PLAN 1

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layout: PLAN 0+000 TO 1+480 date: Dec 06, 2022 14:35 user: Melemon



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■
Pr. Top Bank/Matchline	---	Prop. Clean Out	●
Pr. Ditchline	---	Prop. Curb Stop	●
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	■
Ex. Property Line	---	Prop. Double CB	■
Ex. Centreline of Ditch	---	Prop. Fire Hydrant	●
Ex. Sanitary Forcemain	---	Prop. Sanitary Manhole	●
Ex. Sanitary Sewer	---	Prop. Storm Manhole	●
Ex. Storm Sewer	---	Prop. Water Valve	●
Ex. U/G Telephone	---	Remove & Dispose	⊗
Ex. U/G Cable TV	---		
Ex. U/G Gas	---		
Ex. U/G Hydro	---		
Ex. U/G Traffic Wiring	---		
Ex. Watermain	---		
Ex. Catchbasin	□	Prop. Catchbasin	■
Ex. Clean Out	○	Prop. Clean Out	●
Ex. Curb Stop	○	Prop. Curb Stop	●
Ex. Ditch Inlet CB	□	Prop. Ditch Inlet CB	■
Ex. Double Catchbasin	□	Prop. Double CB	■
Ex. Fire Hydrant	○	Prop. Fire Hydrant	●
Ex. Sanitary Manhole	○	Prop. Sanitary Manhole	●
Ex. Storm Manhole	○	Prop. Storm Manhole	●
Ex. Water Valve	○	Prop. Water Valve	●
Ex. Cut Cross	⊗	Remove & Dispose	⊗
Ex. Sign	⊗		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		



ISSUED FOR PC#2	3	DEC 6, 2022	HC
ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

AREA MUNICIPALITY:
Township of South-West Oxford
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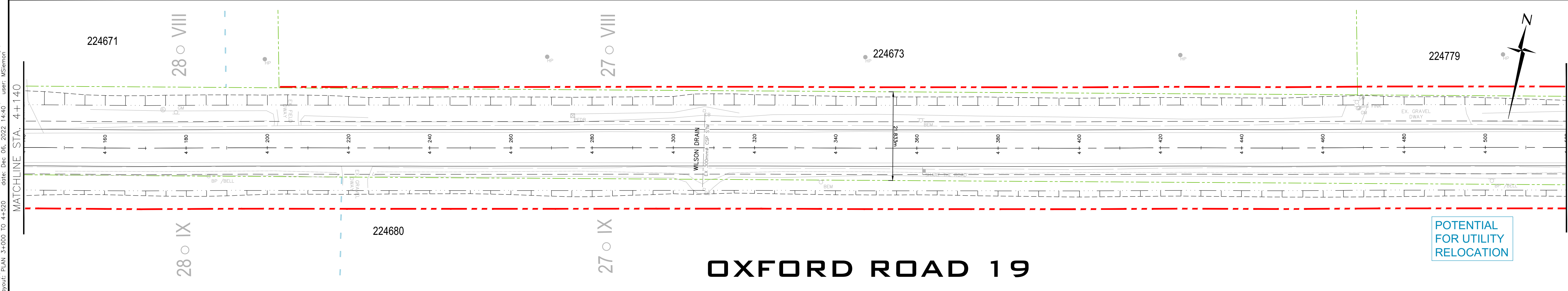
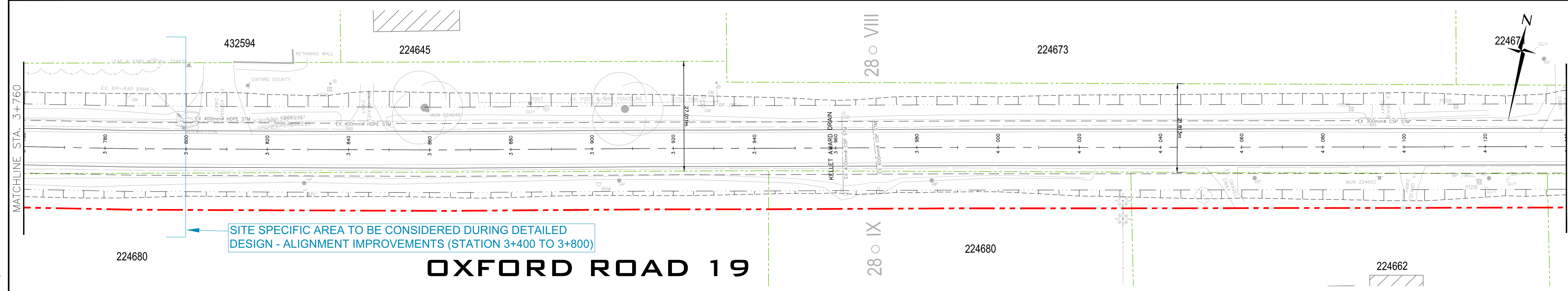
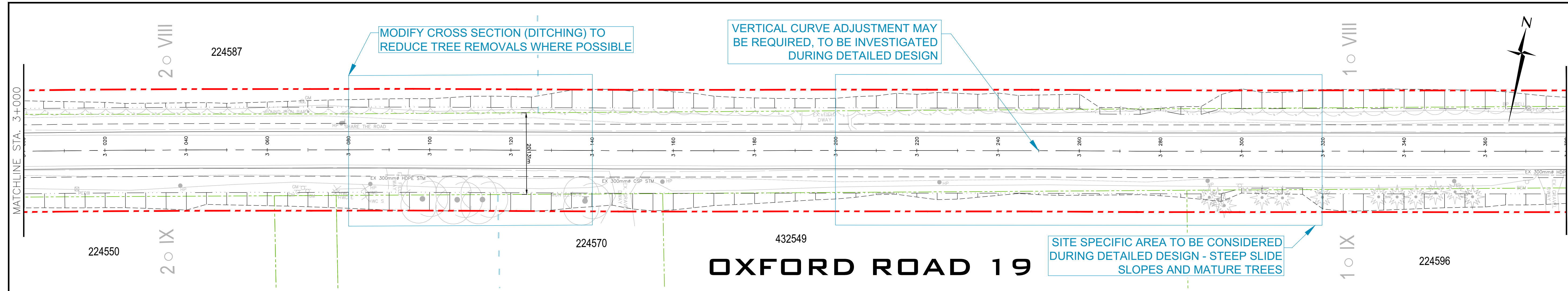
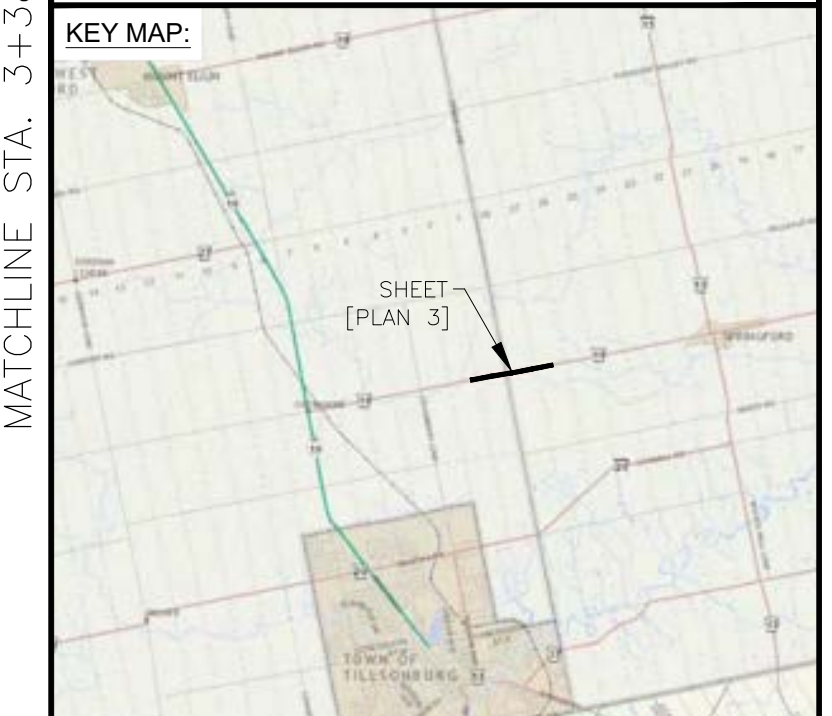
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DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 1+480 TO 3+000

HOR. SCALE - 1:500
STN: 1+480 TO: 3+000
CONTRACT NO:
DWG NO: PLAN 2

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layout: PLAN 1+480 TO 3+000 date: Dec 06, 2022 14:38 user: Melemon



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	CO
Pr. Ditchline	---	Prop. Curb Stop	CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	DICB
Ex. Property Line	---	Prop. Double CB	DCB
Ex. Centreline of Ditch	---	Prop. Fire Hydrant	FH
Ex. Sanitary Forcemain	---	Prop. Sanitary Manhole	SAMH
Ex. Storm Sewer	---	Prop. Storm Manhole	STMH
Ex. U/G Telephone	T	Prop. Water Valve	WV
Ex. U/G Cable TV	C	Remove & Dispose	RD
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TR		
Ex. Watermain	---		
Ex. Catchbasin	CB		
Ex. Clean Out	CO		
Ex. Curb Stop	CS		
Ex. Ditch Inlet CB	DICB		
Ex. Double Catchbasin	DCB		
Ex. Fire Hydrant	FH		
Ex. Sanitary Manhole	SAMH		
Ex. Storm Manhole	STMH		
Ex. Water Valve	WV		
Ex. Cut Cross	---		
Ex. Sign	---		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	---		
Ex. Deciduous Shrub	---		
Ex. Stump	---		
Ex. Coniferous Tree	---		
Ex. Deciduous Tree	---		

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ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
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APPROVED BY: HC	
DATE: NOV 10, 2022	

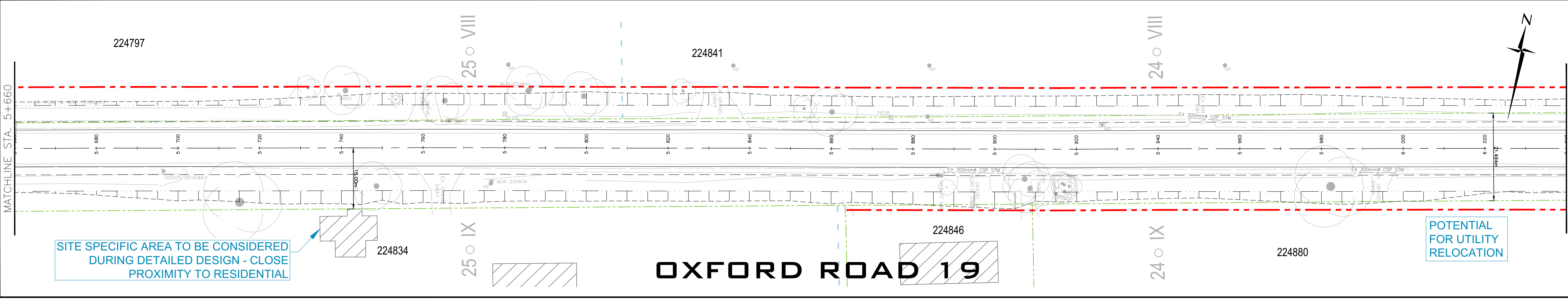
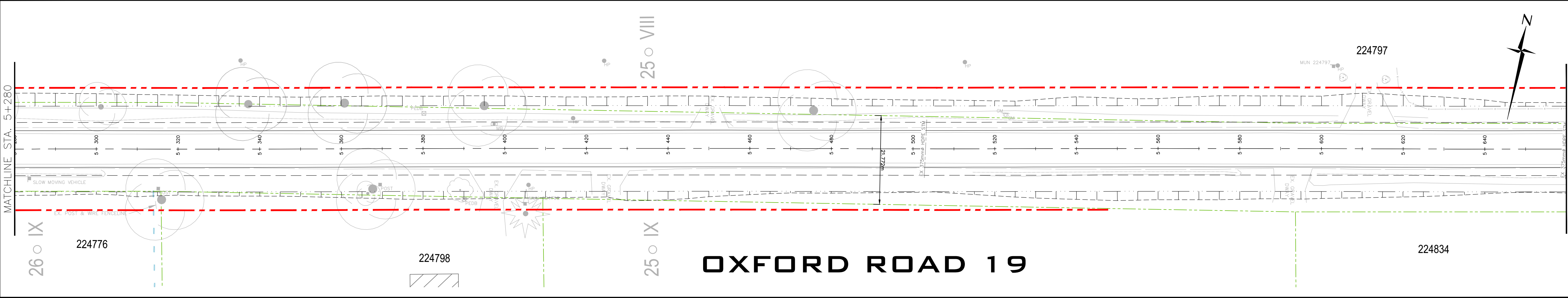
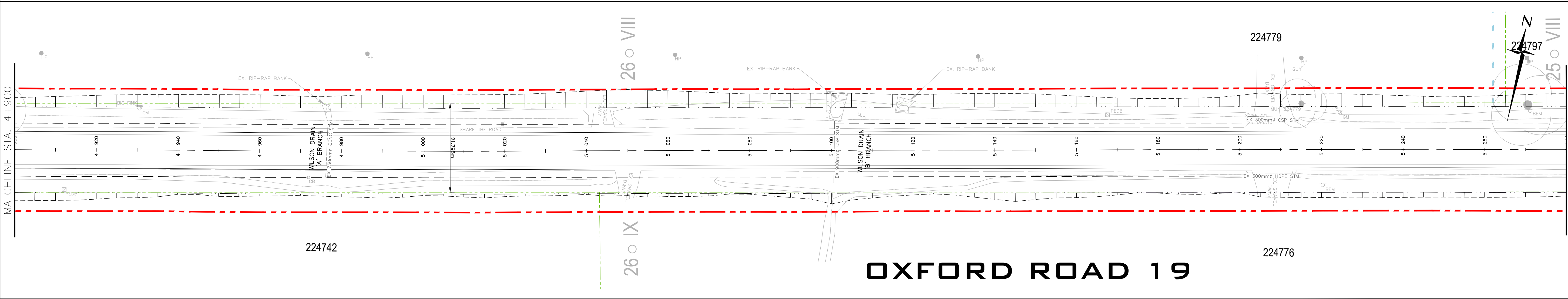
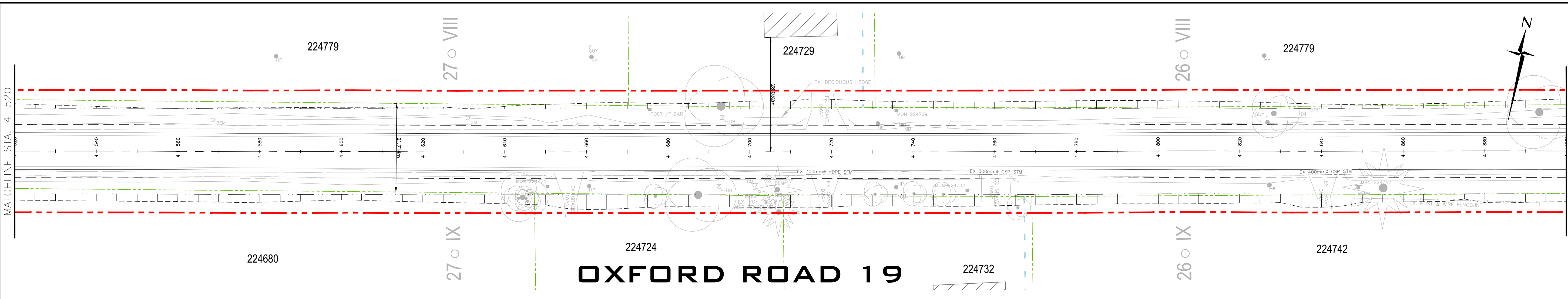
OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 3+000 TO 4+520

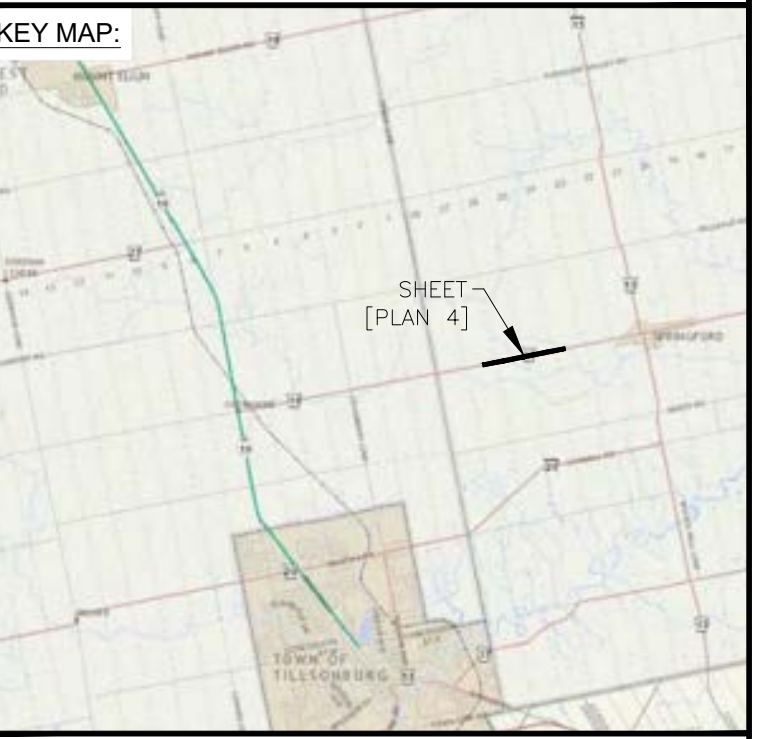
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CONTRACT NO:
DWG NO: PLAN 3

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 Layout: PLAN 4+520 TO 6+040 date: Dec 06, 2022 14:41 user: Melemon



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 Woodstock, Ontario N4S 7Y3
 Phone: 519-539-9800
 Fax: 519-421-4711
 www.oxfordcounty.ca



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■
Pr. Top Bank/Matchline	---	Prop. Clean Out	●
Pr. Ditchline	---	Prop. Curb Stop	●
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	■
Pr. Edge of Pavement	---	Prop. Double CB	■
Ex. Property Line	---	Prop. Fire Hydrant	●
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	●
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	●
Ex. Sanitary Sewer	---	Prop. Water Valve	●
Ex. Storm Sewer	---	Remove & Dispose	⊗
Ex. U/G Telephone	T		
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	□	Prop. Catchbasin	■
Ex. Clean Out	○	Prop. Clean Out	●
Ex. Curb Stop	○	Prop. Curb Stop	●
Ex. Ditch Inlet CB	□	Prop. Ditch Inlet CB	■
Ex. Double Catchbasin	□	Prop. Double CB	■
Ex. Fire Hydrant	○	Prop. Fire Hydrant	●
Ex. Sanitary Manhole	○	Prop. Sanitary Manhole	●
Ex. Storm Manhole	○	Prop. Storm Manhole	●
Ex. Water Valve	○	Prop. Water Valve	●
Ex. Cut Cross	⊗	Remove & Dispose	⊗
Ex. Sign	⊗		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		

ISSUED FOR PCC#2	3	DEC 6, 2022	HC
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ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
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CONTROL POINTS:
 BENCHMARK # ELEVATION=m

AREA MUNICIPALITY:
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 fax (519) 468-2414
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 fax (519) 271-5730
 web www.rjburnside.com

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CHECKED BY: AB
APPROVED BY: HC
DATE: NOV 10, 2022

STAMP:

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
PLAN 4+520 TO 6+040

HOR. SCALE - 1:500
STN: 4+520 TO: 6+040
CONTRACT NO:
DWG NO: PLAN 4

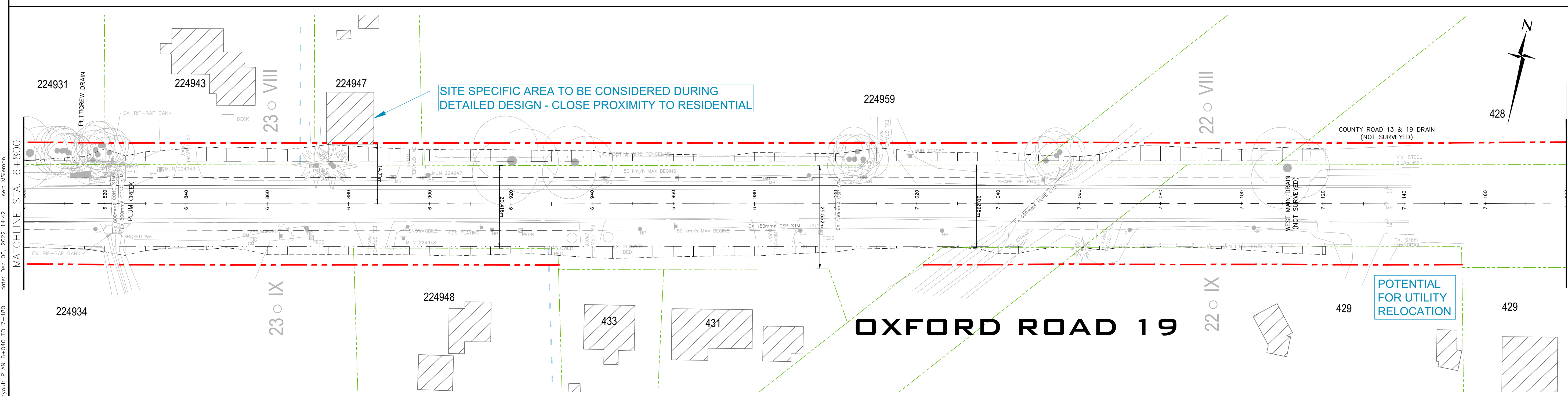
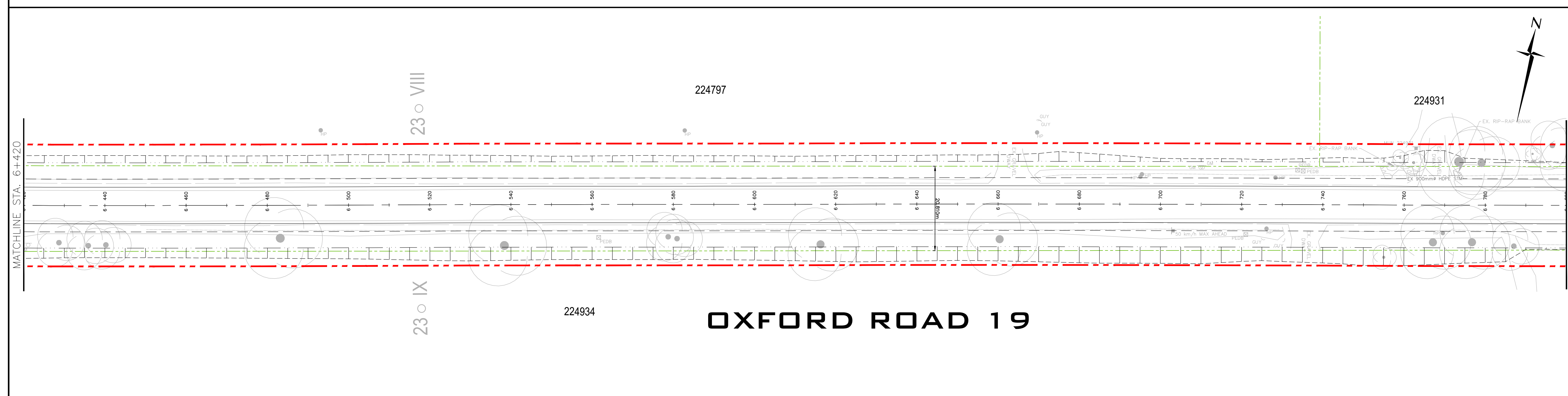
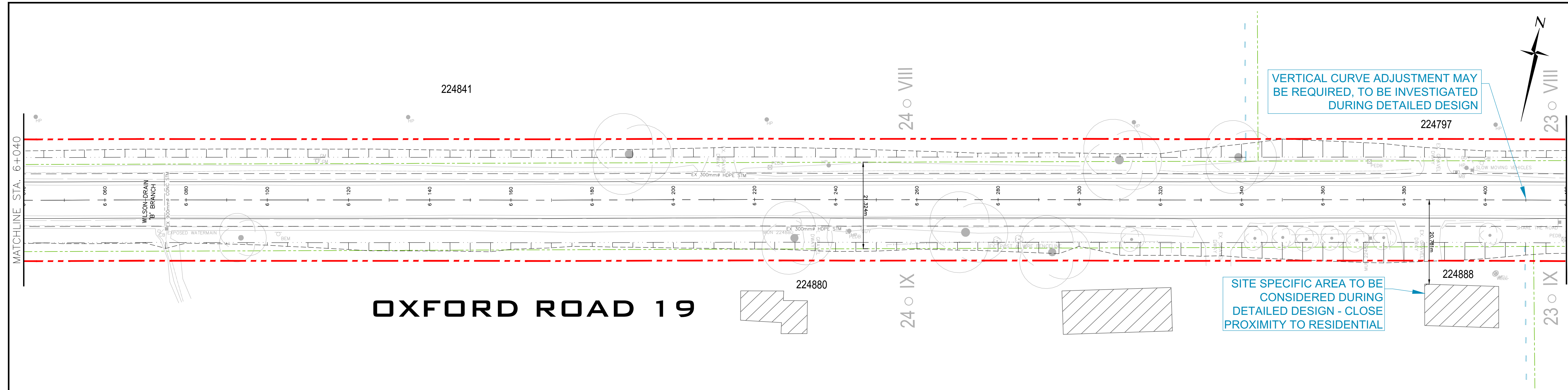
SITE SPECIFIC AREA TO BE CONSIDERED
 DURING DETAILED DESIGN - CLOSE
 PROXIMITY TO RESIDENTIAL

POTENTIAL
 FOR UTILITY
 RELOCATION



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■ CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	● CO
Pr. Ditchline	---	Prop. Curb Stop	● CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	■ DICB
Pr. Edge of Pavement	---	Prop. Double CB	■ DCB
Ex. Property Line	---	Prop. Fire Hydrant	● FHY
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	● SAMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	● STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	● WVAL
Ex. Storm Sewer	---	Remove & Dispose	⊗
Ex. U/G Telephone	---		
Ex. U/G Cable TV	---		
Ex. U/G Gas	---		
Ex. U/G Hydro	---		
Ex. U/G Traffic Wiring	---		
Ex. Watermain	---		
Ex. Catchbasin	□ XCB		
Ex. Clean Out	○ XCO		
Ex. Curb Stop	○ XCS		
Ex. Ditch Inlet CB	□ XDICB		
Ex. Double Catchbasin	□ XDCB		
Ex. Fire Hydrant	○ XSAMH		
Ex. Sanitary Manhole	○ XSAMH		
Ex. Storm Manhole	○ XSTMH		
Ex. Water Valve	○ XWVAL		
Ex. Cut Cross	⊗		
Ex. Sign	⊗		
Ex. Guy Wire	⊗		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		



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ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
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DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
PLAN 6+040 TO 7+180
 HOR. SCALE - 1:500
 STN: 6+040 TO: 7+180
 CONTRACT NO:
 DWG NO: PLAN 5

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 layout: PLAN 6+040 TO 7+180 date: Dec 06, 2022 14:42 user: Meeman



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forceman	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	---
Ex. U/G Cable TV	---
Ex. U/G Gas	---
Ex. U/G Hydro	---
Ex. U/G Traffic Wiring	---
Ex. Watermain	---
Ex. Catchbasin	□ XCB
Prop. Catchbasin	■ CB
Ex. Clean Out	○ CO
Prop. Clean Out	● CO
Ex. Curb Stop	○ CS
Prop. Curb Stop	● CS
Ex. Ditch Inlet CB	□ DICB
Prop. Ditch Inlet CB	■ DICB
Ex. Double Catchbasin	□ XDCB
Prop. Double CB	■ DCB
Ex. Fire Hydrant	○ XSAFH
Prop. Fire Hydrant	● SAFH
Ex. Sanitary Manhole	○ XSTMH
Prop. Sanitary Manhole	● STMH
Ex. Storm Manhole	○ XSTMH
Prop. Storm Manhole	● STMH
Ex. Water Valve	○ XWV
Prop. Water Valve	● WV
Ex. Cut Cross	✕
Remove & Dispose	⊗
Ex. Sign	■
Ex. Guy Wire	---
Ex. Coniferous Shrub	⊙
Ex. Deciduous Shrub	⊙
Ex. Stump	⊙
Ex. Coniferous Tree	⊙
Ex. Deciduous Tree	⊙

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ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
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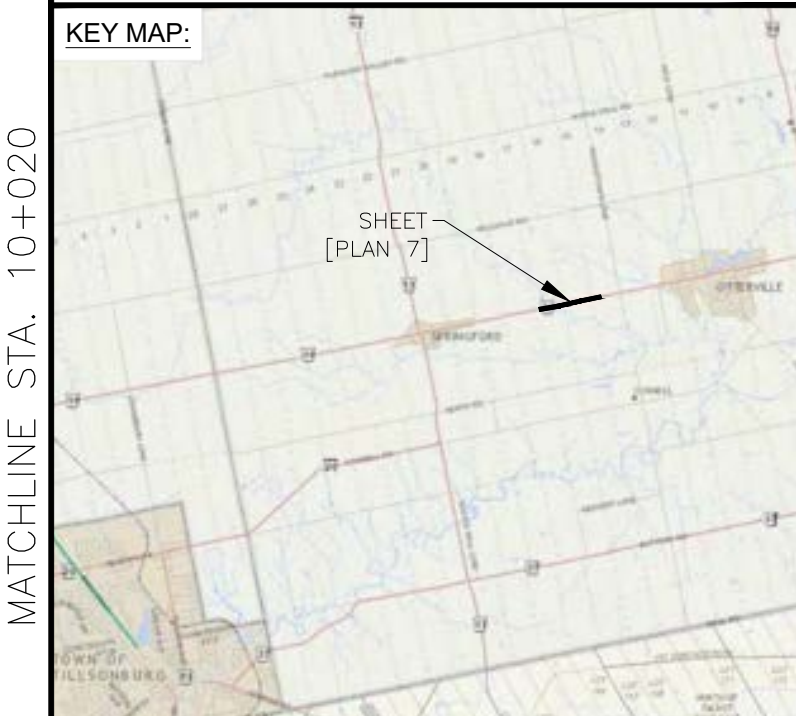
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APPROVED BY: HC	
DATE: NOV 10, 2022	

**OXFORD ROAD 19
 ENVIRONMENTAL
 ASSESSMENT**

PLAN 8+500 TO 9+640

HOR. SCALE - 1:500
 STN: 8+500 TO: 9+640
 CONTRACT NO:
 DWG NO: PLAN 6

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LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	CB
Ex. Clean Out	CO
Ex. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Ex. Fire Hydrant	FH
Ex. Sanitary Manhole	SAMH
Ex. Storm Manhole	STMH
Ex. Water Valve	WV
Ex. Cut Cross	CC
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CSH
Ex. Deciduous Shrub	DSH
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT


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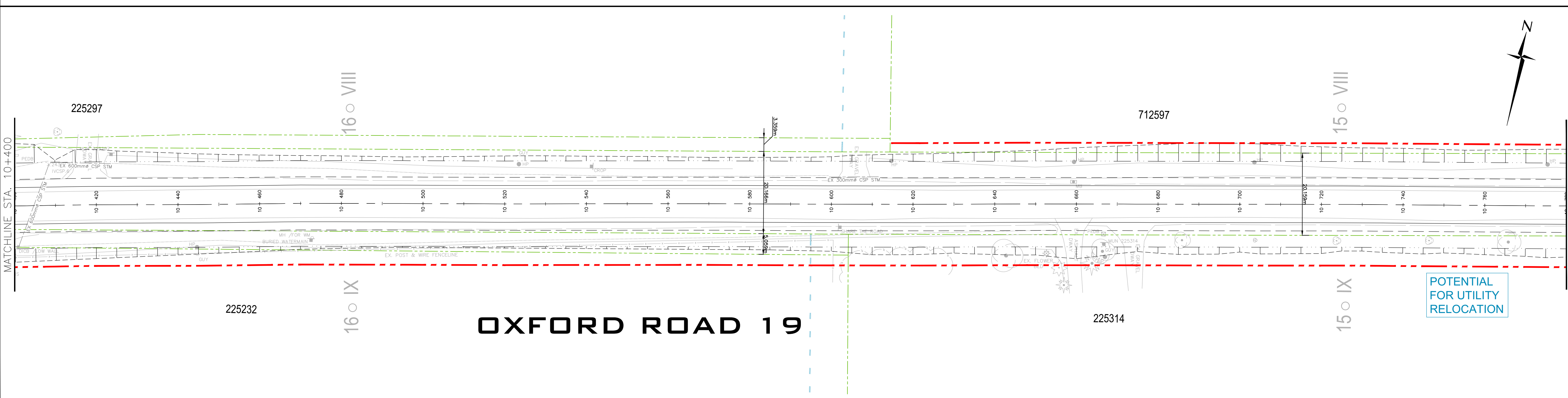
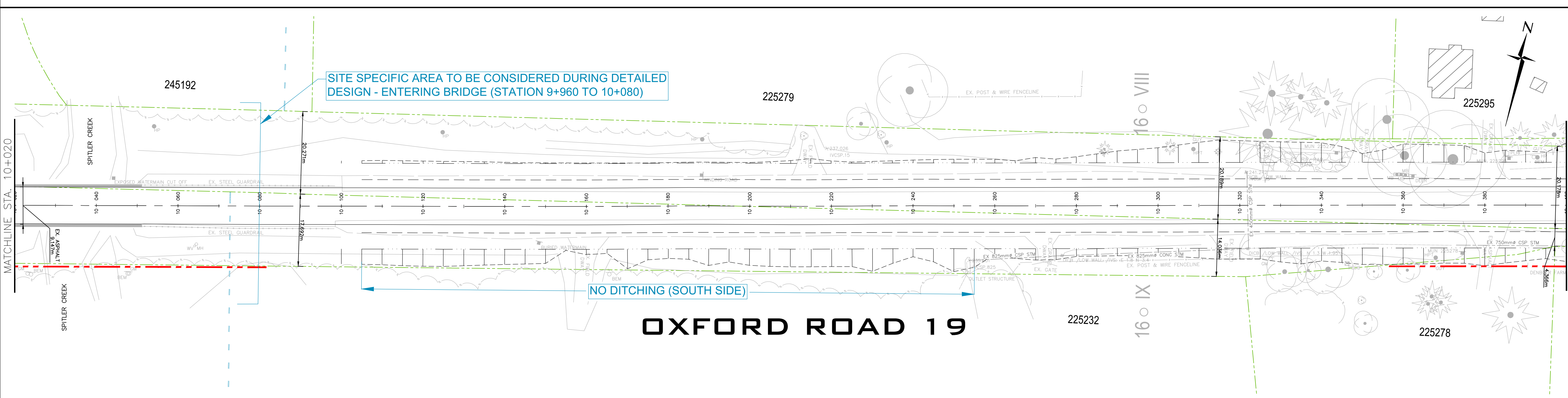
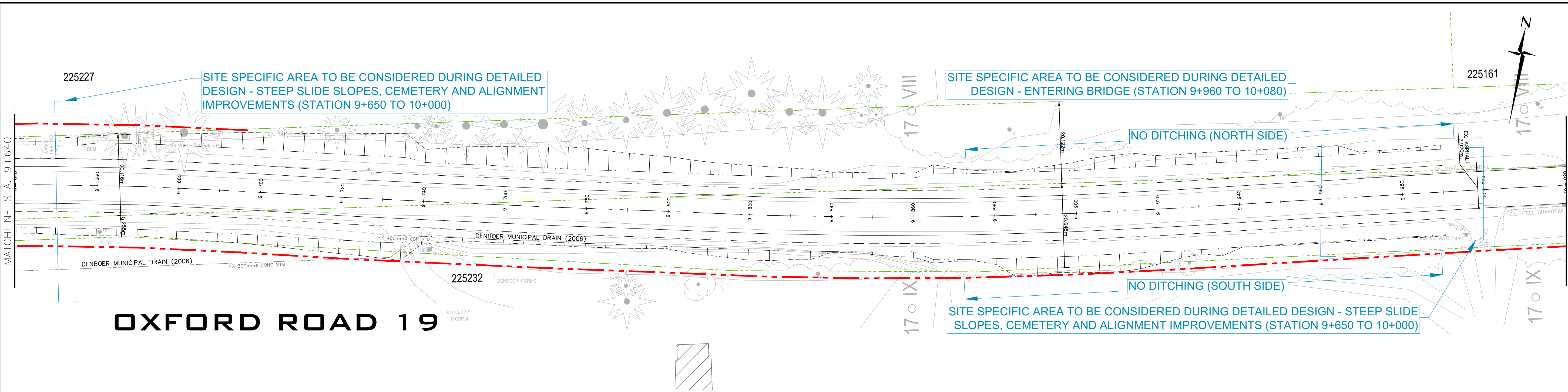


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APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
PLAN 9+640 TO 10+780	
HOR. SCALE - 1:500	
STN: 9+640	TO: 10+780
CONTRACT NO:	
DWG NO: PLAN 7	

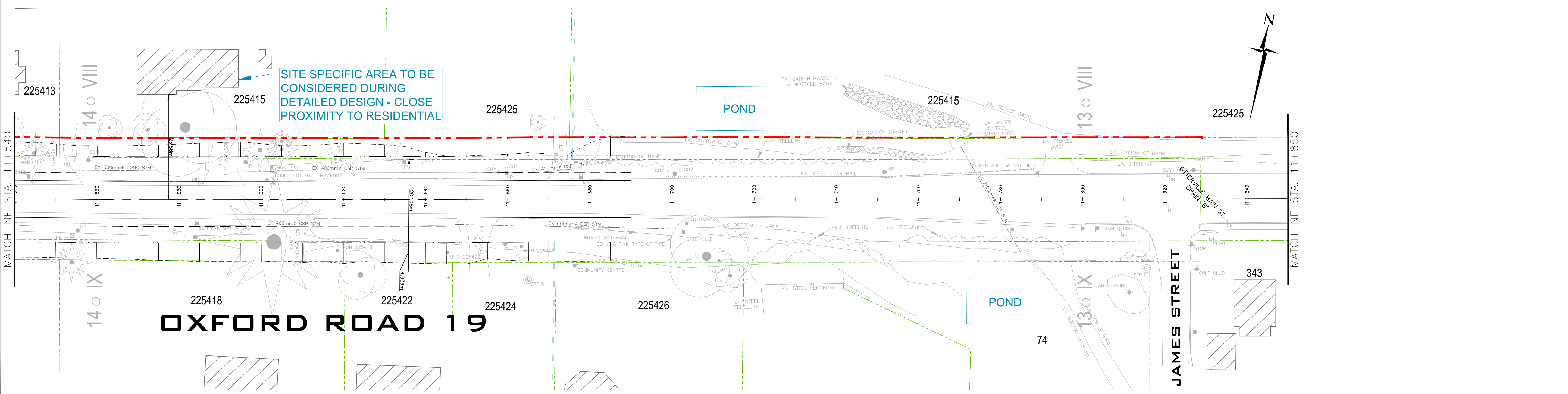
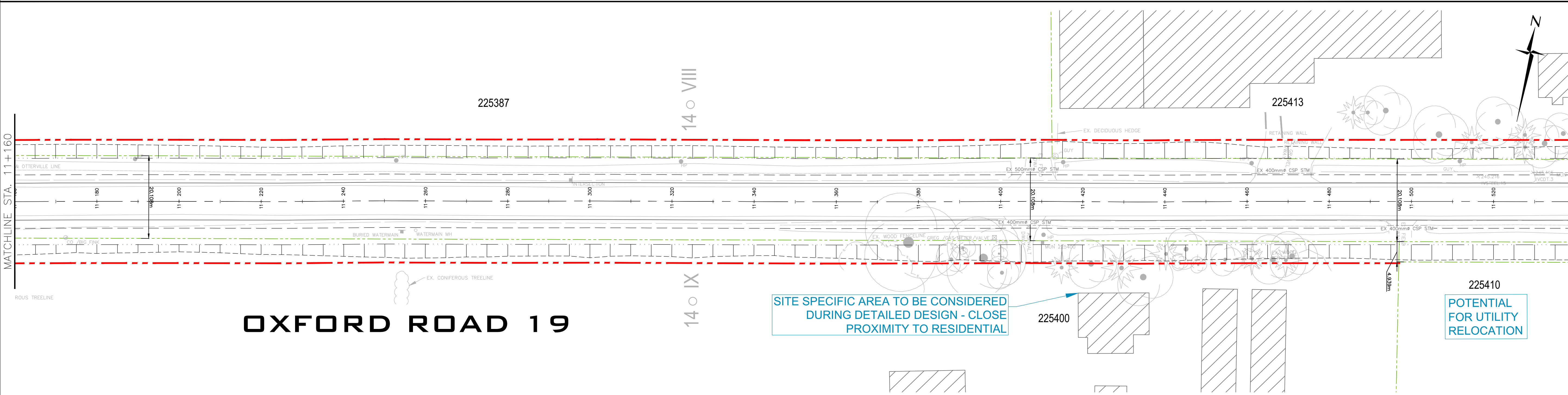
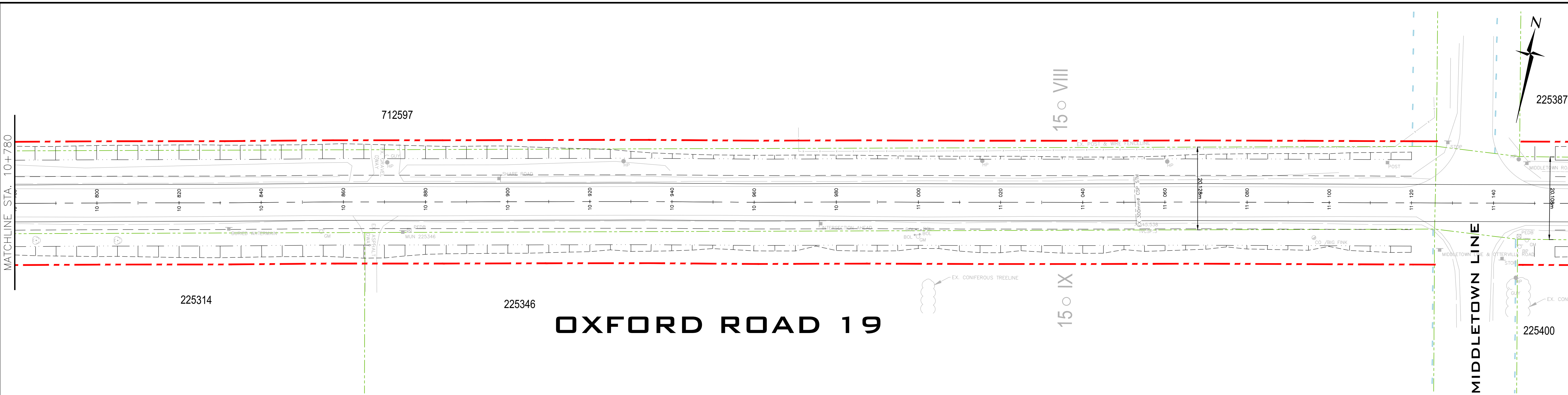


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Layout: PLAN 9+640 TO 10+780 - date: Dec 06, 2022 14:45 user: MStrom



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	---
Ex. U/G Cable TV	---
Ex. U/G Gas	---
Ex. U/G Hydro	---
Ex. U/G Traffic Wiring	---
Ex. Watermain	---
Ex. Catchbasin	---
Ex. Clean Out	---
Ex. Curb Stop	---
Ex. Ditch Inlet CB	---
Ex. Double Catchbasin	---
Ex. Fire Hydrant	---
Ex. Sanitary Manhole	---
Ex. Storm Manhole	---
Ex. Water Valve	---
Ex. Cut Cross	---
Ex. Sign	---
Ex. Guy Wire	---
Ex. Coniferous Shrub	---
Ex. Deciduous Shrub	---
Ex. Stump	---
Ex. Coniferous Tree	---
Ex. Deciduous Tree	---
Prop. Catchbasin	---
Prop. Clean Out	---
Prop. Curb Stop	---
Prop. Ditch Inlet CB	---
Prop. Double CB	---
Prop. Fire Hydrant	---
Prop. Sanitary Manhole	---
Prop. Storm Manhole	---
Prop. Water Valve	---
Remove & Dispose	---



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ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
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BENCHMARK #	ELEVATION=m
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fax (519) 271-3750
web www.rjburnside.com

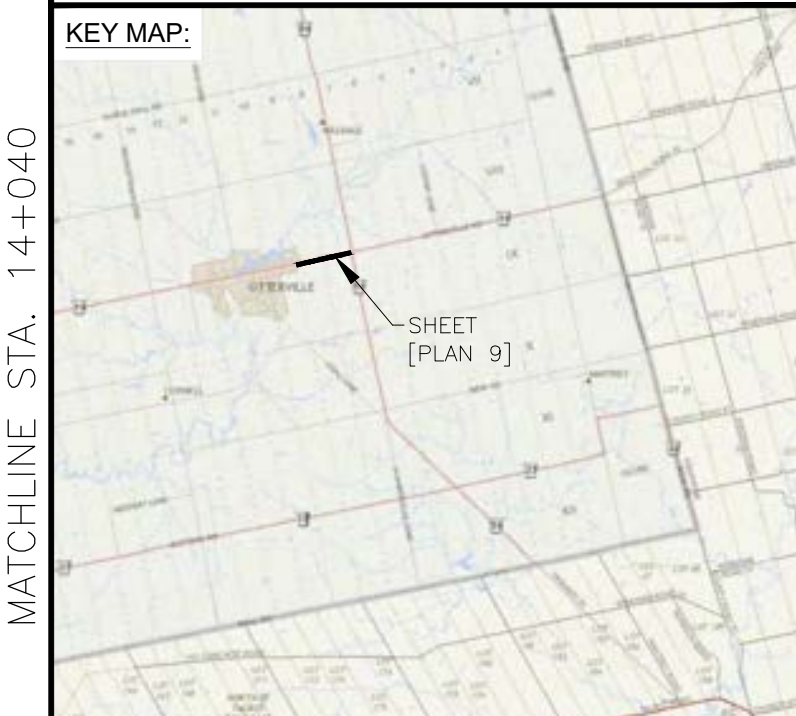
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DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 10+780 TO 11+850

HOR. SCALE - 1:500
STN: 10+780 TO: 11+850
CONTRACT NO:
DWG NO: PLAN 8

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Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	XCB
Prop. Catchbasin	CB
Ex. Clean Out	CO
Prop. Clean Out	CO
Ex. Curb Stop	CS
Prop. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Prop. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Prop. Double Catchbasin	DCB
Ex. Fire Hydrant	FH
Prop. Fire Hydrant	FH
Ex. Sanitary Manhole	SAMH
Prop. Sanitary Manhole	SAMH
Ex. Storm Manhole	STMH
Prop. Storm Manhole	STMH
Ex. Water Valve	WV
Prop. Water Valve	WV
Ex. Cut Cross	CC
Remove & Dispose	RD
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CSH
Ex. Deciduous Shrub	DSH
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT

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ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
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CONTROL POINTS:

BENCHMARK #	ELEVATION=
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AREA MUNICIPALITY:

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 285767 Airport Road, P.O. Box 100
 Norwich, Ontario, N0J 1P0
 telephone (519) 468-2410
 fax (519) 468-2414
 web www.norwich.ca

CONSULTANT:

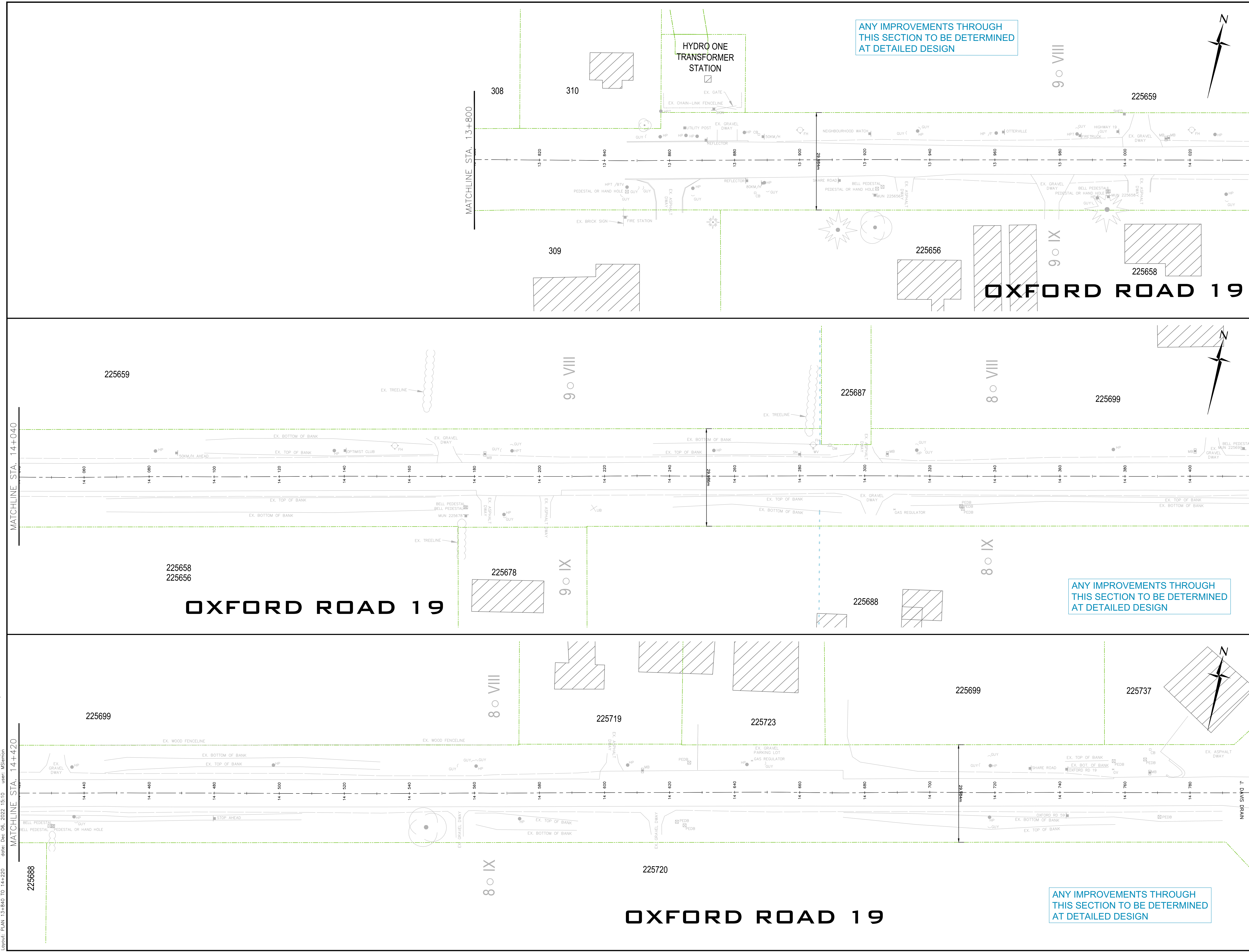
BURNSIDE
 R.J. Burnside & Associates Limited
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 Woodstock, Ontario, N4S 3C4
 telephone (519) 271-5111
 fax (519) 271-5750
 web www.rjburnside.com

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 CHECKED BY: AB
 APPROVED BY: HC
 DATE: NOV 10, 2022

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 13+840 TO 14+800

HOR. SCALE - 1:500
 STN: 13+840 TO: 14+800
 CONTRACT NO:
 DWG NO: PLAN 9

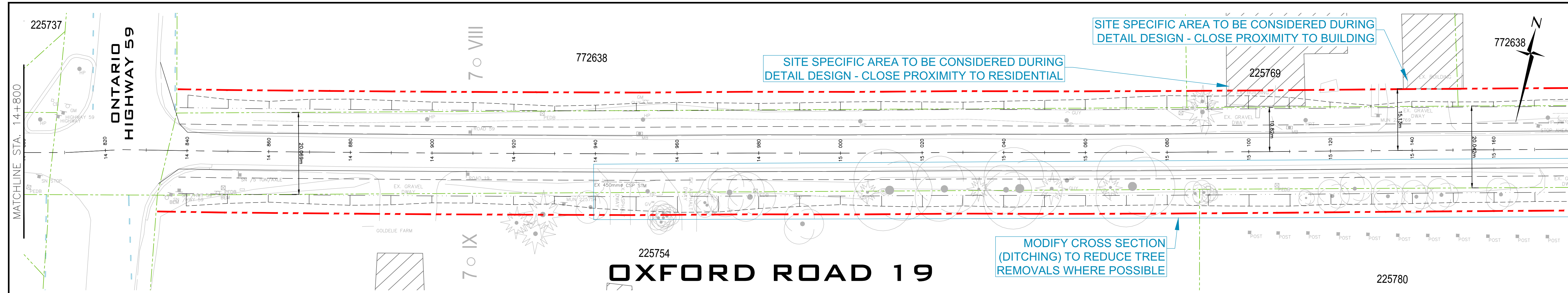


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 Layout: PLAN 13+840 TO 14+220 Date: Dec 06, 2022 15:10 User: MShannon



OxfordCounty
 Growing stronger together
 P.O. Box 1614, 21 Reeve Street
 Woodstock, Ontario N4S 7Y3
 Phone: 519-539-9800
 Fax: 519-421-4711
 www.oxfordcounty.ca

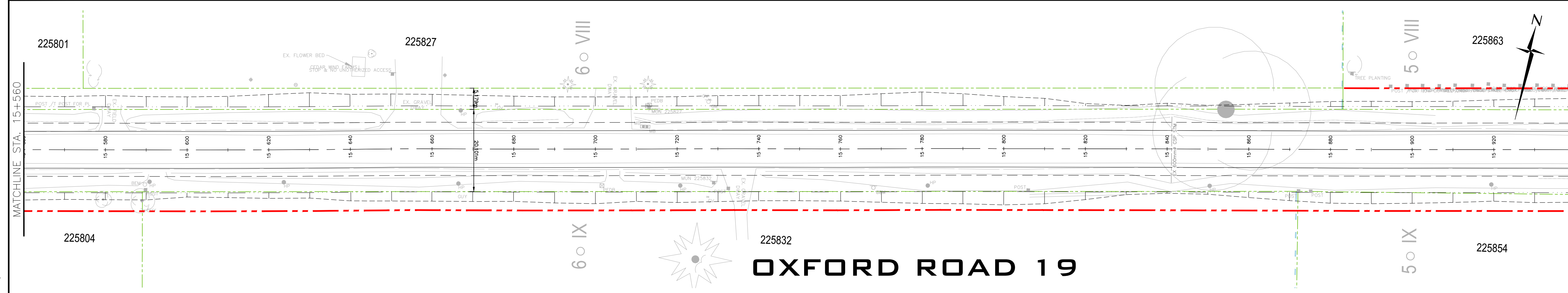
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 SHEET [PLAN 10]



LEGEND:

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- Pr. Top Bank/Matchline
- Pr. Ditchline
- Pr. Edge of Shoulder
- Pr. Edge of Pavement
- Ex. Property Line
- Ex. Centreline of Ditch
- Ex. Sanitary Forcemain
- Ex. Sanitary Sewer
- Ex. Storm Sewer
- Ex. U/G Telephone
- Ex. U/G Cable TV
- Ex. U/G Gas
- Ex. U/G Hydro
- Ex. U/G Traffic Wiring
- Ex. Watermain
- Ex. Catchbasin
- Ex. Clean Out
- Ex. Curb Stop
- Ex. Ditch Inlet CB
- Ex. Double Catchbasin
- Ex. Fire Hydrant
- Ex. Sanitary Manhole
- Ex. Storm Manhole
- Ex. Water Valve
- Ex. Cut Cross
- Ex. Sign
- Ex. Guy Wire
- Ex. Coniferous Shrub
- Ex. Deciduous Shrub
- Ex. Stump
- Ex. Coniferous Tree
- Ex. Deciduous Tree

Prop. Catchbasin
 Prop. Clean Out
 Prop. Curb Stop
 Prop. Ditch Inlet CB
 Prop. Double CB
 Prop. Fire Hydrant
 Prop. Sanitary Manhole
 Prop. Storm Manhole
 Prop. Water Valve
 Remove & Dispose

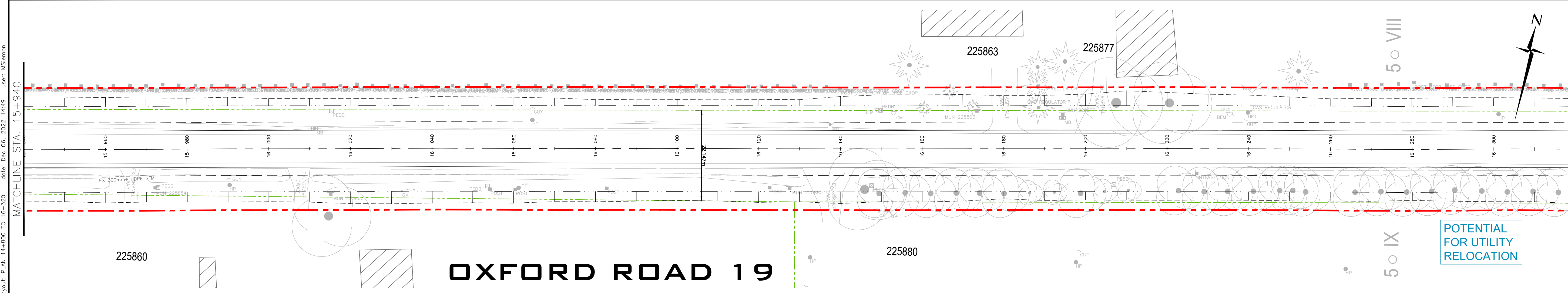


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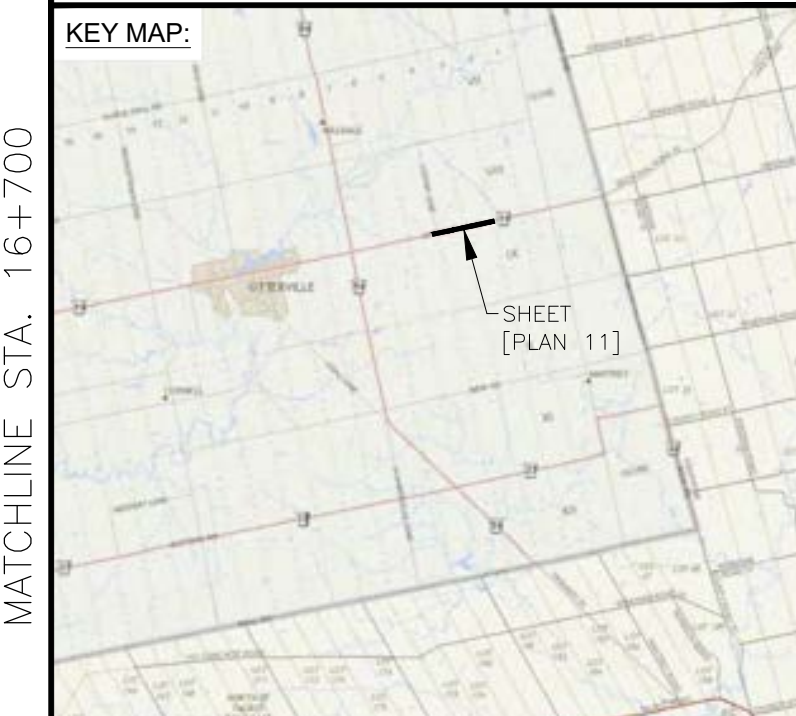
AREA MUNICIPALITY:
 Township of Norwich
 285767 Airport Road, P.O. Box 100
 Norwich, Ontario, N0J 1P0
 telephone (519) 468-2410
 fax (519) 468-2414
 web www.norwich.ca

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 R.J. Burnside & Associates Limited
 35 Perry Street
 Woodstock, Ontario, N4S 3C4
 telephone (519) 271-5111
 fax (519) 271-5730
 web www.rjburnside.com



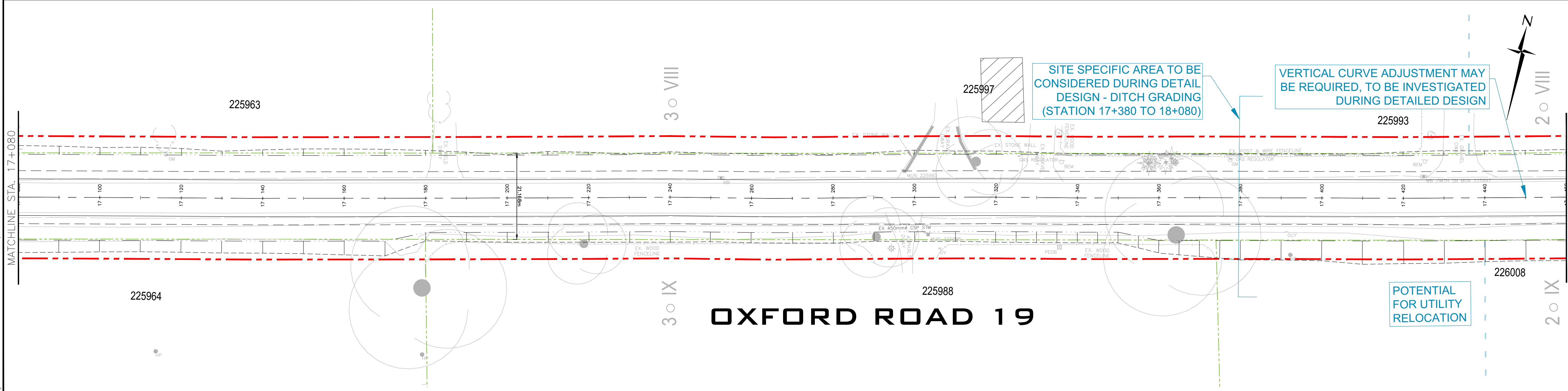
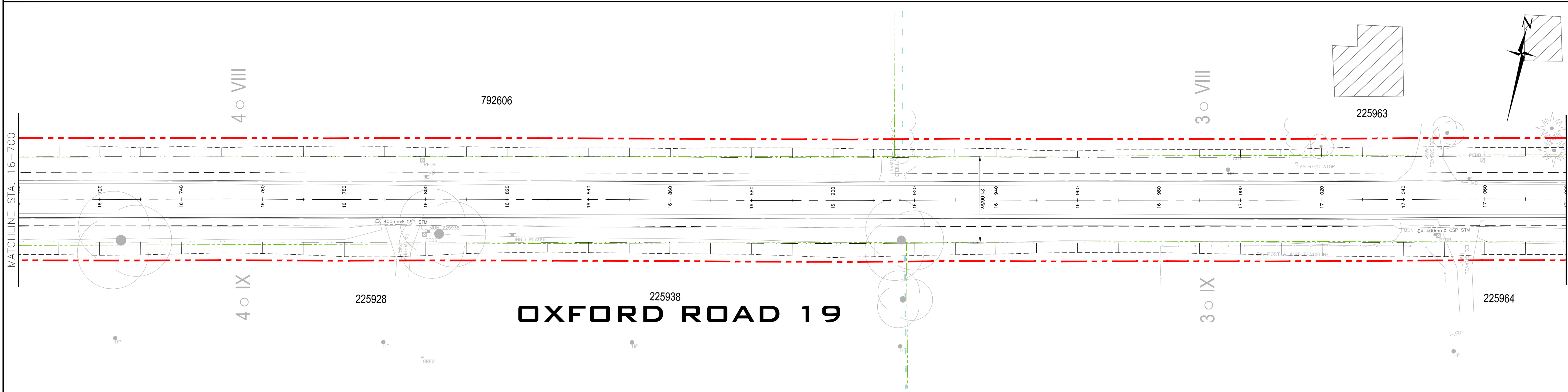
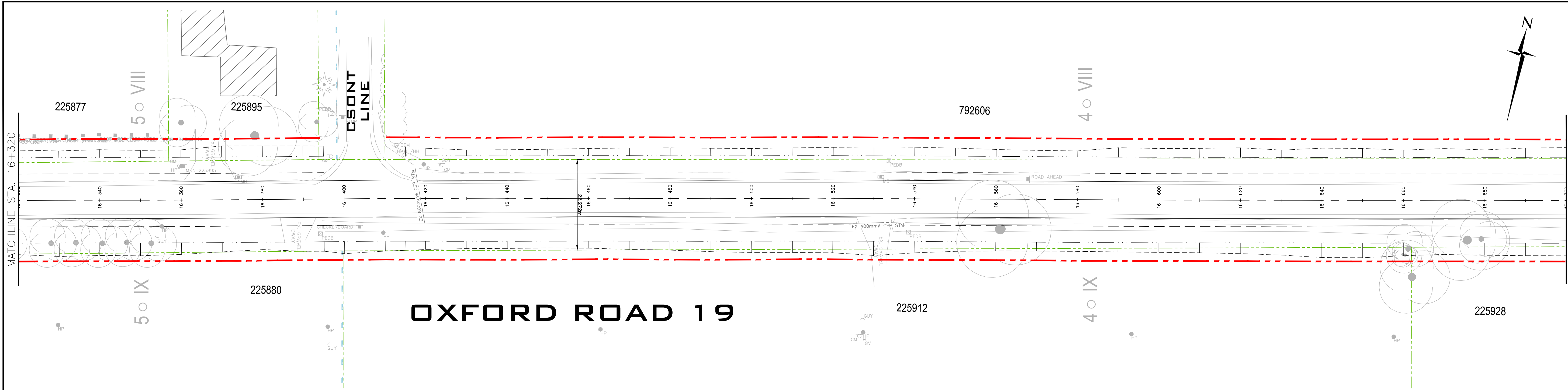
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OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
PLAN 14+800 TO 16+320	
HOR. SCALE - 1:500	
STN: 14+800 TO: 16+320	
CONTRACT NO:	
DWG NO: PLAN 10	

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 layout: PLAN 14+800 TO 16+320 date: Dec 06, 2022 15:49 user: MSimon



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Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	□ XCB
Ex. Clean Out	○ CO
Ex. Curb Stop	○ CS
Ex. Ditch Inlet CB	□ DICB
Ex. Double Catchbasin	□ DCB
Ex. Fire Hydrant	○ XSAHM
Ex. Sanitary Manhole	○ XSTMH
Ex. Storm Manhole	○ XSTMH
Ex. Water Valve	○ XWV
Ex. Cut Cross	○ XCC
Ex. Sign	□ X
Ex. Guy Wire	---
Ex. Coniferous Shrub	○
Ex. Deciduous Shrub	○
Ex. Stump	○
Ex. Coniferous Tree	○
Ex. Deciduous Tree	○
Prop. Catchbasin	■ CB
Prop. Clean Out	● CO
Prop. Curb Stop	● CS
Prop. Ditch Inlet CB	■ DICB
Prop. Double CB	■ DCB
Prop. Fire Hydrant	● XSAHM
Prop. Sanitary Manhole	● XSTMH
Prop. Storm Manhole	● XSTMH
Prop. Water Valve	● XWV
Remove & Dispose	⊗



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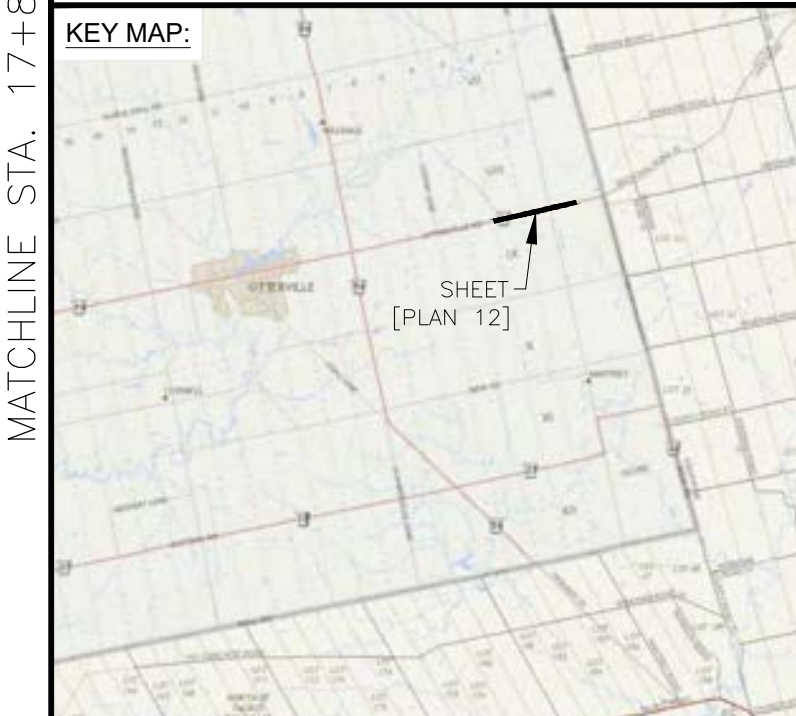
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APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 16+320 TO 17+460

HOR. SCALE - 1:500
STN: 16+320 TO: 17+460
CONTRACT NO:
DWG NO: PLAN 11

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Layout: PLAN 16+320 TO 17+460 Date: Dec 06, 2022 14:50 User: MSimon



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	CO
Pr. Ditchline	---	Prop. Curb Stop	CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	DICB
Pr. Edge of Pavement	---	Prop. Double CB	DCB
Ex. Property Line	---	Prop. Fire Hydrant	FMH
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	SAMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	WV
Ex. Storm Sewer	---	Remove & Dispose	RD
Ex. U/G Telephone	T		
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	CB		
Ex. Clean Out	CO		
Ex. Curb Stop	CS		
Ex. Ditch Inlet CB	DICB		
Ex. Double Catchbasin	DCB		
Ex. Fire Hydrant	FMH		
Ex. Sanitary Manhole	SAMH		
Ex. Storm Manhole	STMH		
Ex. Water Valve	WV		
Ex. Cut Cross	---		
Ex. Sign	---		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	---		
Ex. Deciduous Shrub	---		
Ex. Stump	---		
Ex. Coniferous Tree	---		
Ex. Deciduous Tree	---		


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Norwich, Ontario, N0J 1P0
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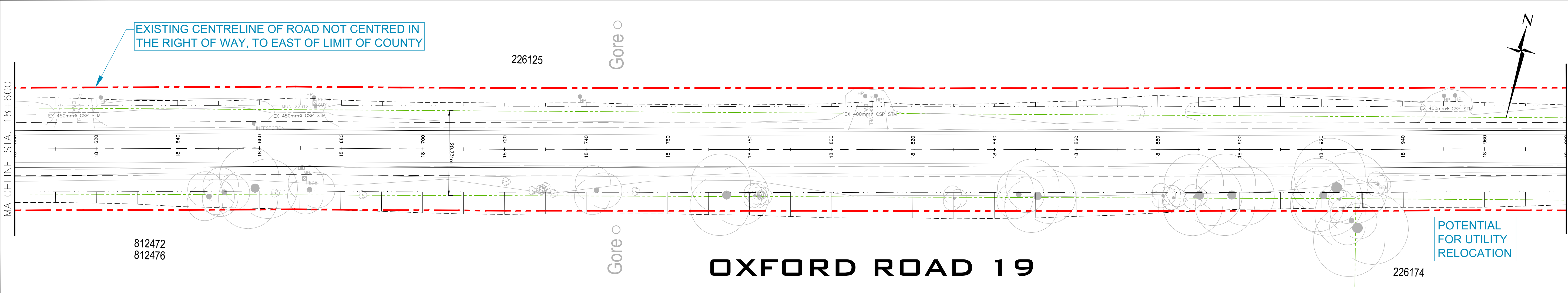
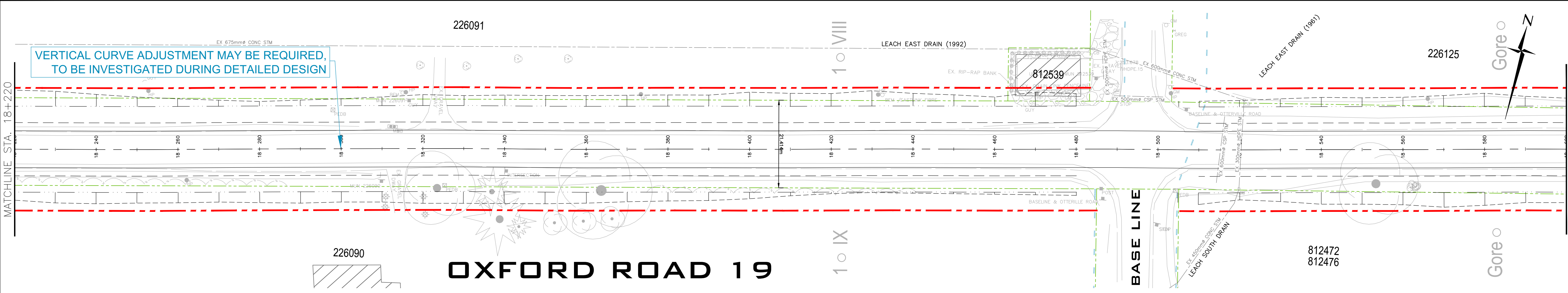
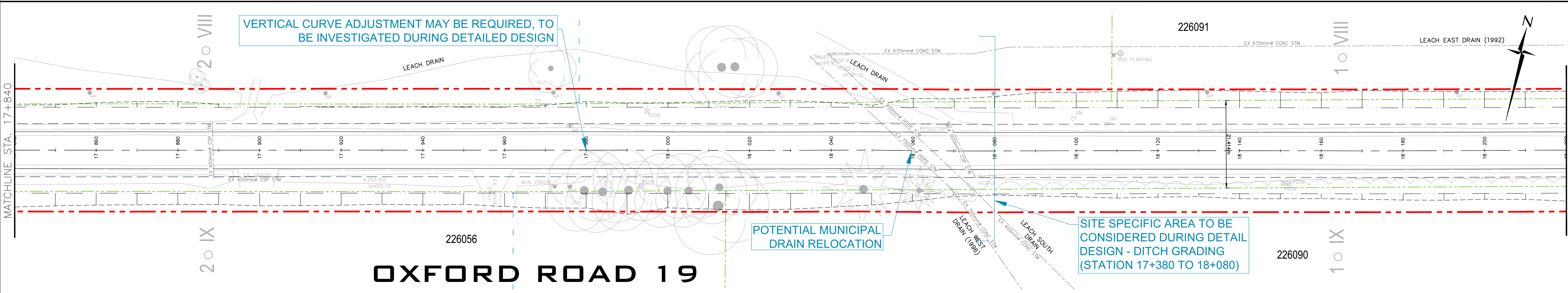
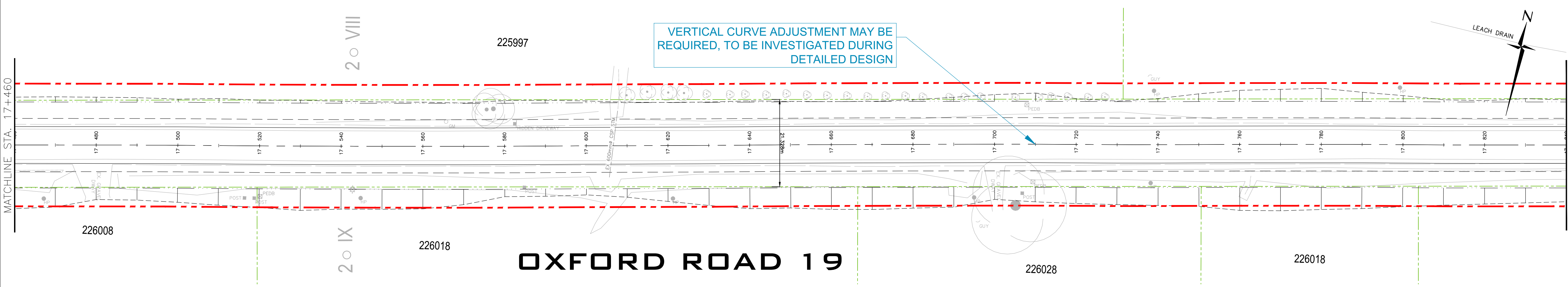
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OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 17+460 TO 18+980

HOR. SCALE - 1:500
STN: 17+460 TO: 18+980
CONTRACT NO:
DWG NO: PLAN 12

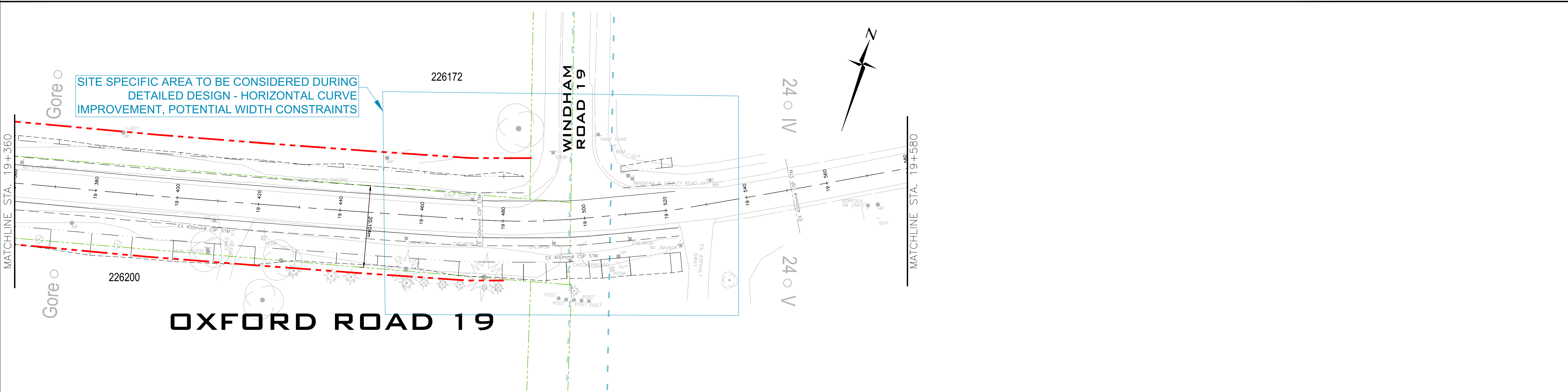
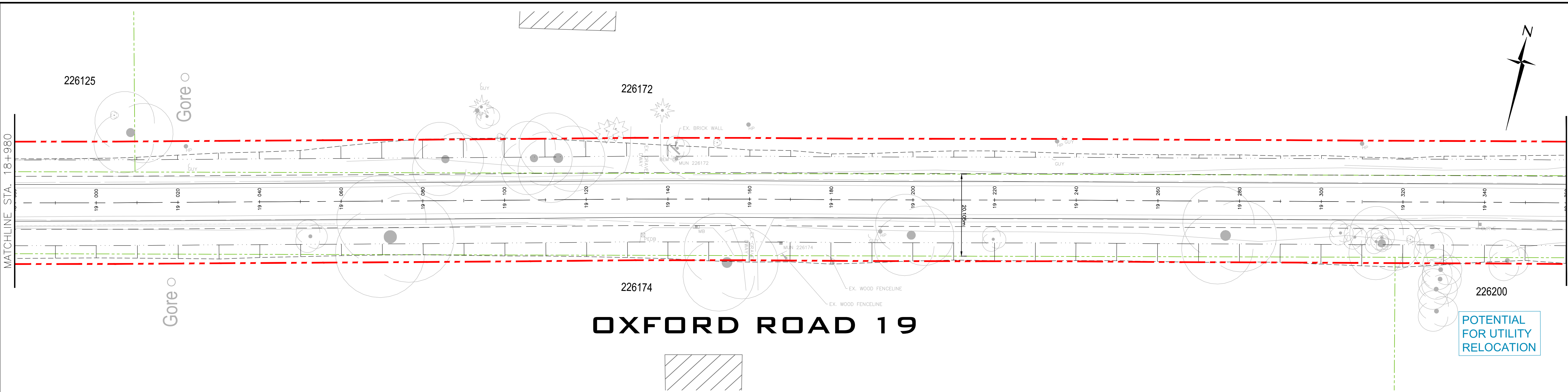


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Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	XCB
Ex. Clean Out	CO
Ex. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Ex. Fire Hydrant	FH
Ex. Sanitary Manhole	SAMH
Ex. Storm Manhole	STMH
Ex. Water Valve	WV
Ex. Cut Cross	CC
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CSH
Ex. Deciduous Shrub	DSH
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT
Prop. Catchbasin	CB
Prop. Clean Out	CO
Prop. Curb Stop	CS
Prop. Ditch Inlet CB	DICB
Prop. Double CB	DCB
Prop. Fire Hydrant	FH
Prop. Sanitary Manhole	SAMH
Prop. Storm Manhole	STMH
Prop. Water Valve	WV
Remove & Dispose	R&D



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 APPROVED BY: HC
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**OXFORD ROAD 19
 ENVIRONMENTAL
 ASSESSMENT**

PLAN 18+980 TO 19+580

HOR. SCALE - 1:500
 STN: 18+980 TO: 19+580
 CONTRACT NO:
 DWG NO: PLAN 13

file: \\SHERLOCK\Shared Work Area\03425 Oxford Road 19\03_ProductionDwg\03425 PLANS-11.dwg
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Stage 1 Archaeological Assessment Oxford Road 19 Corridor Improvements (Lots 1-8, Concessions 8-9, Former Dereham Township and Lots 2-28, Concession 8-9, and the Gore, Former Norwich Township, County of Oxford) South-West Oxford and Norwich Townships, County of Oxford

Supplementary Documentation

Prepared for:

R.J. Burnside & Associates Limited

128 Wellington Street West; Suite 301

Barrie Ontario L4N 8J6

Archaeological Licence: P1017 (Kelly)

PIF P1017-0042-2022

Archaeological Services Inc. File: 21EA-182

1 February 2023



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1.0 Maps

According to Section 7.6 of the *Standards and Guidelines for Consultant Archaeologists (S & G)* administered by the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI 2011), any information that pinpoints the location of an archaeological site (e.g., detailed assessment results mapping, tables of Global Positioning System (GPS) coordinates for site locations) must not be included in the project report and should only be provided in the Supplementary Documentation. This allows the MHSTCI to exclude it from the Ontario Public Register of Archaeological Reports, if necessary. Archaeological site location information is considered by the MHSTCI to be confidential and/or sensitive information that cannot be made public.

The following maps show the approximate location of sites within one kilometre of the Study Area, including those within the Study Area, available from the Ontario Archaeological Sites Database (O.A.S.D.), the original survey methods and registered archaeological site maps from Gary Foster's 1982 survey. Site descriptions and other relevant information relating to all archaeological work conducted for the project are contained in our accompanying Stage 1 assessment report.

Appendix A includes emails with cemetery operators and the Bereavement Authority of Ontario.





Figure 1: Location of Previously Registered Archaeological Sites within One Kilometre of the Study Area (Key Map)



Figure 2: Location of Previously Registered Archaeological Sites within One Kilometre of the Study Area (Sheet 1)



Figure 3: Location of Previously Registered Archaeological Sites within One Kilometre of the Study Area (Sheet 2)



Figure 4: Location of Previously Registered Archaeological Sites within One Kilometre of the Study Area (Sheet 3)



Figure 5: Location of Previously Registered Archaeological Sites within One Kilometre of the Study Area (Sheet 4)



Figure 6: Location of Previously Registered Archaeological Sites within One Kilometre of the Study Area (Sheet 5)

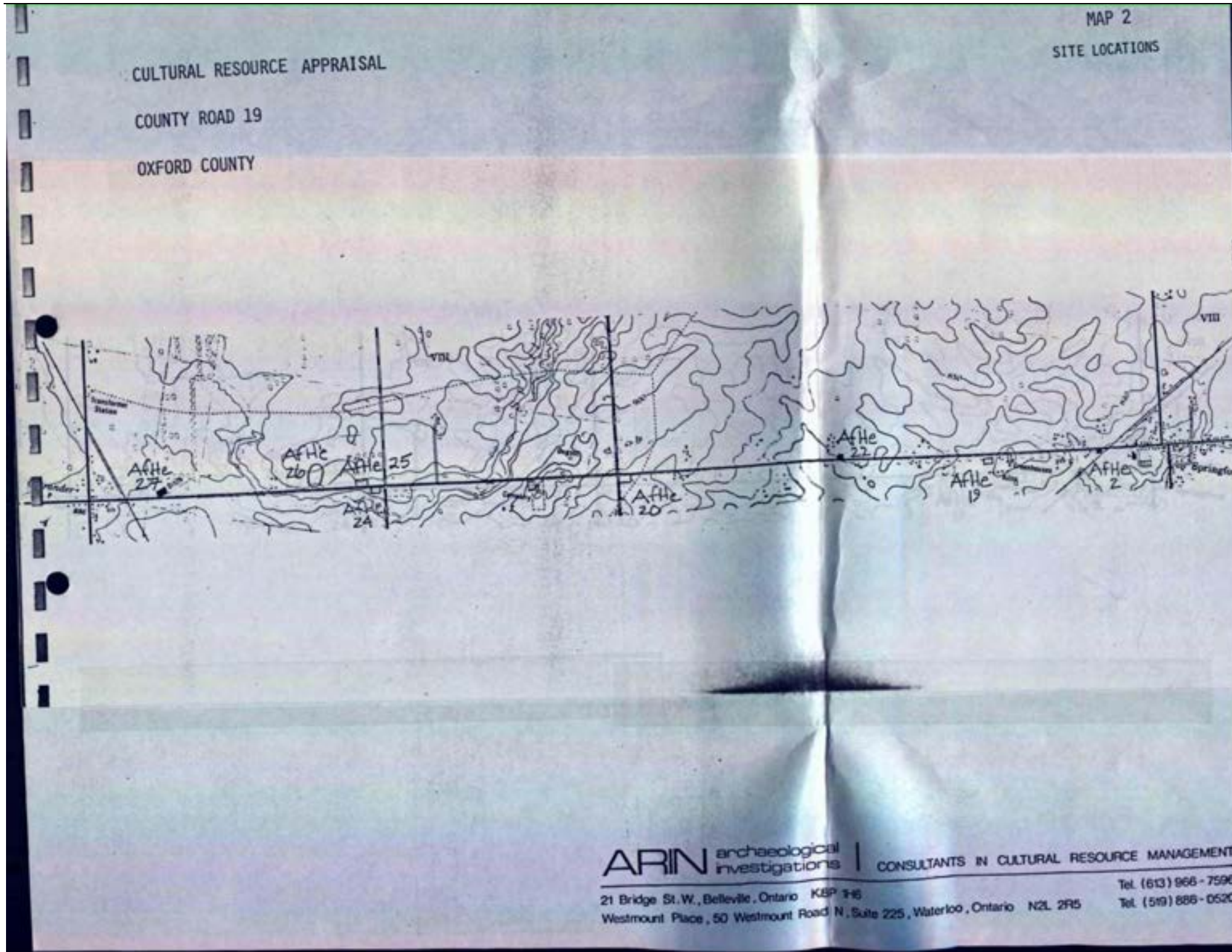


Figure 7 Foster 1982 Registered Archaeological Sites



2.0 Bibliography and Sources

ASI, (Archaeological Services Inc.). (2022). Stage 1 Archaeological Assessment Oxford Road 19 Corridor Improvements (Lots 1-8, Concessions 8-9, Former Dereham Township and Lots 2-28, Concession 8-9, and the Gore, Former Norwich Township, County of Oxford) South-West Oxford and Norwich Townships, County of Oxford.

Ministry of Tourism and Culture. (2011). Standards and Guidelines for Consultant Archaeologists. Cultural Programs Branch, Ontario Ministry of Tourism and Culture.



3.0 Cemetery Email Communications

Attached are emails documenting correspondence between ASI and the cemetery operators and the Bereavement Authority of Ontario. This is to satisfy the Ministry's request for information from cemetery operators about cemetery properties.



Laura Burke

From: Annette Carroll <acarroll@norwich.ca>
Sent: May 6, 2022 10:15 AM
To: Laura Burke
Subject: FW: [V]
Attachments: IMG_1319.jpg; ATT00001.txt; erbtown cemetery.pdf

Project Code: 21EA-182

Hi Laura

This is all that I can find for the Erbtown cemetery - there are no burial records just paperwork showing it was abandoned and the Township took over. I did find on the informationoxford.ca site and it says the following:

Erbtown Cemetery (Free Methodist Cemetery) Site Address
225424 Main St. W
Otterville, ON
Canada

The Erb family donated the land prior to 1861 for the Episcopal Methodist Church and Cemetery. This cemetery is located on Lot 14 Concession 9 of Norwich Township formerly South Norwich Township at the western town limits of Otterville, Ontario. The settlement of Erbtown started around 1850, when Abraham Erb and his four brothers came to this area to operate a sawmill and later build a woolen mill. Abraham Erb built the Erbtown Methodist Church with the burial grounds beside. This church was located near the old Pine Street Friend's Meeting House of the Quakers.

This is all that I have. The only index of records is the one I have found on Find A Grave.ca

Hope this helps a little. The pictures attached are polaroids taken many years ago.

Thank you,
Annette Carroll
Community Services Clerk
Township of Norwich
Norwich, ON
NOJ 1P0
P: 519-863-3733 Ext. 21

The information contained in this email is intended only for the individual or entity to whom it is addressed. Its contents (including any attachments) may contain confidential and/or privileged information. If you are not an intended recipient, you may not use, disclose, disseminate, copy or print its content. If you received this email in error, please notify the sender by reply email and delete and destroy the message.

-----Original Message-----

From: Annette Carroll <acarroll3spring@yahoo.com>
Sent: May 6, 2022 9:59 AM
To: Annette Carroll <acarroll@norwich.ca>

Subject:

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or on clicking links from unknown senders.

Danielle Bella

From: Norwich Archives <archives@norwichdhs.ca>
Sent: Thursday, April 7, 2022 1:40 PM
To: Danielle Bella
Subject: Re: Inquiry About Local Cemeteries [V]

Project Code: 21EA-182

Hi Danielle,

We have the published material on the 3 cemeteries that was done by Oxford County OGS. Extra material on Pine Street **may** be available for the South Norwich Historical Society as their meeting house is right beside the cemetery. On Facebook they are: [Historic Otterville/Mill and Station Museum](#). They will also have info on the Otterville Methodist Episcopal cemetery, north of Otterville. We have a file of collected info on it. As to the family cemetery, there needs to be clarification on the location. 225895 Otterville Road is way east of Otterville. No Black families had land in that area and usually if a family cemetery was created the family actually owned the land at one point, as in the Black family cemetery north of Burgessville. Burkholders had a large blacksmith shop there and they produced the Stillyard Stump Puller that was shipped across Canada. They were very German and had enough sons that they would not need extra labour. I can check the census to verify, if this is the location Mr Pettman refers to. The problem occurs when you Google 225895, you get a totally different location on Otterville Road/ County Road 19 that is 1.35 km west of Middletown Line and west of Otterville. This is an area where Black families did live. So there needs to be double checking of the location. Black Settlement in South Norwich is documented by Joyce Pettigrew in her book: A Safe Haven. We checked and Mr Pettman's memory of a cemetery is not in the book. Again, contact the South Norwich Historical Society as Joyce is still an active member there.

Let us know when you are ready to come. I did roam around St Charles this spring and got some nice photos for a researcher. Next on my list to do is St Peter's.

Janet Hilliker
Archivist

From: [Danielle Bella](#)
Sent: Wednesday, April 6, 2022 5:31 PM
To: archives@norwichdhs.ca
Cc: [Laura Burke](#)
Subject: Inquiry About Local Cemeteries

Hello,

I am an archaeologist researching Oxford Road 19 between Plank Line in Ostrander and Windham Road 19 east of Brown's Corners. Part of my research involves cemeteries within or near my Study Area's boundaries. The Ministry of Heritage, Sports, Tourism and Culture Industries requires us to obtain maps or plans of the legal limits of cemetery properties to help determine if there could be human remains outside of the present-day boundaries. There are three established cemeteries and one possible cemetery that has come up in this research:

- St. Charles Anglican Cemetery on Lots 1-2, Concession 9 in Dereham Township
- Springford Community Cemetery on Lot 17, Concession 8 in Norwich Township
- Pine Street Burial Ground on Lot 13, Concession 9 in Norwich Township
- Possible Black Pioneer Family cemetery on Lot 5, Concession 8 and/or 9 in Norwich Township

The last cemetery was discovered during a personal communication with Charles Pettman. Mr. Pettman is a manager of the St. Charles Anglican Cemetery and a local property owner. He remembers a Black Pioneer Family cemetery within an agricultural field, approximately at 225895 County Road 19. He also remembers a farmer in the 1940s removing the cemetery's white picket fence and headstones.

Do you have any information on these cemeteries? The Bereavement Authority of Ontario has no record of a Black Pioneer Cemetery within this area. One of my colleagues will be conducting a property inspection within the next few weeks and if information is available it would be fantastic to schedule an appointment during your hours on Thursdays between 10am and 4pm.

Thank you,

Danielle Bella, BA (Hon) (She/Her)
Archaeologist | Technical Writer • Environmental Assessment Division



ASI • Providing Archaeological & Cultural Heritage Services
DBella@asiheritage.ca • 416 966 1069 • Fax: 416 966 9723
528 Bathurst Street, Toronto, Ontario, M5S 2P9 • asiheritage.ca



Virus-free. www.avg.com

Eliza Brandy

From: Henry Centen <Henry.Centen@rjburnside.com>
Sent: April 12, 2022 2:26 PM
To: Avid Banihashemi
Cc: Sylvia Waters; Eliza Brandy
Subject: FW: Oxford Road 19 - Past Archaeological Assessments; Cemetery Info [V]

Project Code: 21EA-182

fyi

From: Jesse Keith <jkeith@oxfordcounty.ca>
Sent: Tuesday, April 12, 2022 2:09 PM
To: Heather St. Clair <hstclair@oxfordcounty.ca>; Kim Armstrong <karmstrong@norwich.ca>
Cc: Henry Centen <Henry.Centen@rjburnside.com>
Subject: RE: Oxford Road 19 - Past Archaeological Assessments; Cemetery Info

Thanks Heather & Kim

JESSE KEITH, P. ENG. (HE / HIM) | Project Engineer, Public Works
OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3
WWW.OXFORDCOUNTY.CA | T 519.539.9800 / 1-800-755-0394, ext 3194



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From: Heather St. Clair <hstclair@oxfordcounty.ca>
Sent: April 12, 2022 2:06 PM
To: Jesse Keith <jkeith@oxfordcounty.ca>
Subject: FW: Oxford Road 19 - Past Archaeological Assessments; Cemetery Info

From: Kim Armstrong
Sent: April 12, 2022 2:05 PM
To: Heather St. Clair <hstclair@oxfordcounty.ca>; Henry.Centen@rjburnside.com'
Cc: Tricia Radburn <Tricia.Radburn@rjburnside.com>; Avid Banihashemi <Avid.Banihashemi@rjburnside.com>; Eliza Brandy <ebrandy@asiheritage.ca>; Frank Gross <fgross@oxfordcounty.ca>; Melissa Abercrombie <mabercrombie@oxfordcounty.ca>; Reuben Davis <rdavis@oxfordcounty.ca>
Subject: RE: Oxford Road 19 - Past Archaeological Assessments; Cemetery Info

Good Afternoon

The Township of Norwich has no record of an unmarked cemetery near Csont Line.

Kimberley Armstrong, DPA
Deputy Clerk, Township of Norwich
285767 Airport Road, Norwich, ON
519-468-2410 ext. 226
www.norwich.ca

From: Heather St. Clair <hstclair@oxfordcounty.ca>
Sent: Tuesday, April 12, 2022 10:49 AM
To: Kim Armstrong <karmstrong@norwich.ca>
Subject: FW: Oxford Road 19 - Past Archaeological Assessments; Cemetery Info

Good morning,

Are you aware of any archeological assessments in the area of Otterville Road? Are you also aware of any marked or unmarked cemeteries in the vicinity of Csont Line?

From: Jesse Keith
Sent: April 12, 2022 10:03 AM
To: 'Henry Centen' <Henry.Centen@rjburnside.com>; Heather St. Clair <hstclair@oxfordcounty.ca>
Cc: Tricia Radburn <Tricia.Radburn@rjburnside.com>; Avid Banihashemi <Avid.Banihashemi@rjburnside.com>; Eliza Brandy <ebrandy@asiheritage.ca>; Frank Gross <fgross@oxfordcounty.ca>; Melissa Abercrombie <mabercrombie@oxfordcounty.ca>; Reuben Davis <rdavis@oxfordcounty.ca>
Subject: RE: Oxford Road 19 - Past Archaeological Assessments; Cemetery Info

Hi Henry,

I'm not aware/can't find any past archaeological assessments completed within the study area; however, I'll try further. Urbanization works in Otterville were completed in the mid 1990s – I'll see if we can explore our archives.

I'm attaching a "Heritage Resources Inventory", Appendix 4 from the County's Official Plan. I'm not aware of any further related information that the County has on record. I'm including Heather St. Clair, our Development Planner for Township of Norwich.

Heather – please see below correspondence, and chime in if you have any further info; appreciate it.

Regards,

JESSE KEITH, P. ENG. (HE / HIM) | **Project Engineer, Public Works**
OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3
WWW.OXFORDCOUNTY.CA | T 519.539.9800 / 1-800-755-0394, ext 3194



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From: Henry Centen <Henry.Centen@rjburnside.com>
Sent: April 7, 2022 6:07 AM
To: Jesse Keith <jkeith@oxfordcounty.ca>
Cc: Tricia Radburn <Tricia.Radburn@rjburnside.com>; Avid Banihashemi <Avid.Banihashemi@rjburnside.com>; Eliza Brandy <ebrandy@asiheritage.ca>
Subject: FW: Oxford Road 19

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Hi Jesse,

Our archaeological subconsultant, ASI, is working on the Stage 1 Archaeological Assessment for the OR19 EA. Please see the highlighted questions below and let me know if you have any of the information requested.

Thanks
Cheers
Henry Centen, P. Eng.
R.J. Burnside & Associates Limited

From: Eliza Brandy <ebrandy@asiheritage.ca>
Sent: Wednesday, April 06, 2022 4:53 PM
To: Tricia Radburn <Tricia.Radburn@rjburnside.com>
Subject: RE: Oxford Road 19

Hi Tricia,

We have completed most of our background research and are planning to conduct our site visit in the coming week or two.

We're finding some interesting leads, including local knowledge from a long-time resident near who indicated that a previous archaeological assessment was done in the area for road improvements, possibly in the 1990s? We haven't been able to locate it in the MHSTCI database. Can you inquire with the County if they have any copies of any archaeological assessments previously completed in this study area?

This resident also indicated the presence of a historical Black pioneer burying ground in the vicinity of Csont Line that is currently not marked or registered. We're investigating a bit further into this with the local archive/museum. If the County has any further information about this, that would be helpful as well!

Eliza Brandy, MA (She/They)
Associate Archaeologist | Project Manager • Environmental Assessment Division



ASI • Providing Archaeological & Cultural Heritage Services
EBrandy@asiheritage.ca • 416 966 1069 x 222 • Fax: 416 966 9723
528 Bathurst Street, Toronto, Ontario, M5S 2P9 • asiheritage.ca

Eliza Brandy

From: Michael D'Mello <michael.dmello@thebao.ca>
Sent: April 21, 2022 10:33 AM
To: Laura Burke
Cc: Eliza Brandy; Danielle Bella; Ray Porrill; Carey Smith; Jim Cassimatis; Licensing; Karie Draper; Michael D'Mello
Subject: RE: Stage 1 Archaeological Assessment for three cemeteries on Oxford / Ostrander Road [V]

Project Code: 21EA-182

Hello Laura:

This will acknowledge receipt of your e-mail of April 21, 2022 cc'd below advising that an archaeological assessment will be conducted in the area of the possible "Pettman Cemetery".

We look forward to being advised of the results of that archaeological assessment.

Thank you for contacting the Bereavement Authority of Ontario.

Mike

Michael F. D'Mello | Deputy Registrar

Bereavement Authority of Ontario

Address: 100 Sheppard Avenue East, Suite 505, Toronto, Ontario, M2N 6N5

Direct: 647-483-2648 | Cell: 416-728-5396 | Toll Free: 1-844-493-6356 | Fax: 647-748-2645

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From: Laura Burke <lburke@asiheritage.ca>
Sent: April 21, 2022 10:10 AM
To: Michael D'Mello <michael.dmello@thebao.ca>
Cc: Eliza Brandy <ebrandy@asiheritage.ca>; Danielle Bella <dbella@asiheritage.ca>; Ray Porrill <ray.porrill@thebao.ca>; Carey Smith <carey.smith@thebao.ca>; Jim Cassimatis <Jim.Cassimatis@thebao.ca>; Licensing <licensing@thebao.ca>; Karie Draper <karie.draper@thebao.ca>
Subject: RE: Stage 1 Archaeological Assessment for three cemeteries on Oxford / Ostrander Road

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Good morning Michael,

Thanks to you and Ray for providing the necessary documents.

We have already reached out to the municipality. They were unaware of a possible cemetery within or near our Study Area.

A member of our staff will be doing a field review next week. We hope to find out more information and an exact location of the burials Mr. Pettman remembers.

Thanks as always for your cooperation.

Laura Burke

From: Michael D’Mello <michael.dmello@thebao.ca>
Sent: April 20, 2022 5:26 PM
To: Laura Burke <lburke@asiheritage.ca>
Cc: Eliza Brandy <ebrandy@asiheritage.ca>; Danielle Bella <dbella@asiheritage.ca>; Ray Porrill <ray.porrill@thebao.ca>; Michael D’Mello <michael.dmello@thebao.ca>; Carey Smith <carey.smith@thebao.ca>; Jim Cassimatis <Jim.Cassimatis@thebao.ca>; Licensing <licensing@thebao.ca>; Karie Draper <karie.draper@thebao.ca>
Subject: RE: Stage 1 Archaeological Assessment for three cemeteries on Oxford / Ostrander Road

Hello Laura:

This will acknowledge receipt of your e-mail of April 6, 2022 cc’d below and Ray Porrill’s, Licensing Officer, BAO response to you with the various maps and surveys attached.

Should you require a Cemetery Investigation Authorization for any of the assessments related to each of these cemetery sites, please send us separate e-mail since keeping track of multiple sites in one e-mail becomes a nightmare. Thanks for your cooperation.

With regard to the unlicensed Pettman Cemetery site, you might consider notifying the municipality of the possibility that a cemetery exists in that location so that should there be an application for development the municipality will require an archaeological assessment of that area before a permit is issued. We too will contact the municipality and advise them of the un-identified cemetery site in that area.

Thank you for contacting the Bereavement Authority of Ontario.

Mike

Michael F. D’Mello | Deputy Registrar

Bereavement Authority of Ontario

Address: 100 Sheppard Avenue East, Suite 505, Toronto, Ontario, M2N 6N5

Direct: 647-483-2648 | Cell: 416-728-5396 | Toll Free: 1-844-493-6356 | Fax: 647-748-2645

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From: Ray Porrill <ray.porrill@thebao.ca>
Sent: April 6, 2022 3:05 PM
To: Laura Burke <lburke@asiheritage.ca>; Michael D’Mello <michael.dmello@thebao.ca>
Cc: Eliza Brandy <ebrandy@asiheritage.ca>; Danielle Bella <dbella@asiheritage.ca>
Subject: RE: Stage 1 Archaeological Assessment for three cemeteries on Oxford / Ostrander Road

Hi, Laura.

I've attached the maps I found for Sixth Line, St. Peter's Anglican, St. Charles Anglican, Springford and Pine Street Burying Ground (The Friends) cemeteries.

As for Pettman, the only Pettman I can locate in our system is Charles Pettman who takes care of the St. Charles Anglican Cemetery. We have no record of a black cemetery or a Pettman Cemetery in that area.

Best Regards,

Ray Porrill | Licensing Officer

Bereavement Authority of Ontario | 100 Sheppard Av. East, Suite 505 | Toronto, Ontario, M2N 6N5

T: 647.483.2645 Ext. 211 | F: 647-748-2645

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From: Laura Burke <lburke@asiheritage.ca>

Sent: Wednesday, April 6, 2022 1:31 PM

To: Michael D'Mello <michael.dmello@thebao.ca>; Ray Porrill <ray.porrill@thebao.ca>

Cc: Eliza Brandy <ebrandy@asiheritage.ca>; Danielle Bella <dbella@asiheritage.ca>

Subject: RE: Stage 1 Archaeological Assessment for three cemeteries on Oxford / Ostrander Road

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Good day Michael and Ray,

I am following up today to see if you have found any maps or boundary surveys related to the three licensed cemetery sites in our study area.

I also wanted to let you know that today I spoke with Charles Pettman, one of the managers of the St. Charles Anglican Cemetery and local property owner. He provided me with some interesting historical information for that cemetery, but he did not have any maps to share.

He did however inform me of a fourth possible cemetery within Lot 5 Concession 8 that may also extend to Lot 5 Concession 9. He remembers a Black Pioneer Family cemetery within the farmer's field. It used to be surrounded by a small, white picket fence and that it had stone headstones. He also remembers that a farmer in the 1940's, **took down the white picket fence as well as the headstones**. I asked him specifically if he recalls whether the farmer had disturbed or moved the burials. He said he does not recall that and does not think the burials have been disturbed. I asked him if he could give me an estimate of how big the cemetery was and he said it could have been up to a quarter of an acre and that it may have extended south of Oxford / Ostrander Road.

I realize this is an unregistered, unmarked cemetery but I want to inquire to see if you have any information at all on this possible cemetery. He also mentioned that a former (unknown) researcher named the cemetery in question the Pettman cemetery. He said he owns the adjacent property in Lot 4 and that is indeed his surname, but the cemetery in question is actually in Lot 5 and not related to his family.

I have attached a photo and placed a pin in the general area he described, it is just west of 225895 County Road 19/Offord Road.

Thanks as always for your time,

Laura

From: Michael D'Mello <michael.dmello@thebao.ca>

Sent: March 30, 2022 2:57 PM

To: Laura Burke <lburke@asiheritage.ca>

Cc: Ray Porrill <ray.porrill@thebao.ca>; Michael D'Mello <michael.dmello@thebao.ca>; Carey Smith <carey.smith@thebao.ca>; Licensing <licensing@thebao.ca>; Jim Cassimatis <Jim.Cassimatis@thebao.ca>; Karie Draper <karie.draper@thebao.ca>

Subject: RE: Stage 1 Archaeological Assessment for three cemeteries on Oxford / Ostrander Road

Hi Laura:

This will acknowledge receipt of your 2nd e-mail of March 30, 2022, cc'd below, requesting maps and boundary surveys relating to 3 licensed cemetery sites; Our database provides the following information on the sites you are interested in.

1.

Springford Cemetery – Licensed Cemetery Site No. 03027
Trustees of the Springford Cemetery Board
225203 Otterville Road, On

Trustees of the Springford Cemetery Board – Licensed Operator No. 3292868
Trustees of the Springford Cemetery Board
Hicks, Joan (2028001)
185106 Cornell Road R.R. #3, Tillsonburg, On N4g4g8
P: (519) 842-5618, F: (519) 688-2464

2.

St. Charles Anglican Cemetery – Licensed Site No. 03042
Anglican Synod Of Huron
224570 Ostrander Road, On

Anglican Synod of Huron – Licensed Operator No. 3293010
Anglican Synod of Huron
Pettman, Charles (2027660)
412611 Cranberry Line, R.R. # 7, Tillsonburg, On N4g4h1
P: (519) 842-4357

3.

Pine Street Burying Ground (The Friends) Cemetery – Licensed Site No. 03021
Corporation Of The Township Of Norwich
Ontario

Corporation of the Township of Norwich (System ID No. 3292704)
Corporation of the Township of Norwich
Hovorka, Patrick (2030794)
53 Stover St. South, P.O. Box 38, Norwich, ON N0J 1P0
P: (519) 863-3733 Ext. 23, F: (519) 863-3352

We will review our files and respond in due course.

Thank you for contacting the Bereavement Authority of Ontario.

Mike

Michael F. D’Mello | Deputy Registrar

Bereavement Authority of Ontario

Address: 100 Sheppard Avenue East, Suite 505, Toronto, Ontario, M2N 6N5

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From: Laura Burke <lburke@asiheritage.ca>

Sent: March 30, 2022 9:50 AM

To: Michael D’Mello <michael.dmello@thebao.ca>

Cc: Ray Porrill <ray.porrill@thebao.ca>

Subject: Stage 1 Archaeological Assessment for three cemeteries on Oxford / Ostrander Road

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Hello again!

I spoke to two of the three cemetery operators/managers this past week. Although very helpful and accommodating they did not have official maps or plans to fulfil our requirements to confirm the cemetery boundaries for the Ministry of Heritage, Sport, Tourism and Culture Industries. For the third cemetery, St. Charles Anglican Cemetery, I have called and emailed the cemetery operator but have received no response at all. I will continue trying.

For the following cemeteries can you please send us any burial plot information and maps which confirm the following cemetery boundaries.

Cemetery #1 Springford Community Cemetery

TRUSTEES OF THE SPRINGFORD CEMETERY BOARD

24 Wood Street

PO Box 13

Springford, ON N0J 1X0

Cemetery #2 St. Charles Anglican Cemetery

ANGLICAN SYNOD OF HURON

412611 CRANBERRY LINE, R.R. # 7

TILLSONBURG, ON N4G4H1

Canada

Cemetery [#3](#) Pine Street Burial Ground

Corporation of the Township of Norwich

53 Stover St. South

P.O. Box 38

ESSEX, COUNTY OF

Norwich, ON N0J 1P0

Canada

As always thank you for your time. It is appreciated.

Laura Burke



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Appendix E

Cultural Heritage Resource Assessment

Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment

Oxford Road 19 Corridor Improvements Municipal Class Environmental Assessment

South-West Oxford and Norwich Townships County of Oxford and County of Norfolk, Ontario

Draft Report

Prepared for:

R.J. Burnside & Associates Limited
128 Wellington Street West; Suite 301
Barrie Ontario L4N 8J6

Archaeological Services Inc. File: 21CH-148

June 2022 (Updated January, February, and March 2023)



Executive Summary

Archaeological Services Inc. was contracted by R.J. Burnside & Associates Limited, on behalf of the County of Oxford, to conduct a Cultural Heritage Report as part of the Oxford Road 19 Corridor Improvements Municipal Class Environmental Assessment. The Environmental Assessment involves proposed road improvements to Oxford Road 19 from Highway 19 (Plank Line) to the Norfolk County boundary at Windham Road 19. The project study area consists of approximately 16 kilometres of the Oxford Road 19 right-of-way from Highway 19 to the Norfolk County boundary at Windham Road 19, excluding the settlement centres of Springford and Otterville, and is generally bounded by rural and agricultural properties.

The purpose of this report is to present an inventory of known and potential built heritage resources (B.H.R.s) and cultural heritage landscapes (C.H.L.s), identify existing conditions of the project study area, provide a preliminary impact assessment, and propose appropriate mitigation measures.

The results of background historical research and a review of secondary source material, including historical mapping, indicate a study area with a rural land use history dating back to the early nineteenth century. A review of federal, provincial, and municipal registers, inventories, and databases revealed that there are two known features of cultural heritage value within the Oxford Road 19 Corridor Improvements study area. An additional 30 features were identified during background research and fieldwork.

Based on the results of the assessment, the following recommendations have been developed:

1. Construction activities and staging should be suitably planned and undertaken to avoid unintended negative impacts to identified built heritage resources and cultural heritage landscapes. Avoidance measures may include, but are not limited to: erecting temporary fencing, establishing buffer zones, issuing instructions to construction crews to



avoid identified B.H.R.s and C.H.L.s, etc. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can also be implemented.

2. As there are direct adverse impacts anticipated to the following properties: 225769 Otterville Road (B.H.R. 7), 224570 Ostrander Road (C.H.L. 3), 225400 Otterville Road (C.H.L. 12), and given the potential cultural heritage value of those properties, a resource-specific Cultural Heritage Evaluation Report (C.H.E.R.) should be conducted to determine cultural heritage value or interest (C.H.V.I.). As there are direct impacts anticipated, should the C.H.E.R. determine that the property retains C.H.V.I., a resource-specific Heritage Impact Assessment (H.I.A.) should be conducted to evaluate alternatives, assess potential impacts to the resource, and recommend appropriate mitigation measures.
 - a. The C.H.E.R. and H.I.A. should be completed by a qualified heritage professional with recent and relevant experience as early in detailed design as possible.
3. Direct impacts to C.H.L. 13 (225422 Otterville Road) are anticipated to include grading and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property. As C.H.L. 13 is designated under Part IV of the *Ontario Heritage Act*, and there are direct impacts anticipated, a resource-specific H.I.A. is required as per clause 3.3.2.2 of the *Oxford County Official Plan* (County of Oxford, 2021).¹ Given that the proposed undertaking is not anticipated to result in direct adverse impacts to the property and no known heritage attributes are anticipated to be impacted, it is recommended

¹ The proposed mitigation strategies differ between Recommendation 2, and Recommendations 3 and 4, given that clause 3.3.2.2 of the *Oxford County Official Plan* (County of Oxford, 2021) refers specifically to properties designated under the *Ontario Heritage Act* and that if those properties are altered, there is a requirement for a Heritage Impact Assessment.



that the County of Oxford should consider waiving the requirement for a H.I.A. in this case if suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.

4. As the property at 225947 Otterville Road (C.H.L. 20) is designated under Part IV of the *Ontario Heritage Act* and direct impacts anticipated due to property acquisition, grading, encroachment onto the northern portion of the property resulting in changes to the parcel boundaries, and removal/relocation of the Ontario Heritage Trust plaque, a resource-specific H.I.A. is required as per clause 3.3.2.2 of the *Oxford County Official Plan* (County of Oxford, 2021). However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property.
 - a. The H.I.A. should be completed by a qualified heritage professional with recent and relevant experience as early in detailed design as possible.
 - b. The Ontario Heritage Trust plaque should be removed prior to construction and stored in a secure facility to prevent damage. Following construction activities, this plaque should be reinstalled at its extant location, or in a similarly accessible location based on consultation with the Township of Norwich and the Ontario Heritage Trust.
5. To ensure the following properties are not adversely impacted during construction, baseline vibration monitoring should be undertaken during detailed design:
 - 224261 Ostrander Road (B.H.R. 1),
 - 224943 Otterville Road (B.H.R. 2),
 - 224948 Otterville Road (B.H.R. 3),
 - 225769 Otterville Road (B.H.R. 7),
 - 224570 Ostrander Road (C.H.L. 3),



- 225227 Otterville Road (C.H.L. 7),
- 225279 Otterville Road (C.H.L. 8),
- 225413 Otterville Road (C.H.L. 11),
- 225400 Otterville Road (C.H.L. 12),
- 225422 Otterville Road (C.H.L. 13),
- 225860 Otterville Road (C.H.L. 18),
- 225877 Otterville Road (C.H.L. 19), and
- 225963 Otterville Road (C.H.L. 21).

Should this advance monitoring assessment conclude that the structure(s) on these properties will be subject to vibrations, prepare and implement a vibration monitoring plan as part of the detailed design phase of the project to lessen vibration impacts related to construction.

6. A qualified heritage consultant should be contacted during detailed design to review the designs in order to confirm impacts of the proposed works on the potential C.H.L.s at 225227 Otterville Road (C.H.L. 7), 225659 Otterville Road (C.H.L. 15), 225688 Otterville Road (C.H.L. 16), and 225720 Otterville Road (C.H.L. 17). This would determine whether there would be any adverse impacts to the properties and any subsequent cultural heritage requirements or reporting.
7. Should future work require an expansion of the study area then a qualified heritage consultant should be contacted in order to confirm the impacts of the proposed work on potential heritage resources.
8. The report should be submitted to the Township of South-West Oxford, the Township of Norwich, and the County of Norfolk and the Ministry of Citizenship and Multiculturalism for review and comment, and any other local heritage stakeholders that may have an interest in this project. The final report should be submitted to the Township of South-West Oxford, the Township of Norwich, and the County of Norfolk for their records.



Report Accessibility Features

This report has been formatted to meet the Information and Communications Standards under the *Accessibility for Ontarians with Disabilities Act, 2005* (A.O.D.A.). Features of this report which enhance accessibility include: headings, font size and colour, alternative text provided for images, and the use of periods within acronyms. Given this is a technical report, there may be instances where additional accommodation is required in order for readers to access the report's information. If additional accommodation is required, please contact Annie Veilleux, Manager of the Cultural Heritage Division at Archaeological Services Inc., by email at aveilleux@asiheritage.ca or by phone 416-966-1069 ext. 255.



Project Personnel

- **Senior Project Manager:** Lindsay Graves, M.A., C.A.H.P., Senior Cultural Heritage Specialist, Assistant Manager - Cultural Heritage Division
- **Project Coordinator:** Katrina Thach, B.A. (Hon), Associate Archaeologist, Project Coordinator - Environmental Assessment Division
- Jessica Bisson, B.F.A. (Hon.), Dipl. Heritage Conservation, Project Administrator – Cultural Heritage Division
- **Project Manager:** Lindsay Graves
- **Field Review:** Kirstyn Allam, B.A. (Hon), Advanced Dipl. Applied Museum Studies, Cultural Heritage Analyst, Technical Writer and Researcher - Cultural Heritage Division
- **Report Production:** Kirstyn Allam
- **Graphics Production:** Jonas Fernandez, M.S.c., Manager, Geomatics - Operations Division
- Carolyn Nettleton, B.A., Archaeologist, Geomatics Technician – Operations Division
- **Report Reviewer(s):** Lindsay Graves



Qualified Persons Involved in the Project

Lindsay Graves, M.A., C.A.H.P.

Senior Cultural Heritage Specialist, Assistant Manager - Cultural Heritage Division

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Glossary

Built Heritage Resource (B.H.R.)

Definition: "...a building, structure, monument, installation or any manufactured remnant that contributes to a property's cultural heritage value or interest as identified by a community, including an Indigenous community. built heritage resources are located on property that may be designated under Parts IV or V of the *Ontario Heritage Act*, or that may be included on local, provincial, federal and/or international registers" (Ministry of Municipal Affairs and Housing, 2020, p. 41).

Cultural Heritage Landscape (C.H.L.)

Definition: "...a defined geographical area that may have been modified by human activity and is identified as having cultural heritage value or interest by a community, including an Indigenous community. The area may include features such as buildings, structures, spaces, views, archaeological sites or natural elements that are valued together for their interrelationship, meaning or association. Cultural heritage landscapes may be properties that have been determined to have cultural heritage value or interest under the *Ontario Heritage Act*, or have been included on federal and/or international registers, and/or protected through official plan, zoning by-law, or other land use planning mechanisms" (Ministry of Municipal Affairs and Housing, 2020, p. 42).

Known Built Heritage Resource or Cultural Heritage Landscape

Definition: A known built heritage resource or cultural heritage landscape is a property that has recognized cultural heritage value or interest. This can include a property listed on a Municipal Heritage Register, designated under Part IV or V of the *Ontario Heritage Act*, or protected by a heritage agreement, covenant or easement, protected by the *Heritage Railway Stations Protection Act* or the *Heritage Lighthouse Protection Act*, identified as a Federal Heritage Building, or located within a U.N.E.S.C.O. World Heritage Site (Ministry of Citizenship and Multiculturalism, 2016).



Impact

Definition: Includes negative and positive, direct and indirect effects to an identified cultural heritage resource. Direct impacts include destruction of any, or part of any, significant heritage attributes or features and/or unsympathetic or incompatible alterations to an identified resource. Indirect impacts include, but are not limited to, creation of shadows, isolation of heritage attributes, direct or indirect obstruction of significant views, change in land use, land disturbances (Ministry of Citizenship and Multiculturalism, 2006b). Indirect impacts also include potential vibration impacts (See Section 2.5 for complete definition and discussion of potential impacts).

Mitigation

Definition: Mitigation is the process of lessening or negating anticipated adverse impacts to built heritage resources or cultural heritage landscapes and may include, but are not limited to, such actions as avoidance, monitoring, protection, relocation, remedial landscaping, and documentation of the cultural heritage landscape and/or built heritage resource if to be demolished or relocated (Ministry of Citizenship and Multiculturalism, 2006a).

Potential Built Heritage Resource or Cultural Heritage Landscape

Definition: A potential built heritage resource or cultural heritage landscape is a property that has the potential for cultural heritage value or interest. This can include properties/project area that contain a parcel of land that is the subject of a commemorative or interpretive plaque, is adjacent to a known burial site and/or cemetery, is in a Canadian Heritage River Watershed, or contains buildings or structures that are 40 or more years old (Ministry of Citizenship and Multiculturalism, 2016).

Significant

Definition: With regard to cultural heritage and archaeology resources, significant means “resources that have been determined to have cultural heritage value or interest. Processes and criteria for determining cultural heritage value or interest are established by the Province under the authority of the *Ontario Heritage Act*.



While some significant resources may already be identified and inventoried by official sources, the significance of others can only be determined after evaluation” (Ministry of Municipal Affairs and Housing, 2020, p. 51).

Vibration Zone of Influence

Definition: Area within a 50 metre buffer of construction-related activities in which there is potential to affect an identified cultural heritage resource. A 50 metre buffer is applied in the absence of a project-specific defined vibration zone of influence based on existing secondary source literature and direction provided from the Ministry of Heritage, Sport, Tourism and Culture Industries (Carman et al., 2012; Crispino & D’Apuzzo, 2001; P. Ellis, 1987; Rainer, 1982; Wiss, 1981). This buffer accommodates the additional threat from collisions with heavy machinery or subsidence (Randl, 2001).



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1.0 Introduction

Archaeological Services Inc. was contracted by R.J. Burnside & Associates Limited, on behalf of the County of Oxford, to conduct a Cultural Heritage Report as part of the Oxford Road 19 Corridor Improvements Municipal Class Environmental Assessment. The purpose of this report is to present an inventory of known and potential built heritage resources and cultural heritage landscapes, identify existing conditions of the project study area, provide a preliminary impact assessment, and propose appropriate mitigation measures.

1.1 Project Overview

The Oxford Road 19 Corridor Improvements Municipal Class Environmental Assessment involves proposed road improvements to Oxford Road 19 from Highway 19 (Plank Line) to the Norfolk County boundary at Windham Road 19. The project study area consists of approximately 16 kilometres of the Oxford Road 19 right-of-way from Highway 19 to the Norfolk County boundary at Windham Road 19, excluding the settlement centres of Springford and Otterville, and is generally bounded by rural and agricultural properties.

1.2 Description of Study Area

This Cultural Heritage Report will focus on the project study area with an additional 50 metre buffer (Figure 1). This study area has been defined as inclusive of those lands that may contain built heritage resources or cultural heritage landscapes that may be subject to direct or indirect impacts as a result of the proposed undertaking. Properties within the study area are located in the Township of South-West Oxford, the Township of Norwich, and the County of Norfolk.





Figure 1: Location of the study area. Base Map: ©OpenStreetMap and contributors, Creative Commons-Share Alike License (C.C.-By-S.A.)

2.0 Methodology

The following sections provide a summary of regulatory requirements and municipal and regional heritage policies that guide this cultural heritage assessment. In addition, an overview of the process undertaken to identify known and potential built heritage resources and cultural heritage landscapes is provided, along with a description of how the preliminary impact assessment will be undertaken.

2.1 Regulatory Requirements

The *Ontario Heritage Act* (O.H.A.) (Ontario Heritage Act, R.S.O. c. O.18, 1990 [as Amended in 2021], 1990) is the primary piece of legislation that determines policies, priorities and programs for the conservation of Ontario’s heritage. There are many other provincial acts, regulations and policies governing land use planning and resource development that support heritage conservation, including:

- The *Planning Act* (Planning Act, R.S.O. 1990, c. P.13, 1990), which states that “conservation of features of significant architectural, cultural, historical, archaeological or scientific interest” (cultural heritage resources) is a “matter of provincial interest”. The *Provincial Policy Statement* (Ministry of Municipal Affairs and Housing, 2020), issued under the *Planning Act*, links heritage conservation to long-term economic prosperity and requires municipalities and the Crown to conserve significant built heritage resources or cultural heritage landscapes.
- The *Environmental Assessment Act* (Environmental Assessment Act, R.S.O., 1990), which defines “environment” to include cultural conditions that influence the life of humans or a community. Cultural heritage resources, which includes archaeological resources, built heritage resources and cultural heritage landscapes, are important components of those cultural conditions.



The Ministry of Citizenship and Multiculturalism (hereafter “The Ministry”) is charged under Section 2.0 of the O.H.A. with the responsibility to determine policies, priorities, and programs for the conservation, protection, and preservation of the heritage of Ontario. The *Standards and Guidelines for Conservation of Provincial Heritage Properties* (Ministry of Citizenship and Multiculturalism, 2010) (hereinafter “*Standards and Guidelines*”) apply to properties the Government of Ontario owns or controls that have “cultural heritage value or interest” (C.H.V.I.). The *Standards and Guidelines* provide a series of guidelines that apply to provincial heritage properties in the areas of identification and evaluation; protection; maintenance; use; and disposal. For the purpose of this report, the *Standards and Guidelines* provide points of reference to aid in determining potential heritage significance in identification of built heritage resources and cultural heritage landscapes. While not directly applicable for use in properties not under provincial ownership, the *Standards and Guidelines* are regarded as best practice for guiding heritage assessments and ensure that additional identification and mitigation measures are considered.

Similarly, the *Ontario Heritage Tool Kit* (Ministry of Culture, 2006) provides a guide to evaluate heritage properties. To conserve a built heritage resource or cultural heritage landscape, the *Ontario Heritage Tool Kit* states that a municipality or approval authority may require a heritage impact assessment and/or a conservation plan to guide the approval, modification, or denial of a proposed development.



2.2 Municipal/Regional Heritage Policies

The study area is located within the Township of South-West Oxford and the Township of Norwich in the County of Oxford, and the County of Norfolk. Policies relating to cultural heritage resources were reviewed from the following sources:

- *Oxford County Official Plan* (County of Oxford, 2021)²
- *Norfolk County Official Plan* (Norfolk County, 2021)

2.3 Identification of Built Heritage Resources and Cultural Heritage Landscapes

This Cultural Heritage Report follows guidelines presented in the *Ontario Heritage Tool Kit* (Ministry of Culture, 2006) and *Criteria for Evaluating Potential for Built Heritage Resources and Cultural Heritage Landscapes* (Ministry of Citizenship and Multiculturalism, 2016). The objective of this report is to present an inventory of known and potential built heritage resources and cultural heritage landscapes, and to provide a preliminary understanding of known and potential built heritage resources and cultural heritage landscapes located within areas anticipated to be directly or indirectly impacted by the proposed project.

In the course of the cultural heritage assessment process, all potentially affected built heritage resources and cultural heritage landscapes are subject to identification and inventory. Generally, when conducting an identification of built heritage resources and cultural heritage landscapes within a study area, three stages of research and data collection are undertaken to appropriately establish the potential for and existence of built heritage resources and cultural heritage landscapes in a geographic area: background research and desktop data collection; field review; and identification.

² Note that both the Township of South-West Oxford and the Township of Norwich use the Oxford County Official Plan as their Official Plans.



Background historical research, which includes consultation of primary and secondary source research and historical mapping, is undertaken to identify early settlement patterns and broad agents or themes of change in a study area. This stage in the data collection process enables the researcher to determine the presence of sensitive heritage areas that correspond to nineteenth- and twentieth-century settlement and development patterns. To augment data collected during this stage of the research process, federal, provincial, and municipal databases and/or agencies are consulted to obtain information about specific properties that have been previously identified and/or designated as having cultural heritage value. Typically, resources identified during these stages of the research process are reflective of particular architectural styles or construction methods, associated with an important person, place, or event, and contribute to the contextual facets of a particular place, neighbourhood, or intersection.

A field review is then undertaken to confirm the location and condition of previously identified built heritage resources and cultural heritage landscapes. The field review is also used to identify potential built heritage resources and cultural heritage landscapes that have not been previously identified on federal, provincial, or municipal databases or through other appropriate agency data sources.

During the cultural heritage assessment process, a property is identified as a potential built heritage resources or cultural heritage landscape based on research, the Ministry screening tool, and professional expertise and best practice. In addition, use of a 40-year-old benchmark is a guiding principle when conducting a preliminary identification of built heritage resources and cultural heritage landscapes. While identification of a resource that is 40 years old or older does not confer outright heritage significance, this benchmark provides a means to collect information about resources that may retain heritage value. Similarly, if a resource is slightly younger than 40 years old, this does not preclude the resource from having cultural heritage value or interest.



2.4 Background Information Review

To make an identification of previously identified known or potential built heritage resources and cultural heritage landscapes within the study area, the following sections present the resources that were consulted as part of this Cultural Heritage Report.

2.4.1 Review of Existing Heritage Inventories

A number of resources were consulted in order to identify previously identified built heritage resources and cultural heritage landscapes within the study area. These resources reviewed on 4 and 5 May, 2022, include:

- The Heritage webpage for the Township of Norwich that lists the designated and historically significant properties in the Township (Township of Norwich, 2019b);
- The County of Oxford *Heritage Resources Inventory* (County of Oxford, 2006);
- The Norfolk County *Heritage & Culture Mapping* tool (County of Norfolk, 2020);
- The *Ontario Heritage Act Register* (Ontario Heritage Trust, n.d.c);
- The *Places of Worship Inventory* (Ontario Heritage Trust, n.d.d);
- The inventory of Ontario Heritage Trust easements (Ontario Heritage Trust, n.d.b);
- The Ontario Heritage Trust's *An Inventory of Provincial Plaques Across Ontario*: a PDF of Ontario Heritage Trust Plaques and their locations (Ontario Heritage Trust, 2018);
- Inventory of known cemeteries/burial sites in the Ontario Genealogical Society's online databases (Ontario Genealogical Society, n.d.);
- Canada's Historic Places website: available online, the searchable register provides information on historic places recognized for their heritage value at the local, provincial, territorial, and national levels (Parks Canada, n.d.a);
- Directory of Federal Heritage Designations: a searchable on-line database that identifies National Historic Sites, National Historic Events, National



Historic People, Heritage Railway Stations, Federal Heritage Buildings, and Heritage Lighthouses (Parks Canada, n.d.b);

- Canadian Heritage River System: a national river conservation program that promotes, protects and enhances the best examples of Canada’s river heritage (Canadian Heritage Rivers Board and Technical Planning Committee, n.d.); and,
- United Nations Educational, Scientific and Cultural Organization (U.N.E.S.C.O.) World Heritage Sites (U.N.E.S.C.O. World Heritage Centre, n.d.).

2.4.2 Review of Previous Heritage Reporting

No additional cultural heritage studies are known to have been undertaken within the study area and so none were reviewed as part of this assessment.

2.4.3 Community Information Gathering

The following individuals, groups, and/or organizations were contacted to gather information on known and potential built heritage resources and cultural heritage landscapes, active and inactive cemeteries, and areas of identified Indigenous interest within the study area:

- Julie Middleton, Clerk, Township of South-West Oxford (email communication 6 May 2022). Email correspondence confirmed the previously identified cultural heritage resources and provided information about one additional property in Mount Elgin. However, this property was outside of the study area.
- Lizz Birchall, Administrative/Clerk Services, Township of Norwich (email communication 6 and 9 May 2022). Staff recommended contacting the Museum and/or Archives to confirm the previously identified cultural heritage resources.
- Janet Hilliker, Archivist, Norwich and District Museum and Archives (email communication 16, 19, and 26 May 2022). Email sent to confirm the



previously identified heritage resources (as per direction from Lizz Birchall with the Township of Norwich) and whether any listing or research reports have been conducted on the properties. The Archivist provided information on some of the properties and indicated that a member of the local Heritage Committee would review the list of properties. That response is still outstanding.

- Melissa Coliver, Director, Heritage and Culture, Norfolk County (email communication 6, 12, and 13 May 2022). Email correspondence confirmed that there are no previously identified cultural heritage resources or concerns regarding the study area.
- The Ministry (email communication 6 and 9 May 2022). Email correspondence confirmed that there are no properties designated by the Minister and that there are no known Provincial Heritage Properties within the study area.
- The Ontario Heritage Trust (email communication 6 May 2022). A request to confirm that there are no conservation easements or Trust-owned properties within the study area was sent to the Ontario Heritage Trust. A response was still outstanding at the time of report submission.
- At project start-up in late April 2022, Archaeological Services Inc. (A.S.I.) made a request to the proponent that any engagement with Indigenous communities undertaken as part of this project include a discussion about known or potential built heritage resources or cultural heritage landscapes that are of interest to the respective communities. No feedback was received by the time of report submission.



2.5 Preliminary Impact Assessment Methodology

To assess the potential impacts of the undertaking, identified built heritage resources and cultural heritage landscapes are considered against a range of possible negative impacts, based on the *Ontario Heritage Tool Kit InfoSheet #5: Heritage Impact Assessments and Conservation Plans* (Ministry of Citizenship and Multiculturalism, 2006b). These include:

Direct impacts:

- Destruction of any, or part of any, significant heritage attributes or features; and
- Alteration that is not sympathetic, or is incompatible, with the historic fabric and appearance.

Indirect impacts:

- Shadows created that alter the appearance of a heritage attribute or change the viability of a natural feature or plantings, such as a garden;
- Isolation of a heritage attribute from its surrounding environment, context or a significant relationship;
- Direct or indirect obstruction of significant views or vistas within, from, or of built and natural features;
- A change in land use such as rezoning a battlefield from open space to residential use, allowing new development or site alteration to fill in the formerly open spaces; and
- Land disturbances such as a change in grade that alters soils, and drainage patterns that adversely affect an archaeological resource.

Indirect impacts from construction-related vibration have the potential to negatively affect built heritage resources and cultural heritage landscapes depending on the type of construction methods and machinery selected for the project and proximity and composition of the identified resources. Potential



vibration impacts are defined as having potential to affect an identified built heritage resources and cultural heritage landscapes where work is taking place within 50 metres of features on the property. A 50 metre buffer is applied in the absence of a project-specific defined vibration zone of influence based on existing secondary source literature and direction provided from the Ministry (Carman et al., 2012; Crispino & D'Apuzzo, 2001; P. Ellis, 1987; Rainer, 1982; Wiss, 1981). This buffer accommodates any additional or potential threat from collisions with heavy machinery or subsidence (Randl, 2001).

Several additional factors are also considered when evaluating potential impacts on identified built heritage resources and cultural heritage landscapes. These are outlined in a document set out by the Ministry of Culture and Communications (now Ministry of Citizenship and Multiculturalism) and the Ministry of the Environment entitled *Guideline for Preparing the Cultural Heritage Resource Component of Environmental Assessments* (1992). While this document has largely been superseded in some respects by more current policies and legislation, the guidance provided that continues to be of relevance to this specific project includes the following definitions:

- Magnitude: the amount of physical alteration or destruction which can be expected;
- Severity: the irreversibility or reversibility of an impact;
- Duration: the length of time an adverse impact persists;
- Frequency: the number of times an impact can be expected;
- Range: the spatial distribution, widespread or site specific, of an adverse impact; and
- Diversity: the number of different kinds of activities to affect a heritage resource.

The proposed undertaking should endeavor to avoid adversely affecting known and potential built heritage resources and cultural heritage landscapes and interventions should be managed in such a way that identified significant cultural heritage resources are conserved. When the nature of the undertaking is such



that adverse impacts are unavoidable, it may be necessary to implement alternative approaches or mitigation strategies that alleviate the negative effects on identified built heritage resources and cultural heritage landscapes. Mitigation is the process of lessening or negating anticipated adverse impacts to cultural heritage resources and may include, but are not limited to, such actions as avoidance, monitoring, protection, relocation, remedial landscaping, and documentation of the built heritage resource or cultural heritage landscape if to be demolished or relocated.

Various works associated with infrastructure improvements have the potential to affect built heritage resources and cultural heritage landscapes in a variety of ways, and as such, appropriate mitigation measures for the undertaking need to be considered.

3.0 Summary of Historical Development Within the Study Area

This section provides a brief summary of historical research. A review of available primary and secondary source material was undertaken to produce a contextual overview of the study area, including a general description of physiography, Indigenous land use, and Euro-Canadian settlement.

3.1 Physiography

The study area is situated within the spillways and undrumlined till plains of the Mount Elgin Ridges physiographic region, the sand plains of the Norfolk Sand Plain physiographic region, and the till moraines of the Horseshoe Moraines physiographic region of southern Ontario.

The study area is within a spillway of the Mount Elgin Ridges physiographic region of southern Ontario. The region is made up of a series of ridges and vales between the Thames Valley and the Norfolk sand plain, and covers the south-eastern part of Middlesex, southern Oxford, and part of Elgin and Brant Counties.



The ridges are well drained moraines of calcareous clay or silty clay till, whereas the vales are within glacial spillways of alluvium with imperfect to poor drainage in the hollows. The region has many kettle lakes, such as Mud Lake, Walker Pond, and Whittaker Lake, as well as large swamps, with waterways draining into the Thames River or Lake Erie (L. J. Chapman & Putnam, 1984, pp. 144–146).

The Norfolk Sand Plain physiographic region is a wedge-shaped feature that extends from the Lake Erie shoreline and tapers northward to a point in Brantford on the Grand River (Chapman and Putnam 1984:153-154). The region encompasses an area of 3,134 square kilometres and consists of sands and silts that were deposited as a delta in glacial Lakes Whittlesey and Warren. A massive discharge of meltwater from the Grand River area entered the lakes between the ice front and the moraines to the northwest, building the delta from west to east as the glacier withdrew, thus covering most of the area west of the Galt Moraine with sand. In the vicinity of the subject property, glaciolactustrine deep water sediments belonging to mainly glacial Lake Warren and younger deposits and consisting of stratified to varved silt and clay, minor sand, are overlain by veneer of sand (Zone 10) (Cowan 1972: Map 2240).

The Horseshoe Moraine physiographic region of southern Ontario forms the core of a horse-shoe shaped area flanking the upland that lies to the west of the highest part of the Niagara cuesta (Chapman and Putnam 1984:127-129). The southwestern limb of the region, located in the southern part of Huron County, has a fairly simple landscape consisting of morainic ridges composed of pale brown, hard calcareous, fine-textured till, with a moderate degree of stoniness. Huron clay loam is the most representative soil type, and it occurs widely throughout the region.

Big Otter Creek passes through the village of Otterville which is located between the eastern-most study area and the middle study area. Big Otter Creek is within the Big Otter Creek watershed, which draining 712 square kilometres on the north shore of Lake Erie in southwestern Ontario. This watershed is covered by a substantial agricultural land base, approximately 74 percent. The remainder is 14



percent treed land, seven percent wetland, and five percent urban land (Grand River Conservation Authority, 2011).

Spittler Creek intersects the study area west of James Street, west of Middletown Line, and at several points between Plank Line and West Street. Spittler Creek joins Big Otter Creek south of Otterville and drains an area of 116 kilometres squared. Spittler Creek is a subwatershed of Big Otter Creek. Plumb Creek also intersects the study area west of Springford.

Thus, the study area's physiography, soil type, and location in the vicinity of the Big Otter Creek, Spittler Creek, and Plumb Creek influenced early settlement and its eventual transformation into agricultural lands.

3.2 Indigenous Land Use and Settlement

Southern Ontario has been occupied by human populations since the retreat of the Laurentide glacier approximately 13,000 years ago, or 11,000 Before the Common Era (B.C.E.) (Ferris, 2013).³ During the Paleo period (c. 11,000 B.C.E. to 9,000 B.C.E.), groups tended to be small, nomadic, and non-stratified. The population relied on hunting, fishing, and gathering for sustenance, though their lives went far beyond subsistence strategies to include cultural practices including but not limited to art and astronomy. Fluted points, beaked scrapers, and gravers are among the most important artifacts to have been found at various sites throughout southern Ontario, and particularly along the shorelines of former glacial lakes. Given the low regional population levels at this time, evidence concerning Paleo-Indian period groups is very limited (C. J. Ellis & Deller, 1990).

Moving into the Archaic period (circa 9,000 B.C.E. to 1,000 B.C.E.), many of the same roles and responsibilities continued as they had for millennia, with groups

³ While many types of information can inform the precontact settlement of Ontario, such as oral traditions and histories, this summary provides information drawn from archaeological research conducted in southern Ontario over the last century.



generally remaining small, nomadic, and non-hierarchical. The seasons dictated the size of groups (with a general tendency to congregate in the spring/summer and disperse in the fall/winter), as well as their various sustenance activities, including fishing, foraging, trapping, and food storage and preparation. There were extensive trade networks which involved the exchange of both raw materials and finished objects such as polished or ground stone tools, beads, and notched or stemmed projectile points. Furthermore, mortuary ceremonialism was evident, meaning that there were burial practices and traditions associated with a group member's death (C. J. Ellis et al., 2009; C. J. Ellis & Deller, 1990).

The Woodland period (circa 1,000 B.C.E. to 1650 C.E.) saw several trends and aspects of life remain consistent with previous generations. Among the more notable changes, however, was the introduction of pottery, the establishment of larger occupations and territorial settlements, incipient horticulture, more stratified societies, and more elaborate burials. Later in this period, settlement patterns, foods, and the socio-political system continued to change. A major shift to agriculture occurred in some regions, and the ability to grow vegetables and legumes such as corn, beans, and squash ensured long-term settlement occupation and less dependence upon hunting and fishing. This development contributed to population growth as well as the emergence of permanent villages and special purpose sites supporting those villages. Furthermore, the socio-political system shifted from one which was strongly kinship based to one that involved tribal differentiation as well as political alliances across and between regions (Birch et al., 2021; Dodd et al., 1990; C. J. Ellis & Deller, 1990; Williamson, 1990).

The arrival of European trade goods in the sixteenth century, Europeans themselves in the seventeenth century, and increasing settlement efforts in the eighteenth century all significantly impacted traditional ways of life in Southern Ontario. Over time, war and disease contributed to death, dispersion, and displacement of many Indigenous peoples across the region. The Euro-Canadian population grew in both numbers and power through the eighteenth and



nineteenth centuries and treaties between colonial administrators and First Nations representatives began to be negotiated.

The study area is within Treaty 3, the Between the Lakes Purchase. Following the 1764 Niagara Peace Treaty and the follow-up treaties with Pontiac, the English colonial government considered the Mississaugas to be their allies since they had accepted the Covenant Chain. The English administrators followed the terms of the Royal Proclamation and insured that no settlements were made in the hunting grounds that had been reserved for their use (Johnston, 1964; Lytwyn, 2005). In 1784, under the terms of the “Between the Lakes Purchase” signed by Sir Frederick Haldimand and the Mississaugas, the Crown acquired over one million acres of land in-part spanning westward from near modern day Niagara-on-the-Lake along the south shore of Lake Ontario to modern day Burlington (Aboriginal Affairs and Northern Development Canada, 2016).

3.3 Historical Euro-Canadian Township Survey and Settlement

The first Europeans to arrive in the area were transient merchants and traders from France and England, who followed Indigenous pathways and set up trading posts at strategic locations along the well-traveled river routes. All of these occupations occurred at sites that afforded both natural landfalls and convenient access, by means of the various waterways and overland trails, into the hinterlands. Early transportation routes followed existing Indigenous trails that typically followed the highlands adjacent to various creeks and rivers (ASI 2006). Early European settlements occupied similar locations as Indigenous settlements as they were generally accessible by trail or water routes and would have been in locations with good soil and suitable topography to ensure adequate drainage.

Historically, the study area is located in the former Dereham Township, County of Oxford in Lots 1-8, Concessions 8-9; in the former Norwich Township, County of Oxford in Lots 1-9, 13-20, and 22 – 28, Concessions 8-9, and follows the



concession line between Concessions 8-9 in the Gore also within the former Norwich Township.

3.3.1 Dereham Township

Dereham Township was first formed as part of the County of Norfolk in 1792 but was switched to Oxford County in 1798. Parts of the township were first surveyed in 1799, and then another portion in 1810, with lots and concessions being finished in 1821. A re-survey was conducted in 1822 or 1832 (Shenston, 1852). When George Tillson constructed an iron forge and began the settlement of what is now Tillsonburg in 1825, there were 16 settlers in the entire township. However, the township grew steadily and by 1830, there were 329 residents. That number grew to 776 by 1840. That number jumped significantly over the following two decades. In 1850, the township – which consisted of 67,200 acres – had a population of 2,839 residents and that grew to 3,644 by 1852. Of particular note is that Dereham had 101 black settlers, the overwhelming majority of the 123 total black settlers in all of Oxford County at this time (Shenston, 1852).

Most of the township's residents were involved in agriculture in the nineteenth century, with wheat and oats being the primary crops. A mix of sheep, horses, cows, and pigs were also found scattered throughout the township. Some industry was also taking place. By 1852, the township had 12 sawmills, and lumber was sent down the Otter Creek for export, especially to the United States. There were also two operational grist mills, a carding and fulling mill, and a tannery in the township at this time (Shenston, 1852). Dereham soon became famous for cheese production and is claimed to be the birthplace of commercial cheese making in Canada, exemplified by the township showing a giant block of cheese weighing 7,300 pounds at the New York State Fair of 1866.

The most prominent roadway in the mid-nineteenth century was the Ingersoll and Port Burwell Plank and Gravel road, which ran the length of the whole township (Shenston, 1852).



The Township was annexed into part of the Township of South-West Oxford and the Town of Tillsonburg in 1975 (Mika & Mika, 1977, pp. 543–544).

3.3.2 Ostrander

The western limit of the study area begins at Ostrander. Ostrander is a hamlet at the intersection of Ostrander Road and Plank Line. The 1857 Tremaine’s map shows an H. Ostrander owned Lot 7, Concession 9, located in the southeast corner of what would become the hamlet of Ostrander (Tremaine, 1857). The 1876 Illustrated Atlas of the County of Oxford depicts the community of Ostrander, and H. B. Ostrander as owner of Lot 7, Concession 9 (Walker and Miles, 1876). The 1891 Canada Census has a record of a Henry Ostrander in Dereham Township (Library and Archives Canada, 1891, p. 13). According to the record, Henry was 74 years of age and was married to a Margret A, age 69. They had two children, William age 26 and Anna age 25. The occupation of Henry and William was listed as farmer.

3.3.3 Norwich Township

Norwich Township was originally part of Norfolk County in the territorial division made by Governor Simcoe in 1792. It was attached to the County of Oxford in 1798. The first survey was conducted by a Mr. Hamley in 1809. It was not until 1821 that the gore of land on the east side of Norwich became part of Norwich Township, though that area was not surveyed until 1835 (Shenston, 1852). Settlement was started by Quakers Peter Lossing and Peter De Long, immigrants from New York state, circa 1810 (Stagg, 1987). Farming was the dominant occupation in the nineteenth century. Yet, as early as 1818, the township had one store, one grist mill, and three sawmills. By that same year, the Religious Society of Friends, or Quakers, had established two meeting houses and there was also a school. Several sawmills were operational along the township’s principal river, Otter Creek, by the 1820s. The township’s population rose from 699 in 1820 to 1,215 by 1830. It continued to grow steadily thereafter, reaching 2,419 by 1840



and 4,483 by 1850 (Shenston, 1852). In 1855, the township was divided into North and South halves (Norwich & District Historical Society, 2013).

Agriculture remained the dominant industry in the nineteenth century. By the 1850s, over 25,000 acres of land were under cultivation. Yet other forms of industry were emerging in this decade as well. Most notably, there were twelve sawmills which were capable of sawing over four million feet of lumber annually (Shenston, 1852). The leading village in the township during the first half of the nineteenth century was Otterville, which was settled as early as 1807 when grants were given for the erection of mills. In 1852, Otterville was described as a “beautiful, clean, and flourishing village” with a population of 250. Other communities in the township during the nineteenth century included Norwichville (the town of Norwich), Erbtown, Springford, Cornell, and Hawtrey (Shenston, 1852).

Agriculture continued to dominate the area in the twentieth century, with tobacco being the dominant cash crop. The present boundaries of Norwich Township were established in 1975 when the former village of Norwich amalgamated with the townships of East Oxford, North Norwich, and South Norwich (Township of Norwich, 2019a).

3.3.4 Springford

The study area borders the village of Springford. The earliest residents in the Springford area settled in the early nineteenth century and include Joseph Spitler, John Philips, and John Fox. The village of Springford’s first settler was the Loyalist Josiah Gilbert, and other early residents included Squire Ebenezer Healy, Amos Scott, Amos Palmer, and Willet Post. Healy was the first town warden, taking up the position in 1830. These early residents were largely self-sufficient, having to travel to Otterville for the nearest mill and store. By the mid-nineteenth century, though, Springford began to develop community infrastructure and a commercial base, including sawmills, a blacksmith shop, an inn, and a store. By 1857, the community had a wagon shop, Baptist and Congregationalist Churches, a school,



stores, post office, a hotel, tannery, a cabinet-making shop, a tailoring business, and a shoemaker in addition to multiple residences. During the latter half of the nineteenth century, especially following the arrival of the Brantford, Norwich, and Port Burwell Railway through the community in 1875, other industries and/or cultural buildings were added, including lumbering, a cheese factory, shoemaking, cooperage, hotels, a Methodist Church, a Temperance Hall, a train station, a doctor, a tin smithing business, a brick and tile operation, all of which added to the village's prosperity. However, by the 1880s and 1890s, the village was in decline, with factories further away making goods more cheaply than the local factories (Springford Women's Institute, 2000; Walker and Miles, 1876).

While the community was smaller, there were significant developments in the twentieth century. A Women's Institute was formed in 1903 and Mrs. F.W. Vardon was elected the first president. A Town Hall had been erected in the late nineteenth century, and the Women's Institute took ownership of the it in 1928 for their various functions. While the former Town Hall ultimately closed in 1988, a new community hall was built later that same year, and on its grounds was a plaque dedicated to The Establishment of Free Rural Mail Delivery, which was instigated by the community's most famous citizen, George Wilcox. A water system was installed in 1924. Over time, the Springford Area Athletic Club was formed, an Agricultural Society also was created, and a baseball park was built in 1969. The railway through Springford stopped providing passenger service in 1954, and the line was discontinued altogether in 1980 (Springford Women's Institute, 2000).

3.3.5 Erbtown

The study area passes through the small community of Erbtown, which was founded around 1850 just west of Otterville. The settlement started when Abraham Erb and his four brothers came to operate a sawmill and woolen mill. According to the 1857 Tremaine's map, Sam Erb owned Lots 14-15, Concession 9, and part of Lot 14, Concession 8 (Tremaine, 1857). Jos Erb owned Lot 16, Concession 9. Samuel Erb had emigrated from Pennsylvania to Upper Canada at



some point between his birth in 1792 and his marriage to Mary Lewis of Wentworth Country in 1816. The 1876 Illustrated Atlas of the County of Oxford (Walker and Miles, 1876) shows Erbtown to the west of Otterville, on parts of Lots 13-14, Concession 9, and part of Lot 14, Concession 8. An Abraham Erb owned part of Lot 14, Concessions 8-9. He was married to Mariah Quickfall and they engaged in farming (Lythgoe, 2022). A Jared Erb owned part of Lot 15, Concession 9. The Erb family donated land for an Episcopal Methodist church and cemetery prior to 1861, and Abraham built the church (County of Oxford, 2018a). However, besides the church, the community was not well-established enough to warrant a school, post office, or any other cultural institution by the 1870s, and was not commented upon in the 1876 Illustrated Atlas (Walker and Miles, 1876).

3.3.6 Otterville

Between the middle portion of the study area and the eastern-most portion of the study area is the village of Otterville. Settlement of Otterville began in 1907 on Big Otter Creek when a government grant and machinery was provided to John Earls and Paul Avery for the erection of grist and sawmills (Mika & Mika, 1983, p. 148). The first store and tavern were built in 1816. A tannery and shoe and harness building were started in 1833. A post office opened in 1837. A foundry was built and saw- and woolen mills erected during the 1850s. Twelve steam and fourteen water mills, the A. Parsons Carriage Works, the John Furlong Shingle and Cooperage Factory, and the J.J. Warner Match Factory began during the 1850s. A private bank was opened in 1879, followed by a Trader's Bank within the post office building. St. John's Anglican Church was built in 1854, the Otterville New Conexion Methodist Church was built in 1862 with renovations completed in 1918, and the Baptist Church was built in 1865. The first school was built in the 1830s. A two-storey school was built in 1857, followed by a continuation school in 1924. A continuation and public school was constructed in 1947 which became a central public school in 1947. Otterville became part of Norwich Township on January 1, 1975 (Mika & Mika, 1983, p. 148).



3.3.7 Black History in Oxford County

The institution of slavery existed in Canada and after the end of the American War of Independence (1783) Loyalists who left America that were also enslavers were incentivised by the 1790 *Imperial Act* to bring their slaves duty-free to Canada, resulting in around 3000 enslaved Black people being brought across the border, between 500 and 700 of which were in Upper Canada (Henry, 2022). In 1793, the *Act to Limit Slavery in Upper Canada* was passed to restrict slaves being brought into Canada, though it did not prevent sales of slaves between provinces or to America. Then in 1807 Britain passed the *Abolition of the Slave Trade Act* in an attempt to suppress slave trading throughout its empire. In 1834 the *Slavery Abolition Act* took effect, abolishing slavery in the British colonies of the Caribbean, South America, and North America, emancipating over 780,000 enslaved persons (Rennalls, 2021). The abolition of slavery in the United States came in 1863 with the Emancipation Proclamation and the 1865 Thirteenth Amendment to the United States Constitution.

The *Fugitive Slave Laws* (1793, 1850) passed by the United States Congress criminalized the escape from bondage in America, resulting in many formerly enslaved people – and some Black people who may never have been enslaved – being captured and returned to enslavers in America. The Underground Railroad was one of the systems of Black resistance, which was organized with and for freedom seekers in the United States, assisting in their escape typically to either northern states or to Canada and was started in the early nineteenth-century. The Underground Railroad was commonly assisted by some members of the Society of Friends (otherwise known as Quakers) (Pettigrew, 2006). In Oxford County, Ingersoll was a terminus for the Underground Railroad with one of the largest populations of freedom seekers settling there; however, many other Black communities formed throughout the county such as Otterville (Pettigrew, 2006; Rennalls, 2001).

It is reported that as early as 1829, free Black people had settled in Norwich Township (Pettigrew, 2006, p. 36). Frederick Stover, a Quaker and land agent,



assisted in settling many free Black people from Cincinnati and New York State who had the means to purchase property (Rennalls, 2001). The first registered Black landowner in Norwich Township was Samuel Jones in 1833. By 1842, the School Section Number 18 school had been established along what is now Middletown Line in the north half of Lot 15, Concession 7 near the largest group of Black settlers (Pettigrew, 2006). The land the school sat on was property owned by another Black settler, Charles Joiner who also ran a sawmill. The school disappeared in the 1850s when Norwich Township divided into North and South Norwich (Rennalls, 2001). In 1853, it was reported that 26 Black families lived in Norwich Township (Pettigrew, 2006, p. 45). Dereham Township had 101 Black settlers, the overwhelming majority of the 123 total Black settlers in all of Oxford County at this time (Shenston, 1852). In 1861 there were 51 Black people counted in the census for Dereham Township, however the communities did not seem to stay in those areas and through to 1901 the Black population remained around only 25 (Pettigrew, 2006, pp. 105–106).

Otterville was another important Black community. Land had been purchased in 1856 and five years later an African Methodist Episcopal Church and cemetery were built, and due to the Fugitive Slave Act, changed its name to the British Methodist Episcopal Church (Pettigrew, 2006, p. 47). Large “bush meetings” or camp meetings were held at this church and many people, both Black and White would attend. These meetings would last for several days and those in attendance would travel far to be there. Although the church no longer stands and many of the gravestones are gone, a plaque and cairn now commemorate the location (Rennalls, 2001). The property of the former church and the cemetery is located approximately 620 metres north of Oxford Road 19 on the west side of Church Street/Pick Line. Black families also attended a variety of churches and other congregations in the area.

During the 1880’s the number of Black settlers began to decline as the supply of white pine was depleted and the number of lumber mills in operation dwindled. The Black population went from 165 for the original area of Norwich Township in 1861 to only 27 in 1901. As agriculture on the land became more difficult in the



early-twentieth century and made more challenging by the Great Depression, much of the Black population (along with many other groups of people) moved on from the smaller communities to larger towns and cities such as Ingersoll, Hamilton, and Toronto (Oxford County Archives, 2020; Pettigrew, 2006).

3.3.8 Tillsonburg, Lake Erie and Pacific Railway

Intersecting with the western-most portion of the study area is the Ontario Southland Railway. Originally the Tillsonburg, Lake Erie and Pacific Railroad, the line was constructed beginning in 1895 to connect Port Burwell on Lake Erie with the Grand Trunk Railway Mainline in Ingersoll, approximately 33 miles to the north. The Canadian Pacific Railroad (C.P.) leased the line beginning in 1904 and ceased operations in 1987. In 1998 the line was purchased by the Ontario Southland Railway (O.S.R.) which continues to operate the railroad (Kennedy, 2019).

3.3.9 Brantford, Norwich and Port Burwell Railway

Transecting the study area at Springford is the alignment of the former Brantford, Norwich and Port Burwell Railway. The Norfolk Railway incorporated in 1869 with the intention to build a line from Lake Erie to the Great Western Railway (G.W.R) in Paris, Ontario, but these plans stalled and construction never commenced. In 1874, the company, now The Brantford, Norfolk and Port Burwell Railway (B.N.P.B.R.) was authorized to build from Lake Erie, through Tillsonburg, to connect to the G.W.R. in Brantford. Construction began two years later, and, though intended to reach the great lakes, due to mismanagement and delays the company struggled to finish the 34 miles of track between Brantford and Tillsonburg. Passenger service began, depositing passengers at a station in West Brantford while the company waited on the completion of a bridge over the Grand River. On January 1, 1877, The B.N.P.B.R. was leased to the Canada Southern Railway (C.S.R.), owned by the wealthy and powerful Vanderbilt family. Around this time, the Grand River bridge was completed allowing passengers to disembark, despite the lack of a station, at Market Street in downtown Brantford. However, just four months following the agreement, the company's board were



convinced to revoke the lease with the C.S.R by the G.W.R. who agreed to lease the line in perpetuity and to allow trains to travel an additional mile on their tracks to the Colborne Street Station. In leasing the B.N.P.B.R., the G.W.R. created a loop line from Brantford to their Canada Air Line Railway at Tillsonburg Junction. In 1883 the G.W.R. amalgamated with the Grand Trunk Railway which operated the line until its purchase by the Canadian National Railway (C.N.R.) in 1923. The C.N.R. abandoned the line in sections from 1978-1988 leaving only a small 1-mile spur in operation (Brantford Public Library, n.d.; D. N. M. Smith, n.d.; J. Smith, 2017).

3.3.10 Port Dover and Lake Huron Railway

Transecting the study area is the former alignment of the Port Dover and Lake Huron Railway (P.D.L.H.R). The P.D.L.H.R was incorporated in 1872 to build a rail line from Port Dover to Stratford. To save costs, they were granted permission to acquire the roadbeds and holdings of the former Woodstock and Lake Erie Railway and Harbour Company whose own efforts to build in the region had been halted when an enquiry into their business affairs found evidence of reckless spending, bad faith investments, and bribery. The P.D.L.H.R line from Port Dover to Woodstock opened in 1875 with a further extension to Stratford the following year. The railway company purchased Port Dover Harbour from the federal government in 1877 and in that same year, amalgamated with the Stratford and Huron Railway to form the Port Dover and Lake Huron Railway. In 1881, they amalgamated with the Stratford and Huron Railway and the Georgian Bay and Lake Erie Railway to form a new subsidiary of the Grand Trunk Railway (G.T.R.) called the Grand Trunk, Georgian Bay and Lake Erie Railway which was eventually completely absorbed into the G.T.R. in 1893 (Cooper, 2017a; Delamere, n.d.). In 1923, in an effort to nationalize the railway system, the federal government acquired the G.T.R. following the acquisition of several other major rail companies, creating the Canadian National Railway System (Cooper, 2017b).



3.4 Review of Historical Mapping

The 1857 *Map of the County of Oxford* (Tremaine, 1857) and the 1876 *Illustrated Historical Atlas of the County of Oxford* (Walker and Miles, 1876), were examined to determine the presence of historical features within the study area during the nineteenth century (Figure 2 and Figure 3). Historically, the study area is located in Lot 1-8, Concessions 8-9 in the former Dereham Township; Lots 1-9, 13-20, and 22 – 28, Concessions 8-9 in the former Norwich Township, County of Oxford, and follows the concession line between Concessions 8-9 in the Gore.

It should be noted, however, that not all features of interest were mapped systematically in the Ontario series of historical atlases. For instance, they were often financed by subscription limiting the level of detail provided on the maps. Moreover, not every feature of interest would have been within the scope of the atlases. The use of historical map sources to reconstruct or predict the location of former features within the modern landscape generally begins by using common reference points between the various sources. The historical maps are geo-referenced to provide the most accurate determination of the location of any property on a modern map. The results of this exercise can often be imprecise or even contradictory, as there are numerous potential sources of error inherent in such a process, including differences of scale and resolution, and distortions introduced by reproduction of the sources.

Nineteenth-century mapping shows the study area within a rural, agricultural setting (Figure 2 and Figure 3). The 1857 map (Figure 2) depicts present-day County Road 19, Plank Line, Cranberry Line/Tillson Avenue, Oxford Road 13, Middletown Line, Highway 59, and Base Line following their current alignments, indicating that these are historically surveyed roads. The community of Springford is labelled, with a small grid of three roads to the south. The larger community of Otterville is shown to be developed east of Otter Creek. Spittler Creek and its tributaries are depicted intersecting the study area in several locations. Plank Line and a portion of Oxford Road 19 from Springford to Otterville is labelled as a gravel road. One steam sawmill is located west of Otterville and another to the



east. The Woodstock and Lake Erie Railway and Harbour Company rail line is illustrated just west of the study area, east of Otterville. Later nineteenth-century mapping (Figure 3) shows that Ostrander is now labelled at the west end of the study area at the intersection of County Road 19 and Plank Line and the small community of Erbtown is just west of Otterville. By this time, the Brantford, Norwich and Port Burwell Railway has been constructed just west of Springford and the former Woodstock and Lake Erie Railway and Harbour Company is now the Port Dover and Lake Huron Railway. A sawmill is depicted north of Oxford Road 19 and east of Spittler Creek between Plank Line and Cranberry Line. A church is depicted south of Oxford Road 19, between Cranberry Line and Zenda Line. A schoolhouse is depicted south of Oxford Road 19 and west of Base Line.

In addition to nineteenth-century mapping, historical topographic mapping and aerial photographs from the twentieth century were examined. This report presents maps and aerial photographs from 1909, 1954, 1970 and 1999 (Figure 4 to Figure 7).

The study area continues to express rural or agricultural land use into the twentieth century. The 1909 topographic map (Figure 4) shows that the Brantford, Norwich and Port Burwell Railway and the Port Dover and Lake Huron Railway have become part of the Grand Trunk Railway. A third line, the Canadian Pacific Railway, intersects the study area east of Plank Line. A railway station is located along the line just north of Oxford Road 19. Several bridges carry Oxford Road 19 over Spittler Creek, some have the construction material labelled as wood. The alignment of Oxford Road 19 has changed slightly by this time, part of Oxford Road 19 curves north around Spittler Creek before straightening again. Portions of the Oxford Road 19 roadway are illustrated as metalled while others are unmetalled. Cranberry Line, Zenda Line, and Plank Line north of Oxford Road 19 are also illustrated as metalled while the remainder of the roads intersecting with Oxford Road 19 are unmetalled. A church, post office, and sawmill are shown at the intersection of Plank Line and Oxford Road 19. The church depicted south of Oxford Road 19 between Cranberry Line and Zenda Line is now shown to have a cemetery, behind the church farther south from the study area. A church is



depicted north of Oxford Road 19 west of Oxford Road 13, and a second church is depicted at the southeast corner of the Oxford Road 19 and Oxford Road 13 intersection. A mill is shown northeast of the Oxford Road 19 and Oxford Road 13 intersection along Spittler Creek. West of Middletown Line is a branch of Spittler Creek, and directly to its east is a cemetery. Another cemetery is located between Middletown Line and Church Street, south of Oxford Road 19. A railway station is now located along the Grand Trunk Railway line that is east of Otterville. A school is located south of Oxford Road 19 to the west of Base Line. Wooden houses (as indicated by black squares) and stone or brick houses (as indicated by red squares) are depicted along Oxford Road 19. Otter Creek is shown as a wider channel within Otterville, which has expanded west. Erbtown is no longer labelled on the mapping.

The 1954 aerial photograph (Figure 5) depicts the study area along a road corridor with the continued agricultural uses of the land surrounding the study area. The study area passes through some treed areas along with the rural communities of Springford and Otterville. The former Brantford, Norwich and Port Burwell Railway is now labelled as Canadian National Railway and the former Port Dover and Lake Huron Railway is no longer in use. The watercourses continue to intersect with the study area. The late-twentieth century mapping (Figure 6 and Figure 7) show that overall, the study area remained in a largely agricultural context. There has been some growth in the size of the communities of Ostrander, Springford, and Otterville and an increased presence of structures along Oxford Road, notably the number of kilns and greenhouses on the 1970 mapping (Figure 6). The Canadian Pacific Railway had abandoned the line west of Springford by the time of the 1999 map (Figure 7).





Figure 2: The study area overlaid on the 1857 *Tremaine's Map of the County of Oxford*. Base Map: (Tremaine, 1857)



Figure 3: The study area overlaid on the 1876 *Illustrated Historical Atlas of the County of Oxford*. Base Map: (Walker and Miles, 1876)



Figure 4: The study area overlaid on the 1909 topographic map of Tillsonburg. Base Map: Tillsonburg Sheet (Department of Militia and Defence, 1909)

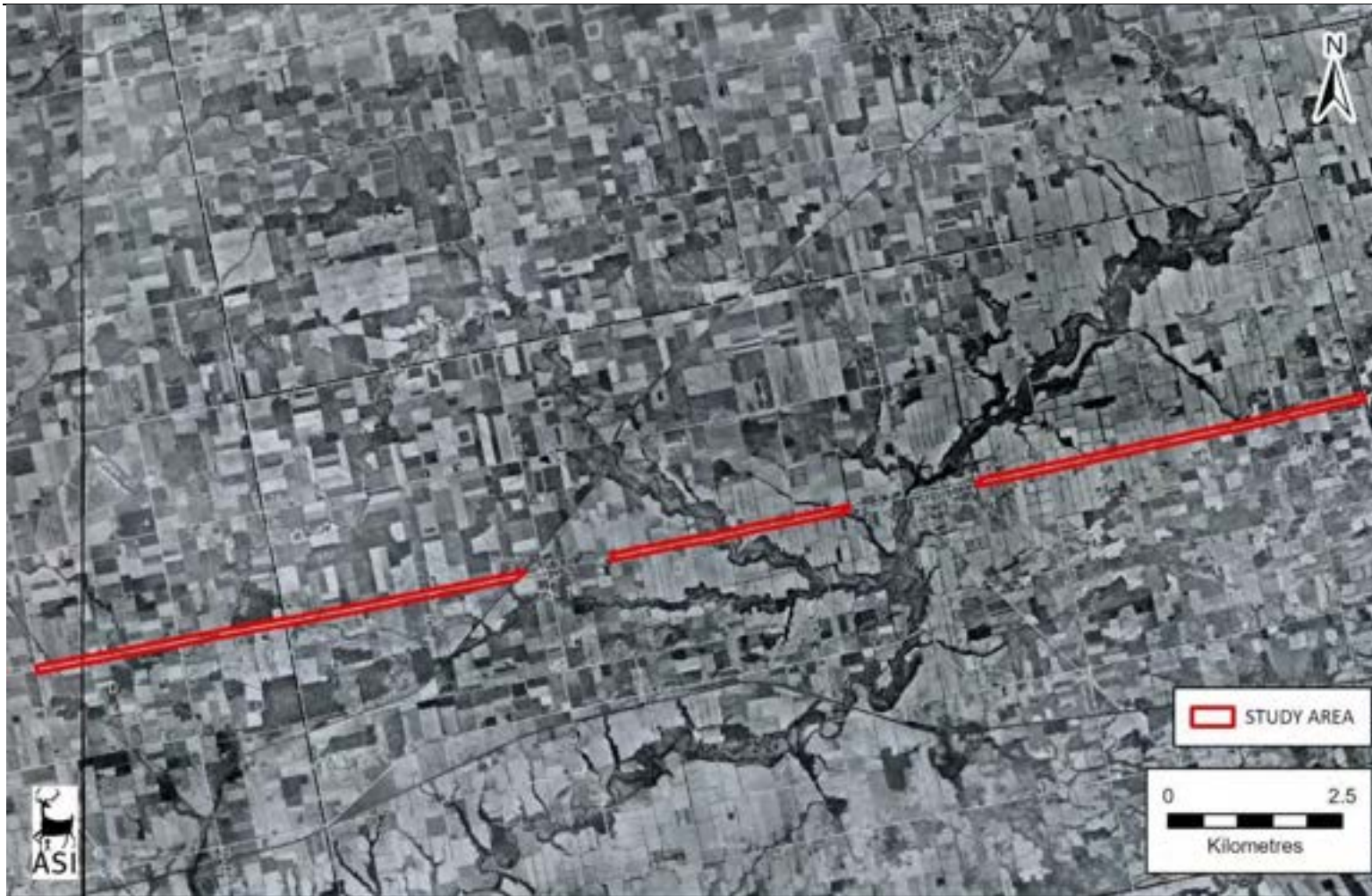


Figure 5: The study area overlaid on the 1954 aerial imagery of Oxford County and Norfolk County.
Base Map: (Hunting Survey Corporation Limited, 1954)

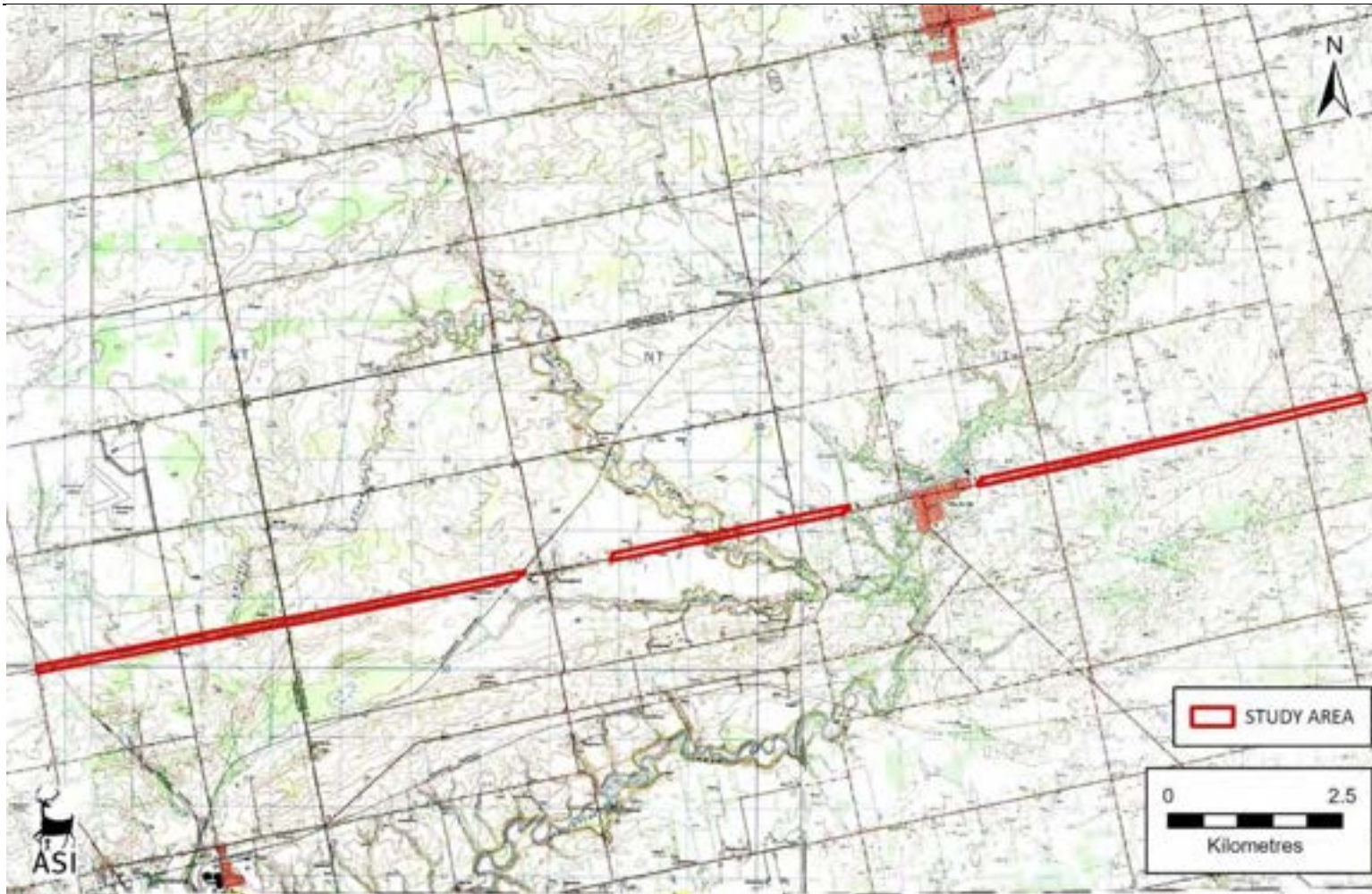


Figure 6: The study area overlaid on the 1970 topographic maps of Mount Elgin, Springford, and Norwich. Base Map: Mount Elgin Sheet, Springford Sheet, and Norwich Sheet (Department of Energy, Mines and Resources, 1970a, 1970c, 1970b)



Figure 7: The study area overlaid on the 1999 topographic map of Tillsonburg. Base Map: Tillsonburg Sheet (Department of Energy, Mines and Resources, 1999)

4.0 Existing Conditions

A field review of the study area was undertaken by Kirstyn Allam of Archaeological Services Inc., on 29 April 2022 to document the existing conditions of the study area from existing rights-of-way. The existing conditions of the study area are described below and captured in Plate 1 to Plate 14.

4.1 Description of Field Review

The Oxford Road 19 study area is located within the Township of South-West Oxford and the Township of Norwich and terminates at the Township of Norwich's border with Norfolk County. The study area is focused on approximately 16 kilometres of the Oxford Road 19⁴ right-of-way from Highway 19 (Plank Line) to the Norfolk County boundary at Windham Road 19, excluding the settlement centres of Springford and Otterville, and is generally bounded by rural and agricultural properties.

The western end of the study area begins at the intersection of County Road 19 and Highway 19 (Plank Line) and travels for approximately 7.1 kilometres where the eastern end of this portion of the study area terminates just west of the settlement centre of Springford, at the former alignment of the Brantford, Norwich and Port Burwell Railway (presently a recreational trail, in a general northeast-southwest alignment). Oxford Road 19 is oriented in a general east-west alignment and Plank Line is oriented in a general north-south alignment. Oxford Road 19 is a paved undivided roadway and features one lane of east-bound and one lane of west-bound vehicular traffic. Oxford Road 19 has narrow gravel shoulders and lacks curbs or sidewalks. The roadway is lined by a mixture of rural and agricultural properties with some commercial properties also. Spittler Creek and Plumb Creek both intersect with the study area. Approximately 245 metres east of the intersection of Oxford Road 19 and Plank Line, the study area is

⁴ Oxford Road 19 is known as Ostrander Road between Plank Line and Zenda Line and as Otterville Road between Zenda Line and Windham Road 19.



transected by the Ontario Southland Railway (the former Tillsonburg, Lake Erie and Pacific Railroad) in a general northwest-southeast alignment. A cast-in-place concrete rigid frame bridge carries Oxford Road 19 over Spittler Creek approximately 1.25 kilometres east of Plank Line. A second bridge, approximately 2.8 kilometres east of Plank Line is a concrete slab on precast T-beam structure which carries Oxford Road 19 also over Spittler Creek. As both bridges were built post-1956 and are considered typical in terms of bridge type and material, they were not identified as potential built heritage resources (Ministry of Tourism, Culture and Sport, 2016).

The study area resumes east of Springford, approximately 800 metres east of Water Street and travels for approximately 3.4 kilometres where it terminates at James Street in the settlement centre of Otterville. The general alignment of Oxford Road 19 remains the same, with one lane for each direction of vehicular traffic, and the road continues to be undivided with narrow gravel shoulders. The roadway continues to be lined primarily with rural and agricultural properties. Big Otter Creek and a tributary of the creek intersect with the study area. A precast concrete I-girder bridge carries Oxford Road 19 over Big Otter Creek approximately 2.6 kilometres east of West Street North. As the bridge was built post-1956 and are considered typical in terms of bridge type and material, it was not identified as potential built heritage resources (Ministry of Tourism, Culture and Sport, 2016).

The final portion of the study area begins approximately 75 metres east of York Street and travels for approximately 5.6 kilometres to the eastern end of the study area at Windham Road 19. The road continues its previous alignment, its cross-section remains unchanged, and it remains lined with mostly rural and agricultural properties with a few commercial properties.





Plate 1: View west to the intersection of Oxford Road 19 and Plank Line.



Plate 2: View east along Oxford Road 19 with rural residential properties (on the left) and commercial properties (on the right).



Plate 3: View north along the Ontario Southland Railway.



Plate 4: The concrete slab on precast T-beam bridge over Spittler Creek along Oxford Road 19, looking west.



Plate 5: View of Spittler Creek, looking south.



Plate 6: View east along Oxford Road 19, a large dairy farm is north of the road (on the left).



Plate 7: Looking southwest along the recreational trail that follows the alignment of the former Brantford, Norwich and Port Burwell rail line.



Plate 8: View east to a large late-twentieth century residence east of Springford.



Plate 9: Looking west along Oxford Road 19, west of Big Otter Creek.



Plate 10: Greenhouses along Oxford Road 19, looking west.



Plate 11: View east as the study area approaches Otterville.



Plate 12: Looking east along Oxford Road 19 with agricultural properties along both sides of the road.



Plate 13: Agricultural properties along Oxford Road 19, east of Csont Line, looking east.





Plate 14: View west at the eastern end of the study area at the intersection of Oxford Road 19 and Windham Road 19.



4.2 Identification of Known and Potential Built Heritage Resources and Cultural Heritage Landscapes



Based on the results of the background research and field review, seven potential built heritage resources (B.H.R.s), two known cultural heritage landscapes (C.H.L.s) and 23 potential C.H.L.s were identified within the study area. These include two properties with designations under Part IV of the Ontario Heritage Act. A detailed inventory of known and potential B.H.R.s and C.H.L.s within the study area is presented below in Table 1. See Figure 8 - Figure 13 for mapping showing the location of identified B.H.R.s and C.H.L.s.







Table 1: Inventory of Known and Potential Built Heritage Resources and Cultural Heritage Landscapes within the Study Area



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
B.H.R. 1	Former Church	224261 Ostrander Road	Potential B.H.R. - Ontario Heritage Trust's Places of Worship Inventory	<p>The former church (presently a residence) is located at the northeast corner of the intersection of Oxford Road 19 (Ostrander Road) and Plank Line. The former church has been converted to a residential property. The potential heritage attributes include the former Knox United Church building that was constructed circa 1900 by the Methodist congregation (Ontario Heritage Trust, n.d.a).</p> <p>The 1909 topographic map (Figure 4) depicts a church in the location of the extant structure.</p>	 <p>Plate 15: The south and east elevations of the former church at 224261 Ostrander Road (A.S.I. 2022).</p>
B.H.R. 2	Residence	224943 Otterville Road	Potential B.H.R. - Identified during background research and field review	<p>The residence is located on the north side of Otterville Road (Oxford Road 19), west of West Street North. The two-storey house is clad in siding and has a rectangular footprint with a rear addition, small western addition, and attached garage on the west side. The house features a hipped roof with a dormer along the southern elevation (front façade). The front façade has two symmetrically placed windows on the second storey and an off-centre front entrance on the main floor, with a large window to the east. A side entrance is located in the small western addition. Potential heritage attributes include the two-storey house, its height and massing, and the hipped roof and dormer.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping</p>	 <p>Plate 16: The south and east elevations of the residence at 224943 Otterville Road (A.S.I. 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
				<p>and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	
B.H.R. 3	Residence	224948 Otterville Road	Potential B.H.R. - Identified during background research and field review	<p>The property is located on the south side of Otterville Road (Oxford Road 19). The one-and-a-half-storey house is clad in siding with an L-shaped footprint, a cross-gabled roof, and an addition at the rear of the structure. Potential heritage attributes include its height and massing, cross-gabled roof, and fenestration.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 17: The north elevation of the residence at 224948 Otterville Road (A.S.I., 2022).</p>
B.H.R. 4	Residence	530 Main Street East	Potential B.H.R. - Identified during background research and field review	<p>The residence is located on the north side of Main Street East (County Road 19), east of Water Street. The two-storey house is clad in siding and has a rectangular footprint with a rear addition. The house features a hipped roof and symmetrical fenestration. A centrally placed entrance is located along the southern elevation (front façade). The potential heritage attributes include the two-storey residence, its height and massing, and hipped roof.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 18: The property at 530 Main Street East, looking north (A.S.I. 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
B.H.R. 5	Residence	343 Main Street West	Potential B.H.R. - Identified during background research and field review	<p>The property is located on the southeast corner of Main Street (Oxford Road 19) and James Street. The two-and-a-half-storey house is clad in red brick with a square footprint, hipped roof with a front dormer, two-storey wrap-around verandah, and a small one-storey addition at the rear of the structure. Potential heritage attributes include its height and massing, position on a corner lot, Edwardian architectural details, fenestration, hipped roof and dormer, and two-storey wrap-around verandah.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 19: The north elevation of the residence at 343 Main Street West (A.S.I., 2022).</p>
B.H.R. 6	Residence	225719 Otterville Road	Potential B.H.R. - Identified during background research and field review	<p>The property is located on the north side of Otterville Road (Oxford Road 19) and west of Highway 59. The one-and-a-half storey house is clad in siding and has a rectangular footprint with a single storey eastern addition. This addition appears to also have a rear addition that connects to a garage. The house features a gable roof with a central gable along the western elevation. The southern elevation in the eastern addition appears to be the main entrance with a covered verandah, and central door flanked by symmetrical windows. The potential heritage attributes include the one-and-a-half storey house, its height and massing including the additions, and the gable roofs.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 20: The south elevation of the residence at 225719 Otterville Road (A.S.I. 2022).</p>

Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
B.H.R. 7	Residence	225769 Otterville Road	Potential B.H.R. - Identified during background research and field review	<p>The property is located on the north side of Otterville Road (Oxford Road 19) and east of Highway 59. The one-and-a-half storey house is clad in siding and has a rectangular footprint, rear saltbox addition, an eastern single storey addition at the rear, and a single storey addition that wraps around the western and southern elevations of the house. The house features a gable roof and asymmetric fenestration. An entrance is located along the southern elevation, and another is at the eastern rear addition. The potential heritage attributes include the one-and-a-half storey house, its height and massing, and gable roof.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 21: The south and west elevations of the residence at 225769 Otterville Road (A.S.I. 2022).</p>
C.H.L. 1	Farmscape	412595 Cranberry Line	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located at the northwest corner of the intersection of Ostrander Road (Oxford Road 19) and Cranberry Line. The property contains a two-storey residence with hipped roof and rectangular footprint, barns, outbuildings, a long tree-lined driveway, mature trees surrounding the house, and active agricultural fields. The potential heritage attributes include the residence, barns and outbuildings, agricultural fields, and mature trees.</p> <p>The 1909 topographic map (Figure 4) depicts a stone or brick house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 22: The farmscape at 412595 Cranberry Line, looking northwest (A.S.I. 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 2	Farmscape	224550 Ostrander Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the south side of Ostrander Road (Oxford Road 19) between Zenda Line and Cranberry Line. The property contains a one-and-a-half-storey farmhouse with a cross-gabled roof and a rear addition, barn, tower silo, and outbuildings. Potential heritage attributes include the residence, barn, silo, outbuildings, agricultural fields, and mature trees.</p> <p>The 1909 topographic map (Figure 4) depicts a stone or brick house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 23: The farmscape at 224550 Ostrander Road, looking south (A.S.I., 2022).</p>
C.H.L. 3	Church and Cemetery	224570 Ostrander Road	<p>Potential C.H.L. - Identified during background research and field review</p> <p>The cemetery is listed on the inventory of known cemeteries/burial sites in the Ontario Genealogical Society's online databases (Ontario Genealogical Society, n.d.)</p>	<p>The property is located on the south side of Ostrander Road (Oxford Road 19), east of Zenda Line. It contains the St. Charles' Chapel of Ease Church building, and the St. Charles Anglican Cemetery. Potential heritage attributes include the red brick, Gothic-Revival church, constructed in 1844 (County of Oxford, 2018b), which has a rectangular footprint, lancet windows, and central tower with battlementing, as well as the attached cemetery, mature trees, and landscaping.</p> <p>The 1876 map (Figure 3) depicts a cemetery in the location of the extant church.</p>	 <p>Plate 24: The north and east elevations of the church at 224570 Ostrander Road and the cemetery behind (A.S.I., 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 4	Farmscape	224671 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the north side of Otterville Road (Oxford Road 19) to the east of Zenda Line. The property contains a two-storey residence with hipped roof with a rectangular footprint and rear addition, barn, long driveway, mature trees, and active agricultural fields. The house has been clad in stucco, has symmetrical fenestration, and a centrally placed entrance along the southern elevation (front façade). The potential heritage attributes include the residence, barn, driveway, active agricultural fields, and mature trees.</p> <p>The 1909 topographic map (Figure 4) depicts a stone or brick house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 25: The farmscape at 224671 Otterville Road, looking north (A.S.I. 2022).</p>
C.H.L. 5	Remnant Farmscape	224724 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The remnant farmscape is located on the south side of Otterville Road (Oxford Road 19) to the east of Zenda Line. The property contains a one-and-a-half storey brick residence with eastern addition, long driveway, and mature vegetation bordering the property. The house was obscured from view by vegetation though it appears to feature a gable roof and symmetrical fenestration along the upper storey. The potential heritage attributes include the residence, long driveway, and the vegetative demarcation along the eastern and western boundaries of the property.</p> <p>The 1909 topographic map (Figure 4) depicts a stone or brick house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 26: The remnant farmscape at 224724 Otterville Road, looking south (A.S.I. 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 6	Farmscape	224742 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the south side of Otterville Road (Oxford Road 19) to the east of Zenda Line. The property contains a two-storey frame residence with hipped roof and rear addition, barn and outbuildings, long driveway, mature trees and vegetation, and active agricultural fields. The house features an internal chimney, a central doorway along the northern elevation (front façade) and symmetrical fenestration. The potential heritage attributes include the residence, barn and outbuildings, mature trees and vegetation, long driveway, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 27: The farmscape at 224732 Otterville Road, looking south (A.S.I. 2022).</p>
C.H.L. 7	Cemetery	225227 Otterville Road	<p>Potential C.H.L. - Identified during background research and field review</p> <p>The cemetery is listed on the inventory of known cemeteries/burial sites in the Ontario Genealogical Society's online databases (Ontario Genealogical Society, n.d.)</p>	<p>The property is located on the north side of Otterville Road (Oxford Road 19), west of Middletown Line. It contains the Springford Community Cemetery. Potential heritage attributes include the cemetery, landscaping, and mature trees.</p> <p>The 1909 topographic map (Figure 4) depicts a cemetery in the location of the Springford Community Cemetery.</p>	 <p>Plate 28: The Springford Community Cemetery, looking north (A.S.I., 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 8	Farmscape	225279 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the north side of Otterville Road (Oxford Road 19), west of Middletown Line. The property contains a one-and-a-half storey residence, detached garage, stable, paddock, pond, mature trees, and active agricultural fields. The house has been clad in siding and has a T-shaped footprint with rear additions. The potential heritage attributes include the residence, mature trees, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 29: The farmscape at 225279 Otterville Road, looking northwest (A.S.I. 2022).</p>
C.H.L. 9	Farmscape	712597 Middletown Line	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located at the northwest corner of Middletown Line and Oxford Road 19. The property contains a one-and-a-half storey residence, garage, outbuildings, greenhouses, paddocks, and active agricultural fields. The residence has been clad in siding, has a gable roof, and T-shaped footprint. The potential heritage attributes include the residence, paddocks, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 30: The farmscape at 712597 Middletown Line, looking northeast (A.S.I. 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 10	Farmscape	225346 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located at the southwest corner of the intersection of Otterville Road (Oxford Road 19) and Middletown Line. The property contains a one-and-a-half storey residence, garage, long driveway, active agricultural fields with windbreaks, and a woodlot. The residence features a T-shaped footprint with a rear addition. The north elevation (front façade) features a centre gable, covered verandah, and central doorway flanked by windows. The potential heritage attributes include the residence, long driveway, and agricultural fields with windbreaks.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 31: The farmscape at 225346 Otterville Road, looking southwest (A.S.I. 2022).</p>
C.H.L. 11	Farmscape	225413 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the north side of Otterville Road (Oxford Road 19), east of Middletown Line. The property contains a two-storey residence with a hipped roof, barns and greenhouses, pond, agricultural fields, and mature vegetation. The residence is clad in siding and has a square footprint. The southern elevation (front façade) features a central door with an awning and symmetrical fenestration. The potential heritage attributes include the residence and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 32: The farmscape at 225413 Otterville Road, looking northeast (A.S.I. 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 12	Farmscape	225400 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the southeast corner of the intersection of Otterville Road (Oxford Road 19) and Middletown Line. The property contains a one-and-a-half storey residence with gable roof, barns and outbuildings, mature vegetation and windbreaks, and active agricultural fields. The house is clad in siding and features a wrap-around verandah. The potential heritage attributes include the residence, barns and outbuildings, mature vegetation and windbreaks, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 33: The farmscape at 225400 Otterville Road, looking south (A.S.I. 2022).</p>
C.H.L. 13	Museum and Community Centre	225422 Otterville Road	Known C.H.L. - Designated under Part IV of the Ontario Heritage Act (By-law 18-90 and 84-77)	<p>The property is located on the south side of Otterville Road (Oxford Road 19) and west of James Street. The property contains an octagonal residence, two cemeteries, and former railway station. The South Norwich Historical Society operates a museum on the property and the Woodlawn Adult Community Centre is also on the property. The two cemeteries on the property are the Pine Street Burying Ground and the Erbtown Cemetery.</p> <p>The known heritage attributes include the octagonal house constructed in 1861 by Thomas Wright (for additional information, please see the by-law) and the former Port Dover and Lake Huron Railway Station which is considered to be symbol of nineteenth-century transportation and communication for a small town and its architecture is an example of a village railroad station (for additional information, please see the by-law). The potential heritage attributes include the Pine Street Burying Ground and the Erbtown Cemetery.</p>	 <p>Plate 34: The octagonal residence and the Pine Street Burying Ground, looking southeast (A.S.I. 2022).</p>



Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
				The 1909 topographic map (Figure 4) depicts a cemetery in the vicinity of the extant ones on the property.	
C.H.L. 14	Farmscape	74 James Street	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located at the southwest corner of the intersection of Otterville Road (Oxford Road 19) and James Street. The property contains two residences, outbuildings, and agricultural fields. The northern residence is a two-storey brick house with hipped roof and a projecting bay along the northern elevation (front façade). The house features a wrap-around verandah and a central doorway along the north elevation. The southern residence appears to date from the twenty-first century.</p> <p>The 1954 aerial photograph (Figure 5) depicts a residence in the vicinity of the extant northern house. Based on historical mapping and the building’s vernacular architecture, the house was likely built in the early-twentieth century.</p>	 <p>Plate 35: The farmscape at 74 James Street, looking south (A.S.I. 2022).</p>
C.H.L. 15	Farmscape	225659 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the north side of Otterville Road (Oxford Road 19) and east of York Street. The property contains a two-storey brick house with hipped roof, outbuildings, drying kilns, long driveway, mature trees and active agricultural fields. The house has a rectangular footprint and rear addition. The residence is partially obscured from the right-of-way by vegetation although appears to have symmetrical fenestration along the upper storey. The potential heritage attributes include the residence, long driveway, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 36: The farmscape at 225659 Otterville Road, looking north (A.S.I. 2022).</p>

Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 16	Farmscape	225688 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the south side of Otterville Road, west of Highway 59. The property contains a one-and-a-half-storey farmhouse with a cross-gabled roof, T-shaped footprint, and a rear addition; an A-frame barn; and outbuildings. Potential heritage attributes include the residence, barn, outbuildings, agricultural fields, and mature trees.</p> <p>The 1909 topographic map (Figure 4) depicts a stone or brick house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 37: The farmscape at 225688 Otterville Road, looking south (A.S.I., 2022).</p>
C.H.L. 17	Farmscape	225720 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located at the southwest corner of the intersection of Otterville Road (Oxford Road 19) and Highway 59. The property contains a one-and-a-half storey residence, barns, mature trees, and active agricultural fields. The frame residence has an H-shaped footprint with gable roof and a central entrance is located in the middle of the northern elevation (front façade). One of the barns is a gambrel roof bank barn. The potential heritage attributes include the residence, gambrel roof bank barn, long driveway, mature trees, agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 38: The farmscape at 225720 Otterville Road, looking south (A.S.I. 2022).</p>

Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 18	Farmscape	225860 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the south side of Otterville Road (Oxford Road 19), west of Csont Line. The property contains a two-storey brick residence, garage, barn, vegetative windbreaks, and active agricultural fields. The residence has a hipped roof and rectangular footprint with a rear addition. The house has an off-centre entrance along the northern elevation (front façade) and symmetrical fenestration. The potential heritage attributes include the residence, barn, vegetative windbreaks, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a brick or stone house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 39: The farmscape at 225860 Otterville Road, looking south (A.S.I. 2022).</p>
C.H.L. 19	Farmscape	225877 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located at the northwest corner of the intersection of Otterville Road (Oxford Road 19) and Csont Line. The property contains two residences, outbuildings, mature trees, and agricultural fields. The western residence is of late-twentieth century construction. The eastern residence is a one-and-a-half storey frame house clad in siding with a wrap-around verandah. The house has a rectangular footprint and rear addition. The southern elevation (front façade) has a central door flanked by windows. The potential heritage attributes include the frame residence and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 40: The farmscape at 225877 Otterville Road, looking north (A.S.I. 2022).</p>

Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 20	Farmscape	225947 Otterville Road (same parcel as 225938 Otterville Road)	Known C.H.L. – Designated under Part IV of the Ontario Heritage Act (By-law 57-88); Ontario Heritage Trust Plaque	<p>The farmscape is located on the south side of Otterville Road between Base Line and Highway 59 and is the site of the former Innisfree Farm. Known heritage attributes include the residence, a former conference centre, tower silo, outbuildings, mature trees, and wood lots. For additional information, please see the by-law designating the Innisfree farmhouse. Known heritage attributes also include an Ontario Heritage Trust Plaque commemorating the birthplace of historian Harold Innis (Ontario Heritage Trust, 2018).</p> <p>The conference centre was built in 1971 by Innis College’s Harold Innis Foundation, who purchased Innisfree Farm from Sam Innis in 1986. The conference centre was built in the Modern style and operated as a library for the works of Harold Innis, an educational and meeting space, and as a rural retreat for students of Innis College until the farm was sold in 1988 (J. Chapman, 2018).</p>	 <p>Plate 41: The farmscape and commemorative plaque at 225947 Otterville Road, looking south (A.S.I., 2022).</p>
C.H.L. 21	Farmscape	225963 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the north side of Otterville Road (Oxford Road 19), east of Csont Line. The property contains a one-and-a-half storey residence with eastern addition, barns, long driveway, vegetative windbreaks, and agricultural fields. The residence has been clad in siding and has a gable roof with an internal chimney. The potential heritage attributes include the residence, long driveway, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 42: The farmscape at 225963 Otterville Road, looking north (A.S.I. 2022).</p>

Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 22	Farmscape	225964 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the south side of Otterville Road (Oxford Road 19), east of Csont Line. The property contains a one-and-a-half storey residence, barns, silos, long tree-lined driveway, and agricultural fields. The residence features an L-shaped footprint with rear addition and gable roof. The potential heritage attributes include the residence, barns, tree-lined driveway, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 43: The farmscape at 225964 Otterville Road, looking north (A.S.I. 2022).</p>
C.H.L. 23	Remnant Farmscape	226028 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the south side of Otterville Road (Oxford Road 19), west of Base Line. The property contains a residence, an overgrown long driveway, and agricultural fields. The residence features a hipped roof and rectangular footprint. The house has been clad in siding and has symmetrical fenestration along the northern elevation (front façade). The potential heritage attributes include the residence, long driveway, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 44: The remnant farmscape at 226028 Otterville Road, looking north (A.S.I. 2022).</p>

Feature I.D.	Type of Property	Address or Location	Heritage Status and Recognition	Description of Property and Known or Potential C.H.V.I.	Photographs/ Digital Image
C.H.L. 24	Farmscape	226091 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the north side of Otterville Road (Oxford Road 19), west of Base Line. The property contains a two-storey Italianate farmhouse with a hipped roof and a large rear addition, a barn, and outbuildings. Potential heritage attributes include the residence, barn, outbuildings, mature trees, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a stone or brick house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 45: The farmscape at 226091 Otterville Road, looking north (A.S.I., 2022).</p>
C.H.L. 25	Farmscape	226125 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>The farmscape is located on the north side of Otterville Road (Oxford Road 19), east of Base Line. The property contains a one-and-a-half-storey residence with a gabled roof and an addition to the eastern elevation, a barn, and multiple outbuildings. Potential heritage attributes include the residence, barn, outbuildings, mature trees, and agricultural fields.</p> <p>The 1909 topographic map (Figure 4) depicts a wooden house in the vicinity of the extant structure. Based on historical mapping and the vernacular architecture of the house, it was likely built in the late-nineteenth or early-twentieth century.</p>	 <p>Plate 46: The farmscape at 226125 Otterville Road, looking north (A.S.I., 2022).</p>

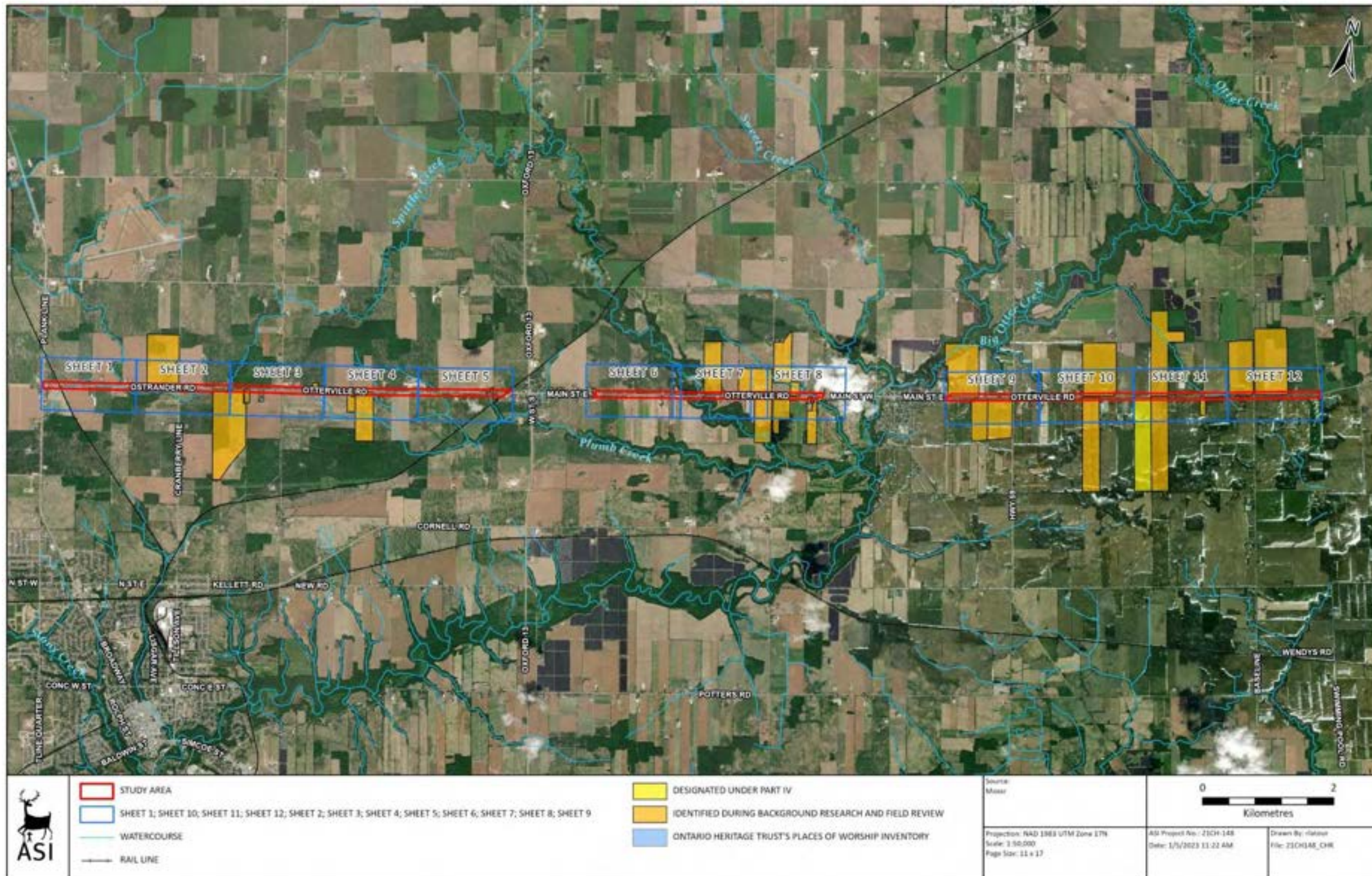


Figure 8: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Overview)



Figure 9: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 1)

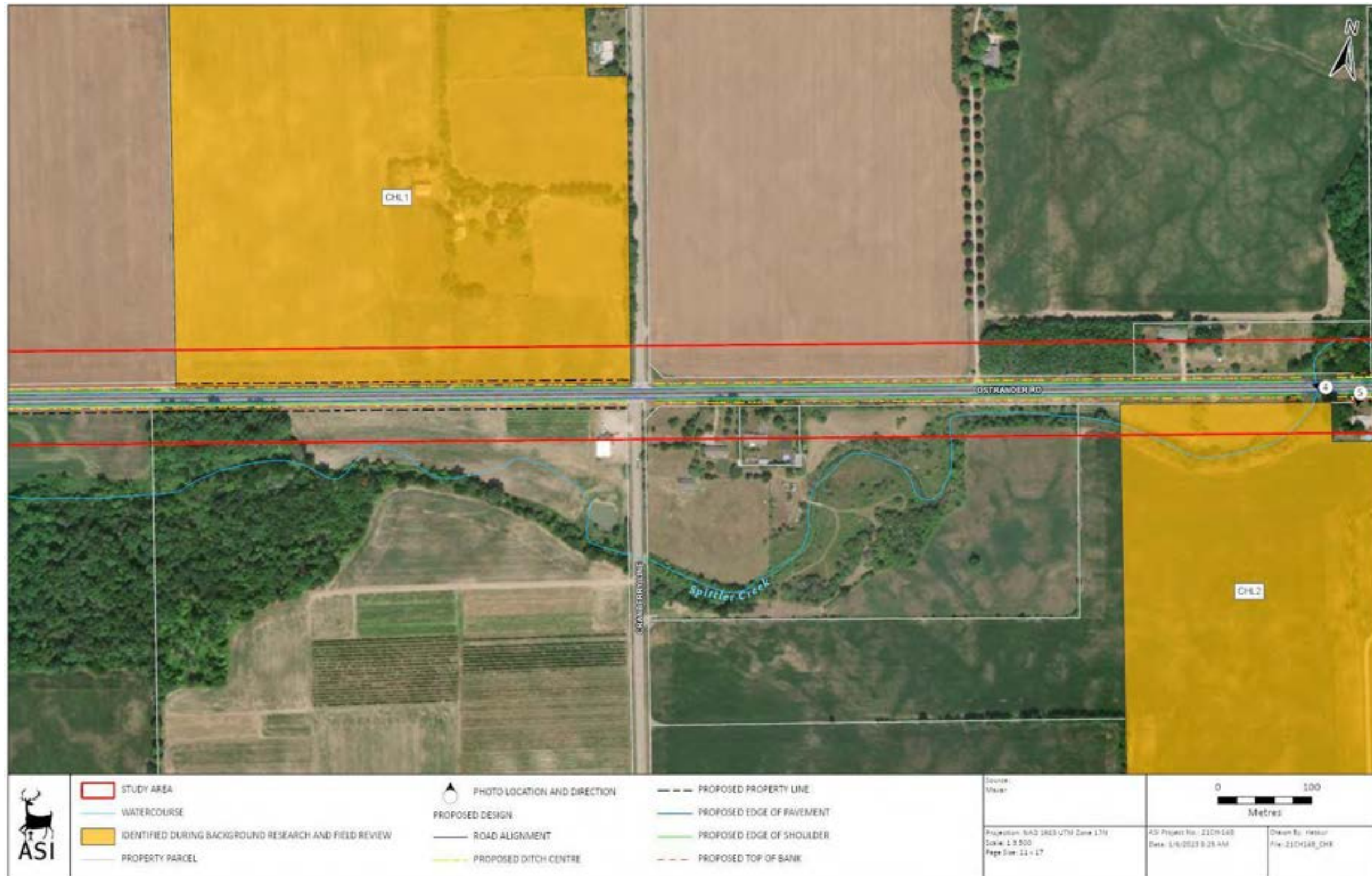


Figure 10: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 2)

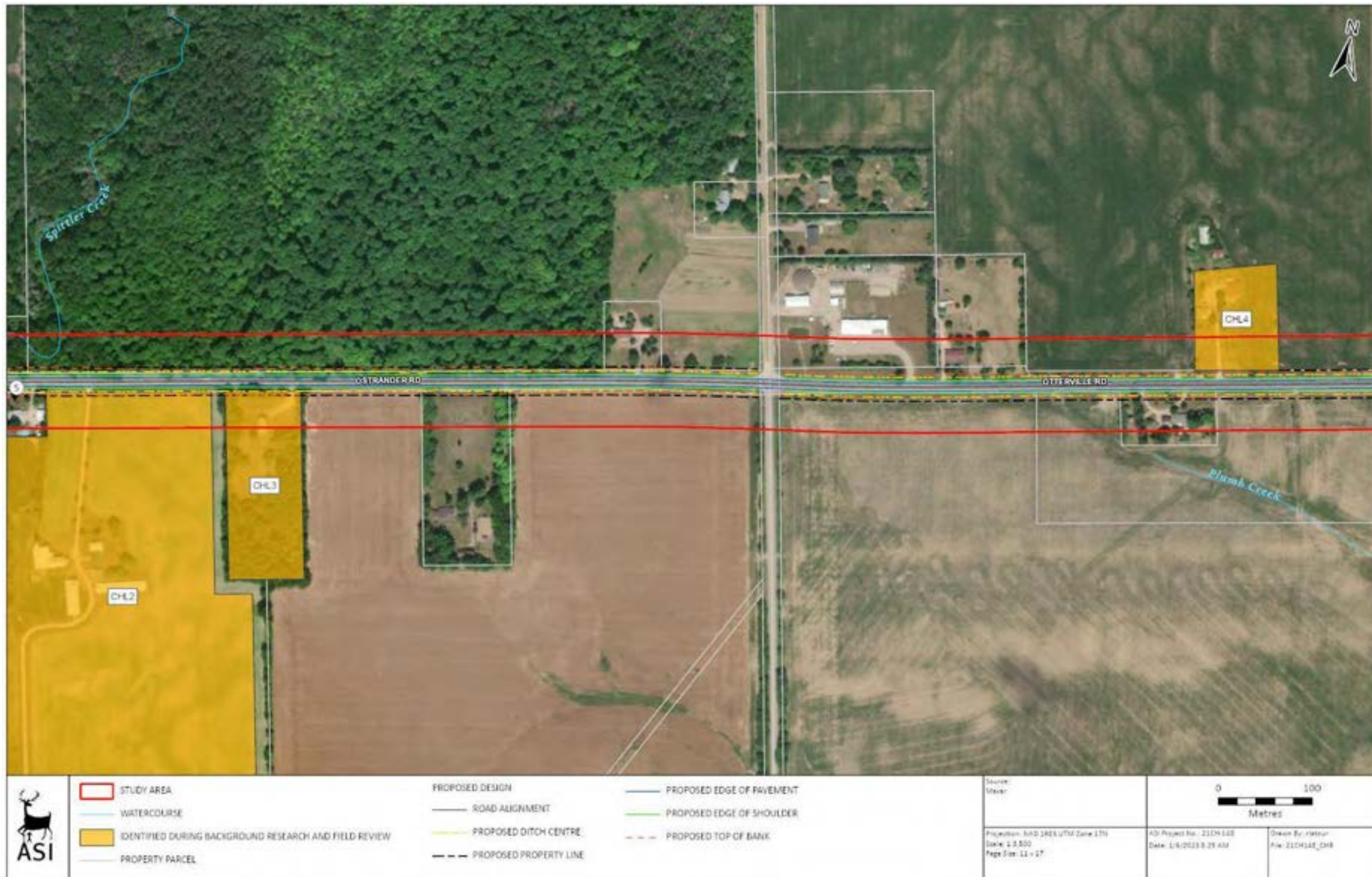


Figure 11: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 3)

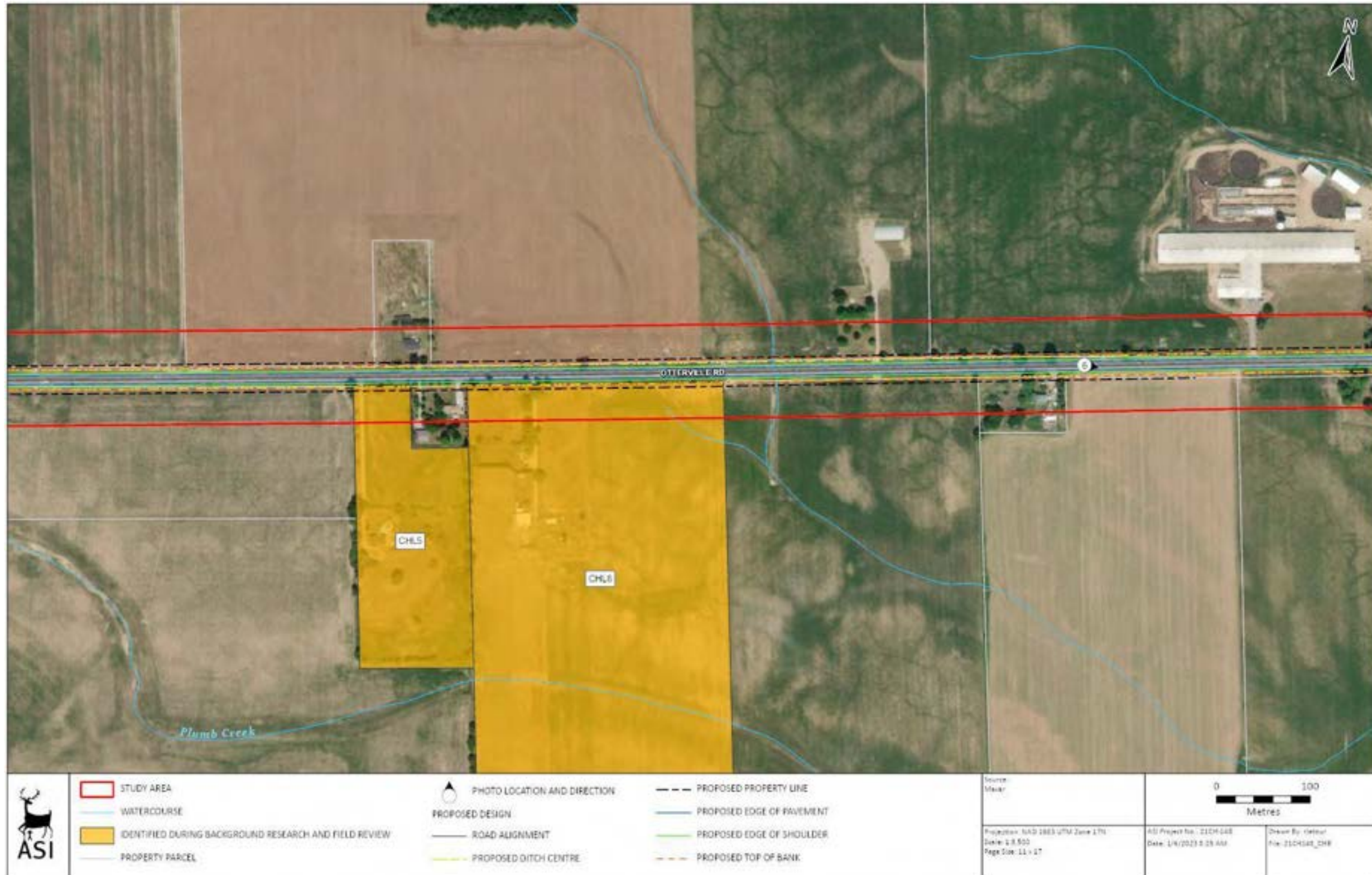


Figure 12: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 4)



Figure 13: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 5)



Figure 14: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 6)



Figure 15: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 7)

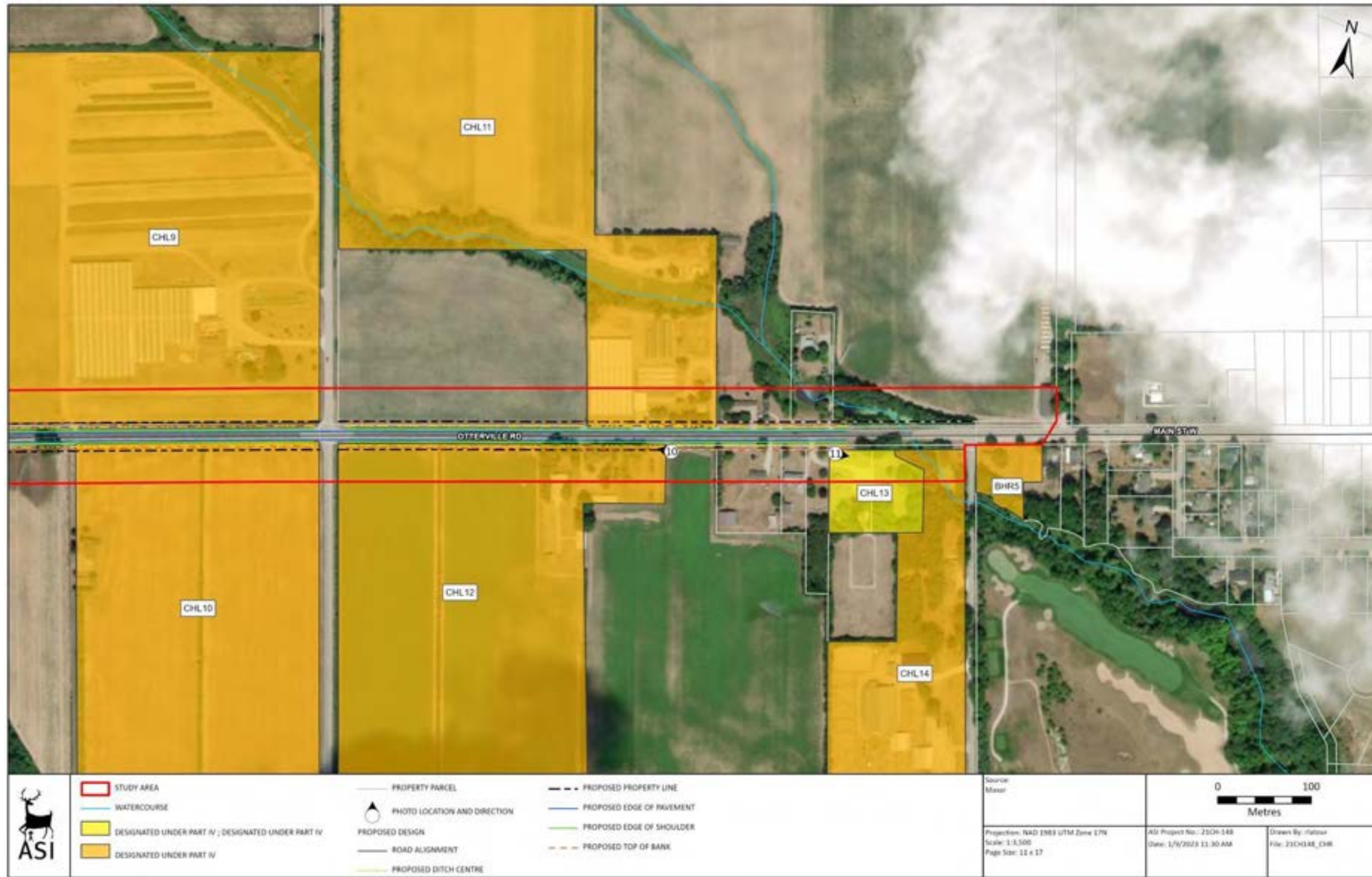


Figure 16: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 8)

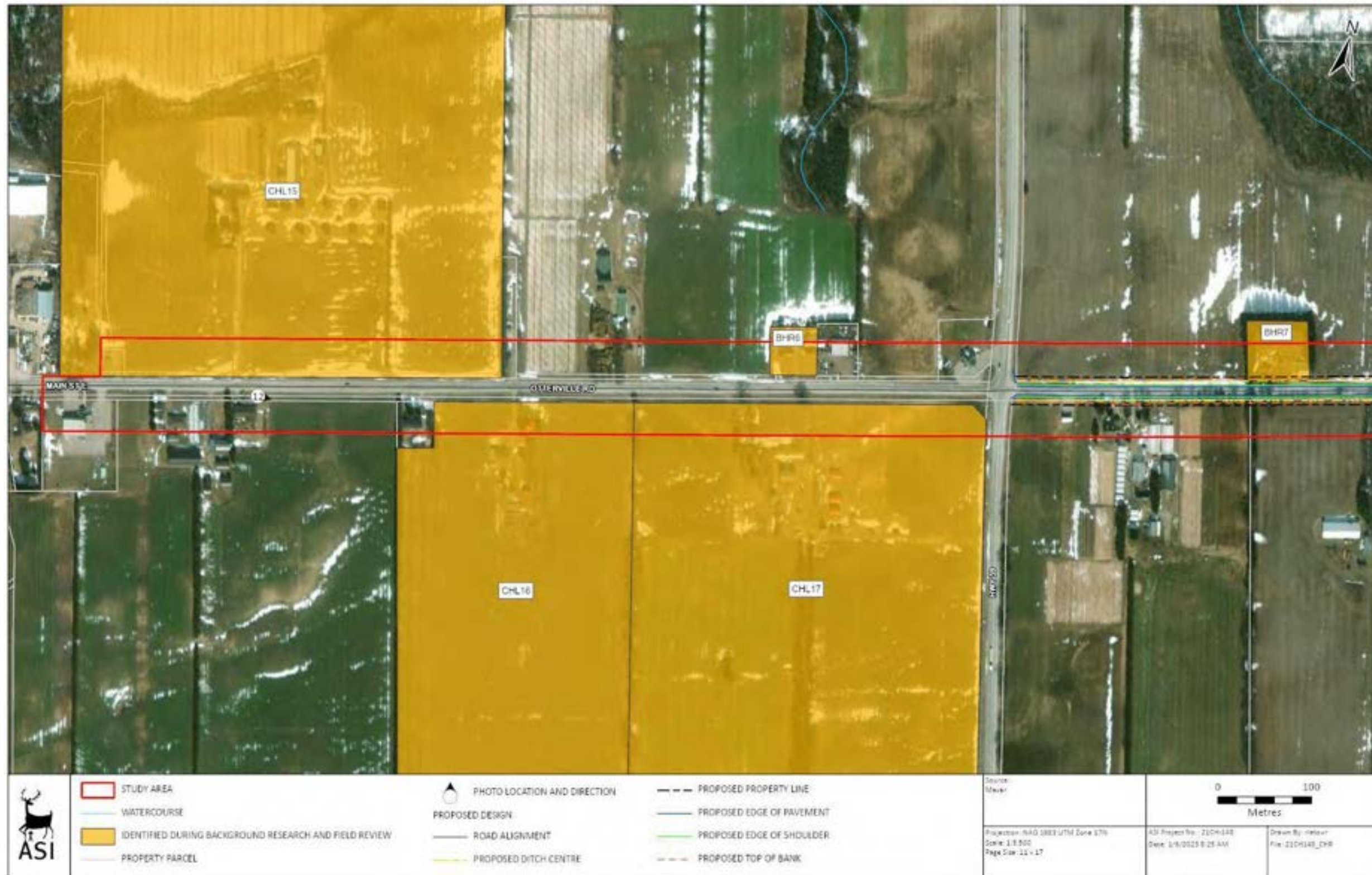


Figure 17: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 9)



Figure 18: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 10)

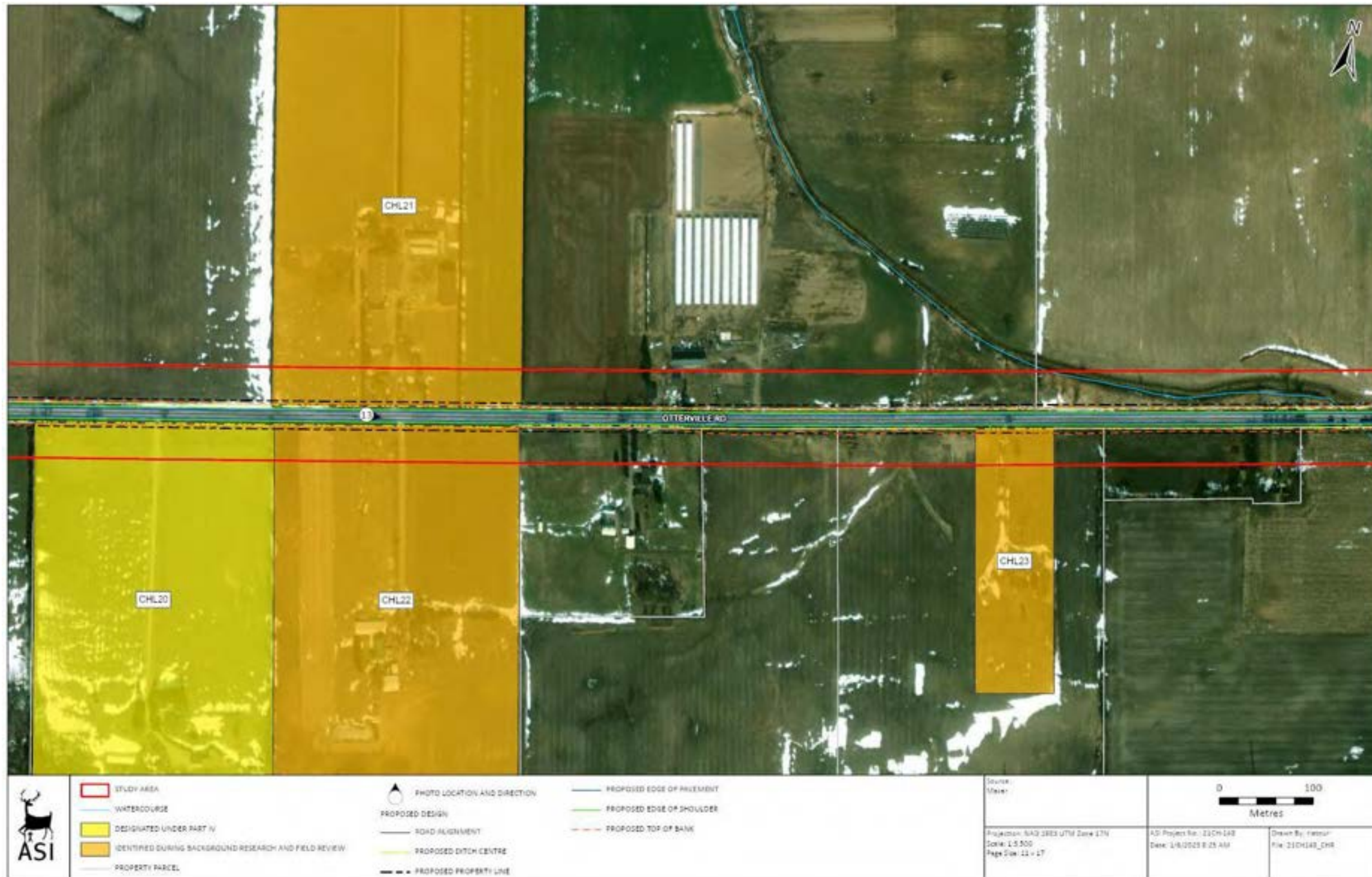


Figure 19: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 11)

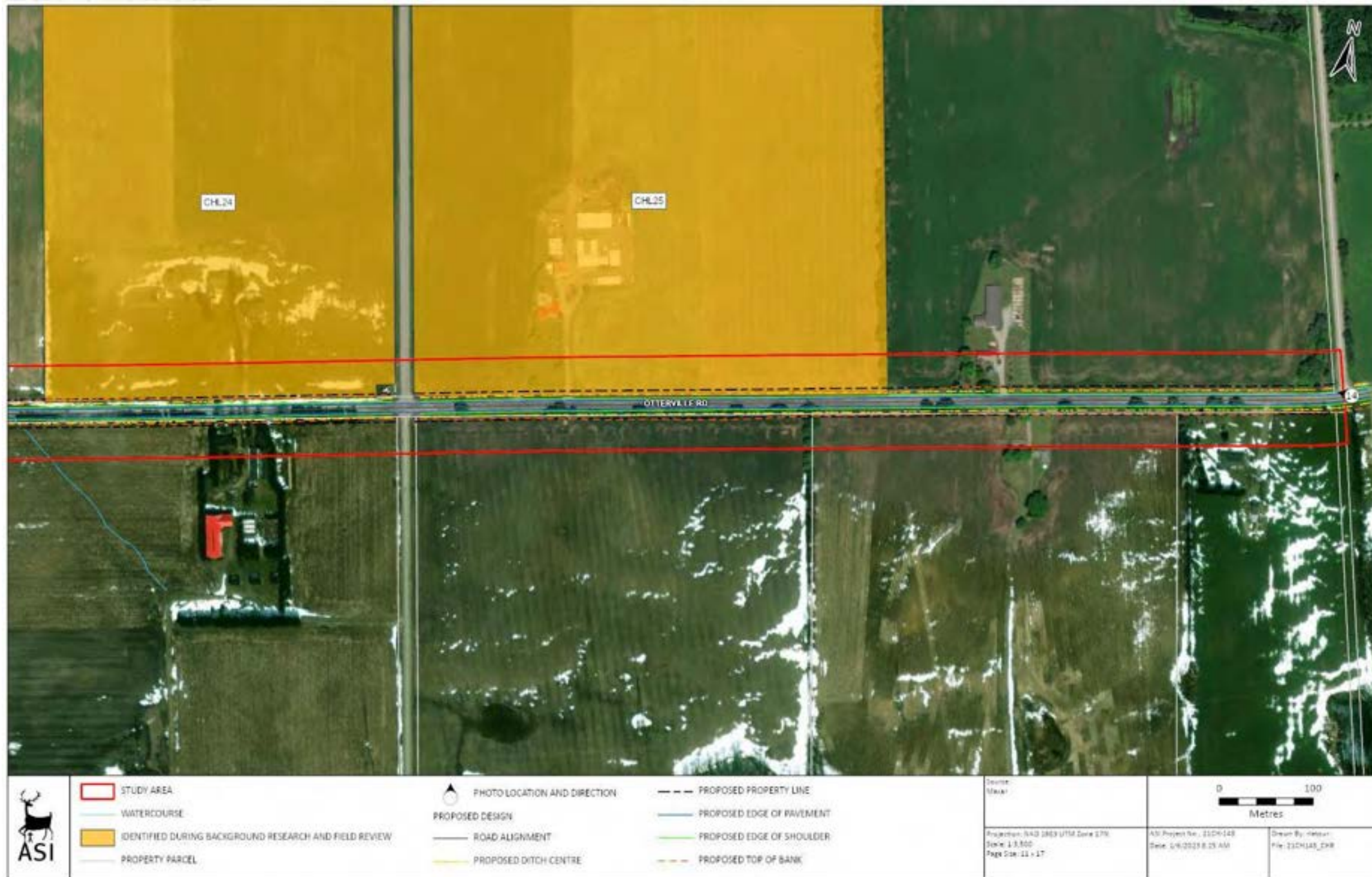


Figure 20: Location of Identified Built Heritage Resources (B.H.R.) and Cultural Heritage Landscapes (C.H.L.) in the Study Area (Sheet 12)

5.0 Preliminary Impact Assessment

The following sections provide more detailed information regarding the proposed project undertaking and analysis of the potential impacts on identified built heritage resources and cultural heritage landscapes.

5.1 Description of Proposed Undertaking

The Oxford Road 19 Corridor Improvements Municipal Class Environmental Assessment involves proposed road improvements to Oxford Road 19 from Highway 19 (Plank Line) to the Norfolk County boundary at Windham Road 19 based on the completion of the 2019 Transportation Master Plan (Paradigm Transportation Solutions Limited & Lura Consulting, 2020) which identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people. The project study area consists of approximately 16 kilometres of the Oxford Road 19 right-of-way from Highway 19 to the Norfolk County boundary at Windham Road 19, excluding the settlement centres of Springford and Otterville.

The proposed undertaking for the Oxford Road 19 Corridor Improvements study area Preferred Design Concept consists of: structural improvements to the road, including widened lanes and shoulders within a widened right-of-way; provide a two-lane road to County requirements with paved shoulder; widened travel lanes and partially paved shoulders; and improvement to the pavement structure (Oxford County & R. J. Burnsides & Associates Limited, 2022). There is minor encroachment and property acquisition across property frontages along the majority of the project study corridor.

Mapping of the proposed undertaking and the study area showing photographic plates and the location of the identified built heritage resources (B.H.R.s) and cultural heritage landscapes (C.H.L.s) is provided in Figure 8 to Figure 20 in Section 4.2. Detailed roll plans of the proposed undertaking are included in Appendix A.

5.2 Analysis of Potential Impacts

Table 2 outlines the potential impacts on all identified B.H.R.s and C.H.L.s within the study area.



Table 2: Preliminary Impact Assessment and Recommended Mitigation Measures

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
B.H.R. 1	224261 Ostrander Road	Potential B.H.R. - Ontario Heritage Trust's Places of Worship Inventory	<p>Direct impacts to B.H.R. 1 are anticipated to include property acquisition and encroachment onto the southern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation should be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the B.H.R.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
B.H.R. 2	224943 Otterville Road	Potential B.H.R. - Identified during background research and field review	<p>Direct impacts to B.H.R. 2 are anticipated to include property acquisition, grading, and encroachment onto the southern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the B.H.R.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>
B.H.R. 3	224948 Otterville Road	Potential B.H.R. - Identified during background research and field review	<p>Direct impacts to B.H.R. 3 are anticipated to include property acquisition, grading, and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation should be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the B.H.R.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
B.H.R. 4	530 Main Street East	Potential B.H.R. - Identified during background research and field review	It is understood that the limits of the proposed work will be confined to the right-of-way and the property adjacent to B.H.R. 4. No direct or indirect adverse impacts to this property are anticipated.	No further work required.
B.H.R. 5	343 Main Street West	Potential B.H.R. - Identified during background research and field review	It is understood that the limits of the proposed work will be confined to the right-of-way and no direct or indirect adverse impacts to this property are anticipated	No further work required.
B.H.R. 6	225719 Otterville Road	Potential B.H.R. - Identified during background research and field review	It is understood that the limits of the proposed work will be confined to the right-of-way east of the intersection of Otterville Road and Highway 59. No direct or indirect adverse impacts to this property are anticipated.	No further work required.

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
B.H.R. 7	225769 Otterville Road	Potential B.H.R. - Identified during background research and field review	<p>Direct impacts to B.H.R. 7 are anticipated to include provisional property acquisition, grading, and encroachment onto the southern portion of the property. However, the proposed preliminary designs are conceptual and a site specific area will be considered during detailed design.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>Where feasible, the proposed design and construction activities related to this project should be designed in a manner that avoids all impacts to B.H.R. 7.</p> <p>Given the potential cultural heritage value of the residence at 225769 Otterville Road, and the anticipated impacts to the property, a resource-specific Cultural Heritage Evaluation Report (C.H.E.R.) should be conducted to determine cultural heritage value or interest (C.H.V.I.).</p> <p>As there are direct impacts anticipated, should the C.H.E.R. determine that the property retains C.H.V.I., a resource-specific Heritage Impact Assessment (H.I.A.) should be conducted to evaluate alternatives, assess potential impacts to the resource, and recommend appropriate mitigation measures.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 1	412595 Cranberry Line	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 1 are anticipated to include property acquisition, grading, and encroachment onto the southern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>
C.H.L. 2	224550 Ostrander Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 2 are anticipated to include property acquisition, grading, and encroachment onto the northern portion of the eastern side of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 3	224570 Ostrander Road	<p>Potential C.H.L. - Identified during background research and field review</p> <p>The cemetery is listed on the inventory of known cemeteries/burial sites in the Ontario Genealogical Society's online databases (Ontario Genealogical Society, n.d.)</p>	<p>Direct impacts to C.H.L. 3 are anticipated to include property acquisition, grading, tree removal, and encroachment onto the northern portion of the church and cemetery property. As the mature trees were identified as a potential heritage attribute, their removal would be considered a direct adverse impact to the property. The proposed preliminary designs are conceptual and modifications to the cross section have been noted to reduce tree removals as possible.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the church and some headstones sit within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>Where feasible, the proposed design and construction activities related to this project should be designed in a manner that avoids all impacts to C.H.L. 3.</p> <p>Given the potential cultural heritage value of the church and cemetery at 224570 Ostrander Road, and the anticipated impacts to the property, a resource-specific C.H.E.R. should be conducted to determine C.H.V.I.</p> <p>As there are direct impacts anticipated, should the C.H.E.R. determine that the property retains C.H.V.I., a resource-specific H.I.A. should be conducted to evaluate alternatives, assess potential impacts to the resource, and recommend appropriate mitigation measures.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>
C.H.L. 4	224671 Otterville Road	<p>Potential C.H.L. - Identified during background research and field review</p>	<p>It is understood that the proposed works will be confined to the Otterville Road right-of-way adjacent to C.H.L. 4. No direct adverse impacts are anticipated.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>No further work required.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 5	224724 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 5 are anticipated to include property acquisition, grading, removal of a few trees along the property frontage, and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>
C.H.L. 6	224742 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 6 are anticipated to include property acquisition, grading, removal of a few trees along the property frontage, and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 7	225227 Otterville Road	<p>Potential C.H.L. - Identified during background research and field review</p> <p>The cemetery is listed on the inventory of known cemeteries/burial sites in the Ontario Genealogical Society's online databases (Ontario Genealogical Society, n.d.)</p>	<p>It is understood that the proposed works will be confined to the Otterville Road right-of-way adjacent to C.H.L. 7. No direct adverse impacts are anticipated.⁵</p> <p>Indirect adverse impacts due to construction related vibration are possible as headstones sit within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>Project designs early on in detailed design should be reviewed by a qualified heritage professional to confirm that there will be no direct adverse impact to the cemetery.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>
C.H.L. 8	225279 Otterville Road	<p>Potential C.H.L. - Identified during background research and field review</p>	<p>It is understood that the proposed works will be confined to the Otterville Road right-of-way adjacent to C.H.L. 8. No direct adverse impacts are anticipated.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>

⁵ The client has confirmed this through email, March 1 2023.

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 9	712597 Middletown Line	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 9 are anticipated to include property acquisition, grading, removal of the post and wire fence, and encroachment onto the southern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>
C.H.L. 10	225346 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 10 are anticipated to include property acquisition, grading, and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 11	225413 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 11 are anticipated to include property acquisition, grading, removal of a few trees along the property frontage, and encroachment onto the southern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 12	225400 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 12 are anticipated to include property acquisition, grading, removal of vegetation, and encroachment onto the northern portion of the property. The proximity of the proposed work to the residence within the C.H.L. and the removal of the trees will alter the setting of the property and is a direct adverse impact.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>Where feasible, the proposed design and construction activities related to this project should be designed in a manner that avoids all impacts to C.H.L. 12.</p> <p>Given the potential cultural heritage value of the farmscape at 225400 Otterville Road, and the anticipated impacts to the property, a resource-specific C.H.E.R. should be conducted to determine C.H.V.I.</p> <p>As there are direct impacts anticipated, should the C.H.E.R. determine that the property retains C.H.V.I., a resource-specific H.I.A. should be conducted to evaluate alternatives, assess potential impacts to the resource, and recommend appropriate mitigation measures.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 13	225422 Otterville Road	Known C.H.L. - Designated under Part IV of the <i>Ontario Heritage Act</i> (By-law 18-90 and 84-77)	<p>Direct impacts to C.H.L. 13 are anticipated to include grading and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>Where feasible, the proposed design and construction activities related to this project should be designed in a manner that avoids all impacts to C.H.L. 13.</p> <p>As C.H.L. 13 is designated under Part IV of the <i>Ontario Heritage Act</i>, and there are direct impacts anticipated, a resource-specific H.I.A. is required as per clause 3.3.2.2 of the <i>Oxford County Official Plan</i> (County of Oxford, 2021). Given that the proposed undertaking is not anticipated to result in direct adverse impacts to the property and no known heritage attributes are anticipated to be impacted, it is recommended that the County of Oxford should consider waiving the requirement for a H.I.A. in this case if suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 14	74 James Street	Potential C.H.L. - Identified during background research and field review	It is understood that the proposed works will be confined to the Otterville Road right-of-way adjacent to C.H.L. 14. No direct or indirect adverse impacts are anticipated. As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.	No further work required.
C.H.L. 15	225659 Otterville Road	Potential C.H.L. - Identified during background research and field review	It is understood that the proposed work is not adjacent to C.H.L. 15. ⁶ No direct or indirect adverse impacts are anticipated.	No further work required.
C.H.L. 16	225688 Otterville Road	Potential C.H.L. - Identified during background research and field review	It is understood that the proposed work is not adjacent to C.H.L. 16. ⁷ No direct or indirect adverse impacts are anticipated.	No further work required.
C.H.L. 17	225720 Otterville Road	Potential C.H.L. - Identified during background research and field review	It is understood that currently the proposed work is not adjacent to C.H.L. 17. ⁸ No direct or indirect adverse impacts are anticipated.	No further work required.

⁶ However, any improvements between 225656 Otterville Road and 225720 Otterville Road will be determined during detailed design.

⁷ However, any improvements between 225656 Otterville Road and 225720 Otterville Road will be determined during detailed design.

⁸ However, any improvements between 225656 Otterville Road and 225720 Otterville Road will be determined during detailed design.

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 18	225860 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 18 are anticipated to include property acquisition, grading, removal of vegetation, and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>
C.H.L. 19	225877 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 19 are anticipated to include property acquisition, grading, removal of vegetation, and encroachment onto the southern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 20	225947 Otterville Road (same parcel as 225938 Otterville Road)	Known C.H.L. – Designated under Part IV of the <i>Ontario Heritage Act</i> (By-law 57-88); Ontario Heritage Trust Plaque	<p>Direct impacts to C.H.L. 20 are anticipated to include property acquisition, grading, encroachment onto the northern portion of the property, and removal/relocation of the Ontario Heritage Trust plaque. However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>Where feasible, the proposed design and construction activities related to this project should be designed in a manner that avoids all impacts to C.H.L. 20.</p> <p>As C.H.L. 20 is designated under Part IV of the <i>Ontario Heritage Act</i>, and there are direct impacts anticipated which include proposed changes to the property parcel boundaries, a resource-specific H.I.A. is required as per clause 3.3.2.2 of the <i>Oxford County Official Plan</i> (County of Oxford, 2021). The H.I.A. should be completed by a qualified heritage professional with recent and relevant experience as early in detailed design as possible.</p> <p>The Ontario Heritage Trust plaque should be removed prior to construction and stored in a secure facility to prevent damage.</p> <p>Following construction activities, this plaque should be reinstalled at its extant location, or in a similarly accessible location based on consultation with the Township of Norwich and the Ontario Heritage Trust.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 21	225963 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 21 are anticipated to include property acquisitions, grading, tree removal, and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property.</p> <p>Indirect adverse impacts due to construction related vibration are possible as the structure sits within 50 metres from the proposed work. These impacts are expected to be limited and temporary. No additional indirect impacts were identified.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>To address the potential for indirect impacts due to construction related vibration, undertake a baseline vibration assessment during detail design to determine potential vibration impacts.</p>
C.H.L. 22	225964 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 22 are anticipated to include property acquisitions, grading, removal of the post and wire fence, and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 23	226028 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 23 are anticipated to include property acquisition, grading, tree removal, and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>
C.H.L. 24	226091 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 24 are anticipated to include property acquisition, grading, and encroachment onto the southern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>

Feature I.D.	Location/Name	Heritage Status and Recognition	Type and Description of Potential/Anticipated Impact	Mitigation Strategies
C.H.L. 25	226125 Otterville Road	Potential C.H.L. - Identified during background research and field review	<p>Direct impacts to C.H.L. 25 are anticipated to include property acquisition, grading, removal of vegetation, and encroachment onto the southern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on potential heritage attributes associated with this property.</p> <p>As the proposed work is located more than 50 metres from the structures within the C.H.L., no indirect adverse impacts are anticipated.</p>	<p>The proposed design and construction activities related to this project should continue to avoid adverse impacts to identified potential heritage attributes. The proposed property acquisition and encroachment should be minimized, where technically feasible. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.</p> <p>Suitable mitigation measures may also include establishing no-go zones with fencing and issuing instructions to construction crews to avoid the C.H.L.</p>

5.3 Summary of Potential Impacts

Based on the preliminary designs provided December 2022, and as presented in Table 2 above, the proposed corridor improvements will result in direct adverse impacts to the known or potential heritage attributes of one B.H.R. (B.H.R. 7) and four C.H.L.s (C.H.L. 3, C.H.L. 12, C.H.L. 13, C.H.L. 20). While encroachment, grading, vegetation removal and property acquisition along property frontages are anticipated for many of the identified B.H.R.s and C.H.L.s, the proposed corridor improvements are not anticipated to have direct adverse impacts to the remaining six B.H.R.s and 20 C.H.L.s. Potential vibration impacts as a result of the proposed construction work may result in indirect adverse impacts to four B.H.R.s and nine C.H.L.s.

6.0 Results and Mitigation Recommendations

The results of background historical research and a review of secondary source material, including historical mapping, indicate a study area with a rural land use history dating back to the early nineteenth century. A review of federal, provincial, and municipal registers, inventories, and databases revealed that there are two known features of cultural heritage value within the Oxford Road 19 Corridor Improvements study area. An additional 30 features were identified during background research and fieldwork.

6.1 Key Findings

A total of seven built heritage resources (B.H.R.s) and 25 cultural heritage landscapes (C.H.L.s) were identified within the study area.

- Of the 32 identified B.H.R.s and C.H.L.s, two properties were designated under Part IV of the *Ontario Heritage Act* (C.H.L. 13 and C.H.L. 20), one property has an Ontario Heritage Trust plaque (C.H.L. 20), one property is listed in the Ontario Heritage Trust's Places of Worship Inventory (B.H.R. 1), and 29 were identified during background research and field review



(B.H.R. 2 – B.H.R. 7, C.H.L. 1 – C.H.L. 11, C.H.L. 12, C.H.L. 14, C.H.L. 15 – C.H.L. 19, and C.H.L. 21 – C.H.L. 25).

- Identified B.H.R.s and C.H.L.s are historically, architecturally, and contextually associated with land use patterns in the Township of South-West Oxford and the Township of Norwich.

6.2 Results of Preliminary Impact Assessment

The proposed corridor improvements will result in direct adverse impacts to the known or potential heritage attributes of to the following one B.H.R. and four C.H.L.s:

- 225769 Otterville Road (B.H.R. 7),
- 224570 Ostrander Road (C.H.L. 3),
- 225400 Otterville Road (C.H.L. 12),
- 225422 Otterville Road (C.H.L. 13),
- 225947 Otterville Road (C.H.L. 20),

While encroachment, grading, vegetation removal and property acquisition along property frontages are anticipated for many of the identified B.H.R.s and C.H.L.s, the proposed corridor improvements are not anticipated to have direct adverse impacts to the remaining six B.H.R.s and 20 C.H.Ls

Potential vibration impacts as a result of the proposed alignment are anticipated to result in potential impacts to the following four B.H.R.s and nine C.H.L.s:

- 224261 Ostrander Road (B.H.R. 1),
- 224943 Otterville Road (B.H.R. 2),
- 224948 Otterville Road (B.H.R. 3),
- 225769 Otterville Road (B.H.R. 7),
- 224570 Ostrander Road (C.H.L. 3),
- 225227 Otterville Road (C.H.L. 7),
- 225279 Otterville Road (C.H.L. 8),
- 225413 Otterville Road (C.H.L. 11),



- 225400 Otterville Road (C.H.L. 12),
- 225422 Otterville Road (C.H.L. 13),
- 225860 Otterville Road (C.H.L. 18),
- 225877 Otterville Road (C.H.L. 19), and
- 225963 Otterville Road (C.H.L. 21).

6.3 Recommendations

Based on the results of the assessment, the following recommendations have been developed:

1. Construction activities and staging should be suitably planned and undertaken to avoid unintended negative impacts to identified built heritage resources and cultural heritage landscapes. Avoidance measures may include, but are not limited to: erecting temporary fencing, establishing buffer zones, issuing instructions to construction crews to avoid identified B.H.R.s and C.H.L.s, etc. Suitable mitigation measures including post construction rehabilitation with sympathetic plantings can also be implemented.
2. As there are direct adverse impacts anticipated to the following properties: 225769 Otterville Road (B.H.R. 7), 224570 Ostrander Road (C.H.L. 3), 225400 Otterville Road (C.H.L. 12), and given the potential cultural heritage value of those properties, a resource-specific Cultural Heritage Evaluation Report (C.H.E.R.) should be conducted to determine cultural heritage value or interest (C.H.V.I.). As there are direct impacts anticipated, should the C.H.E.R. determine that the property retains C.H.V.I., a resource-specific Heritage Impact Assessment (H.I.A.) should be conducted to evaluate alternatives, assess potential impacts to the resource, and recommend appropriate mitigation measures.
 - a. The C.H.E.R. and H.I.A. should be completed by a qualified heritage professional with recent and relevant experience as early in detailed design as possible.



3. Direct impacts to C.H.L. 13 (225422 Otterville Road) are anticipated to include grading and encroachment onto the northern portion of the property. However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property. As C.H.L. 13 is designated under Part IV of the *Ontario Heritage Act*, and there are direct impacts anticipated, a resource-specific H.I.A. is required as per clause 3.3.2.2 of the *Oxford County Official Plan* (County of Oxford, 2021).⁹ Given that the proposed undertaking is not anticipated to result in direct adverse impacts to the property and no known heritage attributes are anticipated to be impacted, it is recommended that the County of Oxford should consider waiving the requirement for a H.I.A. in this case if suitable mitigation measures including post construction rehabilitation with sympathetic plantings can be implemented.

4. As the property at 225947 Otterville Road (C.H.L. 20) is designated under Part IV of the *Ontario Heritage Act* and direct impacts anticipated due to property acquisition, grading, encroachment onto the northern portion of the property resulting in changes to the parcel boundaries, and removal/relocation of the Ontario Heritage Trust plaque, a resource-specific H.I.A. is required as per clause 3.3.2.2 of the *Oxford County Official Plan* (County of Oxford, 2021). However, encroachment is not anticipated to have a direct adverse impact on the known heritage attributes associated with this property.
 - a. The H.I.A. should be completed by a qualified heritage professional with recent and relevant experience as early in detailed design as possible.

⁹ The proposed mitigation strategies differ between Recommendation 2, and Recommendations 3 and 4, given that clause 3.3.2.2 of the *Oxford County Official Plan* (County of Oxford, 2021) refers specifically to properties designated under the *Ontario Heritage Act* and that if those properties are altered, there is a requirement for a Heritage Impact Assessment.



- b. The Ontario Heritage Trust plaque should be removed prior to construction and stored in a secure facility to prevent damage. Following construction activities, this plaque should be reinstalled at its extant location, or in a similarly accessible location based on consultation with the Township of Norwich and the Ontario Heritage Trust.
5. To ensure the following properties are not adversely impacted during construction, baseline vibration monitoring should be undertaken during detailed design:
 - 224261 Ostrander Road (B.H.R. 1),
 - 224943 Otterville Road (B.H.R. 2),
 - 224948 Otterville Road (B.H.R. 3),
 - 225769 Otterville Road (B.H.R. 7),
 - 224570 Ostrander Road (C.H.L. 3),
 - 225227 Otterville Road (C.H.L. 7),
 - 225279 Otterville Road (C.H.L. 8),
 - 225413 Otterville Road (C.H.L. 11),
 - 225400 Otterville Road (C.H.L. 12),
 - 225422 Otterville Road (C.H.L. 13),
 - 225860 Otterville Road (C.H.L. 18),
 - 225877 Otterville Road (C.H.L. 19), and
 - 225963 Otterville Road (C.H.L. 21).

Should this advance monitoring assessment conclude that the structure(s) on these properties will be subject to vibrations, prepare and implement a vibration monitoring plan as part of the detailed design phase of the project to lessen vibration impacts related to construction.

6. A qualified heritage consultant should be contacted during detailed design to review the designs in order to confirm impacts of the proposed works on the potential C.H.L.s at 225227 Otterville Road (C.H.L. 7), 225659 Otterville Road (C.H.L. 15), 225688 Otterville Road



(C.H.L. 16), and 225720 Otterville Road (C.H.L. 17). This would determine whether there would be any adverse impacts to the properties and any subsequent cultural heritage requirements or reporting.

7. Should future work require an expansion of the study area then a qualified heritage consultant should be contacted in order to confirm the impacts of the proposed work on potential heritage resources.
8. The report should be submitted to the Township of South-West Oxford, the Township of Norwich, and the County of Norfolk and the Ministry of Citizenship and Multiculturalism for review and comment, and any other local heritage stakeholders that may have an interest in this project. The final report should be submitted to the Township of South-West Oxford, the Township of Norwich, and the County of Norfolk for their records.



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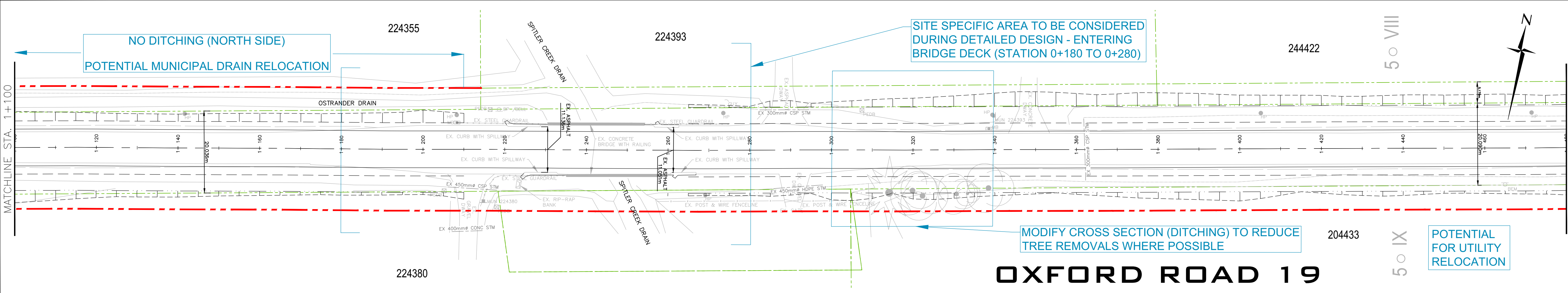
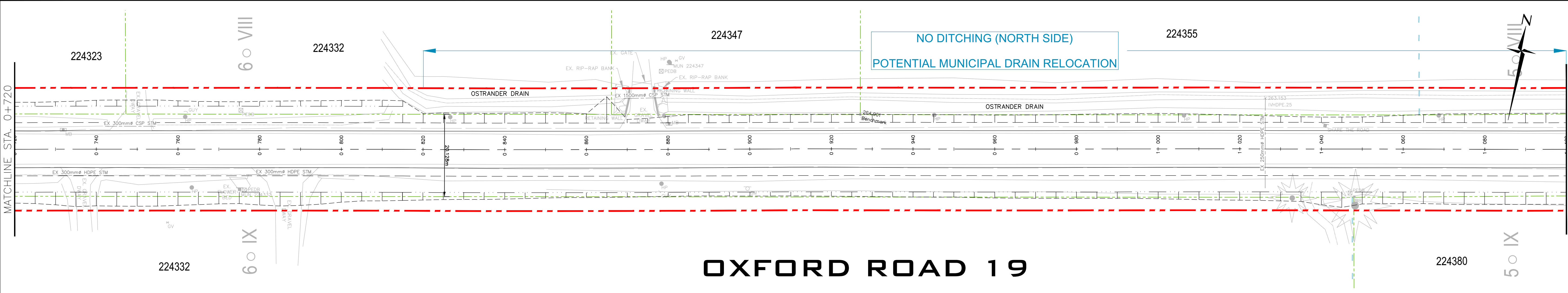
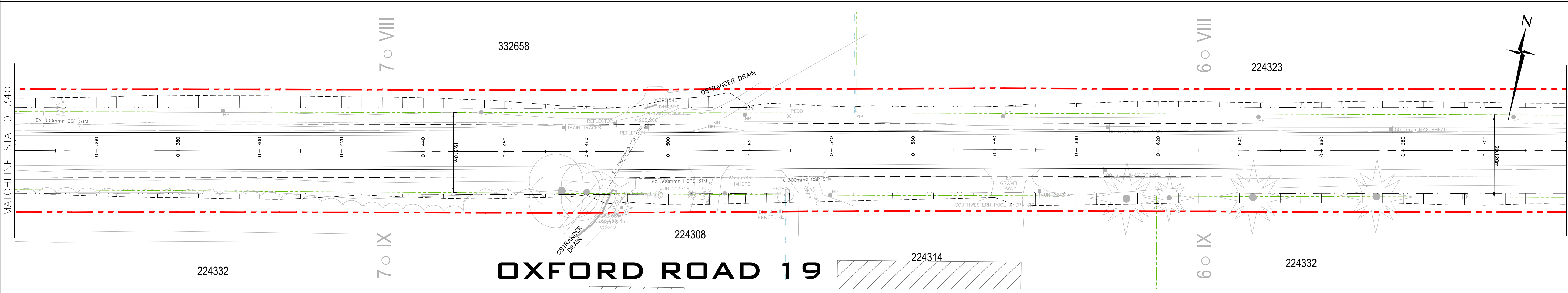
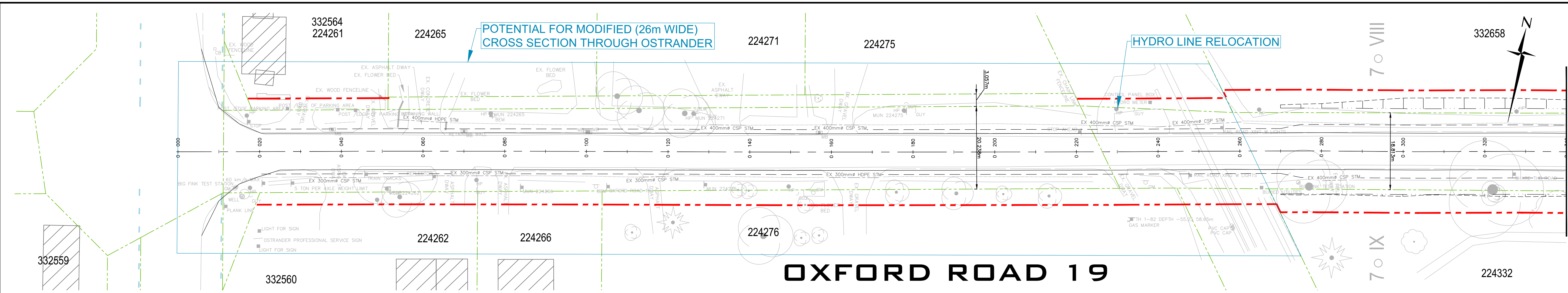
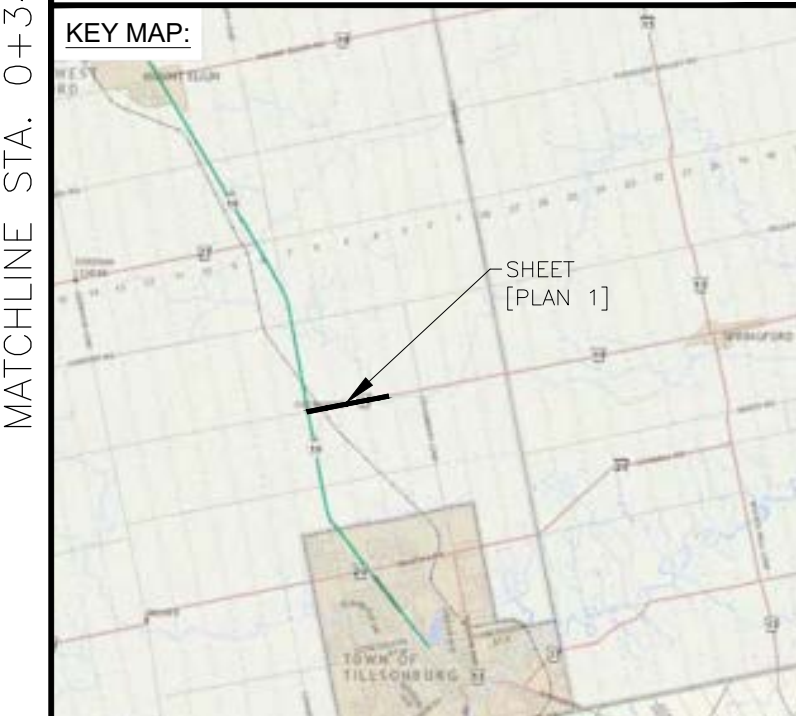
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Appendix A: Roll Plans





LEGEND:

Pr. Property Line	---	Prop. Catchbasin	CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	CO
Pr. Ditchline	---	Prop. Curb Stop	CS
Pr. Edge of Pavement	---	Prop. Ditch Inlet CB	DICB
Ex. Property Line	---	Prop. Double CB	DCB
Ex. Centreline of Ditch	---	Prop. Fire Hydrant	FH
Ex. Sanitary Forcemain	---	Prop. Sanitary Manhole	SAMH
Ex. Storm Sewer	---	Prop. Storm Manhole	STMH
Ex. U/G Telephone	T	Prop. Water Valve	WV
Ex. U/G Cable TV	C	Remove & Dispose	RD
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	CB		
Ex. Clean Out	CO		
Ex. Curb Stop	CS		
Ex. Ditch Inlet CB	DICB		
Ex. Double Catchbasin	DCB		
Ex. Fire Hydrant	FH		
Ex. Sanitary Manhole	SAMH		
Ex. Storm Manhole	STMH		
Ex. Water Valve	WV		
Ex. Cut Cross	---		
Ex. Sign	---		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	---		
Ex. Deciduous Shrub	---		
Ex. Stump	---		
Ex. Coniferous Tree	---		
Ex. Deciduous Tree	---		

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ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
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APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 0+000 TO 1+480

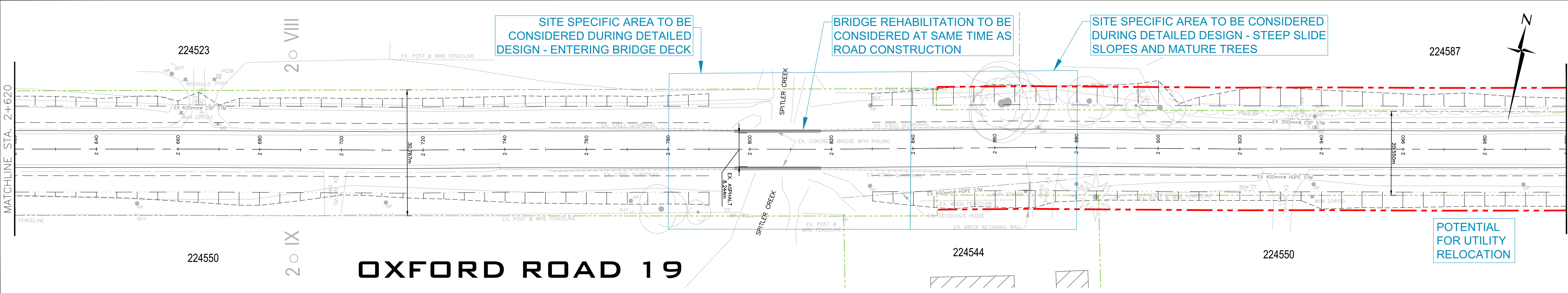
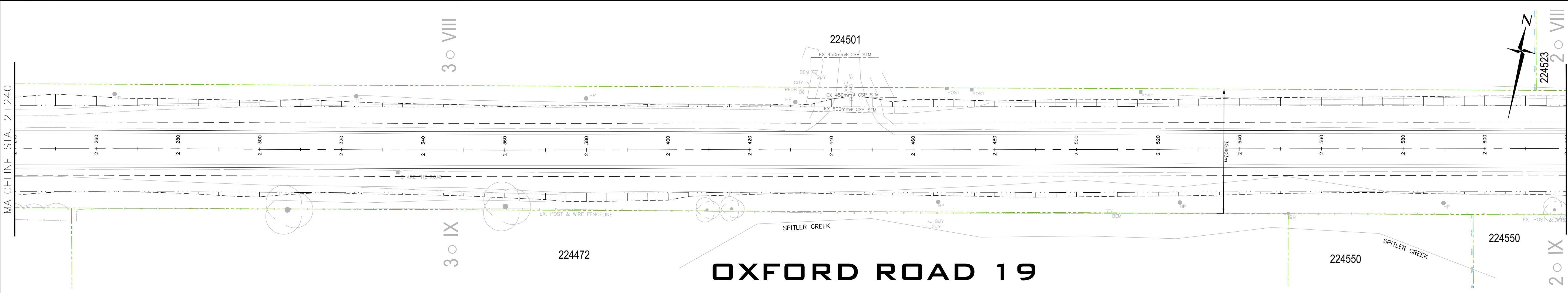
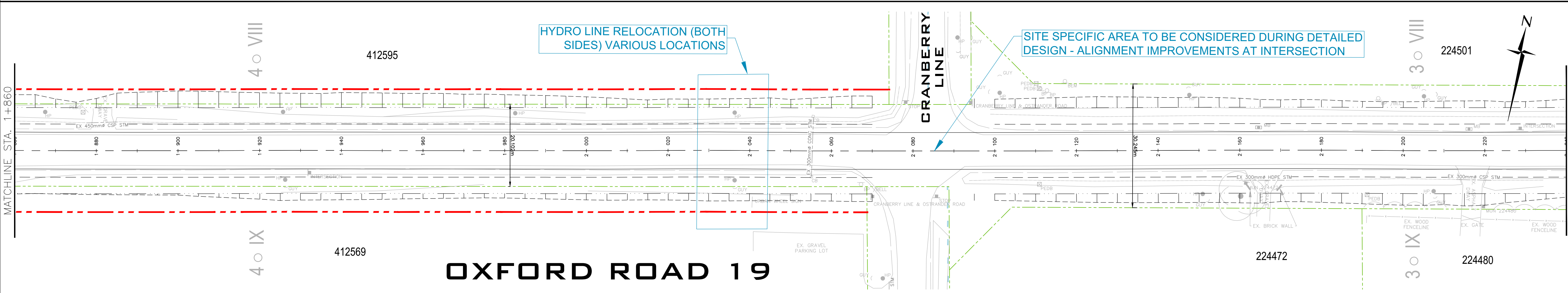
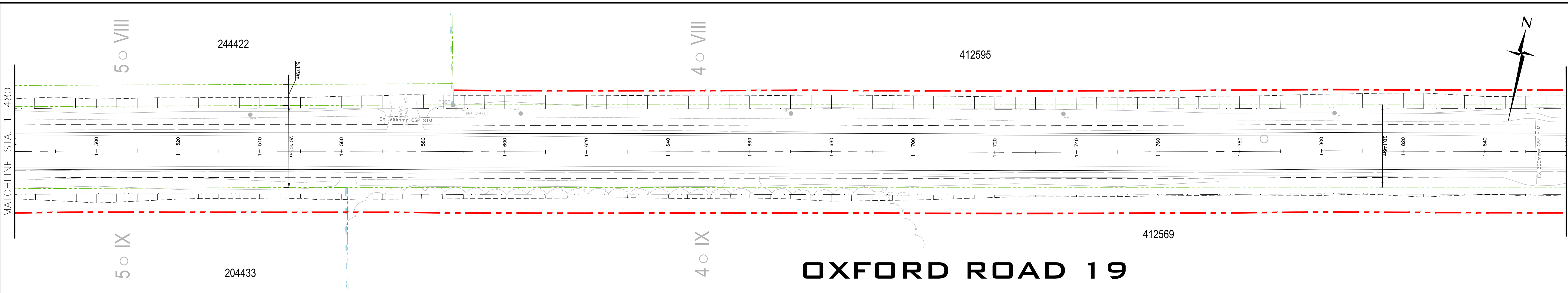
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LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■
Pr. Top Bank/Matchline	---	Prop. Clean Out	●
Pr. Ditchline	---	Prop. Curb Stop	●
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	■
Pr. Centreline of Ditch	---	Prop. Double CB	■
Ex. Property Line	---	Prop. Fire Hydrant	●
Ex. Sanitary Forceman	---	Prop. Sanitary Manhole	●
Ex. Storm Sewer	---	Prop. Storm Manhole	●
Ex. U/G Telephone	---	Prop. Water Valve	●
Ex. U/G Cable TV	---	Remove & Dispose	⊗
Ex. U/G Gas	---		
Ex. U/G Hydro	---		
Ex. U/G Traffic Wiring	---		
Ex. Watermain	---		
Ex. Catchbasin	□	Prop. Catchbasin	■
Ex. Clean Out	○	Prop. Clean Out	●
Ex. Curb Stop	○	Prop. Curb Stop	●
Ex. Ditch Inlet CB	□	Prop. Ditch Inlet CB	■
Ex. Double Catchbasin	□	Prop. Double CB	■
Ex. Fire Hydrant	○	Prop. Fire Hydrant	●
Ex. Sanitary Manhole	○	Prop. Sanitary Manhole	●
Ex. Storm Manhole	○	Prop. Storm Manhole	●
Ex. Water Valve	○	Prop. Water Valve	●
Ex. Cut Cross	⊗	Remove & Dispose	⊗
Ex. Sign	⊗		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		



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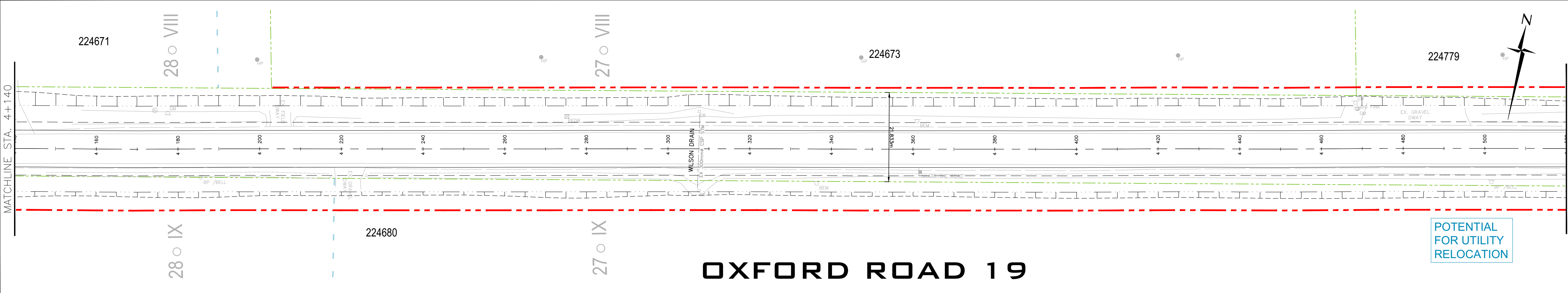
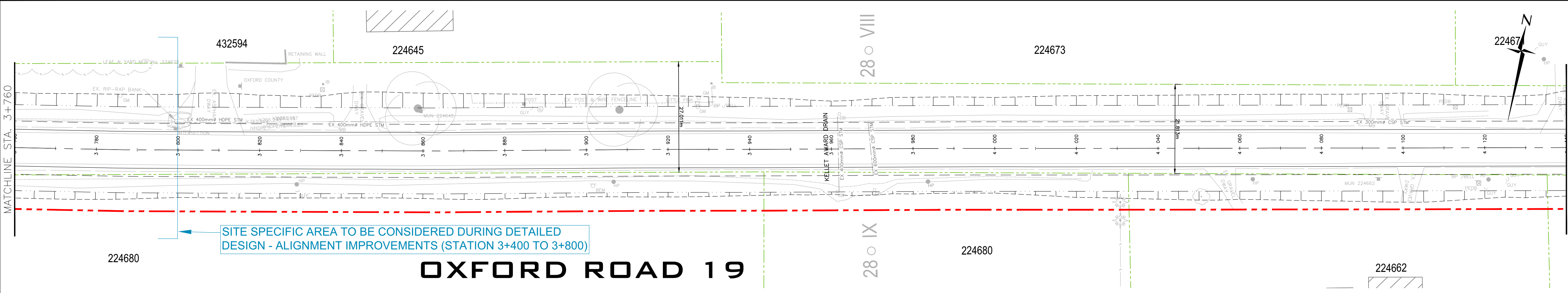
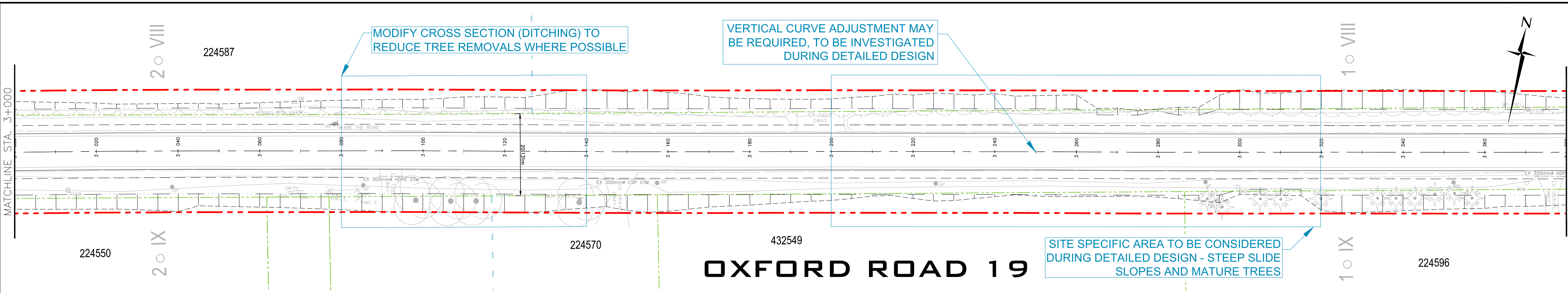
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OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 1+480 TO 3+000

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Pr. Top Bank/Matchline	---	Prop. Clean Out	CO
Pr. Ditchline	---	Prop. Curb Stop	CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	DICB
Pr. Edge of Pavement	---	Prop. Double CB	DCB
Ex. Property Line	---	Prop. Fire Hydrant	FH
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	SAMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	WV
Ex. Storm Sewer	---	Remove & Dispose	RD
Ex. U/G Telephone	T		
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TR		
Ex. Watermain	---		
Ex. Catchbasin	CB		
Ex. Clean Out	CO		
Ex. Curb Stop	CS		
Ex. Ditch Inlet CB	DICB		
Ex. Double Catchbasin	DCB		
Ex. Fire Hydrant	FH		
Ex. Sanitary Manhole	SAMH		
Ex. Storm Manhole	STMH		
Ex. Water Valve	WV		
Ex. Cut Cross	---		
Ex. Sign	---		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	---		
Ex. Deciduous Shrub	---		
Ex. Stump	---		
Ex. Coniferous Tree	---		
Ex. Deciduous Tree	---		

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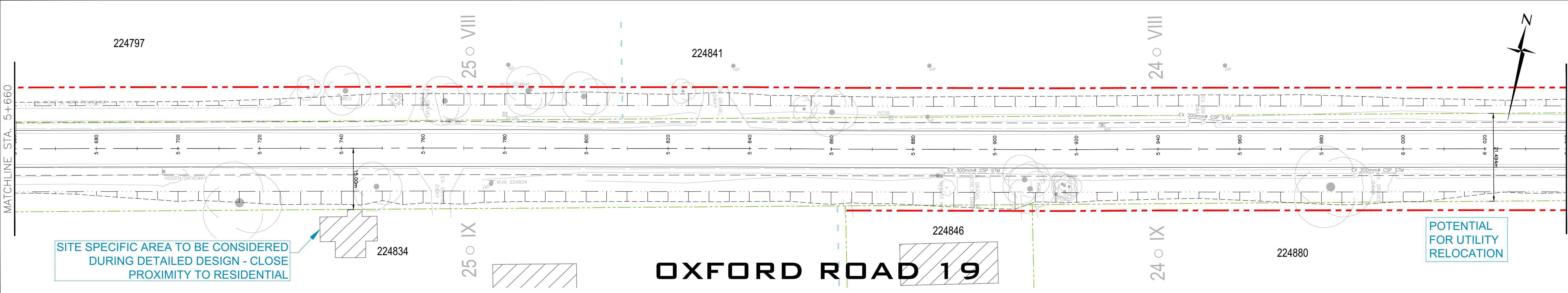
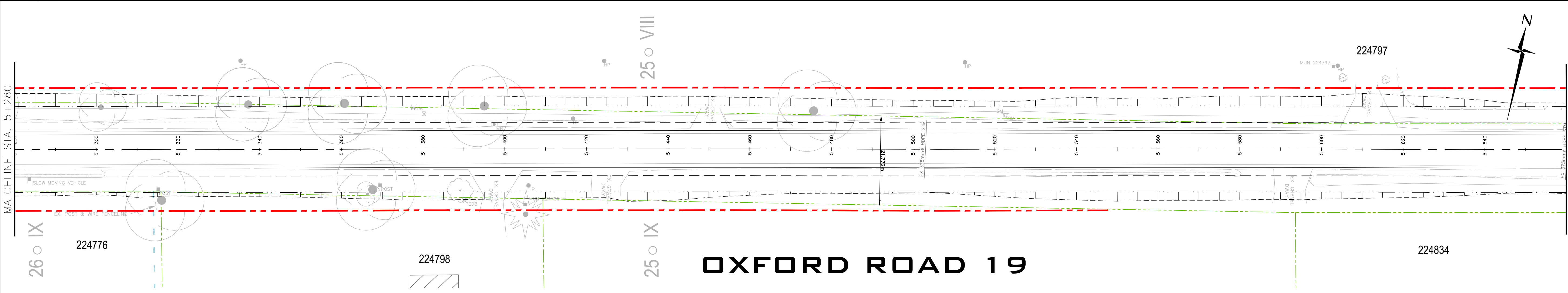
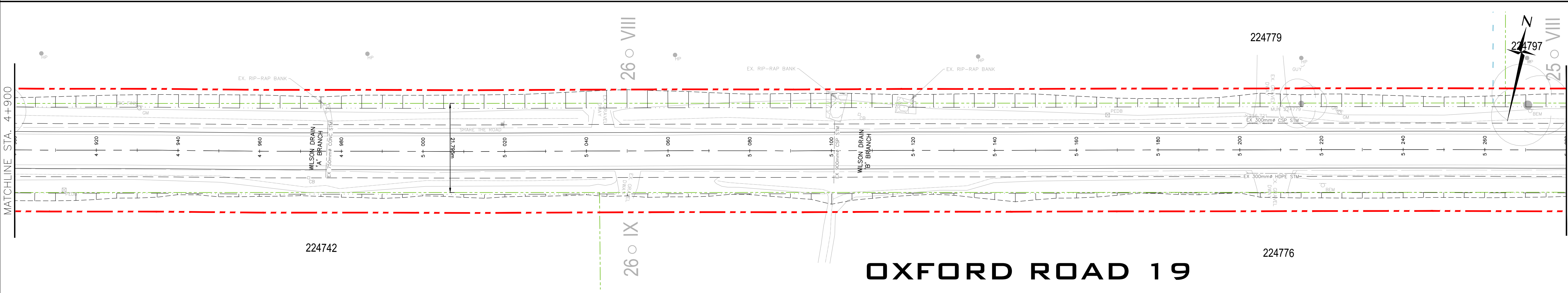
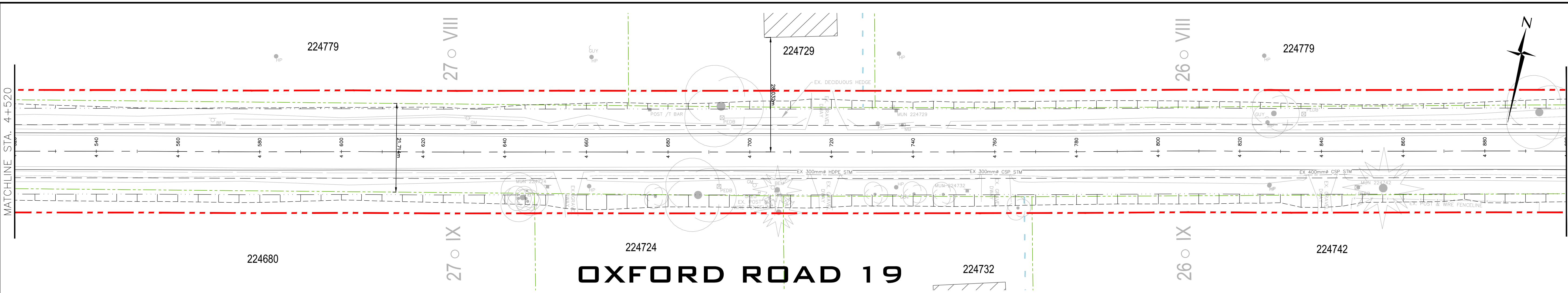
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LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■ CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	● CO
Pr. Ditchline	---	Prop. Curb Stop	● CS
Pr. Edge of Pavement	---	Prop. Ditch Inlet CB	■ DICB
Ex. Property Line	---	Prop. Double CB	■ DCB
Ex. Centreline of Ditch	---	Prop. Fire Hydrant	● FHY
Ex. Sanitary Forcemain	---	Prop. Sanitary Manhole	● SAMH
Ex. Sanitary Sewer	---	Prop. Storm Manhole	● STMH
Ex. Storm Sewer	---	Prop. Water Valve	● WV
Ex. U/G Telephone	---	Remove & Dispose	⊗
Ex. U/G Cable TV	---		
Ex. U/G Gas	---		
Ex. U/G Hydro	---		
Ex. U/G Traffic Wiring	---		
Ex. Watermain	---		
Ex. Catchbasin	□ XCB		
Ex. Clean Out	○ CO		
Ex. Curb Stop	○ CSC		
Ex. Ditch Inlet CB	□ DICB		
Ex. Double Catchbasin	□ DCB		
Ex. Fire Hydrant	○ FHY		
Ex. Sanitary Manhole	○ SAMH		
Ex. Storm Manhole	○ STMH		
Ex. Water Valve	○ WV		
Ex. Cut Cross	⊗		
Ex. Sign	⊗		
Ex. Guy Wire	⊗		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		

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DATE: NOV 10, 2022	

**OXFORD ROAD 19
 ENVIRONMENTAL
 ASSESSMENT**

PLAN 4+520 TO 6+040

HOR. SCALE - 1:500
 STN: 4+520 TO: 6+040
 CONTRACT NO:
 DWG NO: PLAN 4

SITE SPECIFIC AREA TO BE CONSIDERED
 DURING DETAILED DESIGN - CLOSE
 PROXIMITY TO RESIDENTIAL

POTENTIAL
 FOR UTILITY
 RELOCATION



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■ CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	● CO
Pr. Ditchline	---	Prop. Curb Stop	● CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	■ DICB
Pr. Edge of Pavement	---	Prop. Double CB	■ DCB
Ex. Property Line	---	Prop. Fire Hydrant	● FHY
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	● SAMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	● STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	● WVAL
Ex. Storm Sewer	---	Remove & Dispose	⊗
Ex. U/G Telephone	---		
Ex. U/G Cable TV	---		
Ex. U/G Gas	---		
Ex. U/G Hydro	---		
Ex. U/G Traffic Wiring	---		
Ex. Watermain	---		
Ex. Catchbasin	□ XCB		
Ex. Clean Out	○ XCO		
Ex. Curb Stop	○ XCS		
Ex. Ditch Inlet CB	□ XDICB		
Ex. Double Catchbasin	□ XDCB		
Ex. Fire Hydrant	○ XSAMH		
Ex. Sanitary Manhole	○ XSAMH		
Ex. Storm Manhole	○ XSTMH		
Ex. Water Valve	○ XWVAL		
Ex. Cut Cross	⊗		
Ex. Sign	⊗		
Ex. Guy Wire	⊗		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		

ISSUED FOR PCC#2	3	DEC 6, 2022	HC
ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
BENCHMARK #	ELEVATION=m		

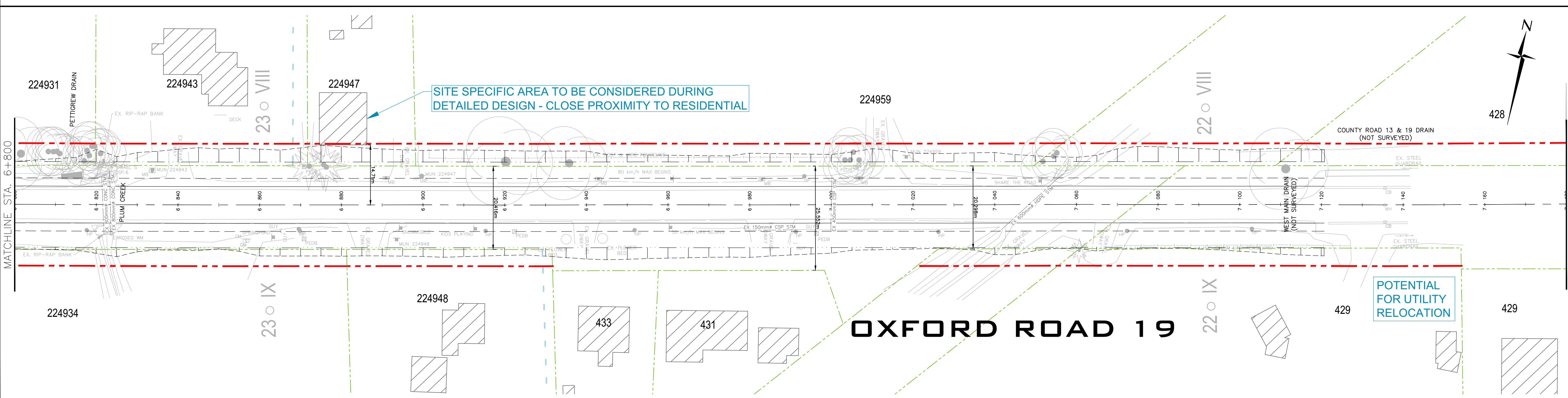
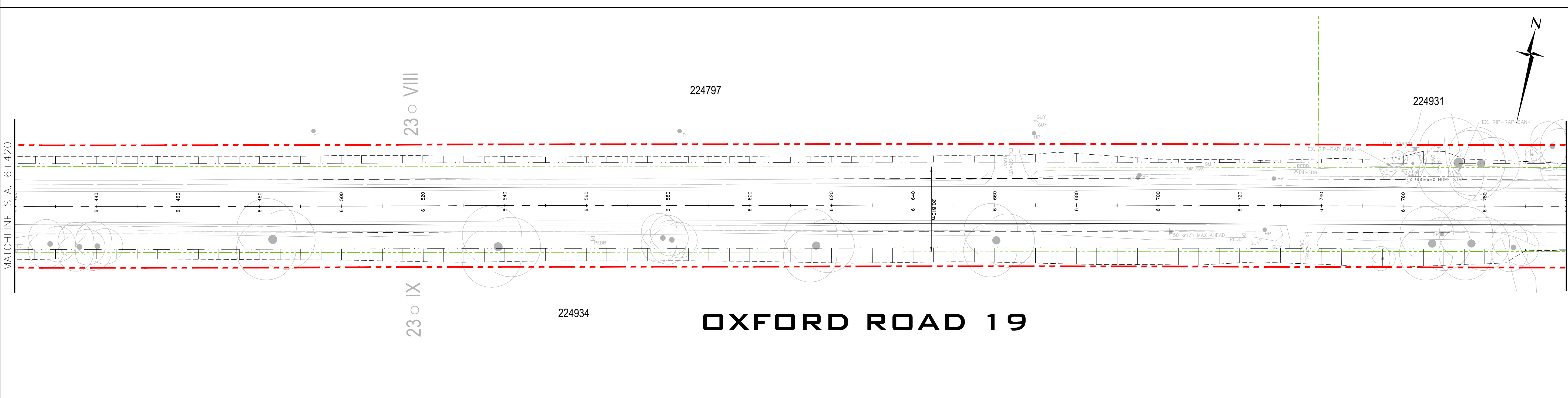
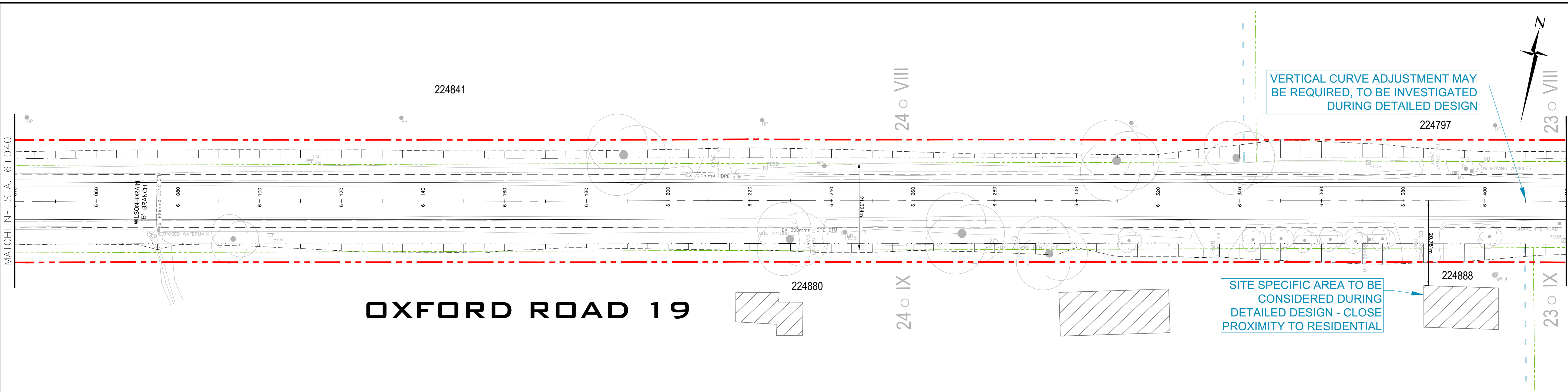
AREA MUNICIPALITY:
Township of Norwich
285767 Airport Road, P.O. Box 100
Norwich, Ontario, N0J 1P0
Telephone (519) 468-2410
Fax (519) 468-2414
Web www.norwich.ca

CONSULTANT:
BURNSIDE
R.J. Burnside & Associates Limited
35 Perry Street
Woodstock, Ontario, N4S 3C4
Telephone (519) 271-5111
Fax (519) 271-5750
Web www.rjburnside.com

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CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
PLAN 6+040 TO 7+180

HOR. SCALE - 1:500
STN: 6+040 TO: 7+180
CONTRACT NO:
DWG NO: PLAN 5



file: \\SHERIDAN\Shared Work Areas\03425 Oxford Road 19\03_Production\Draw\03425 PLANS-1.dwg
layout: PLAN 6+040 TO 7+180 date: Dec 06, 2022 14:42 user: Meemom



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forceman	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	CB
Prop. Catchbasin	CB
Ex. Clean Out	CO
Prop. Clean Out	CO
Ex. Curb Stop	CS
Prop. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Prop. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Prop. Double CB	DCB
Ex. Fire Hydrant	FH
Prop. Fire Hydrant	FH
Ex. Sanitary Manhole	SAMH
Prop. Sanitary Manhole	SAMH
Ex. Storm Manhole	STMH
Prop. Storm Manhole	STMH
Ex. Water Valve	WV
Prop. Water Valve	WV
Ex. Cut Cross	CC
Remove & Dispose	RD
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CS
Ex. Deciduous Shrub	DS
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT

ISSUED FOR PC#2	3	DEC 6, 2022	HC
ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
BENCHMARK #	ELEVATION=m		

AREA MUNICIPALITY:

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 285767 Airport Road, P.O. Box 100
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 telephone (519) 468-2410
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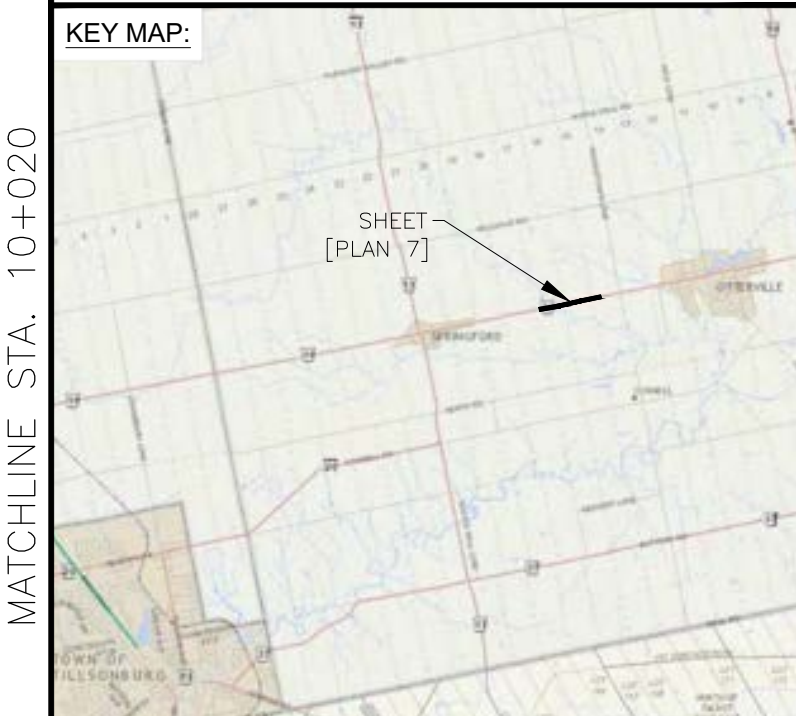
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APPROVED BY: HC	
DATE: NOV 10, 2022	

**OXFORD ROAD 19
 ENVIRONMENTAL
 ASSESSMENT**

PLAN 8+500 TO 9+640

HOR. SCALE - 1:500
 STN: 8+500 TO: 9+640
 CONTRACT NO:
 DWG NO: PLAN 6

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 Layout: PLAN 8+500 TO 9+640 date: Dec 06, 2022 14:44 user: Melemon



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	CB
Ex. Clean Out	CO
Ex. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Ex. Fire Hydrant	FH
Ex. Sanitary Manhole	SAMH
Ex. Storm Manhole	STMH
Ex. Water Valve	WV
Ex. Cut Cross	CC
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CSH
Ex. Deciduous Shrub	DSH
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT

ISSUED FOR PCC#2	3	DEC 6, 2022	HC
ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
BENCHMARK #	ELEVATION=m		

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285767 Airport Road, P.O. Box 100
Norwich, Ontario, N0J 1P0
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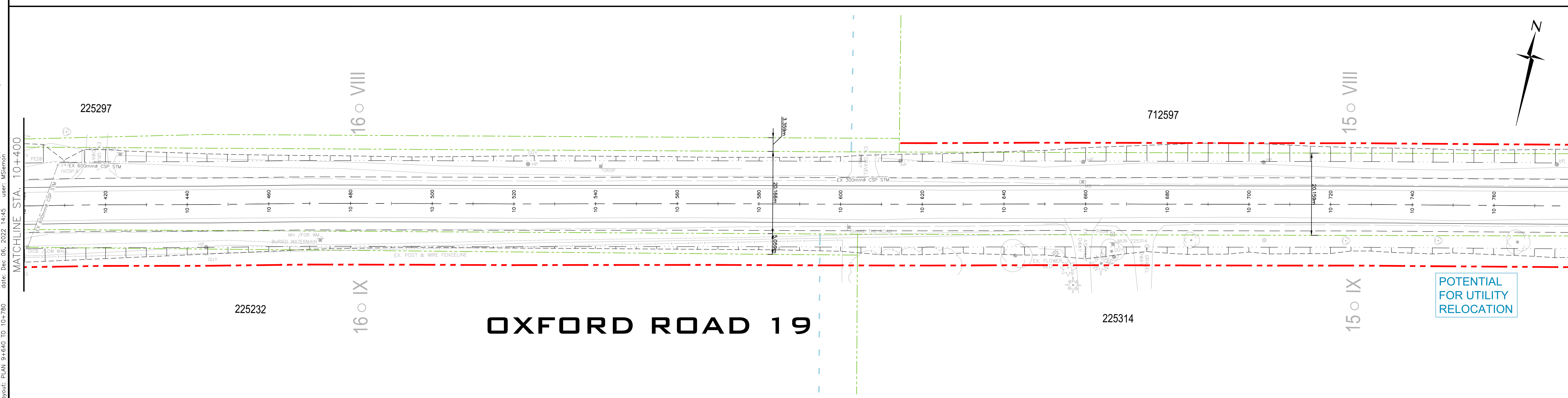
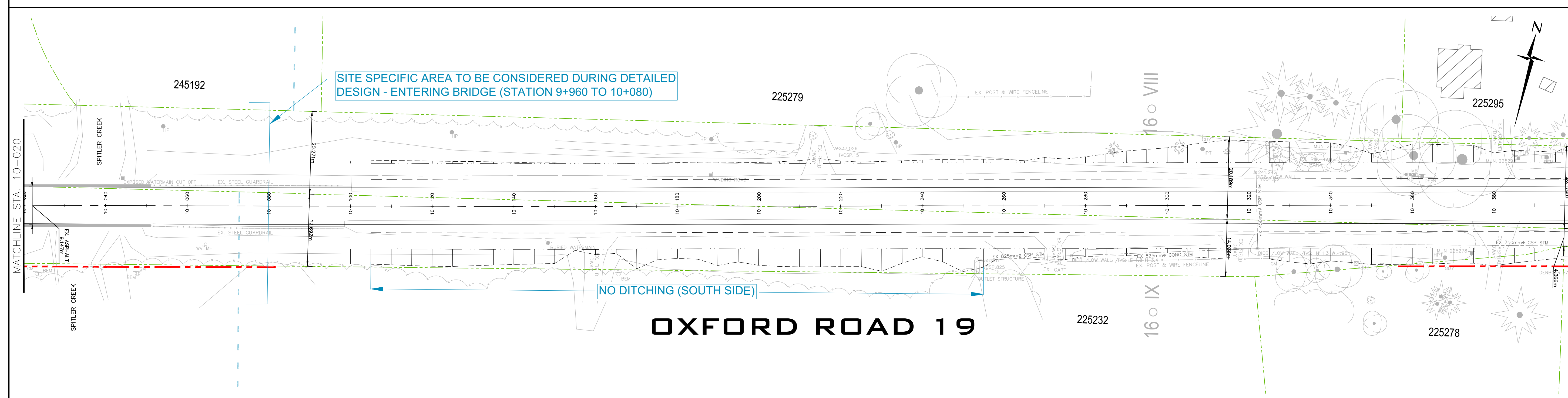
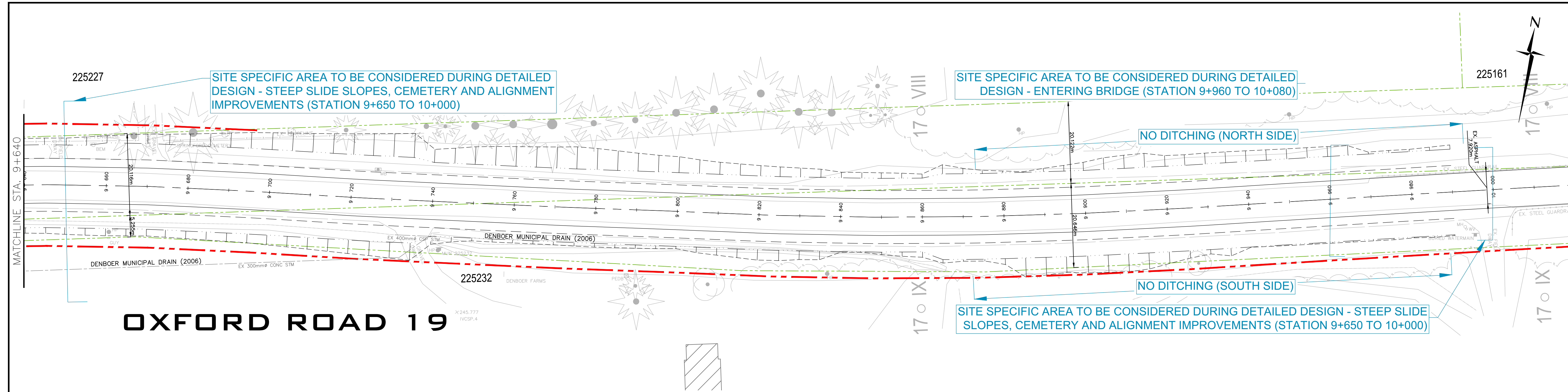
CONSULTANT:
BURNSIDE
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DRAWN BY: MS	
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APPROVED BY: HC	
DATE: NOV 10, 2022	

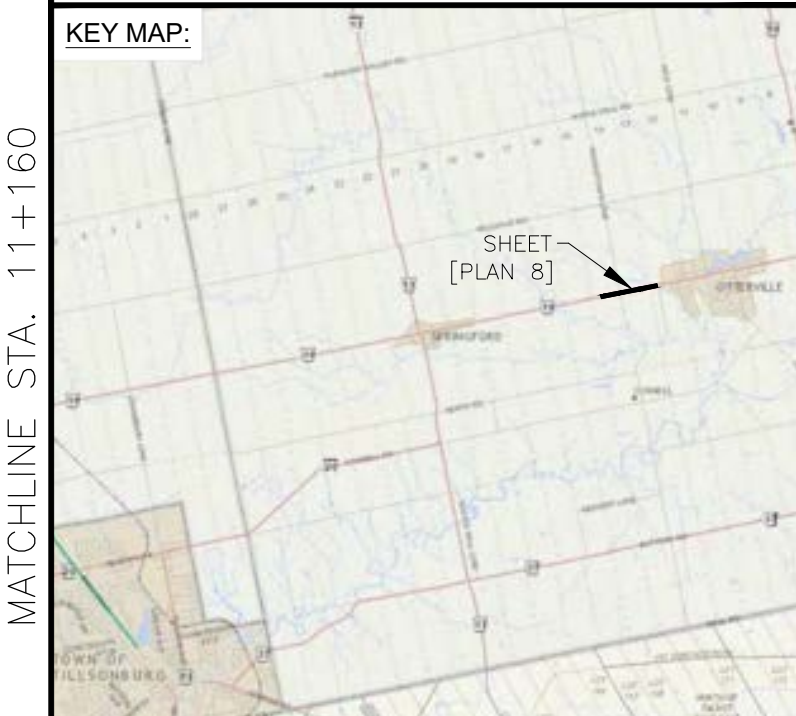
**OXFORD ROAD 19
ENVIRONMENTAL
ASSESSMENT**

PLAN 9+640 TO 10+780

HOR. SCALE - 1:500
STN: 9+640 TO: 10+780
CONTRACT NO:
DWG NO: PLAN 7

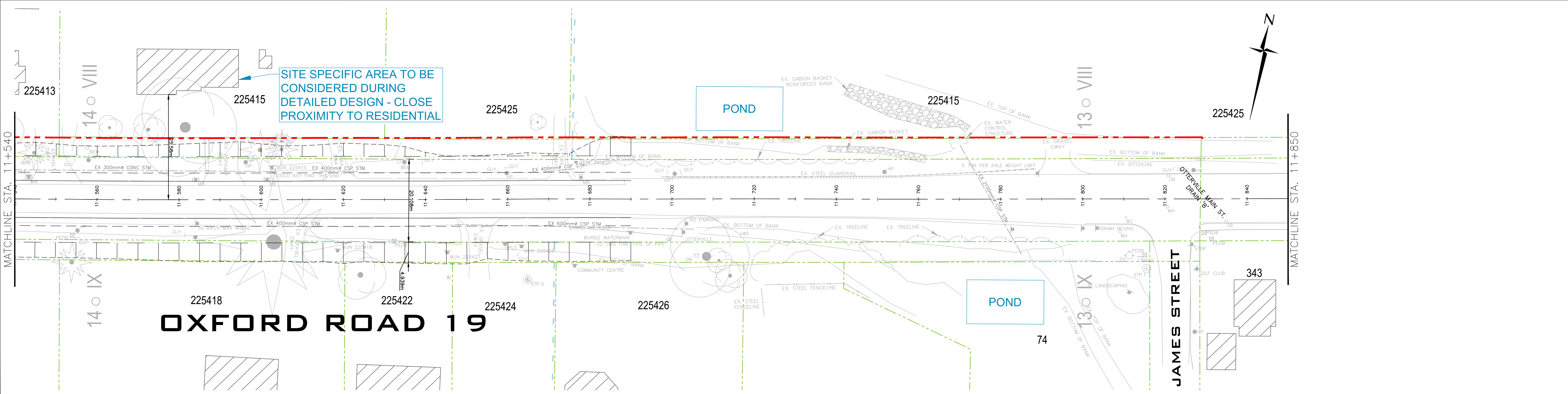
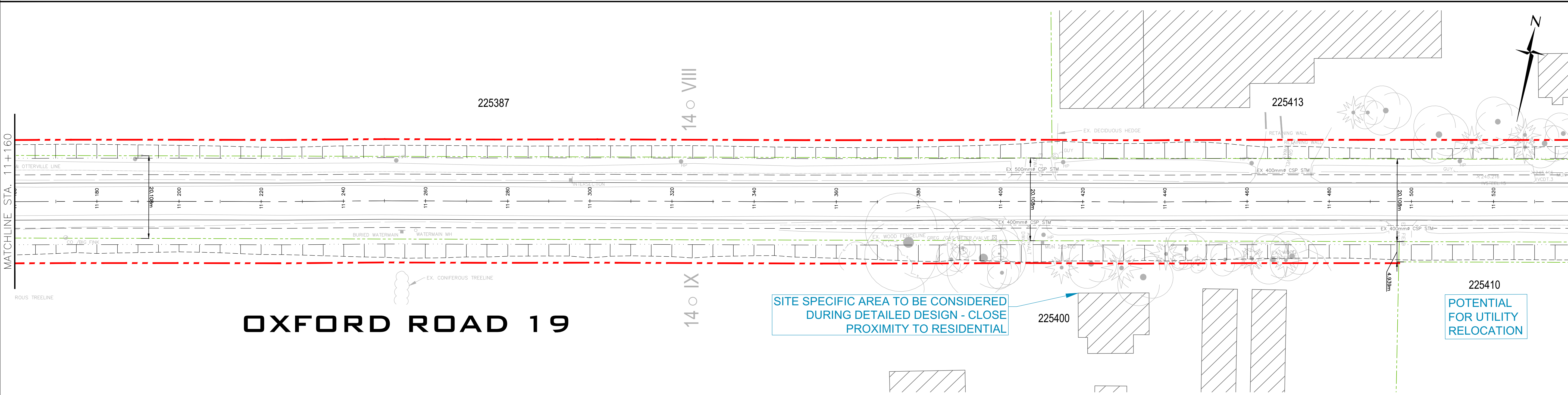
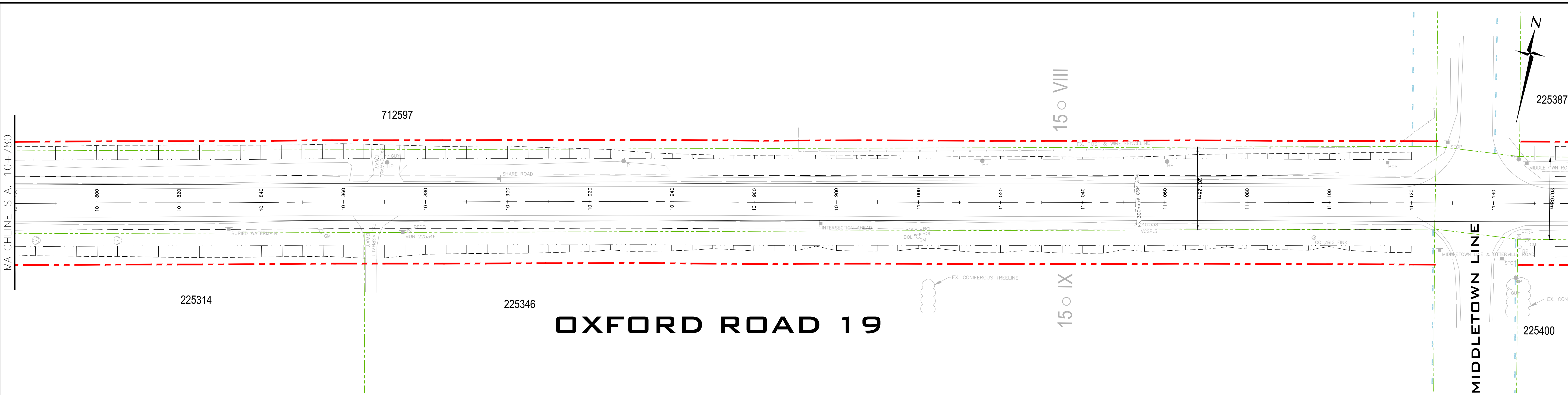


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Layout: PLAN 9+640 TO 10+780 date: Dec 06, 2022 14:25 user: MStrom



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	---
Ex. U/G Cable TV	---
Ex. U/G Gas	---
Ex. U/G Hydro	---
Ex. U/G Traffic Wiring	---
Ex. Watermain	---
Ex. Catchbasin	---
Ex. Clean Out	---
Ex. Curb Stop	---
Ex. Ditch Inlet CB	---
Ex. Double Catchbasin	---
Ex. Fire Hydrant	---
Ex. Sanitary Manhole	---
Ex. Storm Manhole	---
Ex. Water Valve	---
Ex. Cut Cross	---
Ex. Sign	---
Ex. Guy Wire	---
Ex. Coniferous Shrub	---
Ex. Deciduous Shrub	---
Ex. Stump	---
Ex. Coniferous Tree	---
Ex. Deciduous Tree	---
Prop. Catchbasin	---
Prop. Clean Out	---
Prop. Curb Stop	---
Prop. Ditch Inlet CB	---
Prop. Double CB	---
Prop. Fire Hydrant	---
Prop. Sanitary Manhole	---
Prop. Storm Manhole	---
Prop. Water Valve	---
Remove & Dispose	---



ISSUED FOR PCC#2	3	DEC 6, 2022	HC
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ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:

BENCHMARK #	ELEVATION=m
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AREA MUNICIPALITY:
Township of Norwich
285767 Airport Road, P.O. Box 100
Norwich, Ontario, N0J 1P0
telephone (519) 468-2410
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web www.norwich.ca

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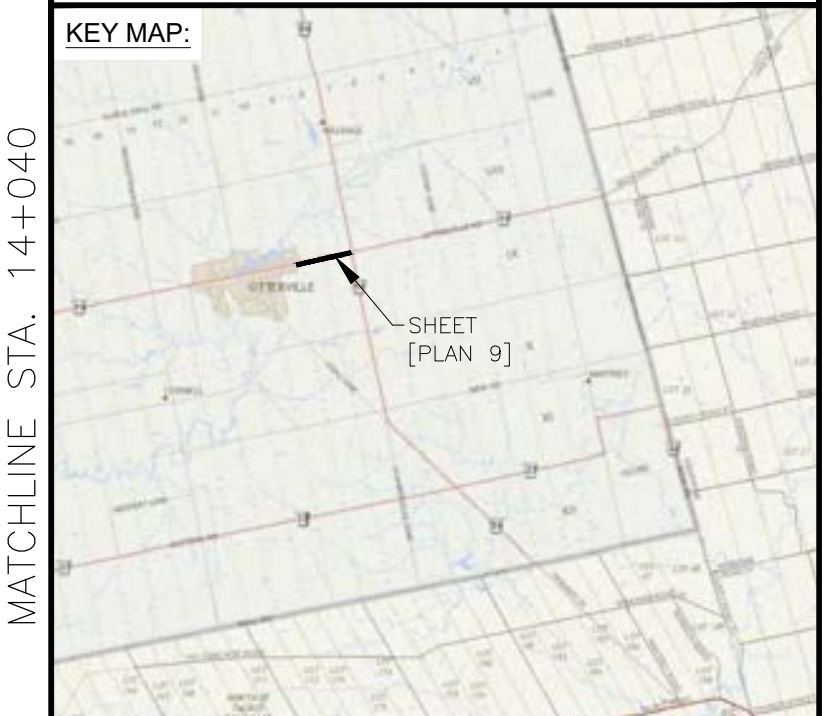
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APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 10+780 TO 11+850

HOR. SCALE - 1:500
STN: 10+780 TO: 11+850
CONTRACT NO:
DWG NO: PLAN 8

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layout: PLAN 10+780 TO 11+850 date: Dec 06, 2022 14:46 user: MSimon



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	CB
Prop. Catchbasin	CB
Ex. Clean Out	CO
Prop. Clean Out	CO
Ex. Curb Stop	CS
Prop. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Prop. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Prop. Double Catchbasin	DCB
Ex. Fire Hydrant	FH
Prop. Fire Hydrant	FH
Ex. Sanitary Manhole	SAMH
Prop. Sanitary Manhole	SAMH
Ex. Storm Manhole	STMH
Prop. Storm Manhole	STMH
Ex. Water Valve	WV
Prop. Water Valve	WV
Ex. Cut Cross	CC
Remove & Dispose	RD
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CSH
Ex. Deciduous Shrub	DSH
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT

ISSUED FOR PC#2	3	DEC 6, 2022	HC
ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:

BENCHMARK #	ELEVATION=
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AREA MUNICIPALITY:

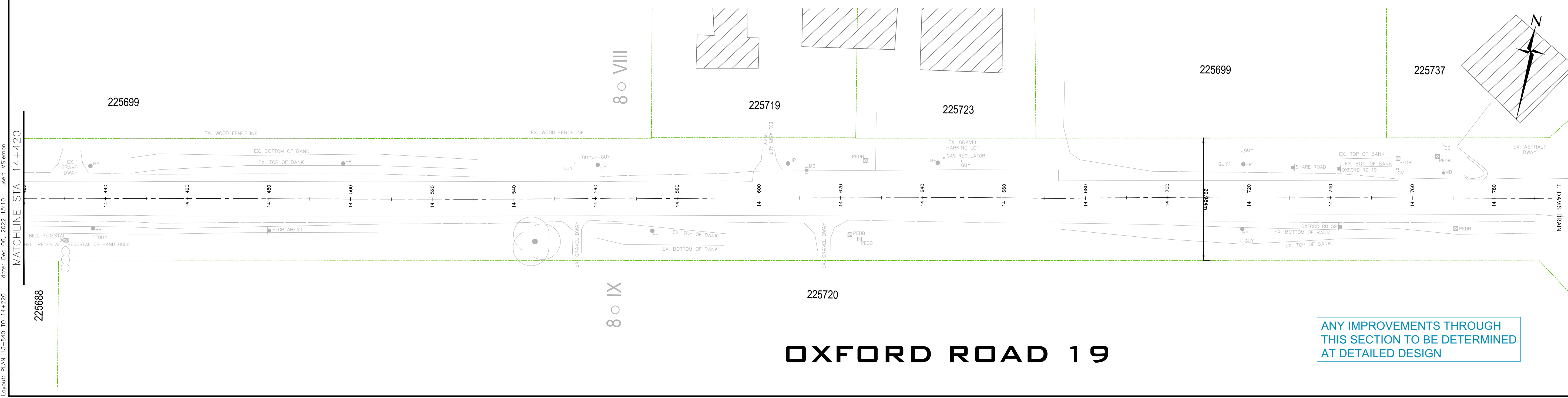
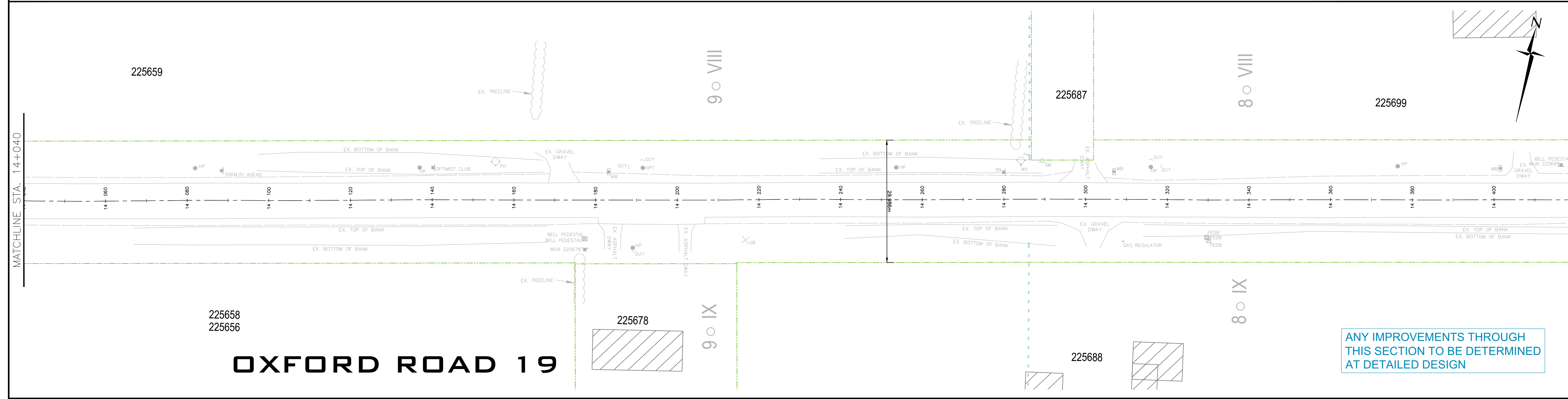
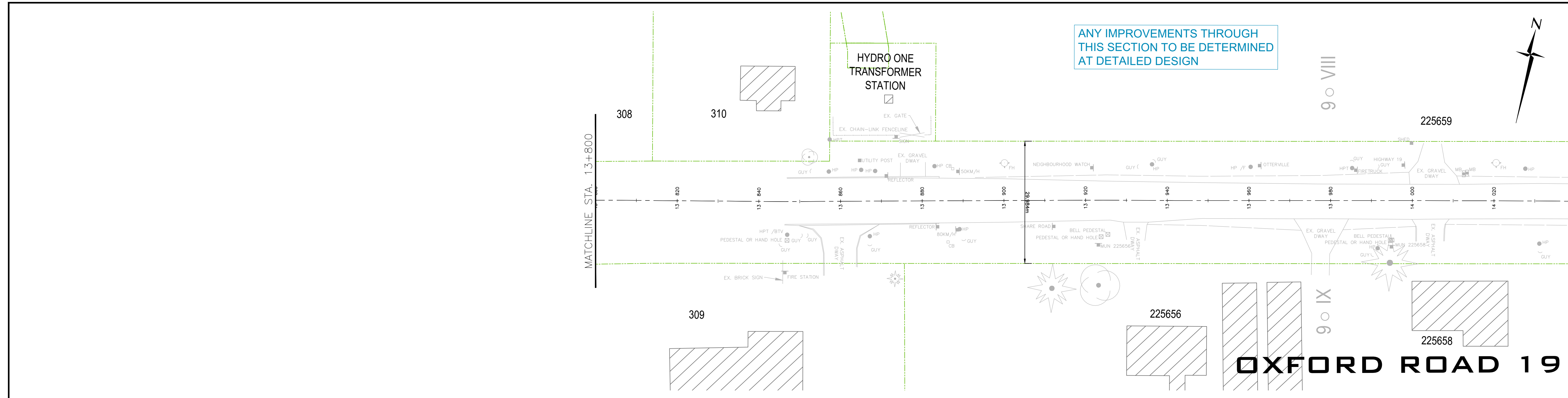
Township of Norwich
285767 Airport Road, P.O. Box 100
Norwich, Ontario, N0J 1P0
telephone (519) 468-2410
fax (519) 468-2414
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CONSULTANT:

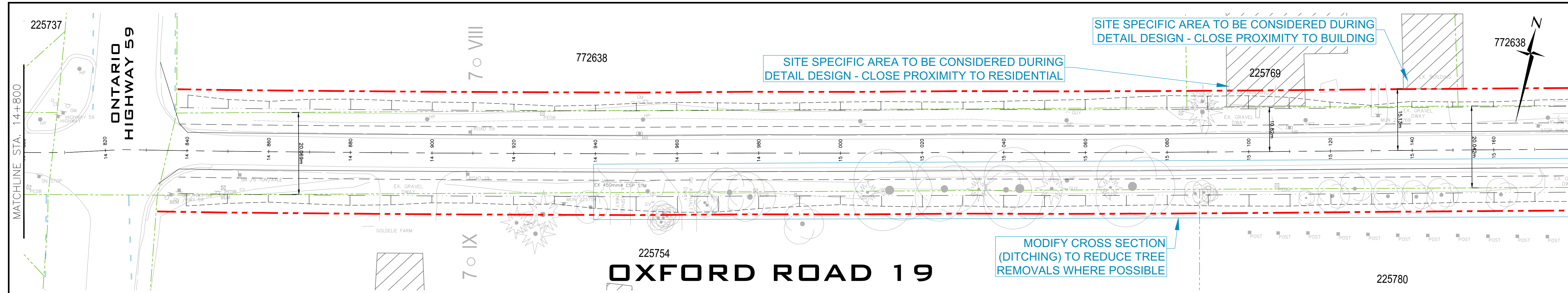
BURNSIDE
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OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
PLAN 13+840 TO 14+800	
HOR. SCALE - 1:500	
STN: 13+840	TO: 14+800
CONTRACT NO:	
DWG NO: PLAN 9	

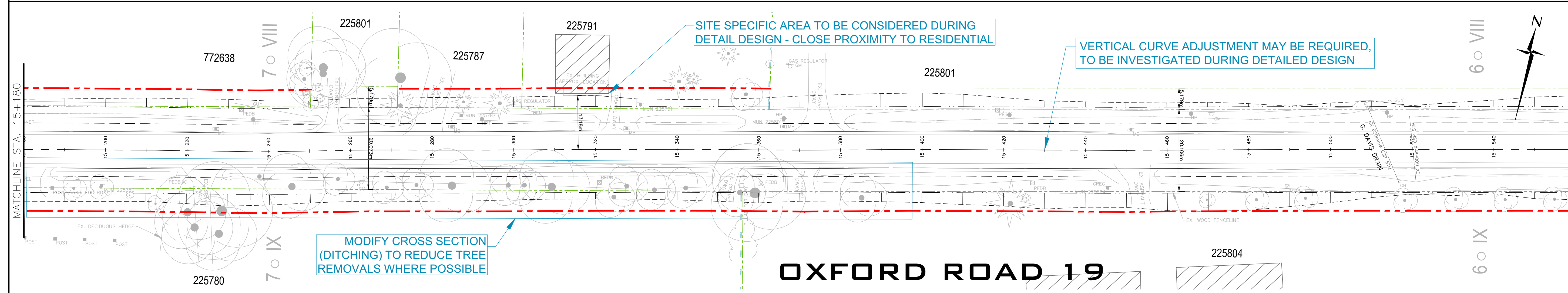


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layout: PLAN 13+840 TO 14+800 date: Dec 06, 2022 15:10 user: MStem



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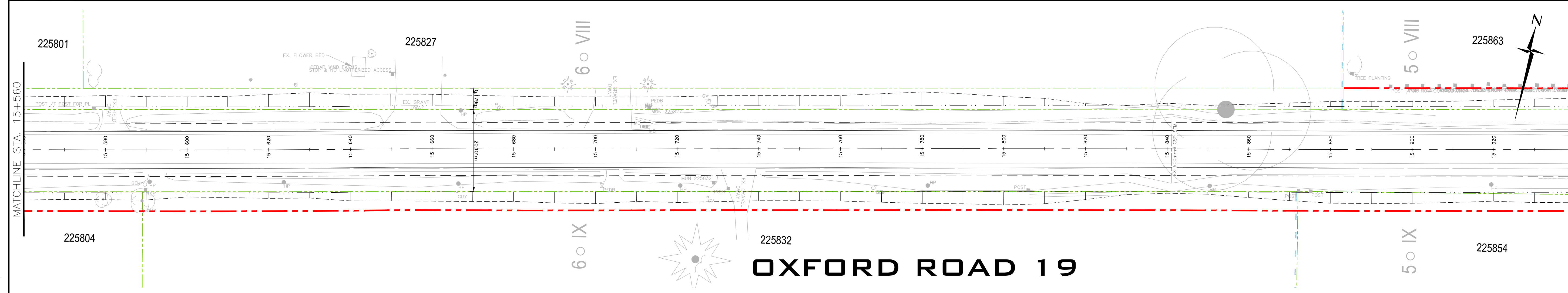
KEY MAP:



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	---
Ex. U/G Cable TV	---
Ex. U/G Gas	---
Ex. U/G Hydro	---
Ex. U/G Traffic Wiring	---
Ex. Watermain	---
Ex. Catchbasin	---
Ex. Clean Out	---
Ex. Curb Stop	---
Ex. Ditch Inlet CB	---
Ex. Double Catchbasin	---
Ex. Fire Hydrant	---
Ex. Sanitary Manhole	---
Ex. Storm Manhole	---
Ex. Water Valve	---
Ex. Cut Cross	---
Ex. Sign	---
Ex. Guy Wire	---
Ex. Coniferous Shrub	---
Ex. Deciduous Shrub	---
Ex. Stump	---
Ex. Coniferous Tree	---
Ex. Deciduous Tree	---

Prop. Catchbasin	CB
Prop. Clean Out	CO
Prop. Curb Stop	CS
Prop. Ditch Inlet CB	DICB
Prop. Double CB	DCB
Prop. Fire Hydrant	FH
Prop. Sanitary Manhole	SAMH
Prop. Storm Manhole	STMH
Prop. Water Valve	WV
Remove & Dispose	R&D



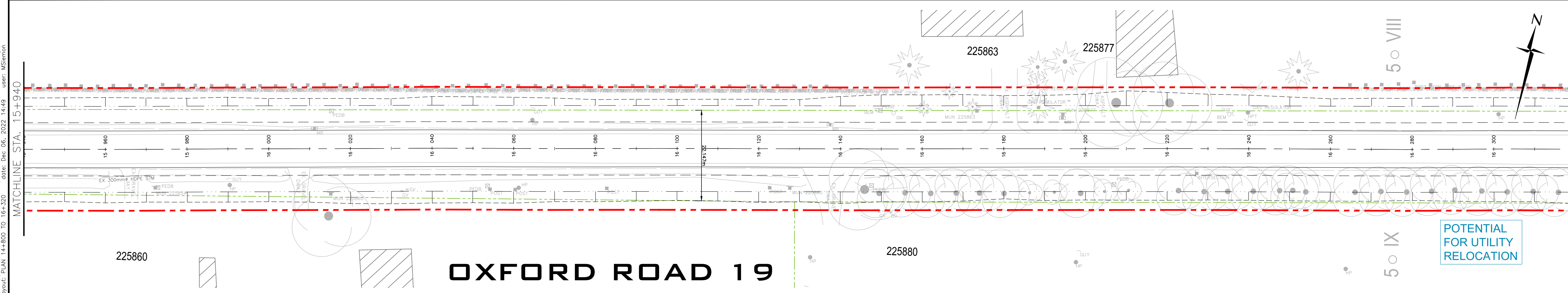
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ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:
 BENCHMARK # ELEVATION=m

AREA MUNICIPALITY:
 Township of Norwich
 285767 Airport Road, P.O. Box 100
 Norwich, Ontario, N0J 1P0
 telephone (519) 468-2410
 fax (519) 468-2414
 web www.norwich.ca

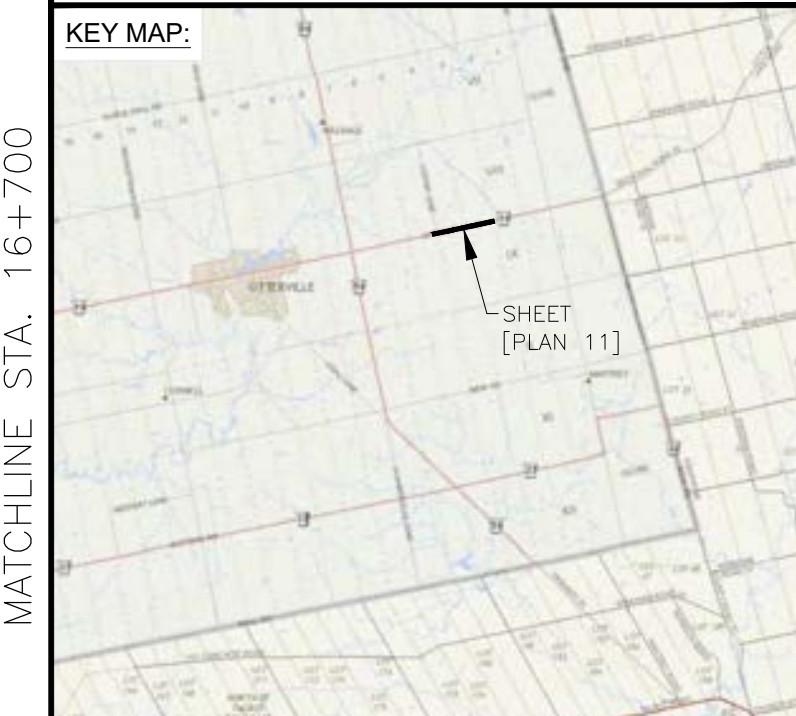
CONSULTANT:

 R.J. Burnside & Associates Limited
 35 Perry Street
 Woodstock, Ontario, N4S 3C4
 telephone (519) 271-5111
 fax (519) 271-5730
 web www.rjburnside.com



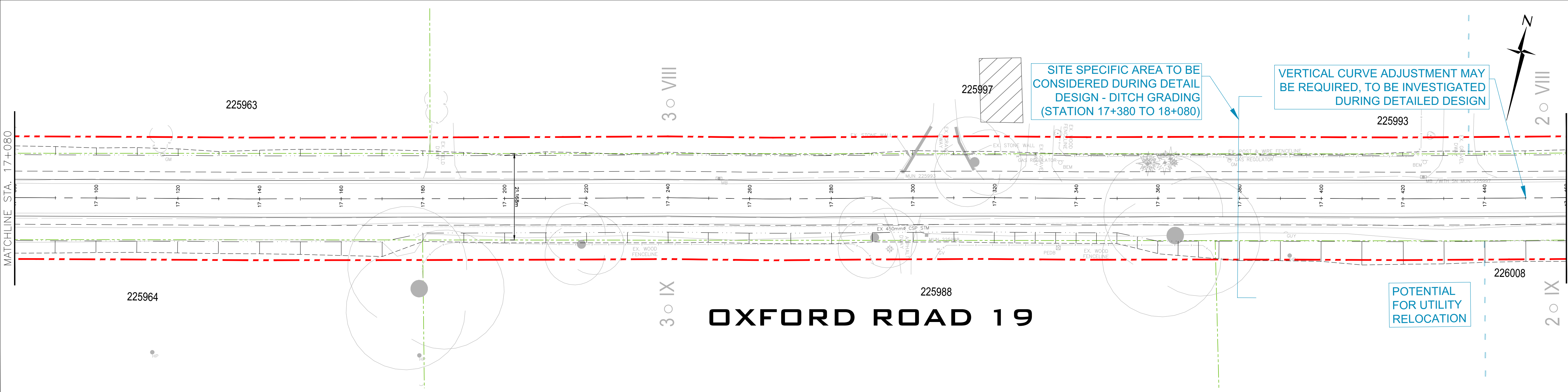
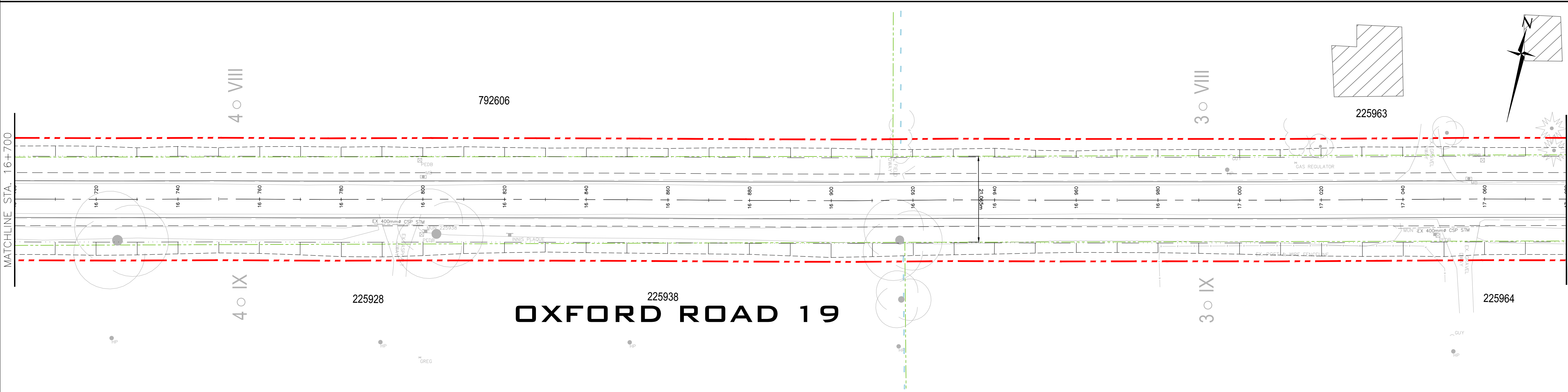
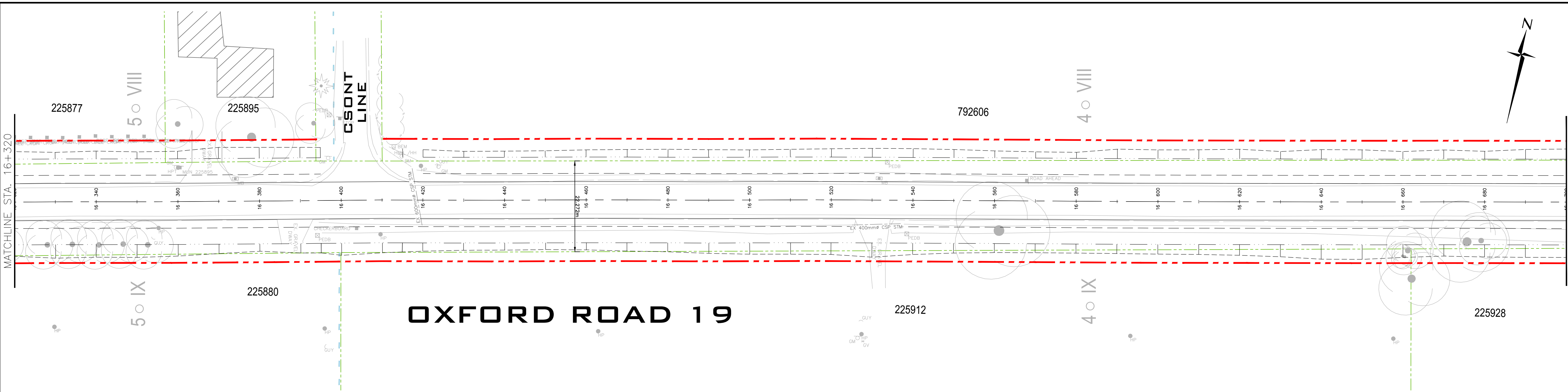
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OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
PLAN 14+800 TO 16+320	
HOR. SCALE - 1:500	
STN: 14+800 TO: 16+320	
CONTRACT NO:	
DWG NO: PLAN 10	

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LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	□ XCB
Ex. Clean Out	○ CO
Ex. Curb Stop	○ CS
Ex. Ditch Inlet CB	□ DICB
Ex. Double Catchbasin	□ DCB
Ex. Fire Hydrant	○ XSAHM
Ex. Sanitary Manhole	○ XSTMH
Ex. Storm Manhole	○ XSTMH
Ex. Water Valve	○ XWV
Ex. Cut Cross	○ XCC
Ex. Sign	□ X
Ex. Guy Wire	---
Ex. Coniferous Shrub	○
Ex. Deciduous Shrub	○
Ex. Stump	○
Ex. Coniferous Tree	○
Ex. Deciduous Tree	○
Prop. Catchbasin	■ CB
Prop. Clean Out	● CO
Prop. Curb Stop	● CS
Prop. Ditch Inlet CB	■ DICB
Prop. Double CB	■ DCB
Prop. Fire Hydrant	● XSAHM
Prop. Sanitary Manhole	● XSTMH
Prop. Storm Manhole	● XSTMH
Prop. Water Valve	● XWV
Remove & Dispose	○



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 BENCHMARK # ELEVATION=m

AREA MUNICIPALITY:
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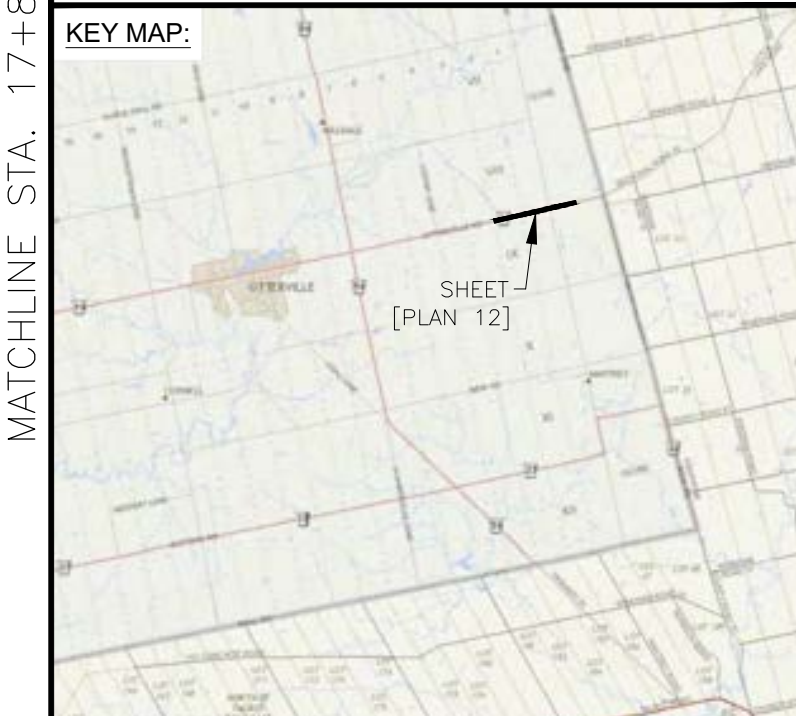
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 DRAWN BY: MS
 CHECKED BY: AB
 APPROVED BY: HC
 DATE: NOV 10, 2022

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 16+320 TO 17+460

HOR. SCALE - 1:500
 STN: 16+320 TO: 17+460
 CONTRACT NO:
 DWG NO: PLAN 11

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 Layout: PLAN 16+320 TO 17+460 Date: Dec 06, 2022 14:50 user: MSimon



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	CO
Pr. Ditchline	---	Prop. Curb Stop	CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	DICB
Pr. Edge of Pavement	---	Prop. Double CB	DCB
Ex. Property Line	---	Prop. Fire Hydrant	FMH
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	SAMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	WV
Ex. Storm Sewer	---	Remove & Dispose	RD
Ex. U/G Telephone	T		
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	CB		
Ex. Clean Out	CO		
Ex. Curb Stop	CS		
Ex. Ditch Inlet CB	DICB		
Ex. Double Catchbasin	DCB		
Ex. Fire Hydrant	FMH		
Ex. Sanitary Manhole	SAMH		
Ex. Storm Manhole	STMH		
Ex. Water Valve	WV		
Ex. Cut Cross	---		
Ex. Sign	---		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	---		
Ex. Deciduous Shrub	---		
Ex. Stump	---		
Ex. Coniferous Tree	---		
Ex. Deciduous Tree	---		

ISSUED FOR PCCH#2	3	DEC 6, 2022	HC
ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:

BENCHMARK #	ELEVATION=m
-------------	-------------

AREA MUNICIPALITY:
Township of Norwich
285767 Airport Road, P.O. Box 100
Norwich, Ontario, N0J 1P0
telephone (519) 468-2410
fax (519) 468-2414
web www.norwich.ca



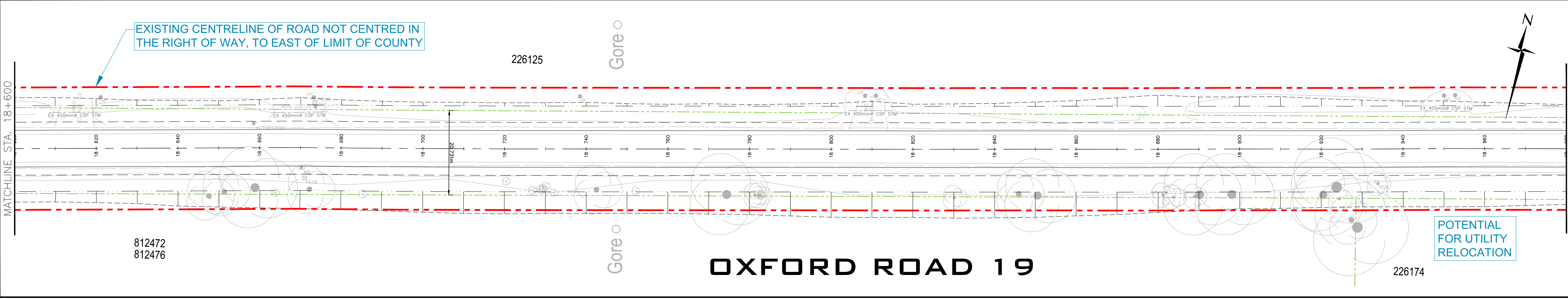
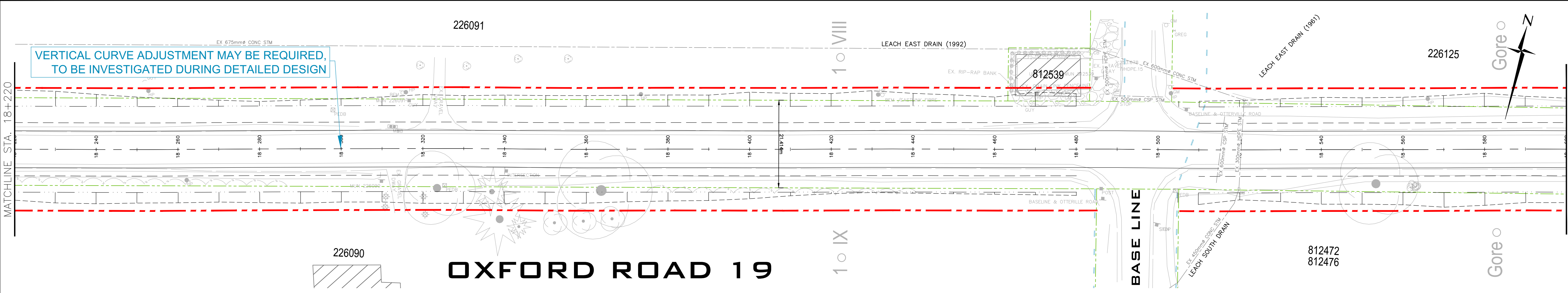
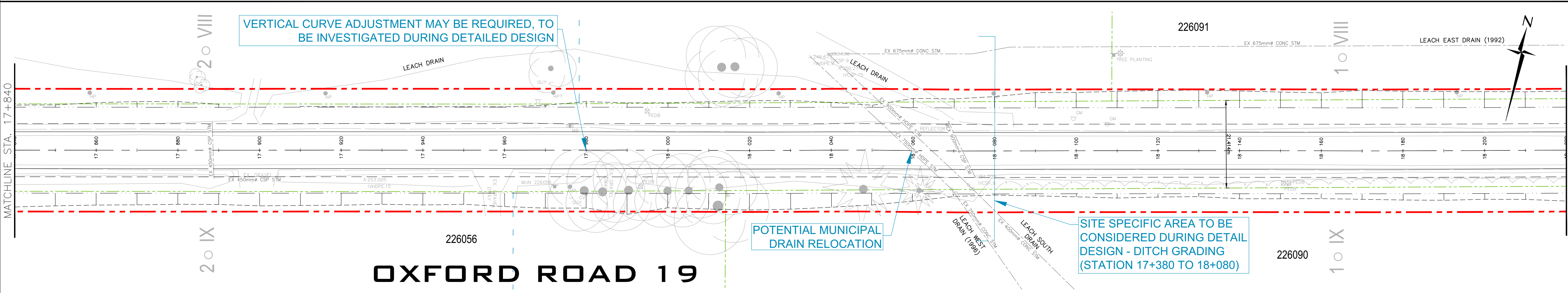
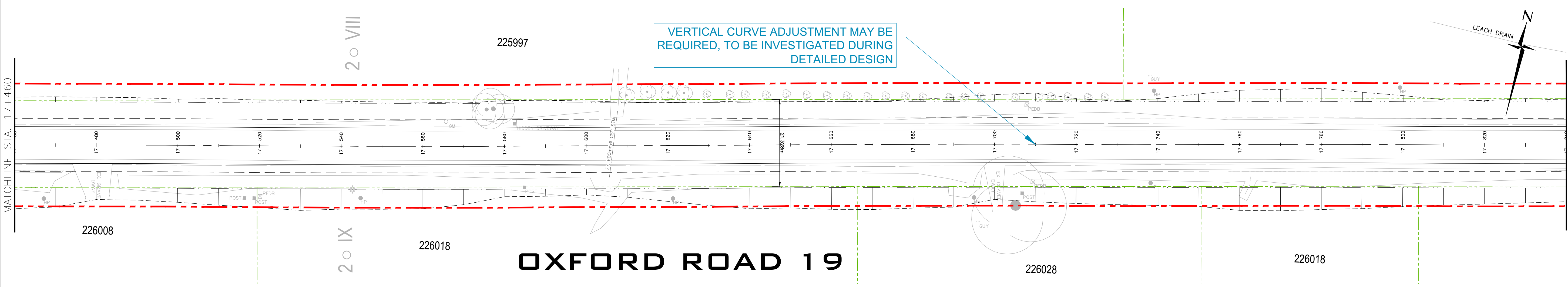
CONSULTANT:
BURNSIDE
R.J. Burnside & Associates Limited
35 Perry Street
Woodstock, Ontario, N4S 3C4
telephone (519) 271-5111
fax (519) 271-3730
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DESIGN BY: HC	STAMP:
DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

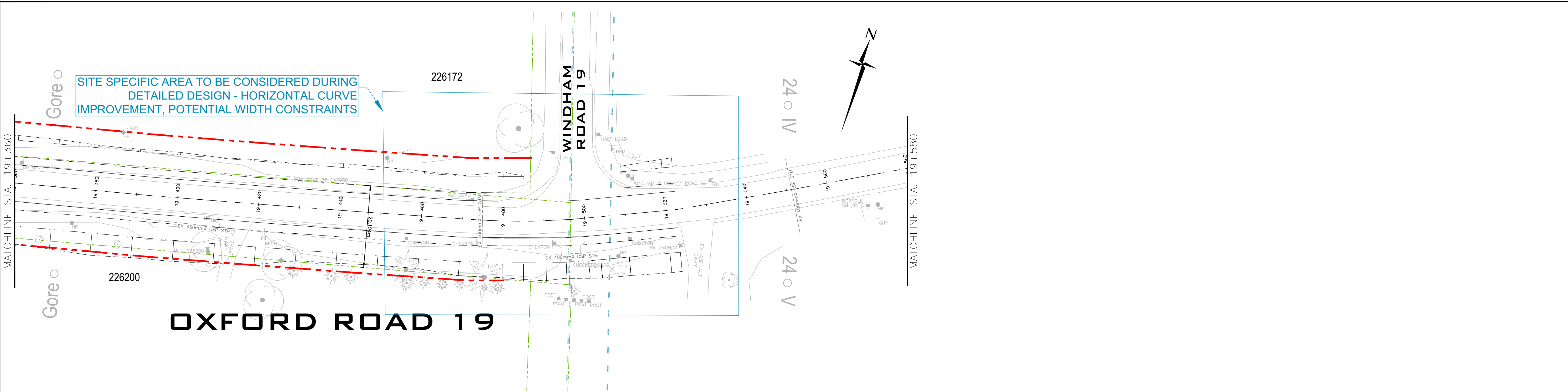
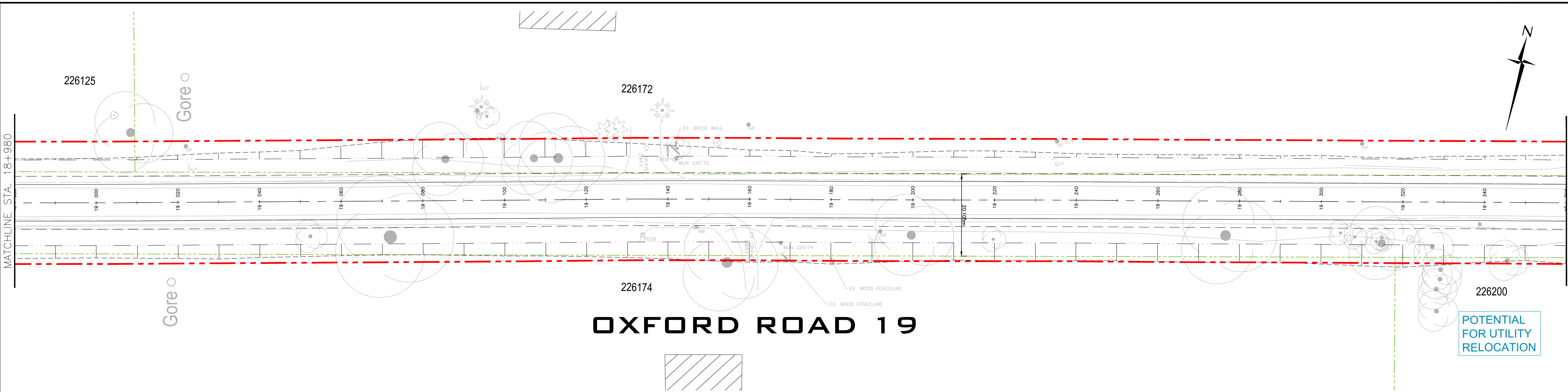
PLAN 17+460 TO 18+980

HOR. SCALE - 1:500
STN: 17+460 TO: 18+980
CONTRACT NO:
DWG NO: PLAN 12



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layout: PLAN 17+460 TO 18+980 date: Dec 06, 2022 14:52 user: MSimon

file: \\SHERLOCK\Shared Work Area\03425 Oxford Road 19\03_ProductionDwg\03425 PLANS-11.dwg
 Layout: PLAN 18+980 TO 19+580 date: Dec 06, 2022 14:53 user: MSimon



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	XCB
Ex. Clean Out	CO
Ex. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Ex. Fire Hydrant	FSAMH
Ex. Sanitary Manhole	SMH
Ex. Storm Manhole	STMH
Ex. Water Valve	WV
Ex. Cut Cross	CC
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CS
Ex. Deciduous Shrub	DS
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT
Prop. Catchbasin	CB
Prop. Clean Out	CO
Prop. Curb Stop	CS
Prop. Ditch Inlet CB	DICB
Prop. Double CB	DCB
Prop. Fire Hydrant	FSAMH
Prop. Sanitary Manhole	SMH
Prop. Storm Manhole	STMH
Prop. Water Valve	WV
Remove & Dispose	R&D

ISSUED FOR PCC#2	No.	DATE	BY
ISSUED FOR CLIENT REVIEW	3	DEC 6, 2022	HC
ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:
 BENCHMARK # _____ ELEVATION=m _____

AREA MUNICIPALITY:
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DESIGN BY: HC	STAMP:
DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

**OXFORD ROAD 19
 ENVIRONMENTAL
 ASSESSMENT**

PLAN 18+980 TO 19+580

HOR. SCALE - 1:500
 STN: 18+980 TO: 19+580
 CONTRACT NO:
 DWG NO: PLAN 13



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Appendix F

Evaluation of Alternative Solutions

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

	Criteria for Evaluating Alignments	Alternative 1: Do Nothing	Alternative 2: Provide regular road maintenance	Alternative 3: Improve road structure within existing ROW	Alternative 4: Widen the ROW and widen partially pave shoulders	Alternative 5: Improve road structure, widen the ROW and widen and partially pave shoulders
		<p>The Alternative Solution option of Do Nothing is a mandatory consideration within the MCEA process and serves as a reference point for comparing other alternative solutions. Under this alternative, the road will continue to operate as a two-way road fully open to the public with no construction or widening. Only routine maintenance will be performed. Half-load restrictions will continue to be applied in the Spring.</p>	<p>The Alternative Solution provides condition maintenance to the road and shoulders to maintain acceptable surface conditions (e.g., crack sealing, gravel shouldering, resurfacing work). Under this alternative, the road will continue to operate as a two-way road fully open to the public with no major rehabilitation, re-construction or widening. Routine operational maintenance will be performed as required. Half-load restrictions will continue to be applied in the Spring.</p>	<p>This Alternative Solution will provide a two-lane road to County requirements with gravel shoulders. Pavement structure will be improved to remove half-load restrictions in the Spring. A minor amount of widening within the existing ROW is proposed on the travel lanes to improve safety.</p>	<p>This Alternative solution will provide a two-lane road to County requirements with a paved/gravel shoulder. No improvement of pavement structure to remove half-load restrictions in the Spring, is proposed. The ROW will be widened to meet the Official Plan requirement of 30.5 m.</p>	<p>This Alternative solution will provide a two-lane road to County requirements with a paved/gravel shoulder. Pavement structure will be improved to remove half-load restrictions in the Spring. The ROW will be widened to meet the Official Plan requirement of 30.5 m.</p>
A	Problem Statement					
1	Addresses Problem Statement	Does not address the Problem Statement	This Alternative partially addresses the Problem Statement by keeping the road in a good state-of-repair. However, without widening the road shoulders or making structural improvements, the transportation of goods and people is not fully optimized.	This Alternative partially addresses the Problem Statement by improving the structural adequacy of the road. However, without widening the road shoulders, the transportation of goods and people is not fully optimized.	This Alternative partially addresses the Problem Statement by widening, as well as potentially partially paving the road shoulders. However, without improving the structural adequacy of the road, the transportation of goods and people is not fully optimized	Fully addresses the Problem Statement, by both improving the structural adequacy of the road and widening/potentially paving the road shoulders.
	Summary Problem Statement	Does not meet POS	Partially meets POS	Partially meets POS	Partially meets POS	Meets POS

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

	Criteria for evaluating alternatives	Alternative 1: Do Nothing	Alternative 2: Provide regular road maintenance	Alternative 3: Improve road structure within existing ROW	Alternative 4: Widen the ROW and widen partially	Alternative 5: Improve road structure, widen the
B	Natural Environment					
1	Impacts to Woodlots, Wetlands and Designated Features (including Provincially Significant Wetlands [PSWs], Environmentally Sensitive [ESAs], Areas of Natural and Scientific Interest [ANSIs], Regional Natural Heritage Systems [NHS])	No impact over existing conditions.	A portion of Otterville PSW is adjacent to the study area, at the edge of the Otterville Settlement. Any potential impact to the wetland would be minimal as improvements are limited to the existing ROW. There are woodlands within and adjacent to the study corridor. As the ROW will not be expanded in this Alternative, impacts to woodlands should be minor.	A portion of Otterville PSW is adjacent to the study area, at the edge of the Otterville Settlement. Any potential impact to the wetland would be minimal as improvements are limited to the existing ROW. There are woodlands within and adjacent to the study corridor. As the ROW will not be expanded in this Alternative, impacts to woodlands should be minor.	A portion of Otterville PSW is adjacent to the study area, at the edge of the Otterville Settlement. There may be potential impacts to the wetland due to widening of the ROW. There are woodlands within and adjacent to the study corridor. Widening in these areas may result in some loss of woodland.	A only portion of Otterville PSW is adjacent to the study area, at the edge of the Otterville Settlement. There may be potential impacts to the wetland due to widening of the ROW. There are woodlands within and adjacent to the study corridor. Widening in these areas may result in some loss of woodland.
	<i>Rating</i>	●	●	●	●	●
2	Impacts to vegetation and vegetation communities	No impact over existing conditions.	No impact over existing conditions.	Impact to vegetation and vegetation communities may occur during construction.	ROW widening likely to result in some impact to vegetation and vegetation community.	ROW widening likely to result in some impact to vegetation and vegetation community.
	<i>Rating</i>	●	●	●	●	●
3	Impact to trees	No impact over existing conditions.	No impact over existing conditions.	Some tree removal may be required due to construction.	Some tree removal may be required.	Some tree removal may be required.
	<i>Rating</i>	●	●	●	●	●
4	Impact to terrestrial habitat including Species at Risk	No impact over existing conditions.	No impact over existing conditions.	Potential impact to terrestrial habitat may occur during construction; risks to be mitigated.	ROW widening will result in increased potential for impact to terrestrial habitat; risks to be mitigated.	ROW widening will result in increased potential for impact to terrestrial habitat; risks to be mitigated.
	<i>Rating</i>	●	●	●	●	●
5	Impact on Fisheries and Aquatic Habitat	No impact over existing conditions.	No impacts are expected to aquatic habitat	No impacts are expected to aquatic habitat	Potential culvert and bridge widening may result in increased potential for impacts to aquatic habitat; risks to be mitigated.	Potential culvert and bridge widening may result in increased potential for impacts to aquatic habitat; risks to be mitigated.
	<i>Rating</i>	●	●	●	●	●

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

6	Impact to surface water and drainage	No impact over existing conditions.	No impact over existing conditions. Minor potential for impact to surface water and drainage to occur during construction; risks to be mitigated.	Potential impact to surface water and drainage to occur during construction; risks to be mitigated.	Potential impact to surface water and drainage to occur during construction; risks to be mitigated.	Potential impact to surface water and drainage to occur during construction; risks to be mitigated.
<i>Rating</i>		●	●	◐	◐	◐
7	Impacts to groundwater, Source Water Protection	No impact over existing conditions.	Portions of the Study Area fall within Wellhead Protection Areas (WHPA), Issue Contributing Areas (ICA), Highly Vulnerable Aquifer Areas(HVA), and Significant Groundwater Recharge Areas (SGRA). Risks to be mitigated.	Portions of the Study Area fall within Wellhead Protection Areas (WHPA), Issue Contributing Areas (ICA), Highly Vulnerable Aquifer Areas(HVA), and Significant Groundwater Recharge Areas (SGRA). Risks to be mitigated.	Portions of the Study Area fall within Wellhead Protection Areas (WHPA), Issue Contributing Areas (ICA), Highly Vulnerable Aquifer Areas(HVA), and Significant Groundwater Recharge Areas (SGRA). Risks to be mitigated.	Portions of the Study Area fall within Wellhead Protection Areas (WHPA), Issue Contributing Areas (ICA), Highly Vulnerable Aquifer Areas(HVA), and Significant Groundwater Recharge Areas (SGRA). Risks to be mitigated.
<i>Rating</i>		●	●	◐	◐	◐
8	Climate Change and Natural hazard impacts (erosion, soil stability, flooding)	Increased frequency and severity of adverse climatological events will impact aging infrastructure.	Increased frequency and severity of adverse climatological events will impact aging infrastructure.	Structural improvements will allow the road to be less susceptible to adverse climatological events.	Paving of shoulders provides increased stabilization of the road bed against adverse climatological events. Increased potential for active transportation can reduce the impacts of auto emissions on increasing greenhouse gases.	Paving of shoulders provides increased stabilization of the road bed against adverse climatological events. Increased potential for active transportation can reduce the impacts of auto emissions on increasing greenhouse gases.
<i>Rating</i>		○	○	◐	●	●
Summary Natural Environment		●	●	◐	◐	◐

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

	Criteria for evaluating alternatives	Alternative 1: Do Nothing	Alternative 2: Provide regular road maintenance	Alternative 3: Improve road structure within existing ROW	Alternative 4: Widen the ROW and widen partially	Alternative 5: Improve road structure, widen the
C	Cultural Environment					
1	Impacts to Cultural Heritage Resources	No impact over existing conditions.	No impact over existing conditions.	No impact over existing conditions.	Potential for impact due to widening of ROW.	Potential for impact due to widening of ROW.
	<i>Rating</i>	●	●	●	●	●
2	Impacts to Archaeological Resources	No impact over existing conditions.	No impact over existing conditions.	No impact over existing conditions.	Potential impact to be confirmed by Stage 1 Archaeological Assessment. There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.	Potential impact to be confirmed by Stage 1 Archaeological Assessment. There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.
	<i>Rating</i>	●	●	●	●	●
	Summary Cultural Environment	●	●	●	●	●

	Criteria for evaluating alternatives	Alternative 1: Do Nothing	Alternative 2: Provide regular road maintenance	Alternative 3: Improve road structure within existing ROW	Alternative 4: Widen the ROW and widen partially	Alternative 5: Improve road structure, widen the
D	Socio-Economic Environment					
1	Impacts to Private Property	No impact over existing conditions.	No impact over existing conditions.	No impact over existing conditions.	Widening will impact properties within the widened ROW, to meet the Official Plan requirements of the County. Consideration will be made to a reduced ROW in areas with significant impacts (e.g., along the residential lots in Ostrander).	Widening will impact properties within the widened ROW, to meet the Official Plan requirements of the County. Consideration will be made to a reduced ROW in areas with significant impacts (e.g., along the residential lots in Ostrander).
	<i>Rating</i>	●	●	●	●	●
2	Compatibility with existing and future land uses	Existing Spring half load restriction poses an issue for the effective movements of goods for local businesses.	Existing Spring half load restriction poses an issue for the effective movements of goods for local businesses.	The road passes through a variety of land uses and provides a connection for residential and commercial purposes. The proposed structural improvements provide benefits to local businesses, allowing effective transport for goods.	The road passes through a variety of land uses and provides a connection for residential and commercial purposes. The proposed improvements to the road cross section provide benefits to local businesses allowing effective transport for goods.	The road passes through a variety of land uses and provides a connection for residential and commercial purposes. The proposed improvements provide benefits to local businesses allowing effective transport for goods.

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

	<i>Rating</i>					
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Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

3	Compatibility with Active Transportation Plans or needs	No impact over existing conditions.	No impact over existing conditions.	No impact over existing conditions.	Based on the County's policies and cycling connectivity considerations, our review recommends that on-road cycling facilities (i.e., paved shoulders) be provided. Through the settlements of Springford and Otterville, Oxford Road 19 provides paved shoulders that can accommodate cycling in this low-speed environment. Extending these cycling opportunities beyond the limits of these settlement has value, particularly since a number of cycling routes and trails intersect with Oxford Road 19.	Based on the County's policies and cycling connectivity considerations, our review recommends that on-road cycling facilities (i.e., paved shoulders) be provided. Through the settlements of Springford and Otterville, Oxford Road 19 provides paved shoulders that can accommodate cycling in this low-speed environment. Extending these cycling opportunities beyond the limits of these settlement has value, particularly since a number of cycling routes and trails intersect with Oxford Road 19. The effectiveness of paved shoulders to accommodate cyclists will depend on the width of shoulder paved.
<i>Rating</i>		<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>	<input checked="" type="radio"/>
4	Conformity to Municipal and Agency Plans and Policies	Does not meet the County's Official Plan ROW requirements.	Does not meet the County's Official Plan ROW requirements.	Does not meet the County's Official Plan ROW requirements.	Meets the Official Plan ROW requirements and supports Active Transportation.	Meets the Official Plan ROW requirements and supports Active Transportation.
<i>Rating</i>		<input type="radio"/>	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>	<input checked="" type="radio"/>
5	Impacts to air quality and noise levels (during and post construction)	No impact on air quality or noise over existing conditions.	Short term nuisance noise and dust emissions expected during the construction phases and will be mitigated.	Potential marginal increase in noise from increases in truck traffic. If identified as a concern, noise assessment study will be completed in Phase 3 of the EA and mitigations will be identified. Short term nuisance noise and dust emissions expected during the construction phases and will be mitigated.	Short term nuisance noise and dust emissions expected during the construction phases and will be mitigated.	Potential marginal increase in noise from increases in truck traffic. If identified as a concern, a noise assessment study will be completed in Phase 3 of the EA and mitigations will be identified. Short term nuisance noise and dust emissions expected during the construction phases and will be mitigated.
<i>Rating</i>		<input checked="" type="radio"/>	<input checked="" type="radio"/>	<input checked="" type="radio"/>	<input checked="" type="radio"/>	<input checked="" type="radio"/>

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

6	Impacts to farms and business operations	No improvement over existing conditions.	No improvement over existing conditions.	No widening to help farms and businesses with the transportation of goods, however they will no longer be subject to the Spring half load restrictions.	Widening will help farms and businesses transport goods.	Widening and structural improvements to remove the Spring half load restrictions will help farms and businesses transport goods.
	<i>Rating</i>	○	○	◐	●	●
7	Provision of safe access to private properties and businesses	No improvement over existing conditions.	No improvement over existing conditions.	No improvement over existing conditions.	Improves the safety for access to private properties and businesses through opportunities for smooth exit/entrance.	Improves the safety for access to private properties and businesses through opportunities for smooth exit/entrance.
	<i>Rating</i>	○	○	◐	●	●
	Summary Socio-Economic Environment	◐	◐	◐	◐	◐

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

	CRITERIA FOR EVALUATING ALTERNATIVES	Alternative 1: Do Nothing	Alternative 2: Provide regular road maintenance	Alternative 3: Improve road structure within existing ROW	Alternative 4: Widen the ROW and widen partially	Alternative 5: Improve road structure, widen the
E	Technical Considerations					
1	Accommodation of all types of traffic and modes of travel	Does not improve the road's ability to accommodate all types of traffic and modes of travel.	Does not improve the road's ability to accommodate all types of traffic and modes of travel	Facilitates the road's ability to accommodate additional truck traffic by removing spring half load restrictions. Does not improve the road's ability to accommodate all modes of travel.	Facilitates the road's ability to accommodate additional modes of travel by providing paved shoulders. Does not improve the road's ability to accommodate trucks.	Improves the road's ability to accommodate all types of traffic and modes of travel.
	<i>Rating</i>					
2	Improvement to operational safety	Does not improve road safety.	Minor improvement to road safety.	Minor improvement to road safety.	Potential for significant improvement to road safety.	Potential for significant improvement to road safety.
	<i>Rating</i>					
3	Road maintenance requirements	No improvements to road maintenance requirements.	No improvements to road maintenance requirements.	May reduce the frequency of preventive road maintenance treatments.	Potential to reduce gravel shoulder maintenance requirements.	May reduce the frequency of preventive road maintenance treatments and gravel shoulder maintenance requirements.
	<i>Rating</i>					
4	Impact to utilities and drainage work/structures.	Does not impact utilities or drainage works/structures.	Does not impact utilities or drainage works/structures.	May impact utilities during construction.	Widening of the shoulders may impact Hydro One Transmission poles/lines or drainage works/structures.	Widening of the shoulders may impact Hydro One Transmission poles/lines or drainage works/structures.
	<i>Rating</i>					
	Summary Technical Considerations					

	CRITERIA FOR EVALUATING ALTERNATIVES	Alternative 1: Do Nothing	Alternative 2: Provide regular road maintenance	Alternative 3: Improve road structure within existing ROW	Alternative 4: Widen the ROW and widen partially	Alternative 5: Improve road structure, widen the
F	Financial Considerations					
1	Capital and O&M cost	No capital cost. Relatively high O&M cost due to road aging.	Lowest low capital cost. Moderate to high O&M cost.	Moderate capital cost. Moderate to high O&M cost.	High capital cost. Moderate O&M cost.	Highest capital cost. Low to moderate O&M cost.
	<i>Rating</i>					
2	Property acquisition cost	No property acquisition is required.	No property acquisition is required.	No property acquisition is required.	Some property acquisition will be required.	Some property acquisition will be required.
	<i>Rating</i>					
	SUMMARY Financial Considerations					

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

CRITERIA FOR EVALUATING ALTERNATIVES	Do Nothing	Alternative 2: Provide regular road maintenance	Alternative 3: Improve road structure within existing ROW	Alternative 4: Wident the ROW and widen partially	Alternative 5: Improve road structure, widen the
OVERALL SUMMARY	Not Carried Forward	Least Preferred	Moderately Preferred	Moderately Preferred	Most Preferred

Oxford Road 19 Schedule C MCEA- Evaluation of Preliminary Preferred Alternatives

ORDER OF PREFERENCE

Most Preferred 

More Preferred 

Moderately Preferred 

Less Preferred 

Least Preferred 



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Appendix G

Evaluation of Alternative Design Concepts

Oxford Road 19 - Evaluation of Alternative Design Concepts

CRITERIA FOR EVALUATING ALTERNATIVES		Alternative 1: Moderately-wide paved shoulders	Alternative 2: Minimally-wide paved shoulders	Alternative 3: Maximally-wide paved shoulders
A NATURAL ENVIRONMENT				
1	Impact to Designated Sites/Species	<p>Removal of limited vegetation within woodland and riparian communities that have potential to provide habitat for SAR species. The extent of impact to terrestrial SAR is anticipated to be low to moderate provided suitable mitigation measures are instated. Clearing is limited to the peripheries of potential habitat.</p> <p>Confirmed present SAR within the ROW are one young Butternut and Barn Swallow. The Butternut specimen will not be impacted; however, additional individuals may be present. Barn Swallow will not be impacted by the proposed road improvements.</p>	<p>Removal of limited vegetation within woodland and riparian communities that have potential to provide habitat for SAR species. The extent of impact to terrestrial SAR is anticipated to be low to moderate provided suitable mitigation measures are instated. Clearing is limited to the peripheries of potential habitat.</p> <p>Confirmed present SAR within the ROW are one young Butternut and Barn Swallow. The Butternut specimen will not be impacted; however, additional individuals may be present. Barn Swallow will not be impacted by the proposed road improvements.</p>	<p>Removal of limited vegetation within woodland and riparian communities that have potential to provide habitat for SAR species. The extent of impact to terrestrial SAR is anticipated to be low to moderate provided suitable mitigation measures are instated. Clearing is limited to the peripheries of potential habitat.</p> <p>Confirmed present SAR within the ROW are one young Butternut and Barn Swallow. The Butternut specimen will not be impacted; however, additional individuals may be present. Barn Swallow will not be impacted by the proposed road improvements.</p>
	<i>Rating</i>	●	●	●
2	Impact to Surface Water Quality	<p>The slightly wider paved roadway may result in a greater amount of roadway runoff that may have an affect on surface water quality. A wider roadway may result in increased amounts of runoff as there is more surface area for water to be displaced. Furthermore, a wider roadway may require a greater accumulation of maintance materials which may have an impact on roadway contaminants.</p>	<p>The slightly narrower-paved road way may have minimal road runoff that may have an affect on surface water quality. A wider roadway may result in increased amounts of runoff as there is more surface area for water to be displaced. Furthermore, a wider roadway may require a greater accumulation of maintance materials which may have on impact roadway contaminants.</p>	<p>The slightly wider paved roadway may result in a greater amount of roadway runoff that may have an affect on surface water quality. A wider roadway may result in increased amounts of runoff as there is more surface area for water to be displaced. Furthermore, a wider roadway may require a greater accumulation of maintance materials which may have on impact roadway contaminants.</p>
	<i>Rating</i>	●	●	●
3	Impact to Ground Water Quality and Quantity	<p>Portions of the Study Area fall within Wellhead Protection Areas (WHPA), Issue Contributing Areas (ICA), Highly Vulnerable Aquifer Areas(HVA), and Significant Groundwater Recharge Areas (SGRA). Risks to be mitigated.</p>	<p>Portions of the Study Area fall within Wellhead Protection Areas (WHPA), Issue Contributing Areas (ICA), Highly Vulnerable Aquifer Areas(HVA), and Significant Groundwater Recharge Areas (SGRA). Risks to be mitigated.</p>	<p>Portions of the Study Area fall within Wellhead Protection Areas (WHPA), Issue Contributing Areas (ICA), Highly Vulnerable Aquifer Areas(HVA), and Significant Groundwater Recharge Areas (SGRA). Risks to be mitigated.</p>
	<i>Rating</i>	●	●	●
4	Impact to Hazard Lands (erosion, slope stability, flooding)	<p>Some potetial impacts in the areas of unstable ditch conditions, which can be mitigated through improval of the ditch embankment. Stability may be upheld with proper stabilizing features. Flooding may not be a major factor as long as run off is conveyed through the ditch and does not pool.</p>	<p>Some potetial impacts in the areas of unstable ditch conditions, which can be mitigated through improval of the ditch embankment. Stability may be upheld with proper stabilizing features. Flooding may not be a major factor as long as run off is conveyed through the ditch and does not pool.</p>	<p>Some potetial impacts in the areas of unstable ditch conditions, which can be mitigated through improval of the ditch embankment. Stability may be upheld with proper stabilizing features. Flooding may not be a major factor as long as run off is conveyed through the ditch and does not pool.</p>
	<i>Rating</i>	●	●	●
5	Impact to Vegetation and Terrestrial Habitat	<p>The following vegetation communities will be impacted by the proposed road improvement project: upland deciduous forest, lowland deciduous forest, thickets, graminoid meadow, mixed meadow, and meadow marsh. Impacts are clearing are anticipated to be low as clearing is limited to the margins of communities. all impacted vegetation communities are commonly occurring communities in southern Ontario.</p> <p>Clearing of hedgerow trees and landscape trees associated with rural properties and farmland will also be required.</p>	<p>The following vegetation communities will be impacted by the proposed road improvement project: upland deciduous forest, lowland deciduous forest, thickets, graminoid meadow, mixed meadow, and meadow marsh. Impacts are clearing are anticipated to be low as clearing is limited to the margins of communities. all impacted vegetation communities are commonly occurring communities in southern Ontario.</p> <p>Clearing of hedgerow trees and landscape trees associated with rural properties and farmland will also be required.</p>	<p>The following vegetation communities will be impacted by the proposed road improvement project: upland deciduous forest, lowland deciduous forest, thickets, graminoid meadow, mixed meadow, and meadow marsh. Impacts are clearing are anticipated to be low as clearing is limited to the margins of communities. all impacted vegetation communities are commonly occurring communities in southern Ontario.</p> <p>Clearing of hedgerow trees and landscape trees associated with rural properties and farmland will also be required.</p>
	<i>Rating</i>	●	●	●

6	Impact to Aquatic Habitat	Six watercrossings fall within the study area , most of provide suitable aquatic habitat for a number of different fish species. No aquatic species at risk are found within the study area. Minimal impact may occur if stucutres are not being replaced. Moderate imapact may occur if strucutres are being modified or replaced.	Six watercrossings fall within the study area , most of provide suitable aquatic habitat for a number of different fish species. No aquatic species at risk are found within the study area. Minimal impact may occur if stucutres are not being replaced. Moderate imapact may occur if strucutres are being modified or replaced.	Six watercrossings fall within the study area , most of provide suitable aquatic habitat for a number of different fish species. No aquatic species at risk are found within the study area. Minimal impact may occur if stucutres are not being replaced. Moderate imapact may occur if strucutres are being modified or replaced.
	Rating	●	●	●
SUMMARY NATURAL ENVIRONMENT		●	●	●

CRITERIA FOR EVALUATING ALTERNATIVES		Alternative 1: Moderately-wide paved shouldrs	Alternative 2: Minimally-wide paved shoudlers	Alternative 3: Maximally-wide paved shouldrs
D SOCIO-CULTURAL ENVIRONMENT				
1	Property Impacts	All alternatives require a similar amount of property.	All alternatives require a similar amount of property.	All alternatives require a similar amount of property.
	Rating	●	●	●
2	Heritage Resources (archaeological features, built heritage, and cultural heritage landscapes)	There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed. There may be some impact to cultural heritage resources (CHL13 and CHL20).	There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed. There may be some impact to cultural heritage resources (CHL13 and CHL20).	There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed. There may be some impact to cultural heritage resources (CHL13 and CHL20).
	Rating	●	●	●
3	Nuisance impacts (noise, traffic, visual impact)	All alternatives have similar noise impacts due to increased truck traffic, resulting from the removal of half-load restriction in the Spring. All alternatives have similar visual impact due to tree and vegetation removal on the widened right-of-way. No significant difference between the alternatives in speeds or noise levels. Some dust created by agricultural equipment travel along gravel shoulders. Temporary impact due to construction activities.	All alternatives have similar impacts due to increased truck traffic, resulting from the removal of half-load restriction in the Spring. All alternatives have similar visual impact due to tree and vegetation removal on the widened right-of-way. No significant difference between the alternatives in speeds or noise levels. Significant dust created by agricultural equipment travel along gravel shoulders. Temporary impact due to construction activities.	All alternatives have similar impacts due to increased truck traffic, resulting from the removal of half-load restriction in the Spring. All alternatives have similar visual impact due to tree and vegetation removal on the widened right-of-way. No significant difference between the alternatives in speeds or noise levels. Temporary impact due to construction activities. Very little dust created by agricultural equipment travel along gravel shoulders. May lengthen the period of temporary construction period impacts if bridges require widening.
	Rating	●	●	●
SUMMARY SOCIO-CULTURAL ENVIRONMENT		●	●	●

CRITERIA FOR EVALUATING ALTERNATIVES		Alternative 1: Moderately-wide paved shouldrs	Alternative 2: Minimally-wide paved shoudlers	Alternative 3: Maximally-wide paved shouldrs
E TECHNICAL ENVIRONMENT				
1	Accomodation of various modes of transpotation and vehicle types	Wider paved shoulders provides increased spacing between traffic (cars, trucks, slower modes such as agricultural vehicles and cyclists). Limited buffer for cyclists or pedestrians.	Minimal asphalt width provides for minimum space between traffic (cars, trucks, slower modes such as agricultural vehicles and cyclists). Cyclists and pedestrians use gravel shoulder.	Wider paved shoulders provides increased spacing between traffic (cars, trucks, slower modes such as agricultural vehicles and cyclists). Increased buffer for cyclists and pedestrians.
	Rating	●	●	●
2	Road safety	Moderate improvement of safety for all travel modes using the corrior. All alternatives allow sufficient space for travel along shoulders and for pullover of disabled vehicles. Some dust created by agricultural equipment travel along gravel shoulders.	Some improvement of safety for all travel modes using the corrior. All alternatives allow sufficient space for travel along shoulders and for pullover of disabled vehicles. Significant dust created by agricultural equipment travel along gravel shoulders.	Significant improvement of safety for all travel modes using the corrior. All alternatives allow sufficient space for travel along shoulders and for pullover of disabled vehicles. Very little created by agricultural equipment travel along gravel shoulders. Minimal gravel shoulder may direct agricultural equipment further onto paved areas and into travel lane.
	Rating	●	●	●

3	Operation & Maintenance requirements	All alternatives meet vehicular operational capacity and physical requirements (e.g., sightlines, geometry) along the corridor. Moderate shoulder maintenance (gravel). Better able to accommodate full range of operating speeds. Moderate improvement to access operations along the corridor (pullover onto shoulder to exit roadway at accesses).	All alternatives meet vehicular operational capacity and physical requirements (e.g., sightlines, geometry) along the corridor. Increased shoulder maintenance (gravel). Lower ability to accommodate vehicles with excessive speeds and / or slow moving vehicles. No improvement to access operations along the corridor.	All alternatives meet vehicular operational capacity and physical requirements (e.g., sightlines, geometry) along the corridor. Minimal shoulder maintenance (gravel) but harder to maintain with grading equipment. Better able to accommodate full range of operating speeds. Significant improvement to access operations along the corridor (pullover onto shoulder to exit roadway at accesses).
	<i>Rating</i>			
4	Design Complexity	Some adjustment of design required to interface the wider asphalt widths with crossings (e.g., bridge at 2+800, 8.24 m width) and accesses. All alternatives result in similar impacts for other facilities along the right-of-way (e.g., utilities, municipal drains, trees).	Maintains existing asphalt interface with crossings (i.e., 3 bridges) and accesses. All alternatives result in similar impacts for other facilities along the right-of-way (e.g., utilities, municipal drains, trees).	Design may not be achievable in area of constraints (e.g., crossings, accesses) without significant adjustment of such constraints (e.g., bridge at 2+800, 8.24 m width; bridge at 10+040, 9.15 m width). Potential for longer temporary construction impacts if structure widenings are required. All alternatives result in similar impacts for other facilities along the right-of-way (e.g., utilities, municipal drains, trees).
	<i>Rating</i>			
SUMMARY TECHNICAL ENVIRONMENT				

CRITERIA FOR EVALUATING ALTERNATIVES	Alternative 1: Moderately-wide paved shoulders	Alternative 2: Minimally-wide paved shoulders	Alternative 3: Maximally-wide paved shoulders
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F ECONOMIC ENVIRONMENT				
1	Comparative costs	Moderate increase in cost over alternative 2, due to partially paved shoulder. Approximately \$118/m for shoulder. All alternatives have similar costs for relocation of utilities or for tree removal, driven by shoulder width requirements, which is the same for each alternative. Some additional cost for adjustment of major bridge crossings.	Lowest cost due to minimal paved shoulder. Approximately \$45/m for shoulder. All alternatives have similar costs for relocation of utilities or for tree removal, driven by shoulder width requirements, which is the same for each alternative.	Higher increase in cost over alternative 2, due to fully paved shoulder. Approximately \$180/m for shoulder. All alternatives have similar costs for relocation of utilities or for tree removal, driven by shoulder width requirements, which is the same for each alternative. Increased additional cost for adjustment of major bridge crossings.
	<i>Rating</i>			
2	Property acquisition costs	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements
	<i>Rating</i>			
SUMMARY ECONOMIC ENVIRONMENT				

CRITERIA FOR EVALUATING ALTERNATIVES	Alternative 1: Moderately-wide paved shoulders	Alternative 2: Minimally-wide paved shoulders	Alternative 3: Maximally-wide paved shoulders
OVERALL SUMMARY	Most Preferred	Least Preferred	Least Preferred

ORDER OF PREFERENCE

- Least Preferred ○
- Less Preferred ◐
- Somewhat Preferred ◑
- Moderately Preferred ◒
- Most Preferred ●



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H

Consultation Summary



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H1

Notices

UNRESERVED ONLINE CONSIGNMENT AUCTION

Consisting of tractors, combines, construction equipment, farm & irrigation equipment, trucks, trailers, & miscellaneous from Shabatara Farms, Komienski Farms, Underhill Farm Supply, Scotland Agromart, Sunrise Equipment Auctions, local dealers, local farmers, bank repos. All items located at Sunrise Equipment Auctions located at 593249 Oxford Rd 13, Norwich, ON.

TUESDAY MARCH 29TH @ 10:00 AM

ONLINE BIDDING OPENS FRIDAY MARCH 25TH @ 8:00AM

TRACTORS, COMBINES & HEADS: Caterpillar 65D track machine, 3pth, 8800hrs; Oliver 2255, 2wd, open (estate units); Case IH MXU100, 4wd, cab, air, CIH 740 ldr, sharp; 2 (two) NH 8240s, 4wd, cab, air, both clean (from the same farm); MF 2680, cab, air, 4wd, MF DL280 ldr, under 2000hrs; Case IH 1896, 4wd, cab, air; MF 4255, cab, air, 4wd, Allied 595 ldr, 2555hrs, no bucket; JD 5525, open, 4wd & JD 542 ldr, power reverser, 2805hrs, sharp; NH T4.75, cab, air, 2wd, 960hrs, as new; JD 7400, cab, air, 4wd; NH TM115, cab, air, 4wd, 2850hrs, nice; NH TN60DA, cab, air, 4wd, Stoll ldr, 850hrs; JD 5225, open, 4wd, turf tires, power reverser, 2203hrs; JD 3039R compact, cab, air, 4wd, ldr, 1209hrs; JD 970 compact, 2wd, ROPS, canopy, only 2681hrs; Case 1210, 2wd & ldr; Ferguson 20-85; Case 1290, 4wd & ldr; JD 5325, cab, air, 4wd, ldr, low hours; IH 140 (late model), direct key start, c/w cult, side dressers, fast hitch (1 point, 20 plate disc & 2 (two) 1 point 2 furrow plows sold separately); 2014 JD 680 combine, 4wd, 2275 engine hours, 1523 separator; 2003 JD 9750 combine, 4wd, 6050 engine hours, 3085 separator; 2015 JD 640 grain head, c/w air reel; Case IH 2020 flex head, 35', used very little.

CONSTRUCTION EQUIPMENT: 2010 Case 590 Super M backhoe, 2wd, extendahoe, cab, heat, 6145hrs, 2nd owner, super sharp, extra buckets & forks sold separately (estate unit); Komatsu D65EX crawler dozer, 12' blade, cab, heat, walks & talks good showing 2652hrs (caution light flashing); Hyundai Robex EX200W2, 4 wheel drive excavator, blade, works good; Cat 305CR mini-ex, cab, heat, wrist-a-twist bucket, plumbed, sharp, 5000hrs.

EQUIPMENT: MF 9800VE, 12 row narrow corn planter, hydraulic fold, Salford Ferti-Go 4S fertilizer unit, markers, trash whippers, only planted 2 seasons, AS NEW (selling on last high bid confirmation); JD 960, 24' hydraulic fold "S" tyne cult, c/w buster bar & rolling harrows; JD boom & tank off JD 4930 sprayer, 90' boom, Greenstar ready, never seen 28%, no rust; Case IH DCX101 discbine; Nuhn Magnum 4000 liquid manure tank spreader, only spread 25 loads, AS NEW; 2013 NH 195 tandem manure spreader, twin beater, hydraulic end gate, clean; NH 1465, 9' haybine, excellent, field ready; NH 451, 3pth hay mower, 7' bar & knife; Ford 7', 3pth flail mower for grass; Woods 6' rotary chopper; MF 4 furrow #82, 3pth plow (only done 50 acres); MF 8', 3pth disc; IH 3pth hay mower; 16' Allied 3pth, hydraulic fold cult; Kongskilde 12' & 10' 3pth cults; Kewanee 110, 8' wheel disc; Terragator 1803 w/75' boom, winterized, local unit, w/controls; Mathews Corp 180, 15', 1000 pto, flail shredder; Shaver post driver, sidemounts, 3pth; Tyler 6 ton fertilizer spreader; 2003 Claas 2200 Quadrant square baler, RotoCut; 2006 Claas 255 Uniwarp round baler, RotoCut; 16' RJ crowfoot packer, w/ transport wheels; 5 shank 3pth ripper; JD 620 ldr & bucket, no brackets; Vicon LZ401, 14' 3pth air seeder, used very little; Wil-Rich RPN20, 8' offset disc, as new; Walinga 508 Agri-Vac sucker/blower, w/3 pipes, used only 5 times; 4 (four) anhydrous bars, 3pth, 5 shank, some with Yetter coulters; 10 (ten) 8 ton running gears (anhydrous) w/holsters; 2 (two) Horst double reach wagons; 3 (three) 4 ton tandem fertilizer spreaders; Baron Brothers 13' lime spreader box, stainless steel; JBM 14' steel flat rack wagon, w/ 1500 gallon flat bottom tank on Horst 14 ton gear & lights; JD 970 12' culti-mulcher; 2 (two) MF 3 furrow plows & 1 (one) 4 furrow; 12' 3pth "C" tyne cult; 12' drag type cult; blades; buckets; 12' steel dump box, no hoist; pallet jack; large assortment of tires (check website for full list of tires).

IRRIGATION & VEGETABLE EQUIPMENT: 2002 Bauer Rainstar E-51 irrigation traveler, excellent (selling on last high bid confirmation); Bauer E4 irrigation reel; Bauer 110-350 irrigation reel; Cadman 3250 irrigation reel, as new; Arkal 3 head filter system on wagon; Approx. 60 Ames 6" irrigation pipe; 74 Wade Rain 4" irrigation pipe w/ sprinkler risers; 75 Wade Rain 4" irrigation pipe; 90 Wade Rain 4" irrigation pipe; Approx. 90 Wade Rain 4" irrigation pipe; large assortment of sprinklers; large selection of misc. fittings; 8 (eight) 48" fans & 8 (eight) 48" vents (sold in lots of 2 & 2); grading belts; Holland 2 row transplanter, as new; incline elevators; stainless steel flume; multiple sizes & lengths of vegetable conveyors; 5 (five) Caisier lazy Susan baling boxes & presses; 1600 gallon, flat bottom water tank.

TRUCKS & TRAILERS: 2010 International LoneStar tandem axle tractor, double bunk, Cummins ISX500, 18 speed, 14/46 axles, air ride, wet line; 2013 Etnyre, tri-axle, removable gooseneck trailer, Honda motor or wetline, 25' working deck, covered wheels, spot to pin on, fourth axle, air ride; 2007 Sterling truck, single axle, Mercedes diesel, automatic, 16' rack, fold down sides, hydraulic tailgate; tandem axle trailer w/ fertilizer box & auger; 1997 Kenworth, tandem axle straight truck, Cat diesel, 10 speed, 26' deck, air ride; 2005 IH 4300 single axle truck, w/ 24' van body, rollup door; 1993 Krohnert 075, 4 axle aluminum tanker, 7500 gallon trailer; 1990 Wall tri-axle trailer, 5th wheel construction trailer, low removable deck; 1992 Homemade 21' hydraulic dump, pup trailer, c/w Elo-Quip box, cross auger, roll tarp, ball hitch; 2011 Ford F150, 2wd pickup; 2002 GMC 2500, 4wd, Duramax truck.

SPECIAL ATTRACTION: 15 antique outboard boat motors, some rare!

CHECK OUT OUR WEBSITE FOR MORE DETAILS

PLEASE NOTE: A great selection of tractors, equipment, vegetable equipment, construction equipment & miscellaneous. Shabatara Farms, Komienski Farms, Underhill Farm Supply, Scotland Agromart, Sunrise Equipment Auctions, local dealers & local farmers are selling everything UNRESERVED except for the Bauer E-51 reel & MF 9800VE selling on last high bid confirmation.

TERMS: A 5% Fee applies to all items to a maximum \$2,000 (Buyer's Premium & Internet Fees). Payments will be received by cash, cheque, debit, credit, certified cheque or bank draft, E-Transfer & Wire Transfer (E-transfer & Wire Transfers preferred) by 4:00pm on March 31st. **Invoices will be emailed out after the auction closes!** If needed, financing must be prearranged prior to bidding. Items to be removed by April 8th. Loading from 8:30 to 3:30 Monday to Friday, No Saturday or Sunday Loading. Owners and Auctioneers are not responsible for accidents during viewings or loading.

AUCTIONEERS

TOM HAMULECKI 519-421-6957
MIKE HAMULECKI 519-535-0202
AUCTION OFFICE 519-424-1562



FOR PICTURES VISIT OUR WEBSITE @ www.sunrise-equipment.com

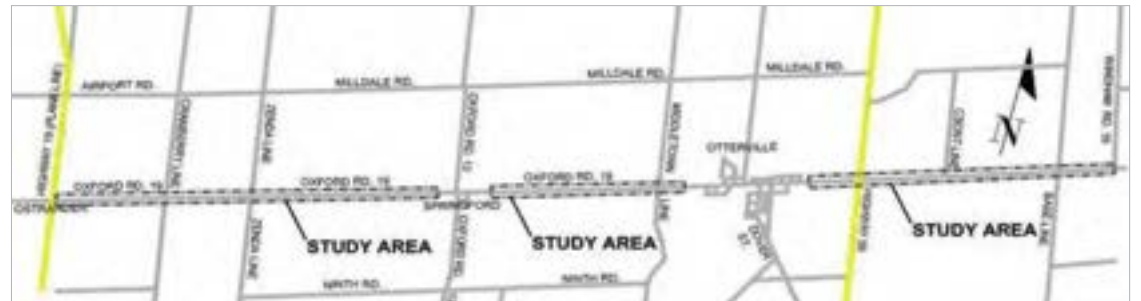
Public Notices



PUBLIC NOTICE

Notice of Study Commencement - Municipal Class Environmental Assessment Study Oxford Road 19 Corridor Improvement

Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

Following completion of Oxford County's 2019 Transportation Master Plan, the County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods.

Accordingly, the County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville - refer to the map herein.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the *Municipal Class Environmental Assessment* (2000, as amended in 2007, 2011 & 2015), approved under the *Ontario Environmental Assessment Act*.

We want to hear from you

This notice signals the commencement of the Class EA Study, a study that will identify and evaluate alternative solutions and alternative design concepts in consultation with adjacent property owners, regulatory agencies, indigenous communities, members of the local business community and the public.

Public consultation will occur during the course of the Study to present and receive comments on the project, alternative solutions and alternative designs for the corridor. Consultation with stakeholders is a key component of the Study process and input will be sought throughout the Study, including at a future public consultation centre which will be announced via a subsequent notice and posted to the County's project webpage: www.oxfordcounty.ca/Projects-studies.

An Environmental Study Report will be prepared and made available for final public review and comment upon completion.

Contacts for information

If you have questions or comments regarding the Study or wish to be added to the Project contact list, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194 | jkeith@oxfordcounty.ca

Henry Centen, P.Eng., Project Manager
R.J. Burnside & Associates Limited
519-340-2003 | henry.centen@rjburnside.com

Comments received during the Study will be considered and documented in the Environmental Study Report.

Information will be collected in accordance with the Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

This notice first issued on March 17, 2022

oxfordcounty.ca



Careers



**Administrative Assistant - Finance
(full-time position)**

The Township of South-West Oxford is seeking an energetic individual to accept the challenge of this entry level position. If you are a self-starter and able to manage priorities in a professional, efficient, and timely manner in an environment with frequent interruptions; you are adaptable and excel in an environment where teamwork is required to meet departmental and organizational goals, this opportunity may be right for you. This position will be responsible for a variety of administrative, coordination and customer service functions to support the Finance Department and to work as a team with other departments. The incumbent will assist with such duties as payable and receivables; updating of financial working papers for audit; and compile data and distribute information pertaining to finance. This position will provide you with the opportunity to gain experience in the workings of municipal government.

Your established skills in customer service and Microsoft Office plus your ability to learn and expand on the uses of software related to electronic records and workflows (Laserfiche) and finance (Great Plains/Microsoft Dynamics) will assist in the streamlining of work processes.

Your positive attitude and proactive approach in serving our community combined with your willingness to engage in team development will assist the municipality in realizing its' goal to develop and deliver municipal services for the growth and well-being of our community.

General Responsibilities:

Under the guidance of the Treasurer the successful candidate will provide support to Building, Emergency, Finance, Health and Parks & Recreation Services.

The job description is on the Township website at www.swox.org

Qualifications:

- Completion of a three (3) year college or specialized program in Business, Accounting or related discipline is preferred; or equivalent work experience
- Excellent interpersonal and public relations skills;
- Proven analytical and problem-solving skills;
- Good attention to detail and strong organizational skills
- Computer skills in Microsoft Office Applications (Excel and Word)
- Displays strong oral and written communication skills
- Ability to provide an appropriate criminal records check;
- Ability to work with and contribute positively to a service-oriented team where team members feel involved, included and supported.

Wages & Benefits (Grade 3 on 2022 Township Grid – entry level position) Annual Salary (35 hours per week): \$44,735.60 to \$55,928.60; plus, a comprehensive health benefits package and OMERS pension plan.

If this challenging career opportunity is for you, please apply in writing outlining your qualifications and experience **no later than 4:30 p.m. (local time) on Monday, April 4, 2022 to:**

South-West Oxford Human Resources

Attn: Brooke Crane
312915 Dereham Line
Mount Elgin, ON N0J 1N0
Email: payroll@swox.org

The Township of South-West Oxford is an equal opportunity employer and is committed to an inclusive, barrier-free environment. Please advise the Township's Human Resource Department if you require any accommodations to ensure you can participate fully and equally during the recruitment and selection process.

We thank all applicants who apply for this position, but only those selected for interviews will be contacted. In accordance with the Municipal Freedom of Information and Protection of Privacy Act, personal information is collected under the authority of the Municipal Act, 2001, S.O. 2001, chapter 25, as amended, and will be used for the purpose of candidate selection only.

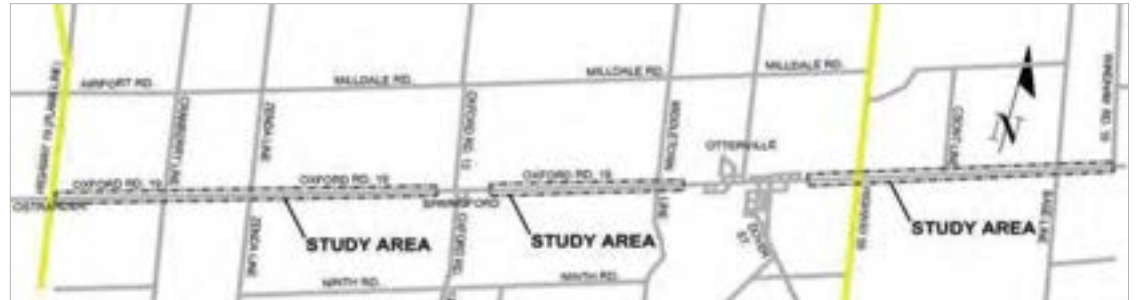
Public Notices



PUBLIC NOTICE

**Notice of Study Commencement - Municipal Class Environmental Assessment Study
Oxford Road 19 Corridor Improvement**

Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

Following completion of Oxford County's 2019 Transportation Master Plan, the County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods.

Accordingly, the County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville - refer to the map herein.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the *Municipal Class Environmental Assessment* (2000, as amended in 2007, 2011 & 2015), approved under the *Ontario Environmental Assessment Act*.

We want to hear from you

This notice signals the commencement of the Class EA Study, a study that will identify and evaluate alternative solutions and alternative design concepts in consultation with adjacent property owners, regulatory agencies, indigenous communities, members of the local business community and the public.

Public consultation will occur during the course of the Study to present and receive comments on the project, alternative solutions and alternative designs for the corridor. Consultation with stakeholders is a key component of the Study process and input will be sought throughout the Study, including at a future public consultation centre which will be announced via a subsequent notice and posted to the County's project webpage: www.oxfordcounty.ca/Projects-studies.

An Environmental Study Report will be prepared and made available for final public review and comment upon completion.

Contacts for information

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 519-539-9800 ext.3194 | jkeith@oxfordcounty.ca

Henry Centen, P.Eng., Project Manager
 R.J. Burnside & Associates Limited
 519-340-2003 | henry.centen@rjburnside.com

Comments received during the Study will be considered and documented in the Environmental Study Report.

Information will be collected in accordance with the Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

This notice first issued on March 17, 2022

oxfordcounty.ca



THE RIGHT CAREER

is waiting for you in the **CLASSIFIEDS!**

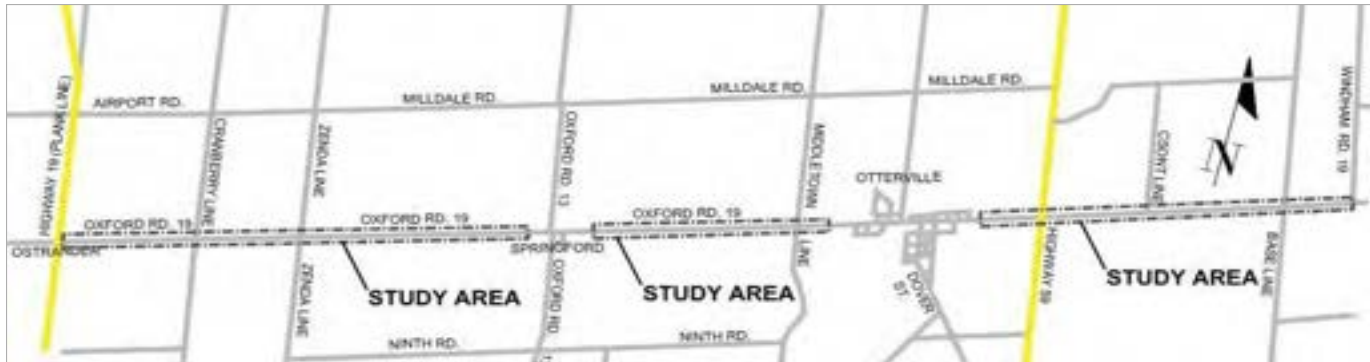


MAKE SURE THAT YOU GET
NOTICED
 BY PURCHASING AD SPACE



Notice of Public Consultation Centre - Municipal Class Environmental Assessment Study Oxford Road 19 Corridor Improvement

Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

Following completion of Oxford County's 2019 Transportation Master Plan, the County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods.

Accordingly, the County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville - refer to the map herein.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (2000, as amended in 2007, 2011 & 2015), approved under the Ontario Environmental Assessment Act.

We want to hear from you

Consultation with stakeholders is a key component of the Study process. As part of the Class EA Study, a Public Consultation Centre (PCC) is being held to present the planning level alternative solutions that are being considered and evaluated for improvements to the Oxford Road 19 Study Area.

Public Consultation Centre

The PCC will be a drop-in format to provide residents/interested parties with an opportunity to review and comment on the recommended preferred alternative solution. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC are as follows:

Date: Thursday, June 9, 2022
Time: 5:00 p.m. - 7:00 p.m.
Location: Springford Community Hall
429 Main St. W, Springford, Ontario

Contacts for information

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194 | jkeith@oxfordcounty.ca

Henry Centen, P.Eng., Project Manager
R.J. Burnside & Associates Limited
519-340-2003 | henry.centen@rjburnside.com

Comments received during the Study will be considered and documented in the Environmental Study Report.

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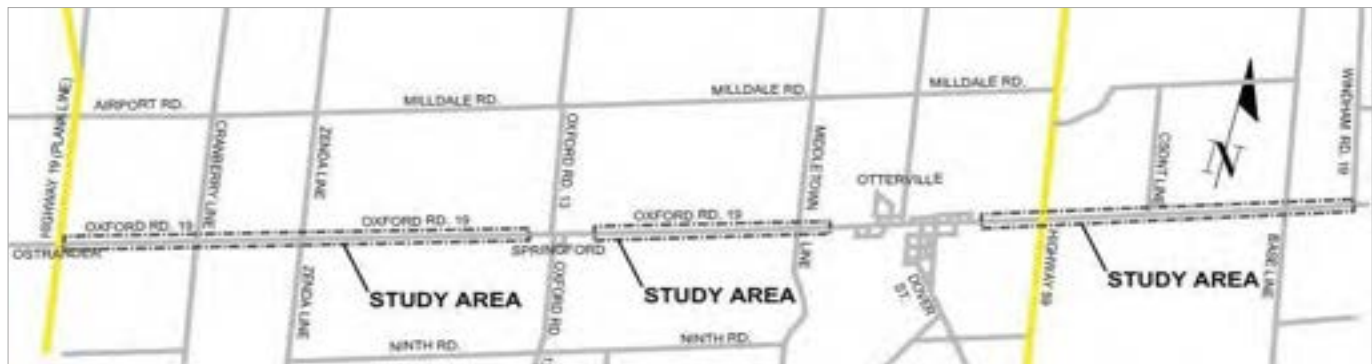
This notice first issued on May 19, 2022

Notice of Public Consultation Centre

Oxford Road 19 Corridor Improvement

Municipal Class Environmental Assessment Study

Following completion of the 2019 Transportation Master Plan, Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

The County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville. Please refer to the map.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (2000, as amended in 2007, 2011 & 2015), approved under the Ontario *Environmental Assessment Act*.

We want to hear from you

Consultation with stakeholders is a key component of the Study process. The first Public Consultation Centre (PCC) was conducted on June 9, 2022, to present alternative solutions and receive input on the recommended preferred solution. PCC#2 is being held to review the conceptual design alternatives that are being considered and evaluated for improvements to the Oxford Road 19 Study Area.

Public Consultation Centre #2

Similar to PCC#1, PCC#2 will be a drop-in format to provide residents/interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its consultant, R.J. Burnside & Associates Limited, will be present to answer questions and discuss next steps in the study. The date and location of PCC#2 are as follows:

Tuesday, December 6, 2022 - 5:00-7:00 p.m.
Springford Community Hall
429 Main St. W, Springford, Ontario

Contacts for information

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194 | jkeith@oxfordcounty.ca

Henry Centen, P.Eng., Project Manager
R.J. Burnside & Associates Limited
519-340-2003 | henry.centen@rjburnside.com

Comments received during the Study will be considered and documented in the Environmental Study Report.

Information will be collected in accordance with the Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

This notice first issued on November 17, 2022

Oxford Rd 19	BEGIN DATE	Mar-22		VERSION NUMBER		1				
County of Oxford	END DATE	Jul-23		DATE PREPARED		June 28/23				
	DURATION			AUTHOR		TR				
STAKEHOLDER	TITLE/ROLE	CONTACT				Upload Info to Consultation Database	Received Notice of Commencement	Received Notice of PCC#1	Received Notice of PCC#2	Removed From/Added to Mailing List
		Name	Address	Email	Phone					
Agencies										
Fisheries and Oceans Canada	Fisheries Protection Program			FisheriesProtection@dfo-mpo.gc.ca	855-852-8320		Y	Y	Y	
Canada Post	Delivery Services Officer, Delivery Planning	Neil Mazey		neil.mazey@canadapost.postescanada.ca			Y	Y	Y	
O.P.P.		Jennifer Ormsby		jennifer.ormsby@opp.ca			Y	Y	Y	
Ministry of Agriculture, Food and Rural Affairs			667 Exeter Road, London ON N6E 1L3	omafra.eanotices@ontario.ca			Y	Y	Y	
Ministry of Agriculture, Food and Rural Affairs	Policy Advisor	Dana Kieffer	667 Exeter Road, London ON N6E 1L3	dana.kieffer@ontario.ca;	519-873-4085		Y	Y	Y	Removed 221117_Email. Dana Kieffer email failed.
Ministry of Agriculture, Food and Rural Affairs	Policy Advisor	Drew Crinklaw	667 Exeter Road, London ON N6E 1L3	drew.crinklaw@ontario.ca	519-873-4085		Y	Y		Removed. 220317_Email. Drew Crinklaw has taken on a new assignment with OMAFRA.
Ministry of Heritage, Sport, Tourism and Culture Industries	Heritage Planner	Joseph Harvey	401 Bay St., Suite 1700, Toronto, ON, M7A 0A7	joseph.harvey@ontario.ca			Y	Y	Y	
Ministry of Heritage, Sport, Tourism and Culture Industries	Team Lead-Heritage (A)	Karla Barboza	401 Bay St., Suite 1700, Toronto, ON, M7A 0A7	karla.barboza@ontario.ca	416-314-7120		Y	Y	Y	

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		Name	Address	Email	Phone					
Ministry of Economic Development, Trade and Tourism	Manager, Cabinet Office Liaison and Policy Support Unit	Damian Dupuy	900 Bay St., 6th Fl., Hearst Block, Toronto, ON, M7A 2E1	damian.dupuy@ontario.ca	416-326-0938		Y	Y	Y	
Ministry of Municipal Affairs and Housing, Western Municipal Service Office	Manager, of Western Municipal Service Office	Erick Boyd	College Park 777 Bay St. 13th Fl., Toronto, ON, M7A 2J3	erick.boyd@ontario.ca	519-873-4033		Y	Y	Y	
Ministry of Transportation	Executive (A) Director	Jennifer Graham Harkness	Macdonald Block Room M1-21, 900 Bay St., Toronto, ON, M7A 1Z8	jennifer.grahamharkness@ontario.ca	416-212-3444		Y	Y	Y	
Ministry of Transportation	Corridor Management Planner	Martin Leyten	Exeter Road Complex, 659 Exeter Rd, London, ON N6E 1L3	Martin.leyten@ontario.ca			Y	Y	Y	
Ministry of Transportation	Corridor Management Planner	Michael Kilgore	Exeter Road Complex, 659 Exeter Rd, London, ON N6E 1L3	Michael.Kilgore@ontario.ca	289-973-8580			Y	Y	Added. Added as replacement to Allan Hodgins.
Ministry of Transportation	Corridor Management Planner	Allan Hodgins	Exeter Road Complex, 659 Exeter Rd, London, ON N6E 1L3	allan.hodgins@ontario.ca	289-973-8580		Y			Removed. 220317_Email. No longer involved with planning related items located within Oxford County.
Ministry of the Environment, Conservation and Parks - Region Technical Support Section				eanotification.swregion@ontario.ca			Y	Y	Y	

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		Name	Address	Email	Phone					
Ministry of the Environment, Conservation and Parks - Regional Office	Environmental Resource Planner & EA Coordinator	Mark Badali		mark.badali1@ontario.ca;			Y	Y	Y	
Ministry of the Environment, Conservation and Parks - Environmental Assessment and Permissions Branch				MEA.NOTICES.EAAB@ontario.ca			Y			
Ministry of the Environment, Conservation and Parks Permissions & Compliance Section				SAROntario@ontario.ca;			Y	Y	Y	

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		Name	Address	Email	Phone					
Ministry of the Environment, Conservation and Parks Permissions & Compliance Section	A/Management Biologist	Brandan Norman		brandan.norman2@ontario.ca;			Y	Y	Y	Removed. 221117_Email. Brandan Norman email failed.
Ministry of Northern Development, Mines, Natural Resources and Forestry (NDMNRF) District Office	Management Biologist, Aylmer District	Jason Webb		Jason.Webb@ontario.ca; MNR.F.Ayl.Planners@ontario.ca;			Y			
Ministry of Northern Development, Mines, Natural Resources and Forestry (NDMNRF) District Office	District Planner, Aylmer/ Guelph	Karina Cerniavskaja	Ontario Government Bld. 1 Stone Road West, Guelph ON	Karina.Cerniavskaja@ontario.ca; MNR.F.Ayl.Planners@ontario.ca;			Y	Y	Y	
County of Oxford	CAO	Michael Duben	21 Reeve Street, P. O. Box 1614 Woodstock, Ontario N4S 7Y3	mduben@oxfordcounty.ca			Y	Y	Y	
County of Oxford	Clerk	Chloe Senior	21 Reeve Street, P. O. Box 1614 Woodstock, Ontario N4S 7Y3	csenior@oxfordcounty.ca			Y	Y	Y	
County of Oxford	Community Planning	Planning	21 Reeve Street, P. O. Box 1614 Woodstock, Ontario N4S 7Y3	planning@oxfordcounty.ca			Y	Y	Y	

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County of Oxford	Development Planner	Heather St. Clair	21 Reeve Street, P. O. Box 1614 Woodstock, Ontario N4S 7Y3	hstclair@oxfordcounty.ca			Y	Y	Y	
County of Oxford	Development Planner	Justin Miller	21 Reeve Street, P. O. Box 1614 Woodstock, Ontario N4S 7Y3	jdmler@oxfordcounty.ca			Y			Removed. Received notification that he will be away from office until further notice.
County of Oxford		Eric Gilbert		egilbert@oxfordcounty.ca			Y	Y	Y	
County of Oxford		Shawn Vanacker		svanacker@oxfordcounty.ca			Y	Y	Y	
County of Oxford		Reuben Davis		rdavis@oxfordcounty.ca			Y	Y	Y	
County of Oxford		Frank Gross		fgross@oxfordcounty.ca			Y	Y	Y	
County of Oxford		Melissa Abercrombie		mabercrombie@oxfordcounty.ca			Y	Y	Y	
County of Oxford		David Simpson		dsimpson@oxfordcounty.ca			Y	Y	Y	
County of Oxford	Paramedic Services			ems@oxfordcounty.ca			Y	Y	Y	
Township of Southwest Oxford	Mayor (and Oxford County Councillor)	David Mayberry	312915 Dereham Line Mt. Elgin ON N0J 1N0	mayor@swox.org	519-485-0477		Y	Y	Y	
Township of Southwest Oxford	CAO	Mary Ellen Greb	312915 Dereham Line Mt. Elgin ON N0J 1N0	cao@swox.org			Y	Y	Y	
Township of Southwest Oxford	Drainage Superintendent & Building Inspector	Daniel Leduc		drainage@swox.org			Y	Y	Y	
Township of Southwest Oxford	Works Superintendent	Adam Prouse		aprouse@swox.org			Y	Y	Y	
Township of Southwest Oxford	Fire Chief	Rob Serson		firechief@swox.org			Y	Y	Y	
Township of Southwest Oxford	Clerk	Julie Forth	312915 Dereham Line Mt. Elgin ON N0J 1N0	clerk@swox.org	519-485-0477 x7023		Y	Y	Y	

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		Name	Address	Email	Phone					
Hydro One Networks Inc.				CentralFBCPlanning@HydroOne.com; SecondaryLandUse@HydroOne.com			Y	Y	Y	
Execulink Telecom		Gregory Csont	P.O. Box 441 Otterville, ON N0J1R0	gcsont1@execulink.com;			Y	Y	Y	
Execulink Telecom	Operations Supervisor	Jeff Soetemans	1127 Ridgeway Rd, Woodstock, Ontario, N4V 1E3, Canada	jeff.soetemans@execulink.com			Y	Y	Y	
Execulink Telecom			1127 Ridgeway Rd, Woodstock, Ontario, N4V 1E3, Canada		(519) 456-7200		Y			
TDL Canada	Oxford identified "The TDL Group" which is the Tim Hortons licensing company. TDL Canada seems more appropriate. Confirm with client?		273 German School Road Paris, ON N3L 3E1	swo@tdlcanada.ca	Tel: 519-448-1667 Fax: 519-448-1563		Y	Y	Y	
Nor-Del Cablevision			P.O. Box 340 Norwich Ontario, N0J 1P0.	nordel@nor-del.com	ocal number: (519) 879-6527 Toll free number: 1-800-563-1954 *Toll free number available only in 519 area codes*		Y	Y	Y	
Bell Canada	Implementation Department	Rick de Bokx	5115 Creekbank Road, 3rd Floor, West Tower Mississauga ON L4W 5R1	richard.de_bokx@bell.ca	905-219-4558		Y	Y	Y	Added. Email requested he be added as correct contact.
Bell Canada	Implementation Department	Scott Moon	5115 Creekbank Road, 3rd Floor, West Tower Mississauga ON L4W 5R1	scott.moon@bell.ca	905-219-4558		Y			Removed. 220317_Email. Retired as of Feb. 28.

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Bell Canada, Municipal Operations Centre	Planner & Manager, ROW Control Centre	John Lachapelle	100 Borough Drive, Floor 5 Blue, Scarborough ON M1P 4WZ	rowcentre@bell.ca			Y	Y	Y	
Cogeco Cable Inc.	Head office, Montréal		1, Place Ville-Marie, Office 3301 Montréal (Québec) H3B 3N2				Y			
Rogers Communications	Rogers Municipal Operations Centre		800 York Street LONDON, ON N5W 2S9	Rogers.MOC@telecon.ca			Y	Y	Y	
Rogers Communications	Planning Coordinator	Marion Wright	3573 Wolfedale Road, Mississauga ON L5C 3T6		905-897-3914; 888-764-3771		Y			
Union Gas Limited	Construction & Project Manager Advisor	Amanda Zocco	Sarnia/ London contact	azocco@uniongas.com			Y	Y	Y	
Union Gas Limited	Sarnia/London contact	Amanda Zocco		azocco@uniongas.com			Y	Y	Y	
Enbridge	Project Manager	Adam Collier	603 Kumpf Drive, Waterloo N2V 1K3	Adam.Collier@enbridge.com	519-885-7400 ext. 5067506 519-635-9488			Y	Y	Added. Name provided by Kvein Schimus.
Enbridge	Project Manager	Kevin Schimus	603 Kumpf Drive, Waterloo N2V 1K3	Kevin.Schimus@enbridge.com	519-885-7400 ext. 5067506 519-635-9488		Y			Removed. No longer managing this area of province.
Enbridge Gas Distribution Inc.	Supervisor, Planning and Design	Vince Cina	500 Consumers Road, North York ON M2J 1P8	vince.cina@enbridge.com			Y	Y	Y	
Enbridge Pipelines Inc.				mark-ups@enbridge.com;			Y	Y	Y	
Enbridge Pipelines Inc.				notifications@enbridge.com;			Y	Y	Y	
TC Energy Corporation (which was previously Trans-Canada Pipelines Ltd. and TransCanada Corp. now merged)	Plannng Co-ordinator, EA contact	Darlene Presley	441 Brant Street, Suite 204 Burlington ON L7R 2G4	dpresley@mhbcpplan.com	905-639-8686 ext. 229 Cell: 705-627-2302		Y	Y	Y	Removed. TCPL does not have any pipelines crossing the study area as outlined on the map provided.

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		Name	Address	Email	Phone					
MHBC Planning, Urban Design & Landscape Architecture	Plannng Co-ordinator, EA contact			TCEnergy@mhbcplan.com			Y	Y	Y	Removed. TCPL does not have any pipelines crossing the study area as outlined on the map provided.
MHBC Planning, Urban Design & Landscape Architecture	Plannng Co-ordinator, EA contact	Darlene Presley	442 Brant Street, Suite 204 Burlington ON L7R 2G4	dpresley@mhbcplan.com	905-639-8686 ext. 229 Cell: 705-627-2302		Y			Removed. TCPL does not have any pipelines crossing the study area as outlined on the map provided.
Trans-Northern Pipelines Inc.		Cliff Lee	45 Vogell Road, Suite 310 Richmond Hill ON L4B 3P6	clee@tnpi.ca; crossingrequesteast@tnpi.ca;			Y			Removed. 220317_Email. Remove TNPI from this distribution list we have no facilities within 45 km.
Zayo				Utility.Circulations@zayo.com	416-649-7527		Y			Removed. 220328_Email. Zayo responded that there are no existing plant in the area indicated in your submission.
Rail										
CN Rail	Manager, Public Works Design and Construction	Stefan Linder	4 Welding Way (off Admin. Road) Vaughan ON L4K 1B9	stefan.linder@cn.ca	905-669-3264		Y	Y	Y	
CN Rail	Manager, Public Works Design and Construction	Michael Vallins		michael.vallins@cn.ca			Y	Y	Y	Removed. 220317_Email. Michael Vallins is on a leave of absence.
Ontario Southland Railway Inc.		Jeff Willsie		jeff.willsie@ontsouthland.com			Y	Y	Y	
Ontario Southland Railway Inc.		Brad Jolliffe	Salford, ON, N0J 1W0	brad.jolliffe@ontsouthland.com			Y	Y	Y	

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		Name	Address	Email	Phone					
Ontario Southland Railway Inc.		Greg Rankin	Salford, ON, N0J 1W0	greg.rankin@ontsouthland.com			Y	Y	Y	
CP Rail, Public Works – Eastern Region		Brian Costigan	Grade Separation and Public At-Grade Crossing Projects	Brian_Costigan@cpr.ca			Y	Y	Y	



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Appendix H2

PCC #1 Summary



BURNSIDE

**Public Consultation Centre Summary
Report**

**Oxford Road 19 Corridor
Improvement**

**Oxford County
Oxford County Public Works**

**Public Consultation Centre Summary
Report**

Oxford Road 19 Corridor Improvement

**Oxford County
Oxford County Public Works**

**R.J. Burnside & Associates Limited
21 Reeve Street, P.O. Box 1614
Woodstock, Ontario N4S 7Y3**

**February 2023
300053425.0000**



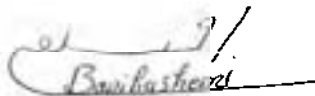
R.J. Burnside & Associates Limited

Report Prepared By:



Sylvia Waters
Project Administrator
SW:Iam

Report Reviewed By:



Avid Banihashemi
Environmental Project Manager

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1.0	Introduction and Background.....	1
2.0	Method of Notification.....	2
3.0	Public Meeting Format.....	2
4.0	Participation Levels and Summary of Comments Received.....	3
5.0	Next Steps.....	5

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Appendices

- Appendix A Newspaper Advertisement
- Appendix B Display Boards
- Appendix C Comment Sheets and Emails

1.0 Introduction and Background

Oxford County's 2019 Transportation Master Plan identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods. Accordingly, Oxford County (County) initiated a Municipal Class Environmental Assessment (MCEA) to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study Area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville. The County has identified the **Problem Statement** as:

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

Alternative Solutions include:

1. Do nothing.
2. Regular maintenance and surface treatments.
3. Structural improvements to road within existing right-of-way.
4. Widen lanes and shoulders within a widened right-of-way – no structural improvements to the road.
5. Structural improvements to the road, including widening lanes and shoulders within a widened right-of-way.

The planning of improvements to Oxford Road 19 are being carried out in accordance with the *Schedule 'C' requirements (Phases 1 to 4)* of the Municipal Engineers Association Municipal Class Environmental Assessment document (October 2000, as amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*.

A key component of the study includes consultation with interested stakeholders. This report documents the Public Consultation Centre (PCC), held on June 9, 2022 from 5:00 p.m. to 7:00 p.m. at Springford Community Hall 429 Main St. W, Springford and summarizes the notification process, the information presented, and the comments received during and after the PCC.

2.0 Method of Notification

The Notice of Commencement was published in the *Oxford Review* on March 17 and March 24, 2022 signalling the commencement of the Study. Details of the date, time, and purpose of the PCC were published in the *Oxford Review* on May 26 and June 2, 2022. A copy of the advertisement is provided in Appendix A.

Notification of the PCC was also mailed/ emailed to regulatory agencies, municipalities, Indigenous communities, and local residents who live within the Study Area and posted on the Oxford County website (www.oxfordcounty.ca/Projects-studies).

3.0 Public Meeting Format

The PCC was arranged as a drop-in format with Display Boards placed around the room. Attendees were greeted upon arrival, were encouraged to sign-in, and were offered a Comment Sheet to provide comments on the project and Alternative Solutions.

Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) were present to discuss the project with interested members of the public and answer questions.

A copy of the Display Boards is provided in Appendix B. The Display Boards covered the following topics:

- Welcome
- Public Consultation Centre Purpose
- Study Area
- MCEA Process
- Problem/Opportunity Statement
- Planning Context
- Existing Land Use
- Supporting Studies
 - Transportation Study
 - Natural Environment Assessment
 - Stage 1 Archaeological Assessment
 - Cultural Heritage Resource Assessment
 - Air and Noise Impact Assessment
 - Stormwater Management Assessment
- Future Growth
- Existing Road and Active Transportation
- Evaluation of Alternatives
- Alternative Solutions
- Next Steps

Participants were requested to provide input to the process by completing the available Comment Sheets. If individuals wished to take Comment Sheets home to fill out later, or to download from the Oxford County website, they were requested to return their comments either via email, fax or to the mailing address provided, by June 23, 2022.

4.0 Participation Levels and Summary of Comments Received

This section provides an overview of the feedback received from participants at the PCC and following the PCC. A total of 20 people attended the PCC excluding the Project Team members.

Written Comment Sheets were received from four stakeholders, as well as one phone call and two emails during comment period of the PCC.

Participant comments are reviewed to provide a better understanding of stakeholder opinions and to provide feedback in the evaluation of the Preferred Alternative. The summary of comments and advice received during the PCC is intended to provide an indication of overall shared issues, opinions and concerns of participants. Copies of individual comments are provided in Appendix C.

Key issues addressed within the written comments relate to:

- Property access during construction.
- Vertical alignment sight line issue (blind spots).
- Horizontal alignment and visibility issues.
- Impacts to road drainage.
- Increase in volume of heavy truck traffic.

Table 4-1: PCC Input

Id Code	Comment	Response (Burnside)
Comment Sheet		
A	Concern about access to owned buildings and businesses in Otterville, and alternative route signage for traffic.	Concerns regarding access to owned buildings and businesses have been noted and will be taken into consideration when developing a commitment to a traffic management plan by the contractor.
B	Concern for blind spot created by large hill on Oxtander Road west of Zandra Line, and level hill in front of St. Charles' Church.	Concerns regarding vertical alignment deficiencies have been noted and will be taken into consideration when developing a Preferred Solution and Preferred Design.
C	Resident notes that the Evaluation Table is a very good summary and should be presented with a detailed explanation allowing public to discuss and ask questions.	The efforts will be taken to present the Evaluation Table with a more detailed explanation at the next PIC allowing public to discuss and ask more detailed questions.
D	Resident notes that the eastern boundary of Oxford Road 19 at connection point with Norfolk County is a very awkward portion of roadway, as it does not line-up in a straight-away but a jog. Many accidents have occurred. Resident also noted that a widening of the roadway and shoulder from Csonk Line to the eastern boundary will impact a municipal drain. Runoff in the spring collects east of Csonk Line into the drainage ditch on south side of road before crossing under road at Csonk Line and emptying further up the concession road.	These concerns have been noted and will be taken into consideration when developing a Preferred Solution and Preferred Design.
One call	Resident enquired whether property was within the Study Area. Resident noted that notice had been received and was planning on attending. Resident also noted that it appears that there is approximately no setback from property line to existing house.	County responded that property in question is in Study Area. Setback concerns have been noted and will be taken into consideration when developing a Preferred Solution and Preferred Design.
Email 1	Requested confirmation of the availability of the PCC exhibits. Resident does not oppose making the road wider, improving the shoulders, and maybe even paving them, or any other work that will make the road safer and better. It was noted that everything the proposals are showing are great and will improve travel for everyone. The concern is the high volume of heavy truck traffic through the villages and the increase in speeding once the road is levelled. Resident would like to see these consequences minimized as much as possible so safety and betterment for some does not become worse for others.	Burnside responded that concerns regarding truck traffic and speeding have been noted and will be taken into consideration when developing a Preferred Solution and Preferred Design. In addition, it was noted that the links to the exhibits have been added to the website.
Email 2	It was noted that the presentation of the project at the PCC was very good. The Resident feels that the road should be totally reconstructed to handle heavy traffic (i.e., large trucks and farm equipment). The resident has observed the deterioration of the road, and the increasing traffic at all hours of the day. Resident feels the road does need wider shoulders, to allow pedestrians and cyclists. The Resident believes that the alignment (as Oxford Road 19 transitions into Norfolk Road 19) should be reconstructed so that the existing curve in the road be straightened allowing more visibility and a safer operation of vehicles.	These suggestions and concerns have been noted and will be taken into consideration when developing a Preferred Solution and Preferred Design.

5.0 Next Steps

Comments and concerns received at PCC #1 will be reviewed for incorporation into the evaluation of a Preferred Alternative and the Environmental Study Report (ESR). Next steps include:

- Confirm Preferred Solution (July-August 2022).
- Complete Transportation Study (July-August 2022).
- Complete additional supporting studies (July-August 2022).
- Develop and Evaluate Alternative Design Concepts (August 2022).
- Public Information Centre # 2 (September 2022).
- Environmental Study Report (October 2022).
- File Environmental Study Report for Schedule C Class EA Study (November 2022).

Documenting the planning and decision-making process of the study, will be prepared for public review and comment at the completion of the study. A Notice of Completion will be issued following the completion of the ESR. The ESR will be available for public review for a minimum 30-day review period.



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Appendix A

Newspaper Advertisement

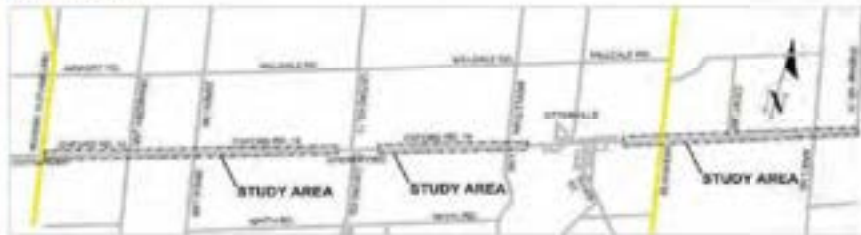
Public Notices



PUBLIC NOTICE

Notice of Public Consultation Centre - Municipal Class Environmental Assessment Study
Oxford Road 19 Corridor Improvement

Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

Following completion of Oxford County's 2019 Transportation Master Plan, the County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods.

Accordingly, the County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which includes the Settlements of Springford and Otterville - refer to the map herein.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (2000, as amended in 2007, 2011 & 2015), approved under the Ontario Environmental Assessment Act.

We want to hear from you

Consultation with stakeholders is a key component of the Study process. As part of the Class EA Study, a Public Consultation Centre (PCC) is being held to present the planning level alternative solutions that are being considered and evaluated for improvements to the Oxford Road 19 Study Area.

Public Consultation Centre

The PCC will be a drop in formal to provide residents/interested parties with an opportunity to review and comment on the recommended preferred alternative solution. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC are as follows:

Date: Thursday, June 9, 2022
Time: 5:00 p.m. - 7:00 p.m.
Location: Springford Community Hall
429 Main St. W., Springford, Ontario

Contacts for information

If you have questions or comments, please contact either of the following project team members:

Janice Keith, P. Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194 | jkeith@oxfordcounty.ca
Henry Conton, P. Eng., Project Manager
R.J. Burnside & Associates Limited
519-340-2003 | henry.conton@rjburnside.com

Comments received during the Study will be considered and documented in the Environmental Study Report.

Information will be collected in accordance with the Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

This notice first issued on May 19, 2022

oxfordcounty.ca



Public Notices



PUBLIC NOTICE

Pesticide Use

For weed and brush control annual spray program

Oxford County will be spraying rural roadsides on County roads in Blandford-Blenheim, East Zorra-Tavistock, Ingersoll, Norwich, South-West Oxford, Tilsburg, Woodstock and Zorra with Clearview Herbicide with aminopyralid (present as potassium salt) and metsulfuron-methyl, Registration No. 29752, and Gateway adjuvant, a non-ionic paraffinic oil blend surfactant, Registration No. 31470, under the Pest Control Products Act (Canada), to control weed and brush growth under the health and safety exception that affects Public Works.

Spraying will take place on the following Oxford County Roads: 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 24, 25, 27, 28, 29, 30, 33, 34, 36, 37, 40, 45, 46, 51, 55, 59, 60 and 119.

Weed spraying is scheduled to begin on or after May 2, 2022 and is scheduled to end on or before June 30, 2022.

The commencement date is approximate and is weather permitting. To learn how the weed control program is progressing in your area contact:

Shawn G. Vanacker, C. Tech., CR6-S, CMM III
Supervisor of Transportation
Oxford County Public Works
519-539-9800 or 1-866-537-7778 (toll-free) | ext. 3106

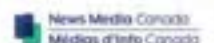
Issued June 2, 2022

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Public Notices



59th Annual General Meeting
DATE: Wednesday, June 15, 2022
TIME: 5:30 to 6:00 pm
LOCATION: Zoom Meeting

You are invited to join the Board of Directors for their annual business meeting to report activities, impact and investments to the community and to:

- 1. Approve the Minutes of the September 15, 2021 AGM;
2. Ratify the Actions of the Board of Directors during 2021/2022;
3. Receive the 2021/2022 Audited Financial Statements;
4. Approve the 2022 Nominating Committee's Report; and
5. Approve Board's Recommended Clarifying Change to By-Law#1

To attend United Way Oxford's Electronic AGM please register by Friday, June 10, 2022 by emailing info@unitedwayoxford.ca or by calling the United Way office at (519) 539-3851 x301 or 1 (877) 280-1391.

Contact: Kelly Gibson, Executive Director, United Way Oxford

Tel: (519) 539-3851 x301 or 1 (877) 280-1391

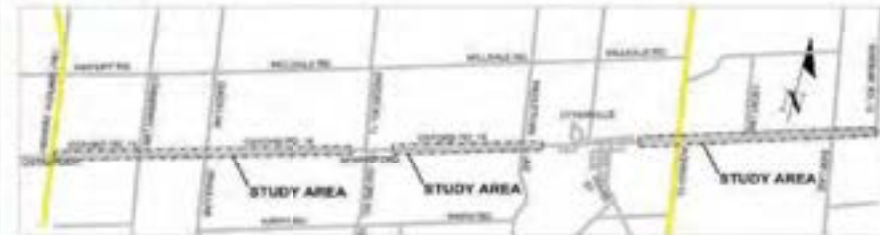
Public Notices



PUBLIC NOTICE

Notice of Public Consultation Centre - Municipal Class Environmental Assessment Study
Oxford Road 19 Corridor Improvement

Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

Following completion of Oxford County's 2019 Transportation Master Plan, the County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods.

Accordingly, the County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (2000, as amended in 2007, 2011 & 2015), approved under the Ontario Environmental Assessment Act.

We want to hear from you

Consultation with stakeholders is a key component of the Study process. As part of the Class EA Study, a Public Consultation Centre (PCC) is being held to present the planning level alternative solutions that are being considered and evaluated for improvements to the Oxford Road 19 Study Area.

Public Consultation Centre

The PCC will be a drop-in format to provide residents/interested parties with an opportunity to review and comment on the recommended preferred alternative solution. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study.

Date: Thursday, June 9, 2022
Time: 5:00 p.m. - 7:00 p.m.
Location: Springford Community Hall, 429 Main St. W., Springford, Ontario

Contacts for information

If you have questions or comments, please contact either of the following project team members:

- Jessie Keith, P.Eng., Project Manager, Oxford County Public Works
Henry Corbett, P.Eng., Project Manager, R.J. Burnside & Associates Limited

Comments received during the Study will be considered and documented in the Environmental Study Report.

Information will be collected in accordance with the Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

This notice first issued on May 19, 2022

oxfordcounty.ca



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Truth be told.



Celebrating 50th Anniversary of Canadian Journalists Association

Mike's Custom Jewellery advertisement with contact info: 519-425-0670

Celebrate! advertisement featuring a baby holding a flower



BURNSIDE

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Appendix B

Display Boards

Appendix B



Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements

PUBLIC CONSULTATION CENTRE / SPRINGFORD HALL
429 MAIN ST W SPRINGFORD, ON
JUNE 9, 2022
5:00 P.M. - 7:00 P.M.

WELCOME

to the Public Consultation Centre for the
Oxford Road 19 Corridor Improvements

Please Sign In

- Meet with Study Team Members
- Review the display materials and discuss your questions and ideas with the Study Team
- Please fill out a comment sheet and return it to the Study Team in person, by email or fax by **June 23, 2022**

STUDY AREA

The County is undertaking a Class EA study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study Area includes approximately 16 kilometres of Oxford Road 19 between Highway 19 (Plank Line), and the Norfolk County boundary (Windham Road 19), which excludes the Settlements of Springford and Otterville.



The site location and approximate extent of the Study Area are shown on the map.

PROBLEM/OPPORTUNITY STATEMENT

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

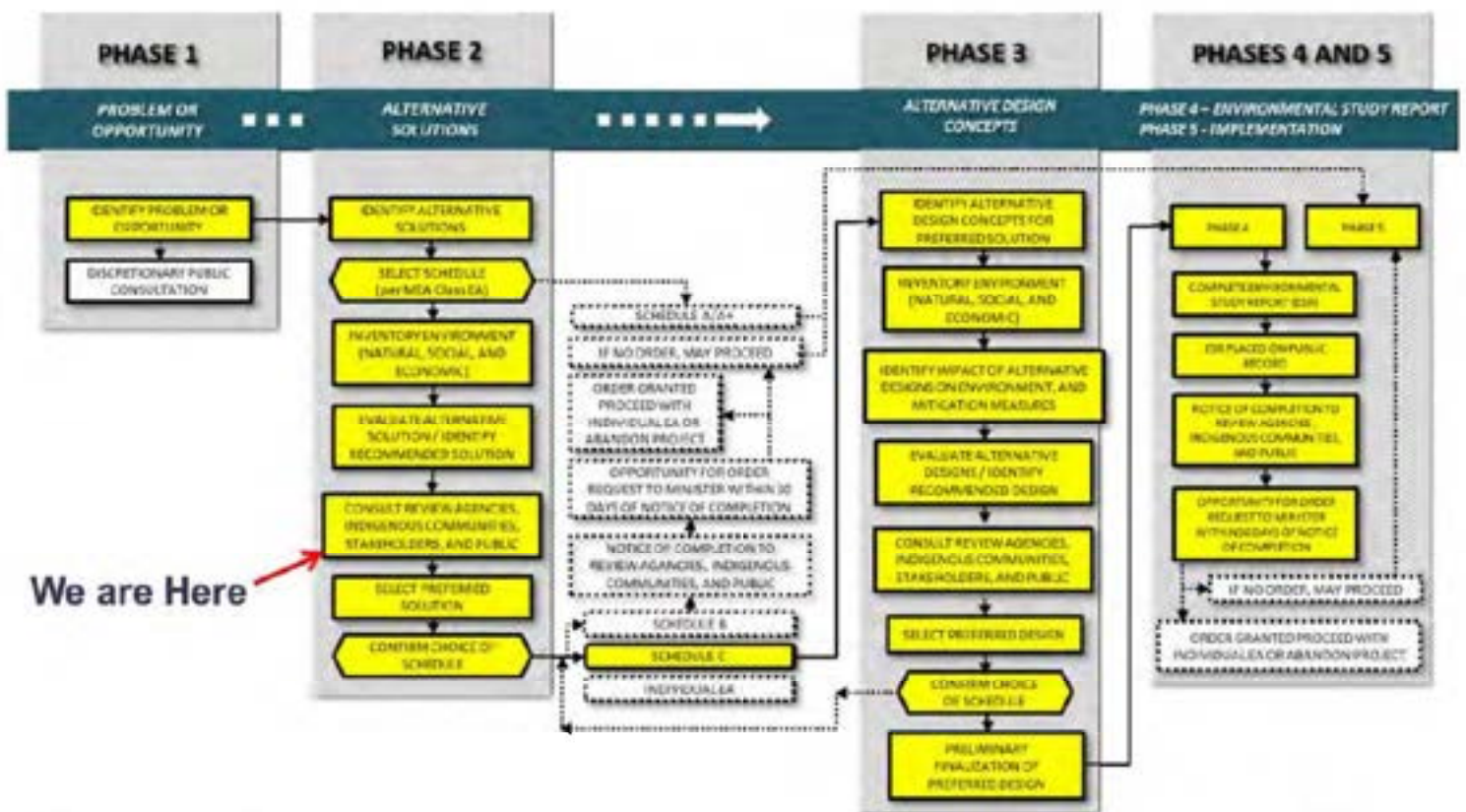


THE EA PROCESS

The Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act.

Nearing completion of the study, an Environmental Study Report (ESR) will be prepared and made available for final public review and comment.

THE EA PROCESS



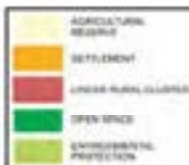
PLANNING CONTEXT

- Provincial Policy Statement, 2020
- County of Oxford Official Plan (consolidation of Official Plan amendments, as of March 31, 2021)
- Oxford County Transportation Master Plan (2019)
- Phase One Comprehensive Review Oxford County (2020)
- Draft Cycling Master Plan (2021)
- TAC Design Guidelines (2017)
- Guidelines from Ontario Traffic Manual – Book 18 (Cycling Facilities)



EXISTING LAND USE

- The two communities of Springford and Otterville within Norwich are zoned as Settlements by the County of Oxford Official Plan.
- Outside of these communities, most land is Agricultural Reserve with lands surrounding Ostrander is a Rural Cluster. All other land in South-West Oxford is Agricultural Reserve.
- Between the villages of Springford and Otterville is Spittler Creek which is zoned as Environmental Protection. This designation falls within the County's Natural Heritage System.
- Within Otterville, the Otter Creek floodplain crosses Oxford Road 19. As floodplain falls within the Open Space designation, this land also falls within the County's Natural Heritage System.
- Below the floodplain to the south of Oxford Road 19, the Environmental Protection designation begins.



SUPPORTING STUDIES

- Transportation Study
- Natural Environment Assessment
 - Terrestrial Habitat Assessment
 - Aquatic Habitat Assessment
- Stage 1 Archaeological Assessment
- Cultural Heritage Resource Assessment
- Air and Noise Impact Assessments
- Stormwater Management Assessment



NATURAL HERITAGE RESOURCES

Designated Features within the Study Area

- Significant valleylands and locally significant natural heritage features
- Watercourses: Spittler Creek, Plumb Creek, and Big Otter Creek.
- Spittler Creek and Big Otter Creek are both associated with significant valleylands
- Otterville Provincially Significant Wetland (PSW) Complex and other wetlands across Spittler Creek

Woodlands



NATURAL HERITAGE RESOURCES

Aquatic habitat

No Species at Risk (SAR) were found through the desktop review. Field work will be conducted in the coming field season to assess aquatic habitat in the Study Area.

- Big Otter Creek is a warm water creek with spring-spawning species such as Largemouth Bass and Yellow Perch.
- Spittler Creek is a cool water creek with observed spring-spawning species.
- Plumb Creek is a cool water creek with fall and spring-spawning species such as Brown Trout, Creek Chub, and Blacknose Dace.



NATURAL HERITAGE RESOURCES

Terrestrial Habitat

The desktop review found several potential Species At Risk (SAR) such as:

Snapping Turtle (Special Concern), Wood Thrush (Special Concern), American Badger (Endangered), American Chestnut (Endangered)

Field work will be conducted in the coming field season to assess ecological communities present in the Study Area.

Bats

The desktop review found records of 4 bat species: Eastern Small-footed Myotis, Little Brown Myotis, Northern Myotis, and Tricoloured Bat

Impact to potential habitat is not expected. Direct impact is to be avoided with timing vegetation removal to be completed outside of the active season for bats.



SOURCE WATER PROTECTION

The Study Area falls under the Long Point Region Source Water Protection Area and Lake Erie Source Water Protection Area.

Wellhead Protection Areas

Three portions of the study area have been identified to fall under Wellhead Protect Areas (i.e., part of Ostrander, north part of Springford and east of Otterville).



Issue Contributing Area

The area west of Otterville is also considered an Issue Contributing Area (ICA), for its potential to contribute elevated concentrations of particular substances to the drinking water source (e.g., chloride, sodium, nitrate).

Highly Vulnerable Aquifer

A significant part of the Oxford Road corridor (i.e., from west of Otterville to east of Oxford Road 59) is also located in a Highly Vulnerable Aquifer (HVA) area.

Significant Groundwater Recharge Areas

A significant part of the study area is also within Significant Groundwater Recharge Areas (SGRAs)

ARCHAEOLOGICAL RESOURCES

The MHTSCI Standards & Guidelines lists criteria that are indicative of archaeological potential. The Study Area meets the following criteria indicative of archaeological potential:

- 27 Previously identified archaeological sites within 1 km (7 of which are within 50 metres);
- Early historic transportation routes (County Road 19 and other intersecting historical roads) and proximity to early settlements (Ostrander, Springford, Erbtown, Otterville);
- Proximity to cemeteries (Springford Community Cemetery, St. Charles Anglican Cemetery, Pine Street Burial Ground, Erbtown Cemetery and the unmarked Pettman Cemetery near Cranberry Line); and
- Water sources: primary, secondary, or past water source (Spittler Creek and Big Otter Creek with their various tributaries) and well-drained soils (Huron clay loam, Fox loamy sand, Honeywood silt loam).

Stage 2 archaeological assessment is recommended in any areas identified as exhibiting archaeological potential beyond the existing right-of-way. Stage 2 archaeological assessment if required, will be completed during the detailed design.



CULTURAL HERITAGE RESOURCES

Two known heritage properties, designated under Part IV of the Ontario Heritage Act, were identified within the project study area. A total of 5 Potential Built Heritage Resources and 23 Potential Cultural Heritage Landscapes were identified within the project study area.



Built Heritage Resources and Cultural Heritage Landscapes throughout the corridor

FUTURE GROWTH

- By 2046, Oxford County is expected to grow by about 47,000 people and employment is expected to grow by 21,000 jobs. While much of this growth will be in Woodstock, Tillsonburg and Ingersoll, Oxford Road 19 provides an important linkage for the movement of goods and people within the County.
- Traffic growth on Oxford Road 19 is impacted by growth in the Township of Norwich and the Township of South-West Oxford, which has historically been low.
- A small amount of subdivision development is forecasted to occur in the Villages of Otterville and Springford.
- Industrial lands in the Town of Norwich have the potential for development or redevelopment, with Oxford Road 19 providing a linkage to this area and to the County of Norfolk.



Oxford Road 19 provides an important east-west link for the movement of goods and people within the County and supports growth and development in this area.

FUTURE GROWTH: DEVELOPMENTS

- A 9-lot subdivision was constructed in Springford over the last few years and 4 lots currently have draft approval.
- About 42 lots have been developed in Otterville over the last several years, with a 34-lot subdivision currently being built. A further 30-lot subdivision has draft approval in the village.
- In 2019, the Township of Norwich had 172 units approved for residential development. Most of the development in Norwich Township is to occur in the Town of Norwich.



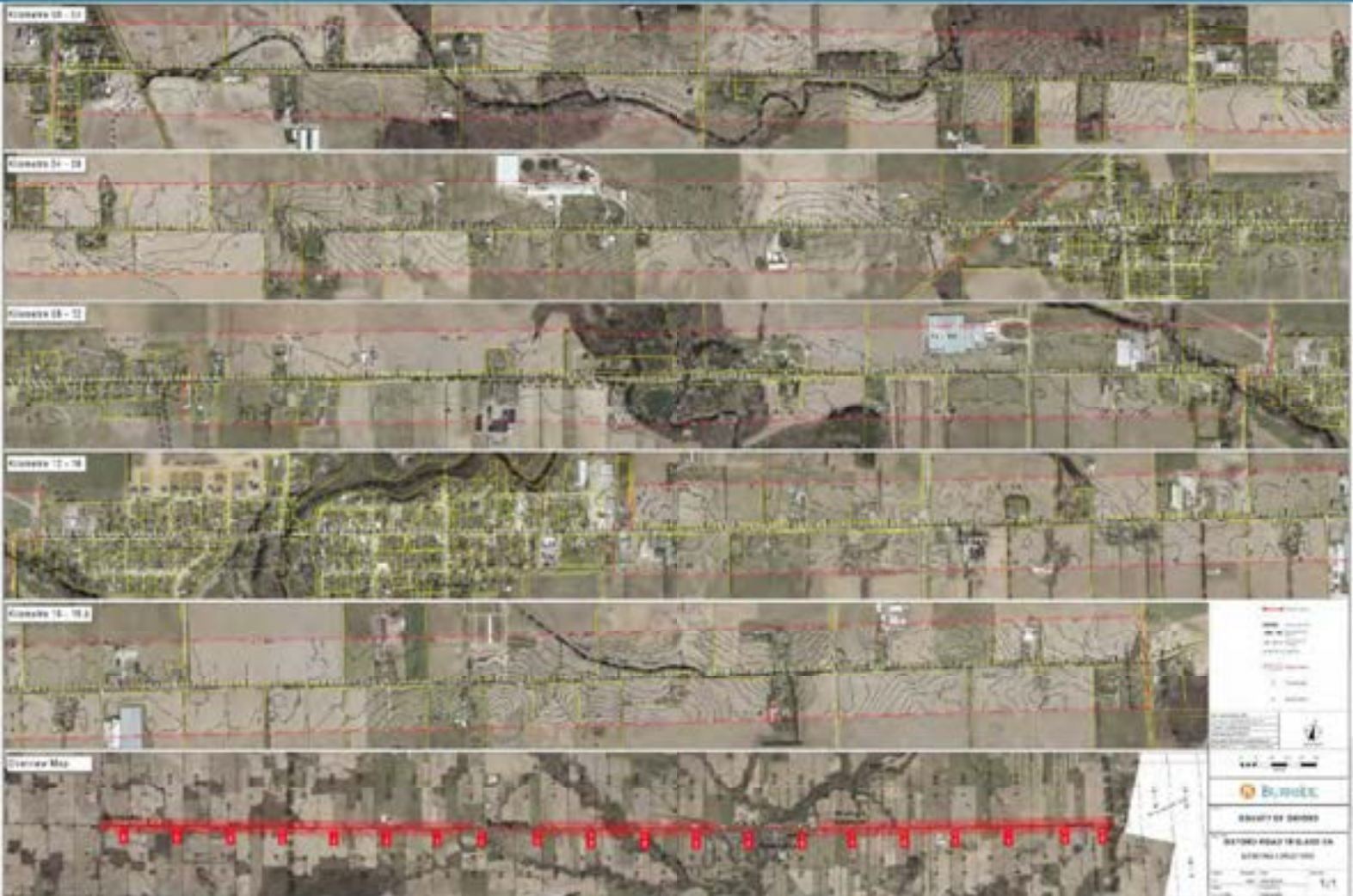
EXISTING ROAD NETWORK AND TRAFFIC CONDITION

- Oxford Road 19 is a significant east-west County road serving the southeast part of the County, connecting the villages of Springford and Otterville and the rural cluster of Ostrander, with connections to Highway 19, Oxford Road 13, Oxford Road 59 and the County of Norfolk.



- Traffic on Oxford Road 19 operates with free flow condition at most intersections within the study area and is only stop controlled at Highway 19 and Oxford Road 59.
- The existing ROW widths, in the rural area of Oxford Road 19 varies from about 20 metres to 30 metres along the corridor, with travel lanes being about 3.35 metres and the existing gravel shoulder widths varying between about 0.6 metres to 2.3 metres.

PROFILE OF EXISTING ROAD ROW CONFIGURATION



TRAFFIC VOLUMES AND TRAFFIC SAFETY

- Annual Average Daily Traffic (AADT) volumes on Oxford Road 19 are forecasted to grow by over 20%, to a total of 4400 vehicles per day (vpd), by 2046. Traffic volumes to the east of Oxford Road 59 are lower (i.e., about 2300 vpd by 2046). Based on these AADT forecasts, no additional travel lanes are required.
- Intersections along Oxford Road 19 corridor are forecasted to have good operations through horizon year 2046, with the exception of the westbound approach to Highway 19, which may warrant the edition of a westbound left-turn lane in this horizon.
- Collision rates along the corridor in the study area (0.71 per 1 million vehicle km's travelled [MVKT] over the last 8 years) as compared to the provincial average collision rate of 1.46 per MVKT (2018), with no significant collision hot spots or patterns.
- The provision of paved shoulders will improve safety for both motor vehicle travel and other users (e.g., pedestrians, cyclists, farm equipment).

EXISTING ROAD CONDITION

STRUCTURAL ADEQUACY AND GEOMETRIC DEFICIENCIES

- Given the typical age of the pavement profile on Oxford Road 19 it is expected that full depth replacement will be required to improve the road structure.
- Geotechnical study indicates the existing pavement structure has an average Granular Base Equivalency (GBE) of about 450 mm, while a GBE of 650 to 750 is recommended.
- The road base is generally over 60 years old and therefore may be near the end of a typical lifecycle for such infrastructure.
- No significant horizontal curves are along Oxford Road 19. A number of vertical sightline deficiencies were identified.



REDUCED LOAD LIMITS ON ROAD NETWORK

- Oxford Road 19 is currently subject to a 5-tonne load limit restriction in the Spring, with the exception of the section from Otterville to Oxford Road 59.
- Improvements to the east-west network of unrestricted roads will improve the connectivity for goods movements to Springford, Otterville, Town of Norwich and north-south unrestricted roads (Highway 19, Oxford Road 13 and Oxford Road 59), as well as to Norfolk County.



ACTIVE TRANSPORTATION

- West section of Oxford Road 19 is within the 5 km Bikeshed Area of Tillsonburg.
- Cycling recreational destination points exist along Oxford Road 19.



- Oxford Road 19 is currently not part of the Primary or Secondary Cycling Networks proposed for the County, although it does provide connectivity to north-south cycling networks (Primary, Secondary and Off-road Trail).

EVALUATION CRITERIA

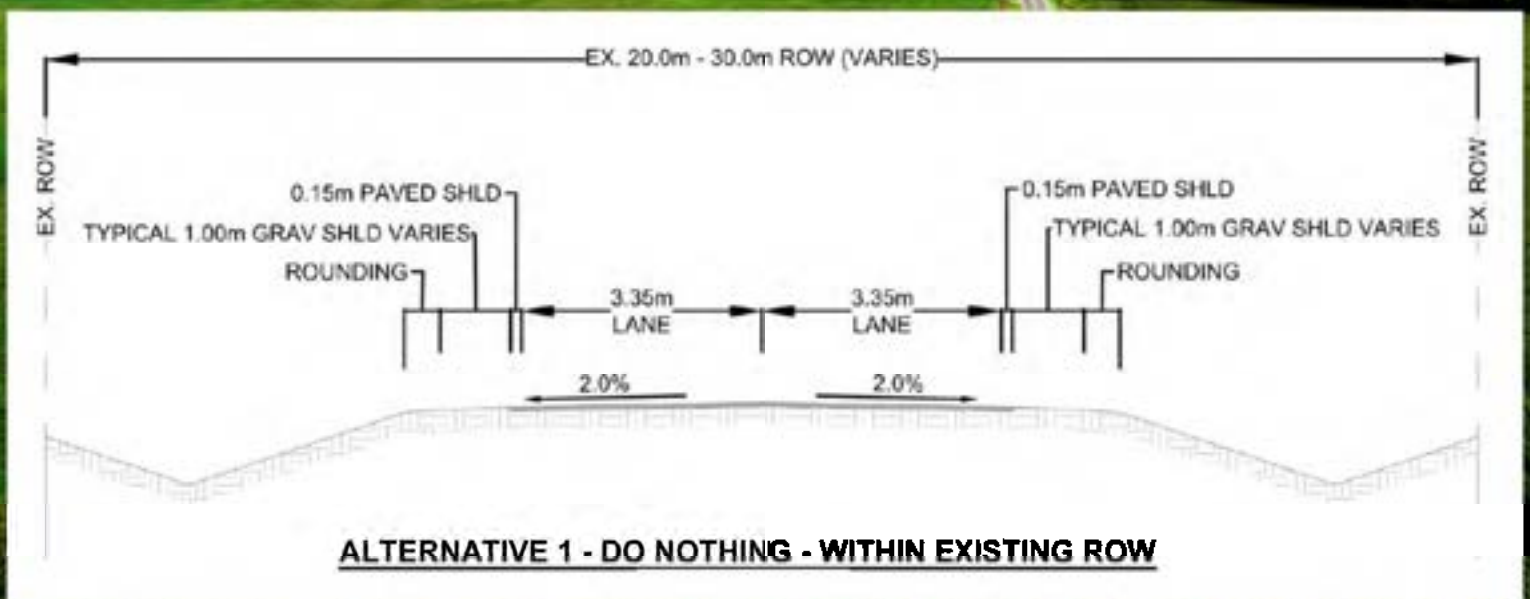
- **Natural Environment**
 - Impacts to vegetation and vegetation communities
 - Impacts to fisheries and aquatic habitat
 - Impacts to terrestrial habitat
 - Impacts to trees
 - Impacts to Woodlots, Wetlands and Designated Features (including PSWs, ESAs, ANSIs, Regional NHS)
 - Impacts to surface water and drainage
 - Impacts to groundwater and source water protection
 - Climate change and natural hazard impacts (erosion, soil stability, flooding)
- **Technical Environment**
 - Accommodation of all types of traffic and modes of travel
 - Improvement to operational safety
 - Road maintenance requirements
 - Impacts to utilities and drainage work/structures
- **Cultural Environment**
 - Impacts to cultural heritage resources
 - Impacts to archaeological resources
- **Socio-Economic Environment**
 - Impacts to private property
 - Compatibility with existing and future land uses
 - Compatibility with active transportation plans or needs
 - Conformity to municipal and agency plans and policies
 - Impacts to air quality and noise levels
 - Impacts to farms and business operations
 - Provision of safe access to private properties and businesses
- **Financial**
 - Capital and operation/maintenance costs
 - Property acquisition cost



Alternative Solutions

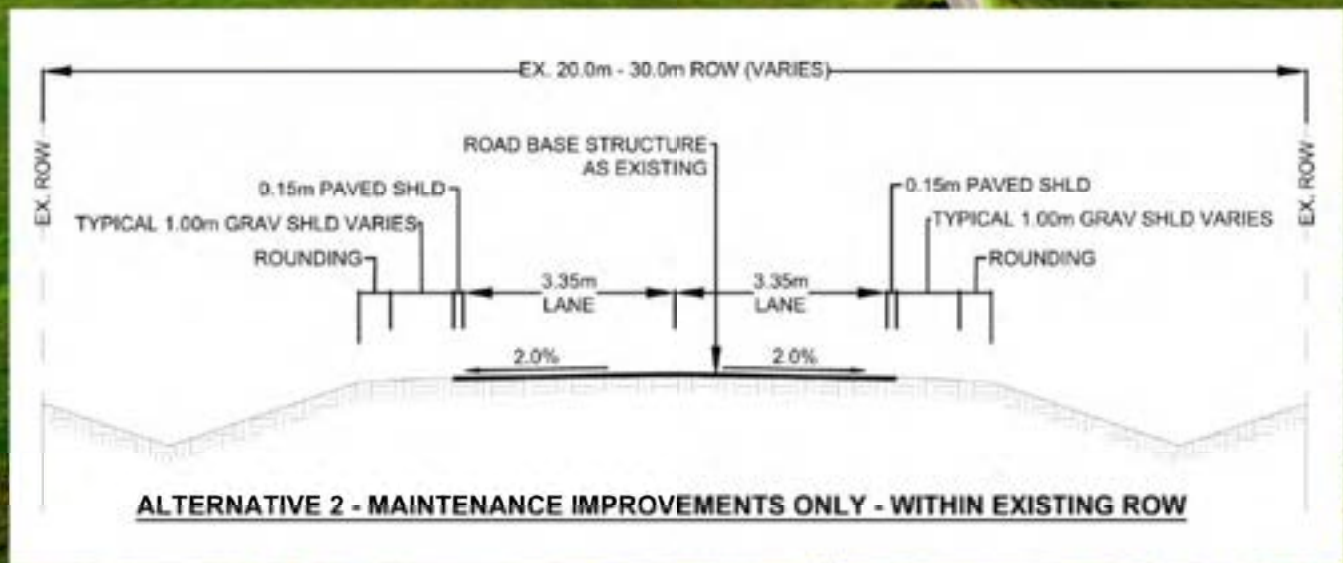
Alternative 1 - Do Nothing.

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.



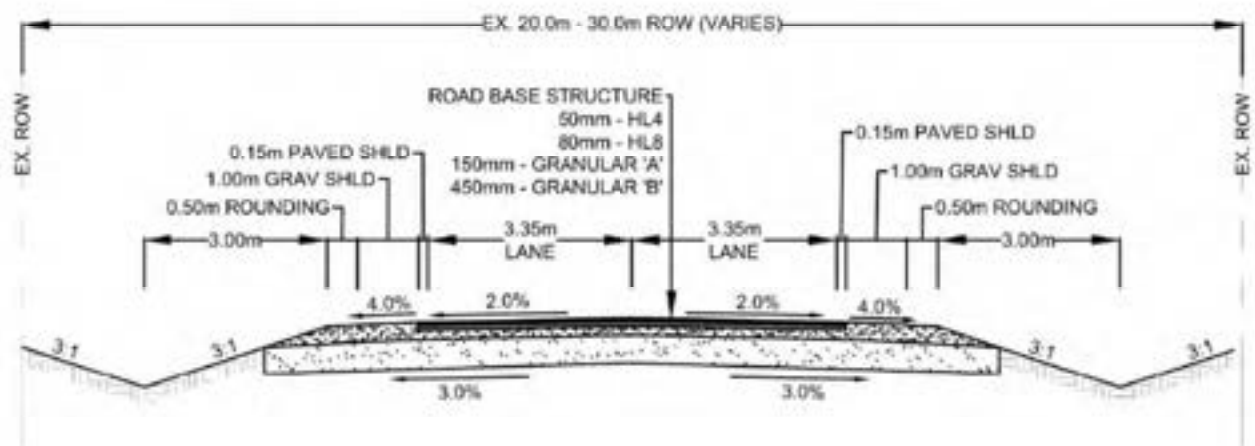
Alternative 2 – Regular Maintenance and Surface Treatments

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.
- All regular maintenance will be performed, as required.



Alternative 3 – Structural Improvements to Road Within Existing ROW

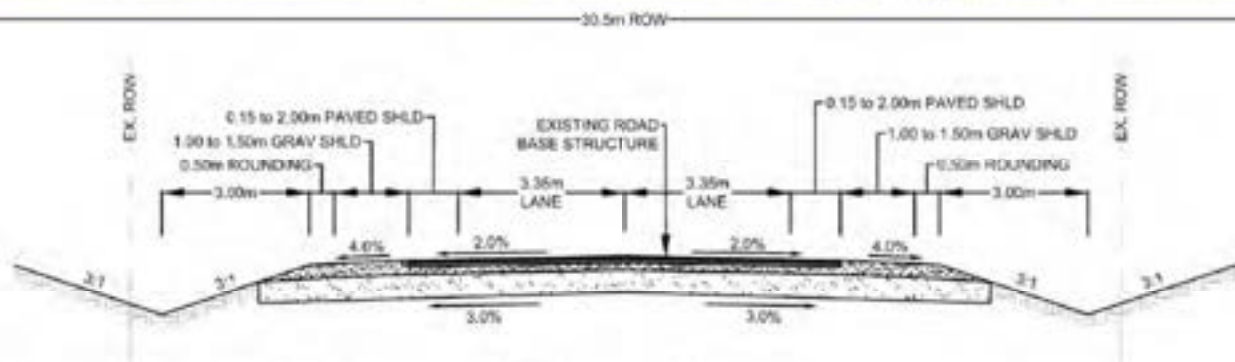
- Provide a two-lane road to County requirement with a gravel shoulder.
- Widening of travel lanes to improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.
- No opportunities for active transportation facilities.



ALTERNATIVE 3 - IMPROVED PAVEMENT STRUCTURE - MINOR LANE WIDENING - WITHIN EXISTING ROW

Alternative 4 – Widened Lanes and Shoulders within a Widened ROW- No Structural Improvements to the Road

- Provide a two-lane road to County requirement with paved shoulder wide.
- Widening of travel lanes to improve safety.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.

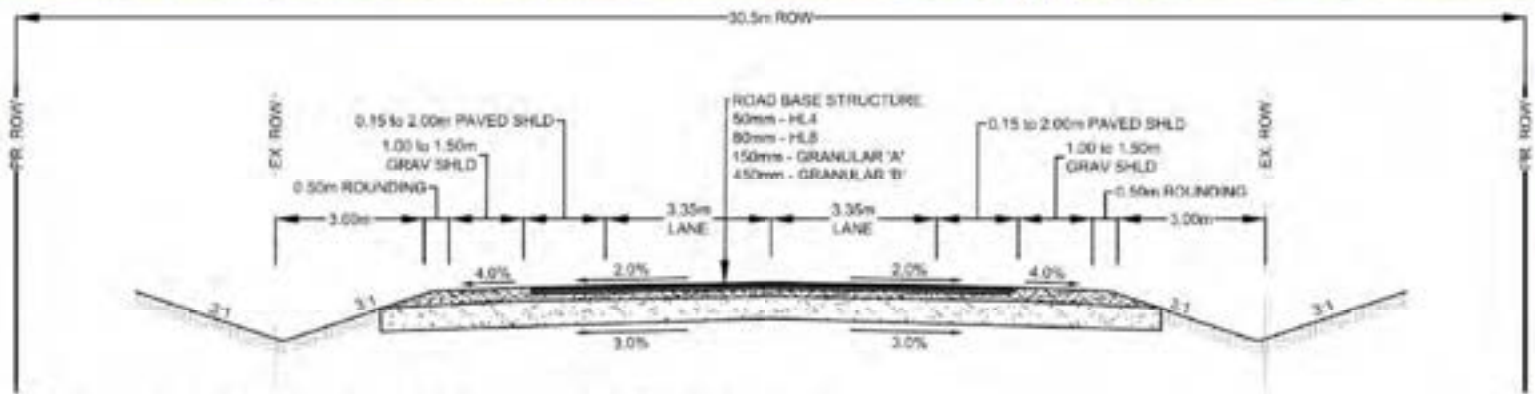


NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 4 - PARTIALLY PAVED SHOULDER - MAINTAIN EXISTING PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

Alternative 5 – Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW

- Provide a two-lane road to County requirements with paved shoulder.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.



NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 5 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

EVALUATION OF ALTERNATIVE SOLUTIONS

Evaluation Criteria	Alternative 1: Do Nothing	Alternative 2: Preventive Maintenance Improvements Only	Alternative 3: Structural Improvements to Road Within Existing ROW	Alternative 4: Widening Lanes and Shoulders within a Widened ROW	Alternative 5: Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW
Natural Environment	No general impact over existing conditions. Increased frequency and severity of adverse climatological events will impact aging infrastructure.	Minor potential impacts to woodlands, surface and ground water. Risks to be mitigated.	Minor potential impacts to woodlands and vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.	Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.	Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.
Cultural Environment	No impact over existing conditions.	No impact over existing conditions.	No impact over existing conditions.	There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.	There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.
Socio-Economic Environment	No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.	No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.	Does not meet the County's Official Plan ROW requirements. provide benefits to local businesses, allowing effective transport for goods.	Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses.	Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses. Provide benefits to local businesses, allowing effective transport for goods.
Technical Environment	Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.	Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.	Facilitates the road's ability to accommodate additional truck traffic. Does not improve the road's ability to accommodate all modes of travel. Some improvements to road safety and maintenance requirements. May impact utilities.	Facilitates the road's ability to accommodate additional modes of travel. Does not improve the road's ability to accommodate trucks. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.	Improves the road's ability to accommodate all types of traffic and modes of travel. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.
Financial Environment	No capital cost. Relatively high O&M cost due to road aging. No property acquisition is required.	Lowest low capital cost. Moderate to high O&M cost. No property acquisition is required.	Moderate capital cost. Moderate to high O&M cost. No property acquisition is required.	High capital cost. Moderate O&M cost. Some property acquisition will be required.	Highest capital cost. Moderate O&M cost. Some property acquisition will be required.
Adherence to POS	Does not meet POS	Partially meet POS	Partially meet POS	Partially meet POS	Fully meets POS
Overall Summary	Not Carried Forward	Not Carried Forward	Not Carried Forward	Not Carried Forward	Carried Forward

Level of Preference: Least Preferred ○ Less Preferred ◐ Moderately Preferred ◑ More Preferred ◒ Most Preferred ◓



Next Steps

- Confirm Preferred Solution (June 2022)
- Complete Transportation Study (June 2022)
- Complete additional supporting studies (July-August 2022)
- Develop and Evaluate Alternative Design Concepts (August 2022)
- Public Information Centre # 2 (September 2022)
- Environmental Study Report (October 2022)
- File Environmental Study Report for Schedule C Class EA Study (November 2022)

Invitation for Participation

Public input is an important component of the decision-making process.

You are invited to provide comments by completing the forms provided and submitting forms to the Study Team members below on or before June 23, 2022.

Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com

THANK-YOU FOR ATTENDING



BURNSIDE

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Appendix C

Comment Sheets and Emails

**Municipal Class Environmental Assessment Study
Oxford Road 19 Corridor Improvements**

I own a Business & Building

COMMENT SHEET

Public Consultation Centre

Time: June 9, 2022
5 pm to 7 pm

Location: Springford Hall,
429 Main St W Springford, ON

Name:	[Redacted]
<i>mailing</i> Address:	[Redacted]
Postal Code:	[Redacted]
Phone:	[Redacted]
Email:	[Redacted]

is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville. This PCC has been scheduled to allow all interested parties an opportunity to discuss issues related to the project, including the alternative solutions being considered. Public input is encouraged and will be given consideration during the planning and design of this project.

Comments/Questions/Suggestions (additional space on back of page):

I own a building and business in Otterville - I am concerned about access to my shop and alternative route signage for traffic

Oxford County and R.J. Burnside & Associates Limited thank you for your involvement in Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. With exception to personal information, all comments will become part of the public record. Project information and comment sheet will be made accessible upon request in accordance with the Accessibility Standard for Information and Communication under the Accessibility for Ontarians with Disabilities Act, 2005.

**Municipal Class Environmental Assessment Study
Oxford Road 19 Corridor Improvements**

COMMENT SHEET

Public Consultation Centre

Time: June 9, 2022
5 pm to 7 pm

Location: Springford Hall,
429 Main St W Springford, ON

Name:

Address:

Postal Code:

Phone:

Email:

is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville. This PCC has been scheduled to allow all interested parties an opportunity to discuss issues related to the project, including the alternative solutions being considered. Public input is encouraged and will be given consideration during the planning and design of this project.

Comments/Questions/Suggestions (additional space on back of page):

Large hill on entrance Rd west of Zenda
Line. We have a blind spot coming out of
our driveway. Mail delivery was stopped at our
house several years ago due to safety concerns of
mail carriers. (We walk east to neighbour for
our mail). Please try and level the hill in front
of St Charles' Church. Vehicles turn around
in our driveway - very dangerous due to the hill.

Oxford County and R.J. Burnside & Associates Limited thank you for your involvement in Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. With exception to personal information, all comments will become part of the public record. Project information and comment sheet will be made accessible upon request in accordance with the Accessibility Standard for Information and Communication under the Accessibility for Ontarians with Disabilities Act, 2005.

**Municipal Class Environmental Assessment Study
Oxford Road 19 Corridor Improvements**

COMMENT SHEET

Public Consultation Centre

Time: June 9, 2022
5 pm to 7 pm

Location: Springford Hall,
429 Main St W Springford, ON

Name:

Address:

Postal Code:

Phone:

Email:

[Redacted Name]
[Redacted Address]
[Redacted Postal Code]
[Redacted Phone]
[Redacted Email]

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Comments/Questions/Suggestions (additional space on back of page):

Evaluation Table.

This is the one display that sums it up. I think the public needs to be presented with a detailed explanation of this board, allowing a presentation to the public and a chance to discuss + ask questions.

Being an insurance broker for 45 years, this ~~is~~ suggestion also allows risk management from a liability perspective.

Oxford County and R.J. Burnside & Associates Limited thank you for your involvement in Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. With exception to personal information, all comments will become part of the public record. Project information and comment sheet will be made accessible upon request in accordance with the Accessibility Standard for Information and Communication under the Accessibility for Ontarians with Disabilities Act, 2005.

eg it may take years for any possible liability exposure to be recognized. Public input helps minimize the exposure.

**Municipal Class Environmental Assessment Study
Oxford Road 19 Corridor Improvements**

COMMENT SHEET

Public Consultation Centre

Time: June 9, 2022
5 pm to 7 pm

Location: Springford Hall,
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Comments/Questions/Suggestions (additional space on back of page):

- ① The eastern boundary of Oxford Road 19 where it connects to Norfolk County is a very awkward portion of roadway. The areas between the two counties do not line-up in a straight-away but an awkward jog. It has been the site of a few collisions including at least one fatality. I find the western approach more challenging to navigate. Perhaps this can be fixed. Over →

Oxford County and R.J. Burnside & Associates Limited thank you for your involvement in Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. With exception to personal information, all comments will become part of the public record. Project information and comment sheet will be made accessible upon request in accordance with the Accessibility Standard for Information and Communication under the Accessibility for Ontarians with Disabilities Act, 2005.

**Municipal Class Environmental Assessment Study
Oxford Road 19 Corridor Improvements**

② A widening of the roadway and shoulder from
Cesnt line to the eastern boundary will also
impact a municipal drain Run-off in the
spring collects east of Cesnt line into
the drainage ditch on the south side of the
road before crossing under the road at Cesnt
line and emptying further up the concession road.

Please complete this Comment Sheet and submit at the PCC or email to either of the Study Team members below on or before June 23, 2022. Your input and comments are appreciated.

Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com

Oxford County and R.J. Burnside & Associates Limited thank you for your involvement in Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements. Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. With exception to personal information, all comments will become part of the public record. Project information and comment sheet will be made accessible upon request in accordance with the Accessibility Standard for Information and Communication under the Accessibility for Ontarians with Disabilities Act, 2005.

Sylvia Waters

From: Henry Centen
Sent: Wednesday, May 18, 2022 6:09 PM
To: [REDACTED]
Cc: Avid Banihashemi; Sylvia Waters
Subject: RE: Oxford Road 19 EA

Hello Linda,
Thank you for the name correction. We will update our records.
We look forward to speaking with you at the upcoming Public Consultation Centre.

Best Regards,
Henry Centen, P. Eng.
R.J. Burnside & Associates Limited
519-340-2003

From: [REDACTED]
Sent: Wednesday, May 18, 2022 5:10 PM
To: Henry Centen <Henry.Centen@rjburnside.com>
Subject:

Mr. Centen: I just received the latest public notice of the Oxford Road 19 corridor improvement. I appreciate knowing about the meeting on June 9, consultation and attend to be there.

I just want to ask you to relay to whomever sends out the notices to please, use my correct name. They must be using an old file. I have reassumed my maiden name. Everything else is correct.

Please replace Linda [REDACTED]

with Linda [REDACTED]

Thank you.

Linda

Sylvia Waters

From: Jesse Keith <jkeith@oxfordcounty.ca>
Sent: Monday, June 06, 2022 3:40 PM
To: Avid Banihashemi; Henry Centen
Cc: Sylvia Waters
Subject: OR19 Class EA - Call from Joyce Pedley, 225769 Otterville Road

Hi Henry/Avid – fyi, for consultation record:

Jesse received call from [REDACTED] homeowner of [REDACTED] at ~11:50am on May 31, 2022:

- Purpose of call from Joyce was to ask me if her property was within the Study Area
 - o I indicated yes
- She mentioned that she recently received the Notice of PCC and that she was planning on attending
- Appears that ~no setback from property line to existing house

JESSE KEITH, P. ENG. (HE / HIM) | Project Engineer, Public Works
OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3
WWW.OXFORDCOUNTY.CA | T 519.539.9800 / 1-800-755-0394, ext 3194



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 Think about our environment. Print only if necessary.

Sylvia Waters

From: Henry Centen
Sent: Friday, June 10, 2022 12:55 PM
To: [REDACTED]
Cc: Jesse Keith; Avid Banihashemi; Mishaal Rizwan; Sylvia Waters
Subject: RE: Oxford County Road 19 EA

Hi again Linda [REDACTED]:

Thanks for your additional comments. Your concerns regarding truck traffic and speeding have been noted. The links to the exhibits were added in the later part of the morning and so they should now be available.

Cheers,
Henry Centen

From: [REDACTED]
Sent: Friday, June 10, 2022 12:38 PM
To: Henry Centen <Henry.Centen@rjburnside.com>
Subject: Re: Oxford County Road 19 EA

Thank you so much Mr. Centen.

I would like to make a couple things clear so I hopefully don't come off as a problem or trouble maker.

I do not oppose making the road wider, improving the shoulders and maybe even paving them and all other work that will make the road safer and better. Everything the proposals are showing are great and I think they will improve travel for everyone.

My concern is what will happen when it's completed in regards to high volume of heavy truck traffic through the villages and the increase in speeding once the road is levelled. I know it is not the intent of the project but each project does have consequences when completed and I would like to see those consequences minimized as much as possible so safety and betterment for some doesn't become worse for others.

Thank you again for listening to me and for sending the link to the boards. I did look this morning but that was not included at the bottom of the page.

If you were at the meeting last night I am sorry I didn't get to meet you. I always like to put a face to those with whom I communicate. Next time.

Linda

On 2022-06-10 11:45, Henry Centen wrote:

Hello Linda [REDACTED]

Thank you for attending the first public consultation centre for the Oxford County Road 19 EA project. We appreciate your interest in this work and assure you that all comments received from the public consultation will be taken into consideration in the development of a preferred solution and preferred design through this EA process.

The display boards are now posted on the County website for this project, which may be accessed by clicking the following link:

<https://www.oxfordcounty.ca/Services-for-You/Studies-and-Construction/Construction-Studies/Details/ArticleId/18464/Oxford-Road-19-Corridor-Improvements-Class-EA-Study>

The link to the boards and comment forms is at the bottom of the webpage.

We look forward to your continuing involvement in this process.

Best Regards,

Henry Centen, P. Eng.

R.J. Burnside & Associates Limited

519-340-2003

From: [REDACTED]

Sent: Thursday, June 09, 2022 8:32 PM

To: Henry Centen <Henry.Centen@rjburnside.com>

Subject:

Mr. Centen,

First I want to apologize for arriving at the meeting tonight, Thursday, June 9, 2022 near closing time. I had written down on my calendar (and my bathroom mirror so I wouldn't forget) the meeting to begin at 7 p.m. I was surprised there were so few people in attendance when I arrived. From a friend who actually arrived on time, I was told there was a very good turnout.

Please convey my apologies to Mr. Taylor and the two ladies, whose names I didn't get, who tried to answer all my question in a very few minutes. I was shocked when the display was being taken down and I hadn't got all the way around. It was a clear sign the meeting was over. It wasn't until I got home and talked to a friend who had attended that I was told the meeting started at 5 p.m. She was, by the way, very excited and pleased with the meeting and the improvements being considered.

I asked one of the consultants in attendance if it were possible to get copies of the display board material via email so I could read them over again in a more relaxed way to get a better understanding. She said it was available online but I can't seem to find it. I admit I am not great at the technological stuff so I ask with great shame if someone could email me all that information or give direction where I could find it. I might not be able to access it but I will try if emailing me copies is not something that can be done.

Again Mr. Centen, I am sorry for my negligence on timing and I promise to do better next time.

Thank you.

Linda [REDACTED]

Sylvia Waters

From: Henry Centen
Sent: Tuesday, June 14, 2022 8:14 AM
To: Avid Banihashemi
Cc: Sylvia Waters; Mishaal Rizwan
Subject: FW: enviromental assessment study oxford re 19 comment sheet june 9th

From: [REDACTED]
Sent: Tuesday, June 14, 2022 8:08 AM
To: jkeith@oxfordcounty.ca
Cc: Henry Centen <Henry.Centen@rjburnside.com>
Subject: enviromental assessment study oxford re 19 comment sheet june 9th

Hi,

Marie and I attended the meeting at Springford on June 9th. The presentation of the project was very good. We feel that the road should be totally reconstructed to handle heavy traffic ie large trucks and farm equipment. We have observed the deterioration of this road and many others in the area. The traffic on this road is increasing at all hours of the day. It does need wider shoulders to allow pedestrians, bikers as well. As Oxford Rd 19 transitions into Norfolk Rd 19 we believe that the intersection should be reconstructed so that the existing curve in the road be straightened allowing more visibility and a safer operation of vehicles.

Regards,

[REDACTED]





BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H3

PCC #2 Summary



BURNSIDE

**Public Consultation Centre #2
Summary Report**

Oxford Road 19 Corridor Improvement

**Oxford County
Oxford County Public Works**

**Public Consultation Centre #2
Summary Report**

Oxford Road 19 Corridor Improvement

**Oxford County
Oxford County Public Works**

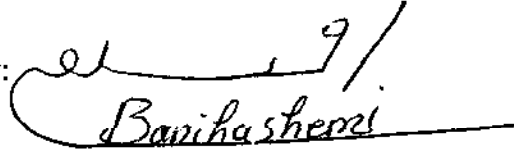
**R.J. Burnside & Associates Limited
21 Reeve Street, P.O. Box 1614
Woodstock, Ontario N4S 7Y3**

**February 2023
300053425.0000**



R.J. Burnside & Associates Limited

Report Prepared By:



Avid Banihashemi, M.Sc., Ph.D.
Project Administrator
ABJ:lam

Report Reviewed By:



Henry Centen, P.Eng.
Project Manager

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Appendices

- Appendix A Newspaper Advertisement
- Appendix B Display Boards
- Appendix C Comment Sheets and Emails

1.0 Introduction and Background

Oxford County's 2019 Transportation Master Plan identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods. Accordingly, Oxford County (County) initiated a Municipal Class Environmental Assessment (MCEA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study Area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville. In Phase 1 of the MCEA Study process, the County identified the **Problem Statement** as:

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

In Phase 2 of MCEA Study process, alternative solutions were identified, evaluated, and presented at the first Public Consultation Centre (PCC#1) on June 9, 2022. Considering public input, the **Preferred Solution** was confirmed as *Alternative 5 – Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW.*

The purpose of PCC#2 was to provide public, agencies, indigenous communities and all interested stakeholders with an opportunity to review the conceptual design alternatives that are being considered and evaluated (Phase 3 of the MCEA Study process) for the preferred solution/improvements to the Oxford Road 19 Study Area.

Alternative Design Concepts include:

1. **Alternative 1:** 9.0 m paved asphalt, Each side with 3.35 m travel lane, 1.15 m paved shoulder and 1.5 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander) - Centered on the existing centreline of the road (typical);
2. **Alternative 2:** 7.0 m paved asphalt Each side with 3.35 m travel lane, 0.15 m paved shoulder and 2.5 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander) - Centered on the existing centreline of the road (typical); and,
3. **Alternative 3:** 10.7 m paved asphalt Each side with 3.35 m travel lane, 2.0 m paved shoulder and 0.65 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander) - Centered on the existing centreline of the road (typical).

The planning of improvements to Oxford Road 19 are being carried out in accordance with the *Schedule 'C' requirements (Phases 1 to 4)* of the Municipal Engineers Association Municipal Class Environmental Assessment document (October 2000, as

Public Consultation Centre #2 Summary Report
February 2023

amended in 2007, 2011 and 2015), which is approved under the *Ontario Environmental Assessment Act*.

A key component of the study includes consultation with interested stakeholders. This report documents the Public Consultation Centre #2 (PCC), held on December 6, 2022 from 5:00 p.m. to 7:00 p.m. at Springford Community Hall 429 Main St. W, Springford and summarizes the notification process, the information presented, and the comments received during and after the PCC #2.

2.0 Method of Notification

The Notice of PCC #2 was published in the *Oxford Review* on November 17 and November 24, 2022, with the details of the date, time, and purpose of the PCC #2, to review the conceptual design alternatives that are being considered and evaluated for the preferred solution/improvements to the Oxford Road 19 Study area. A copy of the advertisement is provided in Appendix A.

Notification of the PCC #2 was also mailed / emailed to regulatory agencies, municipalities, Indigenous communities, and local residents who live within the Study Area and posted on the Oxford County website: (<https://www.oxfordcounty.ca/en/news/oxford-road-19-corridor-improvements-class-ea-study.aspx>).

3.0 Public Meeting Format

The PCC was arranged as a drop-in format with Display Boards placed around the room. Attendees were greeted upon arrival, were encouraged to sign-in, and were offered a Comment Sheet to provide comments on the project.

Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) were present to discuss the project with interested members of the public and answer questions.

A copy of the Display Boards is provided in Appendix B. The Display Boards covered the following topics:

- Welcome
- Study Area
- Problem / Opportunity Statement
- EA Process
- (List of) Supporting Studies
- Natural Heritage Resources
- Source Water Protection
- Archaeological Resources
- Cultural Heritage Resources
- Transportation Study – Design Need Assessment
- Air and Noise Assessment

- Stormwater Assessment
- Preferred Alternative Solutions
- Alternative Design Concepts (1-3)
- Evaluation Criteria
- Evaluation of Alternative Design Concepts
- Preferred Design Concept Plans
- Proposed Mitigation Measures
- Preferred Design Concept Plans – Future Detailed Design Considerations (including supplementary improvements)
- Next Steps

Participants were requested to provide input to the process by completing the available Comment Sheets. If individuals wished to take Comment Sheets home to fill out later, or to download from the Oxford County website, they were requested to return their comments either via email, fax or to the mailing address provided, by January 13, 2023.

4.0 Participation Levels and Summary of Comments Received

This section provides an overview of the feedback received from participants during and following PCC #2. A total of eight people attended PCC #2 excluding the Project Team members.

Two written Comment Sheets and two emails were received from public members during comment period of the PCC #2. Three emails were received from Agencies and three Indigenous communities responded to the PCC#2 Notice during comment period of the PCC #2.

Participant comments are reviewed to provide a better understanding of stakeholder opinions and to provide feedback in the evaluation of the Preferred Design Concept for improvements to the Oxford Road 19. The summary of comments and advice received during PCC #2 is intended to provide an indication of overall shared issues, opinions, and concerns of participants. Copies of individual comments are provided in Appendix C.

Key issues addressed within the written comments relate to:

- Vertical alignment sight line issue (blind spots).
- Horizontal alignment and visibility issues.
- Impact to the residential property.
- Impact to wildlife.
- Liability.

Table 3: PCC #2 Public Comments and Project Team Responses

ID Code	Comment	Project Team Response
Comment Sheet 1	<p>Residents concerned about visibility have witnessed accidents because of visibility. It was noted that houses are close to the busy road, which creates a liability. The Resident suggests that if the road was moved more to the other side up to the hydro lines, less property would be lost, thus safer for Residents and turning farm equipment.</p> <p>The Resident also enquired whether a calculation had been done on the amount of liquid manure travelling on road west/east of Springford, huge environmental exposure.</p>	<p>The preliminary design has flagged horizontal and vertical sightline issues. Additional horizontal alignment alterations will be considered at the detailed design to increase safety of travel through the corridor.</p> <p>The preliminary design team has considered the suggestion carefully and believe that moving the road as suggested will not impact the safety of the road.</p> <p>Calculations on the amount of liquid manure travelling on the road have not been done. Road upgrades are meant to reduce the risk of agricultural vehicle collisions and potential environmental exposure related to such events. These upgrades include widening of shoulders and the inclusion of a paved shoulder.</p>
Comment Sheet 2	<p>Resident requested why the study and assessment excluded Springford and Otterville, as Highway 19 continues through these towns, so the assumption is the traffic flow and maintenance will also be impacted; and whether this area will be covered in a future study?</p>	<p>Those areas have previously been reviewed with considerations to the traffic and safety and the improvement have been completed to these sections. The interfaces between the previously improved sections and the sections that are currently being studied will be reviewed at the detailed design stage.</p>
Email 1	<p>Resident noted they were unable to attend PCC #2 and requested whether there was an executive summary available?</p>	<p>Burnside responded and attached a link to the display boards which were available at PCC #2 and attached the Comment Sheet the Report (ESR) will be made available for public viewing, tentatively in February/March of next year.</p>
Email 2	<p>Resident responded requesting whether there will be further opportunities for input regarding any impacts of this project? Resident requested whether the website regarding the PCC #2 was open now? The Resident asked whether they will be directly advised when the Report is available.</p>	<p>The project website is up and available for public at any time during this project (https://www.oxfordcounty.ca/en/news/oxford-road-19-corridor-improvements-class-ee-study.aspx) and we welcome any additional comments you may have. The project team encouraged the resident to visit the project website occasionally for updates as the project progresses.</p> <p>The Environmental Study Report will be available through the website for 30-days public review and comments. The project team will issue a 'Notice of Completion' that will be mailed/emailed to all that are on the project contact list, including public members, agencies, indigenous communities, and interested stakeholders, to inform them of the start of the 30-day review period.</p>
Email 3	<p>The resident was able to attend this meeting along with his wife. They do have some comments and questions. Many of these were discussed with those in attendance on December 6. [REDACTED]</p> <p>Environmental impact If the road is built as you suggest, this infers that the 20 (or so) trees to the east and on the south side of the road of our home, will be cut down. This will have a large negative impact on the environment. Frequently, there are hawks and sometimes eagles that use these trees. Just this past week we had a red tail hawk land in our blue spruce tree on our property and fly to one of the trees alongside the road that I am referring to. We have had, this past year, red squirrels living on our property and</p>	<p>Response to Email: Burnside responded that all comments are carefully reviewed and considered as the EA study progresses. It was also noted that a PCC#2 Summary Report will be available on the project website once the PCC #2 comment period has closed; and that the Report will include a table of the comments and Project Team responses. The resident was encouraged to visit the project website to review the Report and for project updates.</p> <p>Response to Comments: As part of this EA study, natural environment and wildlife habitat in the Study area has been reviewed to ensure a minimal impact and mitigation measures are being recommended.</p> <p>The concern was that unnecessary trees and land were to be taken from the front of their property. Consideration will be given to minimize tree removal with a focus on drainage and road requirements. Required land is equal distance from the existing centerline of the road, in an effort to best use the existing road base and be fair to landowners.</p>

ID Code	Comment	Project Team Response
	<p>currently there are black squirrels as well. Removing these trees would destroy part of their habitat. I believe that red squirrels are a protected species. We sometimes see snakes, frogs and loads in off to the side of the road where the tree line is, and their habitat would be destroyed. Many small birds' nest in these trees.</p> <p>Proposed Location of new road</p> <p>If the road were rebuilt to the north of the existing road (i.e., closer to the hydro poles and to the east of 2249-66 Otterville Road), these trees could be saved and the habitat for the small animals might not be disturbed. I urge you to consider this option.</p> <p>Liability</p> <p>There is also a liability exposure that if not corrected, could put the county at an increased liability risk. Rebuilding of the road allows an opportunity to fix this existing liability exposure. The exposure is the hill in front of our home. It blocks views to oncoming traffic when traffic is traveling west to east and blocks the view from our driveway to the west. Anyone visiting always comment on how treacherous it is entering or exiting our driveway. Canada Post made us move our mailbox to the east, as to minimize the potential danger which this hill created for their mail delivery. To add to the problem, the neighbours to the northwest of our property have a laneway which is affected by this hill as it blocks the view when exiting / entering the laneway. We have witnessed 2 accidents caused by farm machinery and vehicles colliding when farm machinery is turning into the laneway. Farm machinery was traveling east and turning north into the laneway. Traffic behind the farm machinery is unable to see any laneway, and then attempt to pass the farm machinery, causing the accidents. Taking out the hill to improve the line of vision would be a possible solution. Also moving the road to the north as mentioned above would also assist [REDACTED]</p> <p>[REDACTED]</p>	<p>The preliminary design has flagged horizontal and vertical sightline issues. Further investigation & design consideration to be taken during detailed design to improve sightlines.</p>

Table 2: PCC #2 Agency Comments and Project Team Responses

Hydro One	<p>Thank you for sending us notification regarding (Oxford Road 19 Corridor Improvement). In our assessment, we have confirmed that Hydro One has existing distribution assets within your study area. At this time, we do not have sufficient information to comment on the potential resulting impacts that your project may have on our infrastructure. As such, we must stay informed as more information becomes available so that we can advise if any of the alternative solutions present actual conflicts with our assets, and if so; what resulting measures and costs could be incurred by the proponent. Note that this response does not constitute approval for your plans and is being sent to you as a courtesy to inform you that we must continue to be consulted on your project. Hydro One must be consulted during all stages of your project. Please ensure that all future communications about this and future project(s) are sent to us electronically to secondarylanduse@hydroone.com</p>	<p>Thank you for your response letter dated December 12, 2022 to the Notice of Public Consultation Centre #2 for Oxford Road 19 Municipal Class Environmental Assessment (EA) Study. Kindly, please find below the download link to the preliminary design concept plans completed as part of Stage 3 of the EA Study for your review and input.</p> <p>[REDACTED]</p>
Telecon	Completed markup was provided.	N/A
Enbridge	<p>For utility requests in legacy Union Gas franchise area, please submit a Design and Planning request through Ontario One Call at www.ontarioonecall.ca or 1-800-400-2255. More information about submitting a Design and Planning request can be found here: https://www.ontarioonecall.ca/wp-content/uploads/Design_and_Planning_Best_Practice_2020.pdf</p>	N/A
Execulink	<p>Sorry I could not attend the meeting today. Execulink does have fiber along this road east of Otterville to Baseline, intention to complete next year the section between Springfield and Otterville, and we also run down Zenda Line crossing Rd 19. What do you need from us to help with this project?</p>	<p>As per the Notice, we are still in the Class EA study stage, the preliminary preferred conceptual design (pending public input) includes structural improvements to Oxford Road 19 (to eliminate half load restrictions) and widening of shoulders. Detailed design will follow the study incl. proposed property acquisition process, and eventually utility relocations/construction.</p> <p>Earlier this year, you sent over some plans of Execulink plant existing/planned for the area (attached email correspondence). Was the section in Ostrander completed this year? If plans are available for future fibre between Springfield and Otterville, please share.</p>

Table 3: PCC #2 Indigenous Communities Comments and Project Team Responses

<p>Mississaugas of the Credit First Nation (MCFN)</p>	<p>Thank you for reaching out to MCFN DOCA for consultation. At this time MCFN DOCA has no comments or concerns regarding MCEA Study for Oxford Road 15 Corridor Improvements. Please contact MCFN DOCA if any changes or new information arises about said above project.</p>	<p>N/A</p>
<p>Haudenosaunee Development Institute (HDI)</p>	<p>Currently, the HDI and our nation's do not hold these notifications as engagement or consent. Again, I address HDI and our nation's significant concerns with respect to the proposed project, explicitly towards the Oxford County and RJ Burnside not applying an application and fee with HDI to review the project. How are we to provide feedback and consider engagement when we have no funds to review and/or comment on this proposed project?</p> <p>It is required that the proponent of the project provide a completed application and fee so we can analyze the effects, impairment, and interference within our treaty rights. Once we complete our evaluation, we can begin participation in a meaningful way. You may access the application on the link below and see the provided instructions to complete our application process.</p> <p>Again, this application process provides initial funding for our team to internally review the documents and recognize how this project impacts and interferes within our treaty rights. Once the appropriate measures have been followed through, we will reach out and discuss HDI participation and impacts in a meaningful way. Until then, we ask this proposed project to halt any further.</p>	<p>Response provided (See Appendix C).</p>
<p>Caldwell First Nation (CFN)</p>	<p>We have received your initial submission. Please note that before the project is ready for review, you must provide some additional information: The remaining information involves questions that were marked as "unsure."</p> <ul style="list-style-type: none"> • Habitat or flight paths • Endangered species permit • Landscape agreement • Archaeological assessment <p>When you have this information ready, you can return to this website and submit your information.</p>	<p>A draft copy of the natural environment report as well as Stage 1 Archaeological Assessment report will be provided for CFN review.</p>

5.0 Next Steps

Comments and concerns received at PCC #2 will be reviewed for incorporation into the *Environmental Study Report* (ESR). Next steps include:

- Confirm Preferred Design Concepts (February 2023).
- Finalize Transportation Study (February 2023).
- *Environmental Study Report* (February/March 2023).
- File Environmental Study Report for Schedule C Class EA Study (March/April 2023).

Documenting the planning and decision-making process of the study, will be prepared for public review and comment at the completion of the study. A Notice of Completion will be issued following the completion of the ESR. The ESR will be available for public review for a minimum 30-day review period.



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Appendix A

Newspaper Advertisement

Public Notices



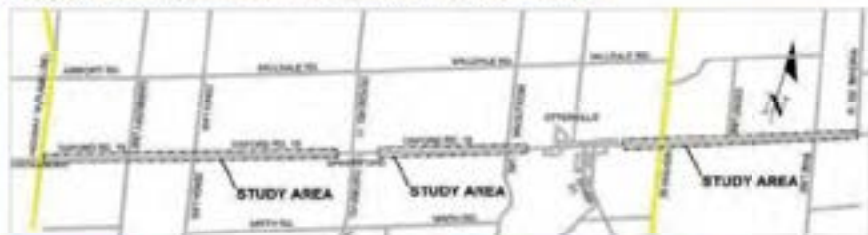
PUBLIC NOTICE

Notice of Public Consultation Centre

Oxford Road 19 Corridor Improvement

Municipal Class Environmental Assessment Study

Following completion of the 2019 Transportation Master Plan, Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

The County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study area includes approximately 16 kilometers of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springfield and Otterville. Please refer to the map.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (2020), as amended in 2007, 2011 & 2015), approved under the Ontario Environmental Assessment Act.

We want to hear from you

Consultation with stakeholders is a key component of the Study process. The first Public Consultation Centre (PCC) was conducted on June 9, 2022, to present alternative solutions and receive input on the recommended preferred solution. PCC#2 is being held to review the conceptual design alternatives that are being considered and evaluated for improvements to the Oxford Road 19 Study Area.

Public Consultation Centre #2

Similar to PCC#1, PCC#2 will be a drop-in format to provide residents/interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its consultant, R.J. Burnside & Associates Limited, will be present to answer questions and discuss next steps in the study. The date and location of PCC#2 are as follows:

Tuesday, December 6, 2022 - 5:00-7:00 p.m.
Springford Community Hall
429 Main St. W., Springford, Ontario

Contacts for information

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P. Eng., Project Manager
 Oxford County Public Works
 519-539-9800 ext.3194 | jkeith@oxfordcounty.ca

Henry Conten, P. Eng., Project Manager
 R.J. Burnside & Associates Limited
 519-340-2003 | henry.conten@rjburnside.com

Comments received during the Study will be considered and documented in the Environmental Study Report. Information will be collected in accordance with the Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

This notice first issued on November 17, 2022

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ANNOUNCEMENTS: 1-866-766-7821 • FAX: 1-866-767-0227

Obituaries



KENNEDY, Margaret Elinor "Madge"

In her 93rd year, Madge Kennedy of Woodstock, passed away peacefully at Woodstock General Hospital on November 7, 2022. Loving mother of Nancy (Arnold) Haskett of Inverkip, and Julie Fagan of Woodstock. Loved Gram of Brandon (Carrie) Haskett of Millbrook, Spencer (Stephanie) Haskett of Embro, Lindsey (Kyle) Marsbridge of Woodstock, Scott (Emily) Fagan of Woodstock, and Jenna (Rob) Fagan of Branford. Predeceased by her loving husband Roy (1992), her dear daughter Susan (1999), her sister Betty (Dor) Campbell, sister-in-law Phyllis Kennedy, and brother-in-law Bernie Kennedy. Proud GG of Connor Marsbridge, Clara, Morgan, Kennedy and Harper Haskett, and Ty Bicorn Shirley. Madge is survived by her sons-in-law Ross Grant and Paul Fagan, two nieces, Barb (Bob) Zaranonello and Joanne (Tom) McHugh, and also her dear friends Elaine and Carson. At Madge's request, cremation has taken place and a private family remembrance will take place at a later date. Memorial donations would be gratefully accepted for Woodstock General Hospital or Sakura House, Woodstock. Personal condolences can be posted at www.gfh.ca. Funeral arrangements have been entrusted to the Glendinning Funeral Home, Plattville.



Events

STARLIGHT BAZAAR
Holy Trinity Church
 1420 Devonshire Ave.
Saturday, November 19th
 Doors Open 10:00 am
 Draws at 2:00 pm

Baking, Crafts,
 Religious Goods, Yesterday's
 Treasures, Penny Sale,
 Raffle and our Tea Room

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Public Notices



PUBLIC NOTICE

Notice of Public Consultation Centre
Oxford Road 19 Corridor Improvement
Municipal Class Environmental Assessment Study

Following completion of the 2019 Transportation Master Plan, Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

The County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville. Please refer to the map.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (2000, as amended in 2007, 2011 & 2015), approved under the Ontario Environmental Assessment Act.

We want to hear from you

Consultation with stakeholders is a key component of the Study process. The first Public Consultation Centre (PCC) was conducted on June 9, 2022, to present alternative solutions and receive input on the recommended preferred solution. PCC#2 is being held to review the conceptual design alternatives that are being considered and evaluated for improvements to the Oxford Road 19 Study Area.

Public Consultation Centre #2

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Tuesday, December 6, 2022 - 5:00-7:00 p.m.
Springford Community Hall
429 Main St. W, Springford, Ontario

Contacts for information

If you have questions or comments, please contact either of the following project team members:

- Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194 | jkeith@oxfordcounty.ca
- Henry Centon, P.Eng., Project Manager
R.J. Burnside & Associates Limited
519-340-2003 | henry.centon@rjburnside.com

Comments received during the Study will be considered and documented in the Environmental Study Report.

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This notice first issued on November 17, 2022

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Appendix B

Display Boards

Appendix B



Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements

PUBLIC CONSULTATION CENTRE 2 / SPRINGFORD HALL
429 MAIN ST W SPRINGFORD, ON
DECEMBER 6, 2022
5:00 P.M. - 7:00 P.M.

WELCOME

to the **Public Consultation Centre 2** for the
Oxford Road 19 Corridor Improvements

Please Sign In

- Meet with Study Team Members
- Review the display materials and discuss your questions and ideas with the Study Team
- Please fill out a comment sheet and return it to the Study Team in person, by email or fax by **January 13, 2023**

STUDY AREA

The County is undertaking a Class EA study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study Area includes approximately 16 kilometres of Oxford Road 19 between Highway 19 (Plank Line), and the Norfolk County boundary (Windham Road 19), which excludes the Settlements of Springford and Otterville.



The site location and approximate extent of the Study Area are shown on the map.

PROBLEM/OPPORTUNITY STATEMENT

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

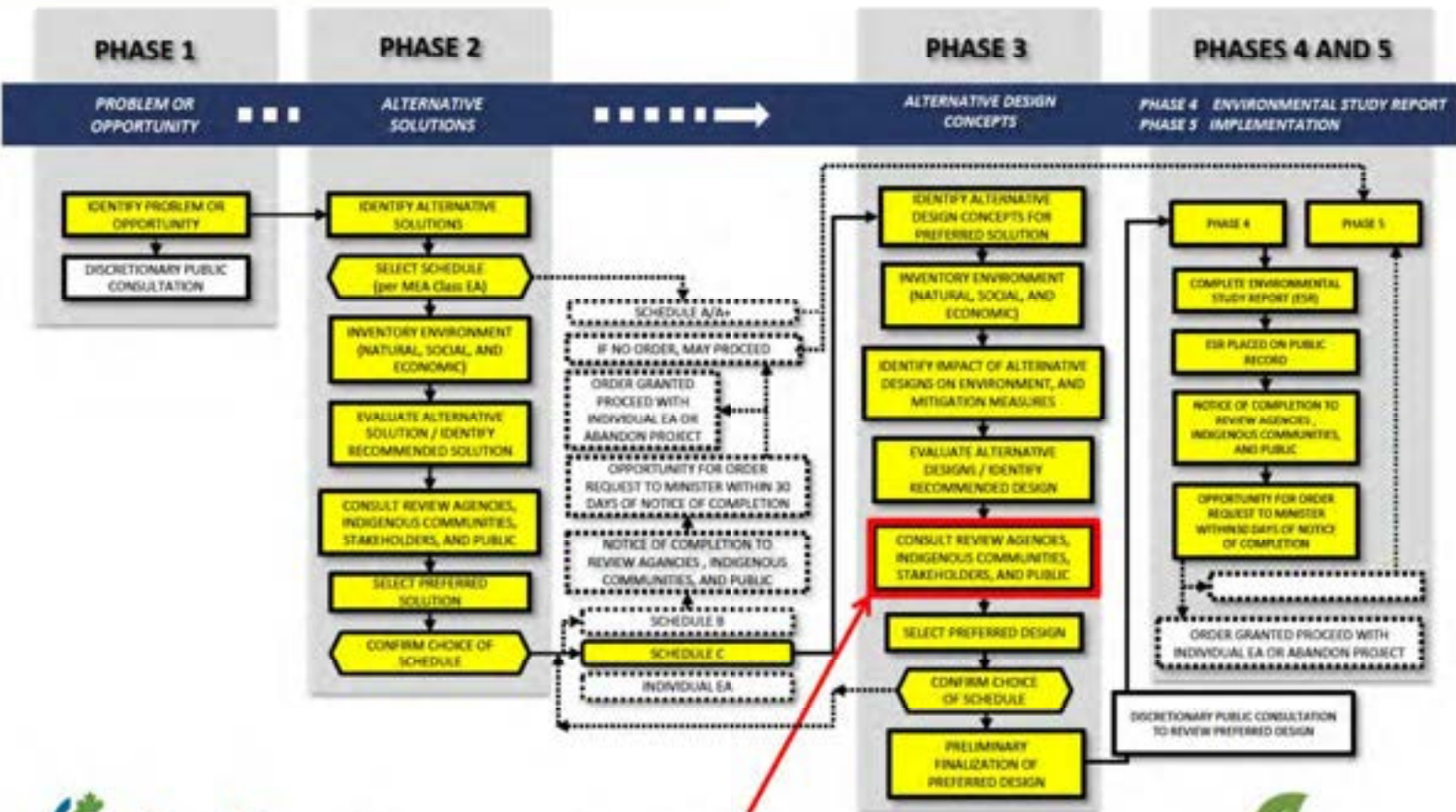


THE EA PROCESS

The Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act.

Nearing completion of the study, an Environmental Study Report (ESR) will be prepared and made available for final public review and comment.

THE EA PROCESS



We are Here

SUPPORTING STUDIES

- Transportation Study (including Illumination Assessment)
- Natural Environment Assessment
 - Terrestrial Habitat Assessment
 - Aquatic Habitat Assessment
- Stage 1 Archaeological Assessment
- Cultural Heritage Resource Assessment
- Air and Noise Impact Assessments
- Stormwater Management Assessment



NATURAL HERITAGE RESOURCES

Designated Features within the Study Area

- Significant valleylands and locally significant natural heritage features
- Watercourses: Spittler Creek, Plumb Creek, and Big Otter Creek.
- Spittler Creek and Big Otter Creek are both associated with significant valleylands
- Otterville Provincially Significant Wetland (PSW) Complex and other wetlands across Spittler Creek
- Woodlands



NATURAL HERITAGE RESOURCES

Aquatic habitat

No Species at Risk (SAR) were found through the desktop review.

- Big Otter Creek is a warm water creek with spring-spawning species such as Largemouth Bass and Yellow Perch.
- Spittler Creek is a cool water creek with observed spring-spawning species.
- Plumb Creek is a cool water creek with fall and spring-spawning species such as Brown Trout, Creek Chub, and Blacknose Dace.

Field work was conducted in the field season to assess aquatic habitat in the Study Area:

- Fish habitat was noted at all six crossings. All crossings has adequate, flow / depth, riparian life, and substrate providing suitable fish habitat.
- Large schools of bait fish were noted throughout both the upstream and downstream reaches of the crossings.
- Spawning Hornyhead Chub (*Nocomius biguttatus*) were observed in the downstream reach and underneath the clear span bridge at two of the crossings.



NATURAL HERITAGE RESOURCES

Terrestrial Habitat

The desktop review found several potential Species At Risk (SAR):

Snapping Turtle (Special Concern), Wood Thrush (Special Concern), American Badger (Endangered), American Chestnut (Endangered)

Field work was conducted in the field season to assess ecological communities present in the Study Area:

- Barn Swallows were observed foraging within agricultural fields. Potential habitat for Barn Swallow is restricted to crossing structures within the Study Area. Barn Swallow nests were not present on crossing structures at the time of field investigations. Consideration should be provided for this species during the detailed design phase as this species is known to nest within bridges and culverts.

A single Butternut was observed on the south side of the ROW. Additional surveys including a Butternut Health Assessment may be required at the detailed design phase if it is anticipated that the specimen will be impacted by road improvements.

Bats

The desktop review found records of 4 bat species: Eastern Small-footed Myotis, Little Brown Myotis, Northern Myotis, and Tricoloured Bat

Impact to potential habitat is not expected. Direct impact is to be avoided with timing vegetation removal to be completed outside of the active season for bats.



SOURCE WATER PROTECTION

The Study Area falls under the Long Point Region Source Water Protection Area and Lake Erie Source Water Protection Area.

Wellhead Protection Areas

Three portions of the study area have been identified to fall under Wellhead Protect Areas (i.e., part of Ostrander, north part of Springford and east of Otterville).



Issue Contributing Area

The area west of Otterville is also considered an Issue Contributing Area (ICA), for its potential to contribute elevated concentrations of particular substances to the drinking water source (e.g., chloride, sodium, nitrate).

Highly Vulnerable Aquifer

A significant part of the Oxford Road corridor (i.e., from west of Otterville to east of Oxford Road 59) is also located in a Highly Vulnerable Aquifer (HVA) area.

Significant Groundwater Recharge Areas

A significant part of the study area is also within Significant Groundwater Recharge Areas (SGRAs)

ARCHAEOLOGICAL RESOURCES

The Ministry of Citizenship and Multiculturalism (MCM) Standards & Guidelines lists criteria that are indicative of archaeological potential. The Stage 1 archaeological assessment showed that:

- 27 Previously identified archaeological sites within 1 km of the Study Area, 2 sites of which are located within the Study Area, and six of which are within 50 m of the Study Area;
- Springford Community Cemetery and Pine Street Burial Ground are located within 20 m of the Study Area. The legal cemetery boundaries shall be avoided by project designs.
- The Pettman Cemetery is approximately 100 m north of the Study Area, however the historic limits are unknown and therefore the lands within the Study Area require Stage 2 archaeological assessment.
- Stage 2 archaeological assessment is recommended in any other areas identified as exhibiting archaeological potential within the impact area of the project.
- Stage 2 archaeological assessment, if required, will be completed during the detailed design.



CULTURAL HERITAGE RESOURCES

Two known heritage properties, designated under Part IV of the Ontario Heritage Act, were identified within the project study area. A total of 5 Potential Built Heritage Resources and 23 Potential Cultural Heritage Landscapes were identified within the project study area.



Built Heritage Resources and Cultural Heritage Landscapes throughout the corridor

TRANSPORTATION STUDY DESIGN NEED ASSESSMENT

- Collision rates along the corridor in the study area (0.71 per 1 million vehicle km's travelled [MVKT] over the last 8 years) as compared to the provincial average collision rate of 1.53 per MVKT (2019), with no significant collision hot spots or patterns.
- No additional travel lanes are required on Oxford Road 19 to beyond year 2046
- A westbound left turn lane may be required at Highway 19 by 2046
- The existing pavement structure should be improved to address the deterioration and to allow for removal of half-load restrictions
- Oxford Road 19 is not part of the County's planned primary or secondary cycling networks, however it does provide connectivity to such cycling networks and trails
- *Improvements to vertical or horizontal alignments of the road to be considered at 20 identified locations in the detailed designs, to improve traffic operations*
- Right-of-way is recommended to be expanded to a minimum width of 30 metres in the rural area and 26 metres in Ostrander
- Improved illumination is warranted at the intersections with Highway 19 and Oxford Road 59.

AIR AND NOISE ASSESSMENT

Noise

- Traffic noise is forecasted to be 1 dBA over the daytime threshold level of 65 dBA (according to the Ministry of Transportation [MTO] Noise Guide) at two houses along the corridor, which triggers further investigation of noise impacts.
- Further investigation of noise levels were completed at the two houses noted above, however no noise mitigation is warranted based on noise levels estimated within the various living spaces.

Air Quality

- There are no houses on the road that will be exposed to a level of air contaminants that exceed criteria, except for the annual benzene impact.
- Annual benzene impact is unaffected by the local road and people would be impacted by the same level anywhere in the area, regardless of how close they were to this or any other road. Therefore, No significant impacts to air quality are forecasted to result from the proposed road improvements.

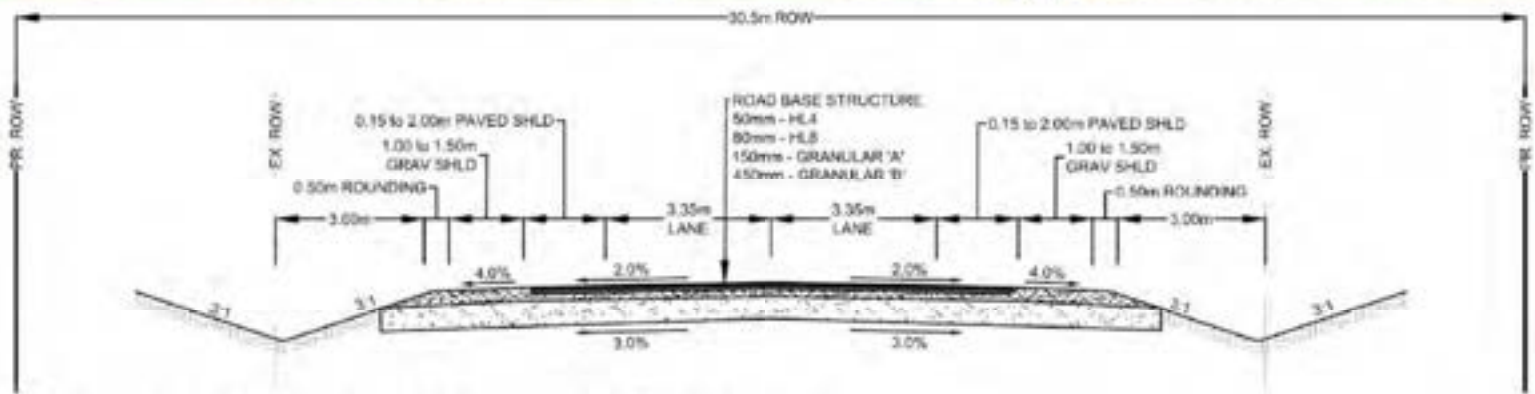
STORMWATER ASSESSMENT

- One structure was identified to have an existing width that will require widening to accommodate wider paved shoulders.
- Vegetated ditches will continue to provide roadside drainage requirements
- Impacts to surface water quantity or quality are expected to be minimal from the proposed project.
- Erosion and sediment control measures will be implemented during the construction phase.

Preferred Alternative Solution

Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW

- Provide a two-lane road to County requirements with paved shoulder.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.



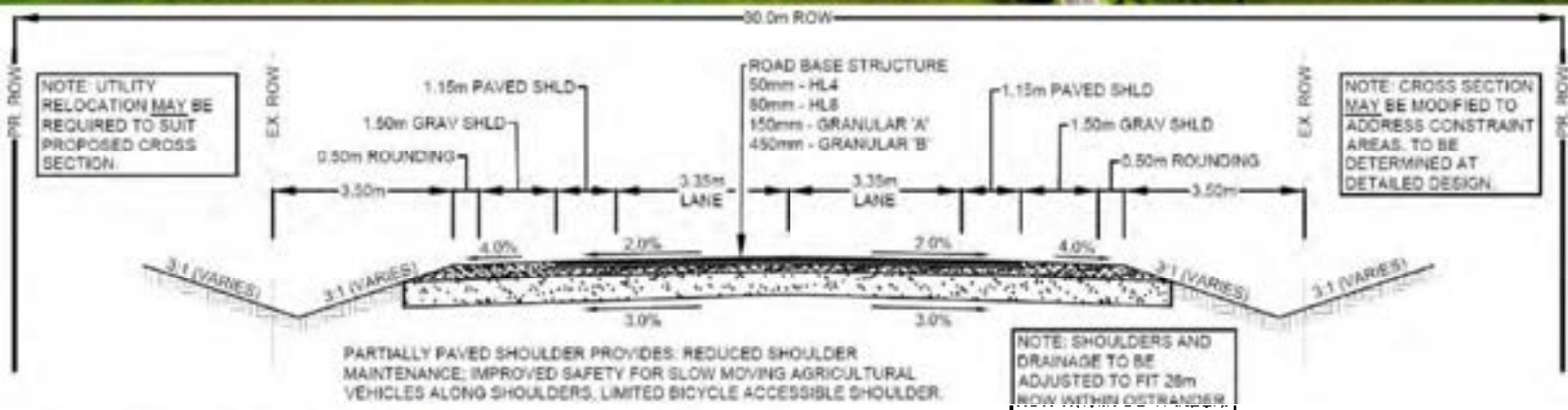
NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 5 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

Alternative Design Concepts

Alternative 1

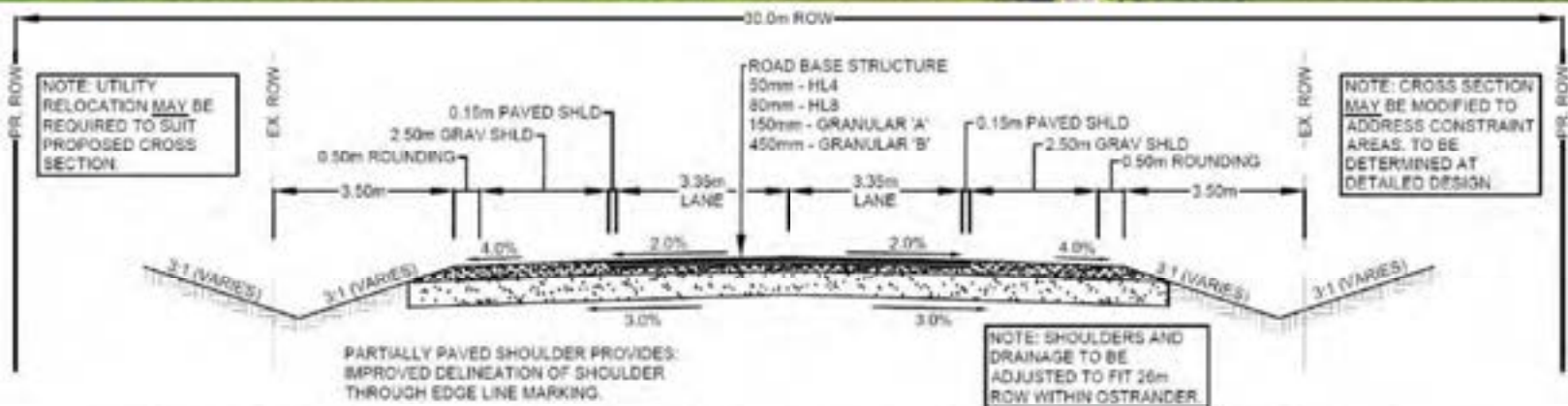
- 9.0 m paved asphalt
- Each side with 3.35 m travel lane, 1.15 m paved shoulder and 1.5 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)



ALTERNATIVE 1 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT
 SCALE 1:150

Alternative Design Concepts Alternative 2

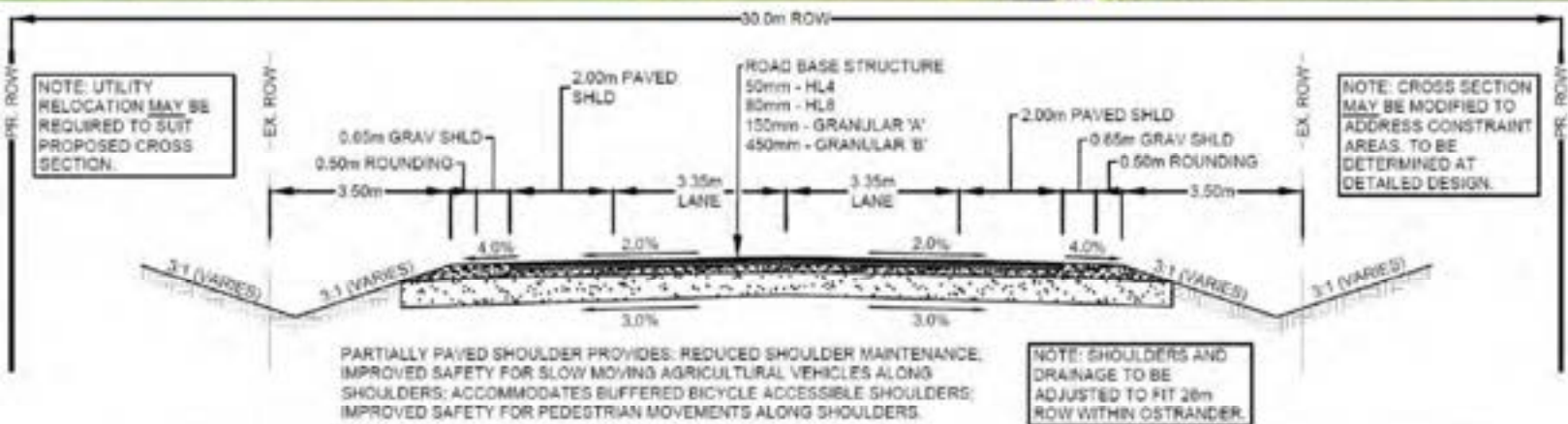
- 7.0 m paved asphalt
- Each side with 3.35 m travel lane, 0.15 m paved shoulder and 2.5 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)



ALTERNATIVE 2 - 0.15m PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN TO MEET OFFICIAL PLAN REQUIREMENT
SCALE 1:150

Alternative Design Concepts Alternative 3

- 10.7 m paved asphalt
- Each side with 3.35 m travel lane, 2.0 m paved shoulder and 0.65 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)



ALTERNATIVE 3 - 2.0m PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

EVALUATION CRITERIA

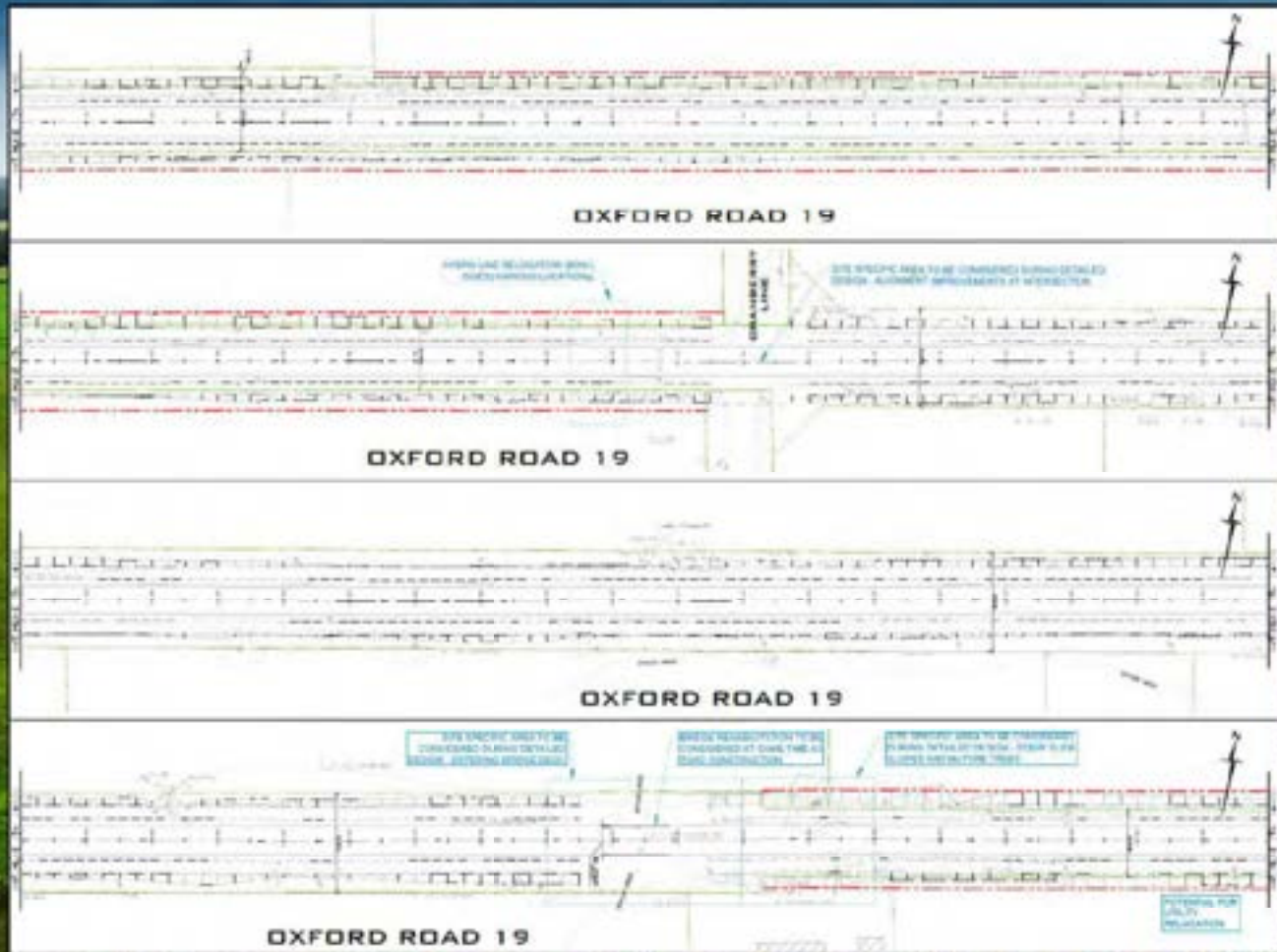
- **Natural Environment**
 - Impacts to vegetation and terrestrial habitat
 - Impacts to fisheries and aquatic habitat
 - Impacts to Hazard Lands (erosion, slope stability, flooding)
 - Impacts to Designated Features/species
 - Impacts to surface water quality and quantity
 - Impacts to groundwater quality and quantity
- **Technical Environment**
 - Accommodation of various modes of transportation and vehicle types
 - Improvement to road safety
 - Road maintenance requirements
 - Design complexity
 - Impacts to utilities and drainage work/structures
- **Socio-Cultural Environment**
 - Impacts to cultural heritage resources
 - Impacts to archaeological resources
 - Private property impacts
 - Nuisance impacts (noise, traffic, visual impact)
- **Financial**
 - Capital and operation/maintenance costs
 - Property acquisition cost

Evaluation Criteria	Alternative 1: 9.0 m paved asphalt Each side with 3.35 m travel lane, 1.15 m paved shoulder and 1.5 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road	Alternative 2: 7.0 m paved asphalt Each side with 3.35 m travel lane, 0.15 m paved shoulder and 2.5 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road	Alternative 3: 10.7 m paved asphalt Each side with 3.35 m travel lane, 2.0 m paved shoulder and 0.65 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road
Natural Environment	Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.	Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.	Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.
Socio-Cultural Environment	Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. Some dust created by agricultural equipment travel along gravel shoulders.	Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. Significant dust created by agricultural equipment travel along gravel shoulders.	Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. May lengthened the temporary construction period impacts if bridges require widening. Very little dust created by agricultural equipment travel along gravel shoulders.
Technical Environment	Wider paved shoulders provides increased spacing between traffic. Limited buffer for cyclists or pedestrians. Moderate improvement of safety for all travel modes using the corridor. Better able to accommodate full range of operating speeds. Moderate improvement to access operations along the corridor (pullover onto shoulder to exit roadway at accesses). Some adjustment of design required to interface the wider asphalt widths with crossings and accesses.	Minimal asphalt width provides for minimum space between traffic. Cyclists and pedestrians use gravel shoulder. Some improvement of safety for all travel modes using the corridor. Lower ability to accommodate vehicles with excessive speeds and / or slow-moving vehicles. No improvement to access operations along the corridor. Maintains existing asphalt interface with crossings (i.e., 3 bridges) and accesses.	Wider paved shoulders provides increased spacing between traffic. Increased buffer for cyclists and pedestrians. Significant improvement of safety for all travel modes using the corridor. Minimal gravel shoulder may direct agricultural equipment further onto paved areas and into travel lane. Better able to accommodate full range of operating speeds. Significant improvement to access operations along the corridor. Design may not be achievable in area of constraints without significant adjustment of such constraints. Potential for longer temporary construction impacts if structure widenings are required.
Economic Environment	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Moderate increase in cost over alternative 2, due to partially paved shoulder. Approximately \$118/m for shoulder. Some additional cost for adjustment of major bridge crossings.	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Lowest cost due to minimal paved shoulder. Approximately \$45/m for shoulder.	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Higher increase in cost over alternative 2, due to fully paved shoulder. Approximately \$180/m for shoulder. Increased additional cost for adjustment of major bridge crossings.
Overall Summary	Most Preferred	Less Preferred	Least Preferred

Level of Preference: Least Preferred ○ Less Preferred ◐ Moderately Preferred ◑ More Preferred ◒ Most Preferred ●

Preferred Design Concepts

Sample Plan



CONCEPT DRAWINGS OF THE FULL CORRIDOR ARE AVAILABLE TO VIEW, SHOWING EXISTING AND PROPOSED ROAD AND RIGHT-OF-WAY DETAILS

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---

Proposed Mitigation Measures

A comprehensive list of proposed mitigation measures will be prepared as part of this EA and included in the Environmental Study Report (ESR).

Some key measures include:

- Health and safety is a priority for Oxford County. Construction will adhere to strict safety guidelines including best practices for vehicle and pedestrian safety.
- Advance notice to local residents of potential traffic impacts from construction.
- Access to residences/farms/pedestrian facilities will be maintained during construction.
- Temporary site fencing and other construction measures will be implemented to minimize noise, vibration, dust, mud and visual impacts.
- The following plans will be prepared by the contractor and implemented during construction: Erosion and Sediment Control Plan; Emergency Response and Communications Plan; Spill Prevention and Contingency Plans; Complaint Response Protocol; Construction Management Plan; Health and Safety Plans; Soil Management Plan; and Traffic Management Plan.

Proposed Mitigation Measures

Natural Heritage Specific Mitigation Measures:

- Plant species loss will be minimized.
- Tree inventory, preservation and protection plan to be prepared at detailed design.
- To reduce the risk of potential impact to wildlife, vegetation clearing will not be completed during appropriate timing to avoid the active period for breeding birds and bat species.
- Installation of temporary fencing as required.
- Any in-water works will occur during appropriate in-water works timing window.
- Refueling and maintenance of construction equipment shall occur within designated areas only. Any hazardous materials used for construction will be handled in accordance to appropriate regulations.
- Spills or depositions into watercourses shall be immediately contained and cleaned up in accordance with provincial regulatory requirements and the contingency plan.

Preferred Design Concepts

Future Detailed Design Considerations:

- Confirm utility relocation requirements (hydro poles, natural gas main, telecommunications)
- Confirm adjustments to Ostrander Municipal Drain
- Widen existing bridge at one location and coordinate with required bridge rehabilitation
- Adjust cross section and/or drainage measures to address significant localized constraints (e.g., existing buildings, residential areas, topography, natural areas, interface with urban areas)
- Features to minimize impact on quality or quantity of stormwater runoff and erosion
- Localized improvements to vertical and/or horizontal alignments to improve sight lines
- Traffic calming near village boundaries
- Finalize right-of-way requirements and property acquisition requirements
- Phasing strategies for construction, considering access and budget constraints



Next Steps

- Confirm Preferred Design Concepts (January 2023)
- Finalize Transportation Study (January 2023)
- Environmental Study Report (February 2023)
- File Environmental Study Report for Schedule C Class EA Study (March 2023)

Invitation for Participation

Public input is an important component of the decision-making process.

You are invited to provide comments by completing the forms provided and submitting forms to the Study Team members below on or before January 13, 2023.

Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com

THANK-YOU FOR ATTENDING



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix C

Comment Sheets and Emails

Municipal Class Environmental Assessment Study Oxford Road 19 Corridor Improvements

Comment Sheet

Name: _____

Public Consultation Centre #2

Address: _____

Time: December 6, 2022
5 p.m. to 7 p.m.

Postal Code: _____

Phone: _____

Location: Springford Hall,
429 Main St. W, Springford, ON

Email: _____

The County of Oxford is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study area includes approximately 16 km of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville. This PCC has been scheduled to allow all interested parties an opportunity to discuss issues related to the project, including the alternative design concepts being considered. Public input is encouraged and will be given consideration during the planning and design of this project.

Comments/Questions/Suggestions (additional space on back of page):

Oxford County and R.J. Burnside & Associates Limited thank you for your involvement in Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements. Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. With exception to personal information, all comments will become part of the public record. Project information and comment sheet will be made accessible upon request in accordance with the *Accessibility Standard for Information and Communication under the Accessibility for Ontarians with Disabilities Act, 2005*.

Municipal Class Environmental Assessment Study Oxford Road 19 Corridor Improvements

Please complete this Comment Sheet and submit at the PCC or email to either of the Study Team members below on or before January 13, 2023. Your input and comments are appreciated.

Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com

Oxford County and R.J. Burnside & Associates Limited thank you for your involvement in Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements. Information will be collected in accordance with the *Freedom of Information and Protection of Privacy Act*. With exception to personal information, all comments will become part of the public record. Project information and comment sheet will be made accessible upon request in accordance with the *Accessibility Standard for Information and Communication under the Accessibility for Ontarians with Disabilities Act, 2005*.

RE: 053425-Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Thu, 1/26/2023 2:34 PM

To: [REDACTED]

Cc: jkeith@oxfordcounty.ca <jkeith@oxfordcounty.ca>; Henry Centen <Henry.Centen@rjburnside.com>; mark.badali1@ontario.ca <mark.badali1@ontario.ca>; 053425 Oxford Road 19 Class EA <053425OxfordRoad19ClassEA@rjburnside.com>

Hello Mike,

I apologize for not responding back to your supplementary notes below sooner.

The project website is up and available for public at any time during this project (<https://www.oxfordcounty.ca/en/news/oxford-road-19-corridor-improvements-class-ea-study.aspx>) and we welcome any additional comments you may have. We encourage you to visit the project website occasionally for updates as it progresses.

The Environmental Study Report will be available through the website for 30-days public review and comments. The project team will issue a 'Notice of Completion' that will be mailed/emailed to all that are on the project contact list, including public members, agencies, indigenous communities, and interested stakeholders, to inform them of the start of the 30-day review period.

If you have any additional questions or comments please do not hesitate to contact me.

Warm regards,
Avid

Avid Banihashemi
Environmental Project Manager

R.J. Burnside & Associates Limited | www.rjburnside.com
Office: +1 800-265-9662 Direct: +1 226-486-1562

From: [REDACTED]

Sent: Friday, December 23, 2022 2:45 PM

To: Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Cc: jkeith@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>; mark.badali1@ontario.ca; Sylvia Waters <Sylvia.Waters@rjburnside.com>; 053425 Oxford Road 19 Class EA <053425OxfordRoad19ClassEA@rjburnside.com>

Subject: Re: 053425-Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hi Avid.

Thanks for the reply.

- Specific to my comments, will I be able to have more input if necessary regarding any impact they might have on this project?
- Regarding the PCC comment window. Am I able to access this window now?
- Regarding the project website - will you be advising me when this becomes available?

I don't want to miss any cutoff date which could disqualify me to discuss my concerns with your team. I would like to work together with your team without needing to seek professional advice from others.

Thanks,

[REDACTED]

On Friday, December 16, 2022 at 02:29:18 p.m. EST, Avid Banihashemi <avid.banihashemi@rjburnside.com> wrote:

Hello Mike,

Thank you for your interest in this project! We are happy that you were able to attend the PCC2 and to share your comments and concerns with the project team. The team will review your comments carefully and closely and they will be considered as the EA study progresses.

A PCC#2 Summary Report will also be prepared once the PCC comment window is closed and will be available on the project website. The Summary Report will include responses to the comments we receive on PCC#2. We encourage you to visit the project website to review the Report once it becomes available, and for additional project updates.

Thank you,

Warm regards,

Avid

Avid Banihashemi
Environmental Project Manager

R.J. Burnside & Associates Limited | www.rjburnside.com
Office: +1 800-265-9662 Direct: +1 226-486-1562

From: [REDACTED]

Sent: Tuesday, December 13, 2022 3:02 PM

To: Sylvia Waters <Sylvia.Waters@rjburnside.com>;
Cc: jkash@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>; Avid Banbhashemi <Avid.Banbhashemi@rjburnside.com>; mark.badalil@ontario.ca
Subject: Re: 053425-Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hello

I was able to attend this meeting along with my [REDACTED]. We do have some comments and questions. Many of these were discussed with those in attendance on December 6.

Environmental impact

If the road is built as you suggest, this infers that the 20 (or so) trees to the east and on the south side of the road of our home, will be cut down. This will have a large negative impact on the environment. Frequently, there are hawks and sometimes eagles that use these trees. Just this past week we had a red tail hawk land in our blue spruce tree on our property and fly to one of the trees along side the road that I am referring to.

We have had, this past year, red squirrels living on our property and currently there are black squirrels as well. Removing these trees would destroy part of their habitat. I believe that red squirrels are a protected species.

We sometimes see snakes, frogs and toads in off to the side of the road where the tree line is and their habitat would be destroyed. Many small birds nest in these trees.

Proposed Location of new road.

If the road were rebuilt to the north of the existing road (ie closer to the hydro poles and to the east of 224846 Otterville Road), these trees could be saved and the habitat for the small animals might not be disturbed.

I urge you to consider this option.

Liability.

There is also a liability exposure that if not corrected, could put the county at an increased liability risk. Rebuilding of the road allows an opportunity to fix this existing liability exposure.

The exposure is the hill in front of our home. It blocks views to oncoming traffic when traffic is travelling west to east, and blocks the view from our driveway to the west. Anyone visiting always comment on how treacherous it is entering or exiting our driveway, Canada Post made us move our mailbox to the east, as to minimize the potential danger which this hill created for their mail delivery. To add to the problem, the neighbours to the northwest of our property have a laneway which is affected by this hill as it blocks the view when exiting / entering the laneway. We have witnessed 2 accidents caused by farm machinery and vehicles colliding when farm machinery is turning into the laneway. Farm machinery was travelling east and turning north into the laneway. Traffic behind the farm machinery are unable to see any laneway, and then attempt to pass the farm machinery, causing the accidents.

Taking out the hill to improve the line of vision would be a possible solution. Also moving the road to the north as mentioned above would also assist.

Please note that I am a [REDACTED] having owned and sold [REDACTED] one company which I worked with for [REDACTED] an [REDACTED]

Thanks for reading this email. We know that traffic will increase, and that we and yourselves have one chance of doing this correctly. I hope these comments are constructive.

[REDACTED]

[REDACTED]

On Thursday, November 17, 2022 at 12:10:39 p.m. EST, Sylvia Waters <sylvia.waters@rjburnside.com> wrote:

On behalf of Oxford County, please see attached Notice of Public Consultation Centre # 2 (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC #2 will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC #2 are as follows:

Date: Tuesday December 6, 2022

Time: 5:00 p.m. - 7:00 p.m.

Location: *Springford Community Hall, 429 Main St. W., Springford, Ontario*

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager

Oxford County Public Works

519-539-9800 ext.3194

jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,

Project Manager

R.J. Burnside & Associates Limited

519-340-2003

henry.centen@rjburnside.com



R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-9662 Direct: +1 705-797-4379
www.rjburnside.com

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Thank you.

Fwd: Road 19

Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Fri 12/9/2022 2:17 PM

To: 053425 Oxford Road 19 Class EA <053425OxfordRoad19ClassEA@rjburnside.com>

Cc: Sylvia Waters <Sylvia.Waters@rjburnside.com>

2 attachments (5 MB)

PCC #2 - Display Boards.pdf; PCC #2 - Comment Sheet.pdf

For filing

From: Jesse Keith <jkeith@oxfordcounty.ca>

Sent: Friday, December 9, 2022 8:37:08 AM

To: [REDACTED]

Cc: Henry Centen <Henry.Centen@rjburnside.com>; Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Subject: RE: Road 19

Good Morning Greg,

Sorry to hear that you weren't able to attend the 2nd Public Consultation Centre (PCC) for this project. Please find attached a slideshow of the display boards which were available for viewing at PCC#2. I've also attached the comment sheet that was provided. Both of these documents are in the process of being posted to the Study webpage: <https://www.oxfordcounty.ca/en/news/oxford-road-19-corridor-improvements-class-ea-study.aspx>

Public input is encouraged, so please don't hesitate to contact me with any input or questions that you may have. The final Environmental Study Report (ESR) will be made available for public viewing, tentatively in February/March of next year.

Best Regards,

JESSE KEITH, P. ENG. (HE / HSM) | Project Engineer, Public Works
OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3
www.oxfordcounty.ca | T 519.539.9800 / 1-800-755-0394, ext 3194



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Think about our environment. Print only if necessary.

----- Original Message -----

From: [REDACTED]

Sent: December 8, 2022 4:17 PM

To: Jesse Keith <jkeith@oxfordcounty.ca>

Subject: Road 19

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or on clicking links from unknown senders.

Hello Jesse,

Unfortunately I was not able to attend the open house on Tuesday in regards to the reconstruction of County Road 19.

Is there an executive summary available or somewhere I am able to access reports or recommendations?

Respectfully,

[REDACTED]

Sent from my iPhone

Avid Banihashemi

From: Avid Banihashemi
Sent: Tuesday, December 13, 2022 4:18 PM
To: SecondaryLandUse@HydroOne.com
Cc: jkeith@oxfordcounty.ca; Henry Centen; Reuben Davis; 053425 Oxford Road 19 Class EA
Subject: FW: Hydro One Response: 20221212-NoticeOfPIC2-Oxford Road 19 Corridor Improvement
Attachments: 20221212-NoticeOfPIC2-Oxford Road 19 Corridor Improvement.pdf

Hello,

Thank you for your response letter dated December 12, 2022 to the Notice of Public Consultation Center #2 for Oxford Road 19 Municipal Class Environmental Assessment (EA) Study. Kindly, please find below the download link to the preliminary design concept plans completed as part of Stage 3 of the EA Study for your review and input.

Warm regards,
Avid

https://rjburnside-my.sharepoint.com/:f:/p/avid_banihashemi/Eo1YvYOCON5KgYJ4vO6Lir0BBXyQZqGVof0Z38DWoFBY7Q?e=xMX9RR

-----Original Message-----

From: SUN Hongxia <https://urldefense.proofpoint.com/v2/url?u=http-3A__Susan.SUN-40HydroOne.com&d=DwIGaQ&c=euGZstcaTDllvimEN8b7jXrwqOf-v5A_CdpgnVfiiMM&r=4k04vHjTlbb55CDolNfi5L-amiHwA4ez_tPKr0Gzado&m=6mjpbHMNwEyRxaq5-38Dk-SdkAGEqsAI4UT6Bz4drTI&s=oMLWDBUIAYy9zmWgjn51C94s489snECOaDY-2VU3Ewg&e=>> On Behalf Of SECONDARY LAND USE Department
Sent: December 12, 2022 10:40 AM
To: Jesse Keith <jkeith@oxfordcounty.ca>
Cc: SECONDARY LAND USE Department <https://urldefense.proofpoint.com/v2/url?u=http-3A__Department.SecondaryLandUse-40hydroone.com&d=DwIGaQ&c=euGZstcaTDllvimEN8b7jXrwqOf-v5A_CdpgnVfiiMM&r=4k04vHjTlbb55CDolNfi5L-amiHwA4ez_tPKr0Gzado&m=6mjpbHMNwEyRxaq5-38Dk-SdkAGEqsAI4UT6Bz4drTI&s=6YwXprms-GJvrZeoHa1keQGG85H4TILFOh3gcQ2kt5U&e=>>
Subject: Hydro One Response: 20221212-NoticeOfPIC2-Oxford Road 19 Corridor Improvement

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or on clicking links from unknown senders.

Please see the attached for Hydro One's Response.

Hydro One Networks Inc

SecondaryLandUse@HydroOne.com

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Hydro One Networks Inc.

483 Bay Street
8th Floor South Tower
Toronto, Ontario M5G 2P5

HydroOne.com

December 12, 2022

Re: Oxford Road 19 Corridor Improvement

Attention:
Jesse Keith, P.Eng.,
Project Manager
Oxford County Public Works

Thank you for sending us notification regarding (Oxford Road 19 Corridor Improvement). In our assessment, we have confirmed that Hydro One has existing distribution assets within your study area.

At this time, we do not have sufficient information to comment on the potential resulting impacts that your project may have on our infrastructure. As such, we must stay informed as more information becomes available so that we can advise if any of the alternative solutions present actual conflicts with our assets, and if so; what resulting measures and costs could be incurred by the proponent. Note that this response does not constitute approval for your plans and is being sent to you as a courtesy to inform you that we must continue to be consulted on your project.

Hydro One must be consulted during all stages of your project. Please ensure that all future communications about this and future project(s) are sent to us electronically to secondarylanduse@hydroone.com

Sent on behalf of,

**Secondary Land Use
Asset Optimization
Strategy & Integrated Planning
Hydro One Networks Inc.**

FW: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Tue 11/29/2022 11:04 AM

To: 053425 Oxford Road 19 Class EA <053425OxfordRoad19ClassEA@rjburnside.com>

Cc: Sylvia Waters <Sylvia.Waters@rjburnside.com>

For filing

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Sent: Tuesday, November 29, 2022 11:02 AM

To: Avid Banihashemi <Avid.Banihashemi@rjburnside.com>; Henry Centen <Henry.Centen@rjburnside.com>

Subject: FW: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

FYI

From: mark-ups <Mark.ups@enbridge.com>

Sent: Monday, November 28, 2022 1:35 PM

To: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Subject: RE: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

For utility requests in legacy Union Gas franchise area, please submit a Design and Planning request through Ontario One Call at www.ontarioonecall.ca or 1-800-400-2255.

More information about submitting a Design and Planning request can be found here:

https://www.ontarioonecall.ca/en-content/uploads/Design_and_Planning_Best_Practice_2020.pdf



Enbridge Gas Inc. Former Franchise Areas



From: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Sent: Thursday, November 17, 2022 12:13 PM

To: [REDACTED]

Subject: [External] 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

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On behalf of Oxford County, please see attached Notice of Public Consultation Centre # 2 (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC #2 will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC #2 are as follows:

Date: Tuesday December 6, 2022

Time: 5:00 p.m. - 7:00 p.m.

Location: Springford Community Hall, 429 Main St. W., Springford, Ontario

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com



Sylvia Waters
Technical Administrator, EPA

R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-8682 Direct: +1 705-797-4379
www.rjburnside.com

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Thank you.

RE: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Notifications <Notifications@enbridge.com>

Wed 11/30/2022 9:03 PM

To: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Thank you for your email.

Enbridge will review all planning and development notifications to determine their proximity and potential to impact our liquid pipeline network. If your notification is in proximity to an Enbridge liquid transmission pipeline, a formal response will be prepared and emailed by your specified deadline.

Planning and development notifications not in proximity to an Enbridge transmission pipeline will not receive a response. Please continue to send in planning and development notifications to notifications@enbridge.com. You can view the approximate locations of Enbridge liquid transmission pipelines on our web map [here](#).

We appreciate the opportunity to work with you in support of Enbridge's damage prevention and integrity management program and look forward to collaborating in the future.

Sincerely,
Enbridge Damage Prevention.

Nous avons bien reçu votre courriel et vous en remercions.

Enbridge examinera toutes les notifications de planification et de développement afin de déterminer leur proximité et leur potentiel d'impact sur notre réseau de pipelines de liquides. Si votre notification se trouve à proximité d'un pipeline de transport de liquides d'Enbridge, une réponse officielle sera préparée et envoyée par courriel dans le délai indiqué.

Les notifications de planification et de développement qui ne se trouvent pas à proximité d'un pipeline de transmission d'Enbridge ne recevront pas de réponse. Veuillez continuer à envoyer vos notifications de planification et de développement à notifications@enbridge.com. Vous pouvez voir les emplacements approximatifs des pipelines de transport de liquides d'Enbridge sur notre carte Web [ici](#).

Toutes les autres demandes non liées à l'utilisation des terres seront transmises au contact approprié d'Enbridge. Nous répondrons à toutes les demandes et questions d'ordre général.

Nous apprécions l'opportunité de travailler avec vous pour soutenir le programme de prévention des dommages et de gestion de l'intégrité d'Enbridge et nous espérons pouvoir collaborer à l'avenir.

Sincèrement,
Prévention des dommages d'Enbridge

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>
Sent: Thursday, November 17, 2022 10:13 AM

[Redacted content]

Subject: [External] 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

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- Time: 5:00 p.m. - 7:00 p.m.
- Location: Springford Community Hall, 429 Main St. W., Springford, Ontario

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
519-340-2003



Sylvia Waters
Technical Administrator, EPA

R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
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Thank you.

FW: G226042_RE: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Michael Simon <Michael.Simon@rjburnside.com>

Wed 12/1/2022 8:38 AM

To: 053425 Oxford Road 19 Class EA <053425OxfordRoad19ClassEA@rjburnside.com>

2 attachments (346 KB)

G226042 - Response letter.pdf, G226042.dwg

From: Henry Centen <Henry.Centen@rjburnside.com>

Sent: Tuesday, December 06, 2022 1:50 PM

To: Michael Simon <Michael.Simon@rjburnside.com>

Cc: Jeremy Taylor <Jeremy.Taylor@rjburnside.com>; Amy Boddy <Amy.Boddy@rjburnside.com>

Subject: FW: G226042_RE: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Fyi. Utility coordination OR19.

HC

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Sent: Tuesday, December 06, 2022 1:45 PM

To: Avid Banhashemi <Avid.Banhashemi@rjburnside.com>; Henry Centen <Henry.Centen@rjburnside.com>

Subject: FW: G226042_RE: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Fyi

From: Mohan, Anusree <Anusree.Mohan@telecon.ca>

Sent: Wednesday, November 30, 2022 10:59 AM

To: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Subject: G226042_RE: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hi,

Please find the attached completed markup request.

Regards,

Anusree Mohan

CAO, Engineering - Central Canada

E : Anusree.Mohan@telecon.ca

7777 Weston Rd, Woodbridge (Ontario) L4L 0G9



telecon.ca

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Sent: Thursday, November 17, 2022 12:13 PM

To: [REDACTED]

Subject: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

On behalf of Oxford County, please see attached Notice of Public Consultation Centre #2 (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

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Date: Tuesday December 6, 2022

Time: 5:00 p.m. - 7:00 p.m.

Location: Springford Community Hall, 429 Main St. W., Springford, Ontario

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com



Sylvia Waters
Technical Administrator, EPA

R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-9662 Direct: +1 705-797-4379
www.rjburnside.com

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Thank you.

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FW: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Fri 11/18/2022 9:13 AM

To: 053425 Oxford Road 19 Class EA <053425OxfordRoad19ClassEA@rjburnside.com>

2 attachments (4 MB)

FW: Oxford Road 19 Corridor Improvement; Notice of Public Consultation Centre #2.pdf

From: Jesse Keith <jkeith@oxfordcounty.ca>
Sent: Thursday, November 17, 2022 3:34 PM
To: 'Jeff Soetemans' <Jeff.Soetemans@execulinktelecom.ca>
Cc: planning@execulinktelecom.ca; Henry Centen <Henry.Centen@rjburnside.com>; Avid Banihashemi <Avid.Banihashemi@rjburnside.com>
Subject: RE: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hi Jeff,

Thanks for reaching out. As per the Notice, we are still in the Class EA study stage; the preliminary preferred conceptual design (pending public input) includes structural improvements to Oxford Road 19 (to eliminate half load restrictions) and widening of shoulders. Detailed design will follow the study incl. proposed property acquisition process, and eventually utility relocations/construction.

Earlier this year, you sent over some plans of Execulink plant existing/planned for the area (attached email correspondence). Was the section in Ostrander completed this year? If plans are available for future fibre between Springford and Otterville, please share.

Thanks,

JESSE KEITH, P. ENG. (HE / HIM) | Project Engineer, Public Works
OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3
www.oxfordcounty.ca | T 519.539.9800 / 1-800-755-0394, ext 3194



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Think about our environment. Print only if necessary.

From: Jeff Soetemans <Jeff.Soetemans@execulinktelecom.ca>
Sent: November 17, 2022 1:06 PM
To: Jesse Keith <jkeith@oxfordcounty.ca>
Cc: planning@execulinktelecom.ca
Subject: FW: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

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Hi Jesse,
Sorry I could not attend the meeting today. Execulink does have fiber along this road east of Otterville to Baseline, intention to complete next year the section between Springford and Otterville, and we also run down Zenda Line crossing Rd 19. What do you need from us to help with this project?

Thanks,

Jeff



Jeff Soetemans
Operations Manager
619 Main St N, Burgessville
tel: 519.456.1094
fax: 519.456.7803
other: 519.902.8940
email: jff.soetemans@execulinktelecom.ca
www.execulink.ca

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Sent: Thursday, November 17, 2022 12:13 PM

To: [Redacted]

Subject: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

On behalf of Oxford County, please see attached Notice of Public Consultation Centre # 2 (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC #2 will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC #2 are as follows:

Date: Tuesday December 6, 2022
Time: 5:00 p.m. - 7:00 p.m.
Location: Springford Community Hall, 429 Main St. W., Springford, Ontario

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Center, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.center@rjburnside.com



R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-9662 Direct: +1 705-797-4379
www.rjburnside.com

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Thank you.

FW: 053425-Mississaugas of the Credit First Nation - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Mon 12/5/2022 4:28 PM

To: 053425 Oxford Road 19 Class EA <053425OxfordRoad19ClassEA@rjburnside.com>

Cc: Henry Centen <Henry.Centen@rjburnside.com>; Sylvia Waters <Sylvia.Waters@rjburnside.com>

For filing

From: Abby LaForme <[REDACTED]>

Sent: Monday, December 05, 2022 4:19 PM

To: Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Cc: Sylvia Waters <Sylvia.Waters@rjburnside.com>; jkeith@oxfordcounty.ca

Subject: RE: 053425-Mississaugas of the Credit First Nation - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Good Afternoon,

Thank you for reaching out to MCFN DOCA for Consultation.

At this time MCFN DOCA has no comments or concerns regarding MCEA Study for Oxford Road 19 Corridor Improvements.

Please contact MCFN DOCA if any changes or new information arises about said above project.

Thank you

Abby (LaForme) Lee
Acting Consultation Coordinator



Mississaugas of the Credit First Nation (MCFN)
Department of Consultation & Accommodation (DOCA)
4065 Highway 6, Hagersville, ON N0A 1H0
Ph: (905) 768 - 4260
Email: [REDACTED]

From: Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Sent: Monday, December 5, 2022 4:13 PM

To: Abby LaForme <[REDACTED]>

Subject: FW: 053425-Mississaugas of the Credit First Nation - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Avid Banihashemi
Environmental Project Manager

R.J. Burnside & Associates Limited | www.rjburnside.com
Office: +1 800-265-9662 Direct: +1 226-486-1562

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Sent: Thursday, November 17, 2022 12:11 PM

To: [REDACTED]

Cc: jkeith@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>; Avid Banihashemi <Avid.Banihashemi@rjburnside.com>; mark.badali1@ontario.ca; Badali, Mark (MECP) <mark.badali1@ontario.ca>

Subject: 053425-Mississaugas of the Credit First Nation - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hello Abby

On behalf of Oxford County, please see attached Notice of Public Consultation Centre # 2 (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC #2 will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC #2 are as follows:

Date: Tuesday December 6, 2022
Time: 5:00 p.m. - 7:00 p.m.
Location: Springfield Community Hall, 429 Main St. W., Springfield, Ontario

The Project Team would be pleased to meet with your community at any time during the EA Study to answer our questions or to discuss any concerns you may have. If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com



R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-9662 Direct: +1 705-797-4379
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Thank you.

RE: Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Wed 2/8/2023 9:32 AM

To: [REDACTED]

Cc: jkeith@oxfordcounty.ca <jkeith@oxfordcounty.ca>; Henry Centen <Henry.Centen@rjburnside.com>; Reuben Davis <rdavis@oxfordcounty.ca>

2 attachments (4 MB)

Oxford Road 19 EA - HDI Email Response - Final - Via Email .pdf; Attachment 053425 Oxford Road 19 EA PCC2 Boards.pdf

Hi [REDACTED]

I apologize for not including the enclosure the letter in my email below. Attached please find both the letter and the enclosure.

Best regards,

Avid

Avid Banihashemi
Environmental Project Manager

R.J. Burnside & Associates Limited | www.rjburnside.com
Office: +1 800-265-9662 Direct: +1 226-486-1562

From: Avid Banihashemi

Sent: Wednesday, February 08, 2023 9:02 AM

To: [REDACTED]

Cc: jkeith@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>; Reuben Davis <rdavis@oxfordcounty.ca>

Subject: RE: Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Good morning [REDACTED]

On behalf of Oxford County, kindly please find attached a response to your email below.

Best regards,

Avid

From: Janice Williams <[REDACTED]>

Sent: Wednesday, December 07, 2022 10:27 AM

To: Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Cc: [REDACTED]

[REDACTED]

[REDACTED]

Subject: Re: Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Sg: no/Hello Avid Banihashemi,

Currently, the HDI and our nation's do not hold these notifications as engagement or consent. Again I address HDI and our nation's significant concerns with respect to the proposed project, explicitly towards the Oxford County and RJ Burnside not applying an application and fee with HDI to review the project. How are we to provide feedback and consider engagement when we have no funds to review and/or comment on this proposed project?

It is required that the proponent of the project provide a completed application and fee so we can analyze the effects, impairment, and interference within our treaty rights. Once we complete our evaluation, we can begin participation in a meaningful way. You may access the application on the link below and see the provided instructions to complete our application process.

[Development - Haudenosaunee Confederacy](#)

Click on the PDF file download and complete the application. Once this is completed, please mail to:

Haudenosaunee Development Institute

44 Sixth Line

Caledonia, Ontario

N3W 1Y9

Payment can be made as a cheque and mailed to HDI P.O.Box number:

P.O.Box office: P.O.Box 714 Ohsweken ON, N0A 1M0

Again, this application process provides initial funding for our team to internally review the documents and recognize how this project impacts and interferes within our treaty rights. Once the appropriate measures have been followed through, we will reach out and discuss HDI participation and impacts in a meaningful way. Until then, we ask this proposed project to halt any further.

Nys:wef/Thank-you,

Raechele Williams

HDI Environmental Supervisor

Haudenosaunee Development Institute

16 Sunrise Court, Suite 402B Ohsweken, ON

P.O. Box 714

Ph: 519-445-4222



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On Thu, Nov 10, 2022 at 3:23 PM Avid Banhashemi <Avid.Banhashemi@rjburnside.com> wrote:

Dear Ms. Williams,

On March 17, 2022, your community was sent a letter from R.J. Burnside & Associates, on behalf of Oxford County, regarding Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment (Class EA) Study with three attachments, Notice of Commencement, Study Area map and Project Response Form, as the first step to initiate the consultation process for this project. On May 19, 2022, in follow-up to that letter, your community was sent the Notice of Public Consultation Centre #1 (NoPCC #1), for the Oxford Road 19 Corridor Improvement Class EA Study, held on June 9, 2022. The notices above were followed by receipt confirmation calls.

We are following up with HDI regarding your email dated June 7, 2022 which highlighted concerns with respect to the Oxford Road 19 Corridor Improvement project, particularly with Oxford County not submitting an application and fee to HDI for project review. At this time, Oxford County is not able to submit an application and provide a fee to HDI for this project. However, Oxford County is committed to the open flow of information and to ensuring that there are opportunities for Indigenous communities to provide input during the Study. In line with this commitment, the County would be happy to share the draft Stage I Archaeological Assessment Report and the draft Natural Heritage Report that are underway as part of this study, once they become available, or any other findings of the study that HDI may be interested in as the study progresses. The Project Team would also be pleased to meet with your community at any time during the EA process to discuss the findings of the study to date, receive the communities' input on these findings to assist with the decision making, answer any questions you may have or to discuss any community concerns with the project.

Thank you in advance for your participation. Should you have any questions or require additional information, please contact the undersigned by phone 519-539-9800, ext. 3194, fax 519-421-4711 or email keith@oxfordcounty.ca

Best regards,

Avid Banhashemi (on behalf of Oxford County's Oxford Road 19 Improvements EA Study Team)



R.J. Burnside & Associates Limited
292 Speedvale Avenue West, Unit 20, Guelph, Ontario N1H 1C4
Office: +1 800-265-9662 Direct: +1 226-486-1562
www.rjburnside.com

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Thank you.

February 6, 2023

Haudenosaunee Development Institute

**RE: Oxford Road 19 Corridor Improvements
Municipal Class Environmental Assessment Study – Schedule C
Response Letter to December 7, 2022 Communication**

Dear Ms. Williams,

We are following up with HDI regarding your email dated December 7, 2022 which highlighted concerns with respect to the Oxford Road 19 Corridor Improvements project.

On behalf of Oxford County, I would like to thank you for your interest in this project. Please rest assured that the Project Team understands HDI's concerns on the potential impacts of this project. At this time, Oxford County will not be engaging in HDI's application process. However, the County would like to offer HDI technical resources on any areas of their concern, to assist with a thorough understanding of the project and any potential impacts of concern.

The County deeply believes in transparent and open flow of communication and information. In line with that, we would be happy to share the findings of our research on any of the study areas that HDI may be interested in; and as noted above, provide technical resources to ensure that these findings address your concerns, particularly in the areas of natural heritage, along with the mitigation measures identified in the stage 1 archaeological assessment. We are also happy to provide in depth review of the road corridor preliminary designs.

The County is also aiming to share the draft supporting technical reports and / or memorandums for natural heritage and stage 1 archaeological assessment as soon as they become available, along with a high-level summary of the study findings, potential impacts, and proposed mitigation measures. In the meantime, we respectfully invite you to review the attached copy of Public Consultation Centre #2 (PCC#2) boards presented at the PCC#2 event held on December 6, 2022.

The Project Team would be pleased to meet with HDI at any time during the EA process to discuss the findings of the study to date, receive the communities' input on these findings and / or discuss how and when the County's technical resources can assist HDI with a full understanding of the findings of the project to date.

Thank you again for your interest in this project. Should you have any questions or require additional information, please contact the undersigned by phone 519-539-9800, ext. 3194, fax 519-421-4711 or email jkeith@oxfordcounty.ca



Sincerely,

A handwritten signature in black ink that reads "Jesse Keith". The signature is written in a cursive, slightly slanted style.

Jesse Keith
Project Manager
Oxford County Public Works

Encl. *Public Consultation Centre #2 Boards*

cc: Henry Centen, Project Manager, R.J. Burnside & Associates,
henry.centen@rjburnside.com

Reuben Davis, Supervisor of Engineering Services, Oxford County Public Works
rdavis@oxfordcounty.ca

Avid Banihashemi

From: Sylvia Waters
Sent: Thursday, November 17, 2022 3:31 PM
To: Avid Banihashemi
Subject: FW: consultwithcaldwell.ca - Project Submission

Importance: High

Follow Up Flag: Follow up
Flag Status: Flagged

FYI

From: CFN Consultation Coordinator <noreply@consultwithcaldwell.ca>
Sent: Thursday, November 17, 2022 3:26 PM
To: Sylvia Waters <Sylvia.Waters@rjburnside.com>
Subject: consultwithcaldwell.ca - Project Submission

Michelle McCormack
Consultation Coordinator

Zack Hamm
Consultation Coordinator

Caldwell First Nation
14 Orange Street
Leamington | ON | N8H 1P5

We have received your initial submission

Please note that before the project is ready for review, **you must provide some additional information:**

The remaining information involves questions that were marked as "unsure"

- Habitat or flight paths
- Endangered species permit
- Landscape agreement
- Archaeological assessment

When you have this information ready, you can return to this website and submit your information.

If you have questions, please contact CFN's Consultation Coordinator: ecc@caldwellfirstnation.ca or [519-322-1766 ext. #1243](tel:519-322-1766).

*Please note that simply sending an email to the Consultation Coordinator or to a Chief and Council member does not constitute consultation.

*Please be advised that an administrative fee will be charged for a meeting with Chief and Council.



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H4

Correspondence with Indigenous Communities



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H4-A

Summary

Mishaal Rizwan

From: Badali, Mark (MECP) <Mark.Badali1@ontario.ca>
Sent: Wednesday, December 15, 2021 2:21 PM
To: Mishaal Rizwan
Subject: RE: MCEA - Indigenous Communities List

Follow Up Flag: Follow up
Flag Status: Flagged

Good afternoon,

Thank you for your email. The list you provided in your email below is representative of the following communities that the ministry recommends be engaged, as they may be impacted by or have an interest in the project:

- Aamjiwnaang First Nation
- Bkejwanong (Walpole Island)
- Caldwell First Nation
- Chippewas of Kettle and Stony Point
- Chippewas of the Thames First Nation
- Oneida Nation of the Thames
- Eelūnaapèewii Lahkèewiit (Delaware Nation or Moravian of the Thames)
- Munsee-Delaware Nation

Please be aware that the above community list may change should new information become available on project impacts and/or communities' areas of interest.

The ministry will provide a formal letter of acknowledgement, including guidance regarding the ministry's areas of interests, upon receipt of the Notice of Commencement for this Class EA project.

Thank you,

Mark Badali (he/him)
Regional Environmental Planner (REP) – Southwest Region
Project Review Unit | Environmental Assessment Branch
Ontario Ministry of the Environment, Conservation and Parks
Mark.Badali1@ontario.ca | (416) 457-2155

From: Mishaal Rizwan <Mishaal.Rizwan@rjburnside.com>
Sent: November 17, 2021 9:15 AM
To: Badali, Mark (MECP) <Mark.Badali1@ontario.ca>
Cc: EA Notices to SWRegion (MECP) <eanotification.swregion@ontario.ca>
Subject: MCEA - Indigenous Communities List

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hello,

We are contacting the MECP to request confirmation of the recommended list of Indigenous communities to contact as part of a Municipal Class Environmental Assessment (MCEA) Schedule C for the Oxford Road 19 Corridor Improvements for the County of Oxford.

The County is undertaking a Study for 16 km of the corridor from Highway 19 to the boundary of the County of Norwich.

A Notice of Commencement for the project will be provided to the MECP in the near future via the Southwestern Region (eanotification.swregion@ontario.ca) one window email process along with the Project Information Form.

Based on the County's previous project experience the Study Team understands that the following communities may have interested in this project:

West Central Region (Ontario)

- Mississaugas of the New Credit First Nation
- Six Nations of the Grand River
- Haudenosaunee Confederacy Chiefs Council and Haudenosaunee Development Institute

Southwest Region (Ontario)

- Aamjiwnaang First Nation
- Bkejwanong Territory (Walpole Island First Nation)
- Chippewas of Kettle and Stony Point First Nation
- Chippewas of the Thames First Nation
- Caldwell First Nation
- Oneida Nation of the Thames ONYOTA'A:KA
- Munsee-Delaware Nation
- Delaware Nation

Could you please confirm if this list is representative all of the communities which may be interested in the study and should be engaged in regards to this study? Please let me know if you require any additional information.

Thank you,



Mishaal Rizwan
Environmental Planner

R.J. Burnside & Associates Limited
292 Speedvale Avenue West, Unit 20, Guelph, Ontario, N1H 1C4
Office: 800-265-9662 Direct Line: 226-343-7014
www.rjburnside.com



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Mishaal Rizwan

From: Henry Centen
Sent: Friday, March 4, 2022 6:21 AM
To: Tricia Radburn
Cc: Mishaal Rizwan
Subject: FW: Municipal Class EA Study - Oxford Road 19 Corridor Improvements

Follow Up Flag: Follow up
Flag Status: Flagged

fyi

From: Badali, Mark (MECP) <Mark.Badali1@ontario.ca>
Sent: Thursday, March 03, 2022 1:11 PM
To: Jesse Keith <jkeith@oxfordcounty.ca>
Cc: Henry Centen <Henry.Centen@rjburnside.com>; Reuben Davis <rdavis@oxfordcounty.ca>; Melissa Abercrombie <mabercrombie@oxfordcounty.ca>; Frank Gross <fgross@oxfordcounty.ca>
Subject: RE: Municipal Class EA Study - Oxford Road 19 Corridor Improvements

Good afternoon,

As the project lies completely in the Treaty 3 area, the ministry's original advice with respect to MECP Southwest Region was around including those signatories and the closest Nanfan treaty community. However, as the study area is so close to the Treaty 4, the ministry concurs with the full list of communities provided in the County's letter dated February 18, 2022 and recommends that the County engage the following communities as an interest based notification:

- Mississaugas of the Credit First Nation
- Six Nations of the Grand River (both Elected Council and Haudenosaunee Confederacy Chiefs Council)
- Aamjiwnaang First Nation
- Bkejwanong (Walpole Island)
- Caldwell First Nation
- Chippewas of Kettle and Stony Point
- Chippewas of the Thames First Nation
- Oneida Nation of the Thames
- Eelūnaapèewii Lahkèewiit (Delaware Nation or Moravian of the Thames)
- Munsee-Delaware Nation

As previously noted, please be aware that the above community list may change should new information become available on project impacts and/or communities' areas of interest. The ministry will provide a formal letter of acknowledgement, including guidance regarding the ministry's areas of interests, upon receipt of the Notice of Commencement for this Class EA project.

Thank you,

Mark Badali ([he/him](#))
Regional Environmental Planner (REP) – Southwest Region

Project Review Unit | Environmental Assessment Branch
Ontario Ministry of the Environment, Conservation and Parks
Mark.Badali1@ontario.ca | (416) 457-2155



From: Jesse Keith <jkeith@oxfordcounty.ca>
Sent: March 2, 2022 9:47 AM
To: Badali, Mark (MECP) <Mark.Badali1@ontario.ca>
Cc: 'Henry Centen' <Henry.Centen@rjburnside.com>; Reuben Davis <rdavis@oxfordcounty.ca>; Melissa Abercrombie <mabercrombie@oxfordcounty.ca>; Frank Gross <fgross@oxfordcounty.ca>
Subject: RE: Municipal Class EA Study - Oxford Road 19 Corridor Improvements

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Hi Mark,

Thanks kindly for the update, hope to here from you soon.

Best Regards,

JESSE KEITH, P. ENG. (HE / HIM) | **Project Engineer, Public Works**
OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3
WWW.OXFORDCOUNTY.CA | T 519.539.9800 / 1-800-755-0394, ext 3194



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 Think about our environment. Print only if necessary.

From: Badali, Mark (MECP) <Mark.Badali1@ontario.ca>
Sent: February 28, 2022 8:33 AM
To: Jesse Keith <jkeith@oxfordcounty.ca>
Cc: 'Henry Centen' <Henry.Centen@rjburnside.com>; Reuben Davis <rdavis@oxfordcounty.ca>; Melissa Abercrombie <mabercrombie@oxfordcounty.ca>; Frank Gross <fgross@oxfordcounty.ca>
Subject: RE: Municipal Class EA Study - Oxford Road 19 Corridor Improvements

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Hi Jesse,

Thank you for your email, I hope that you have been keeping well too.

Due to some staff transitions our Indigenous Advisors will require more time to reconfirm the appropriate list of Indigenous Communities for this Class EA project. I will provide that to you as soon as possible.

Best regards,

Mark Badali ([he/him](#))

Regional Environmental Planner (REP) – Southwest Region
Project Review Unit | Environmental Assessment Branch
Ontario Ministry of the Environment, Conservation and Parks
Mark.Badali1@ontario.ca | (416) 457-2155



From: Jesse Keith <jkeith@oxfordcounty.ca>

Sent: February 18, 2022 11:46 AM

To: Badali, Mark (MECP) <Mark.Badali1@ontario.ca>

Cc: 'Henry Centen' <Henry.Centen@rjburnside.com>; Reuben Davis <rdavis@oxfordcounty.ca>; Melissa Abercrombie <mabercrombie@oxfordcounty.ca>; Frank Gross <fgross@oxfordcounty.ca>

Subject: Municipal Class EA Study - Oxford Road 19 Corridor Improvements

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Hello Mark,

I hope you are doing well. Oxford County is commencing a Schedule 'C' Municipal Class EA Study to consider improvements for Oxford Road 19 Corridor. As per our Oxford County protocol and attached enquiry letter I am asking that MECP reconfirm the appropriate Indigenous Communities which require interest-based consultation for this study, and whether you are aware of any asserted potential impacts to Aboriginal or treaty rights which might arise from this study project.

A Draft Notice of Study Commencement, larger Study Area map, and brief Project Backgrounder are also attached to the letter for your information and comment. Once we hear back from you, the Notice will be finalized and we will submit it along with the 'Project Information Form' to eanotification.swregion@ontario.ca (in conjunction with various circulation/publication methods to notify potential stakeholders; Indigenous Communities; public; etc.)

Your assistance with fulfilling this request at your earliest availability (and preferably by **Friday, February 25th** if at all possible) would be greatly appreciated.

Best Regards,

JESSE KEITH, P. ENG. (HE / HIM) | Project Engineer, Public Works

OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3

WWW.OXFORDCOUNTY.CA | T 519.539.9800 / 1-800-755-0394, ext 3194



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 Think about our environment. Print only if necessary.

PROJECT NAME	Oxford Rd 19	BEGIN DATE	Mar-22	VERSION NUMBER	1																
CLIENT	County of Oxford	END DATE	Jul-23	DATE PREPARED	June 28/23																
POINT OF CONTACT		DURATION		AUTHOR	TR																
PROJECT DESCRIPTIVE																					
CATEGORY	STAKEHOLDER	TITLE/ROLE	CONTACT		Upload Info to Consultation Database	Received Notice of Commencement	Received Notice of PCC#1	Received Notice of PCC#2	Received Archaeological Assessment for Review	Removed From/Added to Mailing List	Phone Call Log/Email Notes										
			Name	Email																	
Indigenous Communities																					
D - Indigenous	Aamjiwnaang First Nation	Aamjiwnaang Administration Office	Chief Chris Plain	rogers@aamjiwnaang.ca		Y	Y	Y	Y	Sent June 9/23	220317_Email. Aamjiwnaang.chief@gmail.com failed. Sent to alternative email. 220602_Call. Burnside spoke with reception. It was noted that notices should be sent to the Band Manager, Jamie Maness (jmaness@aamjiwnaang.ca).										
D - Indigenous	Aamjiwnaang First Nation	Band Manager	Jamie Maness	jmaness@aamjiwnaang.ca		Y	Y	Y	Y	Removed. June 15/23 email indicating no longer in consultation position.	220602_Call. Burnside spoke with reception. It was noted that notices should be sent to the Band Manager, Jamie Maness (jmaness@aamjiwnaang.ca). 220602_Email. Burnside forwarded earlier email to Jamie Maness and also left a message on voice mail. 221205_call. Follow up call on the Notice of PCC2 by Avid B. Voice msg left for Jamie Maness.										
D - Indigenous	Aamjiwnaang First Nation	Band Manager	Harrison Plain	hplain@aamjiwnaang.ca					Y	Sent June 9/23	Added. June 15/23 as per email from Maness. 230615_Email. J. Maness requested that the Archaeological Report be forwarded to Harrison Plain. 230802_Burnside left message re: follow-up on Archaeological Report. He is in a meeting. 230803_Follow-up email sent. 230823_Call re: Archaeological Report follow-up. He is in meeting, voicemail box was full.										
D - Indigenous	Bkejwanong Territory (Wapole Island First Nation)	Consultation Manager	Dean Jacobs	dean.jacobs@wfn.org		Y	Y	Y	Y	Sent June 9/23	220607_Call. Burnside spoke with D. Jacobs. Notice of PIC #1 was received, communities is interested in being kept informed and has no comment at this time. 221205_call. Follow up call on the Notice of PCC2 by Avid B. Voice msg left for Dean Jacobs.										
D - Indigenous	Bkejwanong Territory (Wapole Island First Nation)	Consultation Manager	Janet Macbeth	janet.macbeth@wfn.org		Y	Y	Y	Y	Sent June 9/23											
D - Indigenous	Bkejwanong Territory (Wapole Island First Nation)	Project Review Coordinator	Larissa Wrightman	larissa.wrightman@wfn.org					Y	Sent June 9/23	Added: July 31 2023. New contact. 230801_No phone extension found for a call. 230803_Burnside sent follow-up email.										
D - Indigenous	Chippewas of Kettle and Stony Point First Nation	Chief	Jason Henry	Jason.Henry@kettlepoint.org		Y	Y	Y	Y	Sent June 9/23											
D - Indigenous	Chippewas of Kettle and Stony Point First Nation	Consultation Advisor, CKSPFN	Emily Ferguson	consultation@kettlepoint.org		Y	Y	Y	Y	Sent June 9/23	220615_Email. Emily Ferguson sent appreciation from the community, and noted that staff will review the documentation and return comments shortly. 220708_Email. Emily Ferguson noted staff have been delayed in reviewing the project file, and requested an update on the project to date. It was noted that the community is interested in the environmental impact and archaeological studies associated with the project, and if documents could be sent if available. 220803_Email. Burnside responded that staff are reviewing the draft Stage 1 Archaeological Assessment Report and will share the draft report for review before it is finalized. ESR noted that the natural environment supporting studies are ongoing and their results will be included in the final ESR Report. It was noted that the PCC #1 material also provides study findings to date. 230801_No phone number found to follow up on Archaeological Report. 230803_Email follow-up on archaeological report.										
D - Indigenous	Chippewas of Kettle and Stony Point First Nation	FN Manger and CAO	Claire Sault	claire.sault@kettlepoint.org		Y	Y			Removed. No longer in consultation position.	220607_Call. Burnside spoke with reception, who noted that Valerie George was no longer there, and position had not been filled. It was recommended that Burnside speak with Claire Sault. A voice mail was then left for C. Sault. 220607_Email. Burnside forwarded NoPCC #1. It is noted that our understanding that Valerie George is no longer working there and the position has not been filled, and that it was recommended that notices be forwarded to yourself at this time. 220614_Email. C. Sault acknowledged recent voicemail and have cc'd the Consultation department to follow up with Burnside.										
D - Indigenous	Chippewas of Kettle and Stony Point First Nation	Consultation Coordinator	Valerie George	valerie.george@kettlepoint.org		Y	Y			Removed. No longer in consultation position.	220607_Call. Burnside spoke with reception. It was noted that Valerie George was no longer there, and position had not been filled. It was recommended that Burnside speak with Claire Sault. A voice mail was then left for C. Sault.										
D - Indigenous	Chippewas of Kettle and Stony Point First Nation	Acting FN Manger	Jodie George					Y													
D - Indigenous	Chippewas of the Thames First Nation Consultation Portal					Y	Y	Y	Y	Removed. June 27/23 email requesting to be removed from the project mailing list.											
D - Indigenous	Chippewas of the Thames First Nation	Chief	Jacqueline French	jfrench@cottfn.com	Y	Y				Removed. June 27/23 email requesting to be removed from the project mailing list.											

PROJECT NAME	Oxford Rd 19	BEGIN DATE	Mar-22	VERSION NUMBER	1								
CLIENT	County of Oxford	END DATE	Jul-23	DATE PREPARED	June 28/23								
POINT OF CONTACT		DURATION		AUTHOR	TR								
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CATEGORY	STAKEHOLDER	TITLE/ROLE	CONTACT		Upload Info to Consultation Database	Received Notice of Commencement	Received Notice of PCC#1	Received Notice of PCC#2	Received Archaeological Assessment for Review	Removed From/Added to Mailing List	Phone Call Log/Email Notes		
			Name	Email									
D - Indigenous	Chippewas of the Thames First Nation	Consultation Coordinator	Fallon Burch	consultation@cottfn.com	Y	Y	Y			Removed. June 27/23 email requesting to be removed from the project mailing list.	220329_Email+Letter. COTTFN confirmed receipt of information through Nations Connects on March 23, 2022. It was noted that project screening identified no concerns with project or information presented, at this time; and requested to be kept informed through Nations Connect, if there are any substantive changes. COTTFN also requested the project engage First Nations in closer proximity (e.g., Six Nations of the Grand River, Mississaugas of the Credit First Nation). Attached as per Appendix 'D' of the Windamagawin, an invoice was sent. 220408_Email (Burnside)+Letter (County). The County, in letter dated April 8, 2022 in response to communities letter of March 29, 2022 noted appreciation for communities consultation response, and acknowledge the communities screening has identified no concerns with respect to this project or information that has been presented at this time; and noted that Six Nations of the Grand River and MCFN, have been engaged. County noted a commitment to open flow of information and to ensuring that there are meaningful opportunities for the Chippewas of the Thames First Nation to provide input to project. Stage 1 Archaeological Assessment will be completed and be part of the ESR, upon request the Stage 1 AA can be made available. 220422_NationsConnect. Fallon Burch responded, based on internal screening community does not see much of an impact to treaty and traditional aboriginal rights. However, if there are any substantive changes to the project proposal. 220602_Portal. Burnside uploaded PCC #1 to portal. 220610_NationsConnect Fallon Burch responded, noting appreciation of notice, unfortunately, a representative was unable to attend. F. Burch requested whether information is available electronically or on website? 220621_NationsConnect. Burnside responded that the Stage 1 Archaeological Assessment will be completed and be part of the ESR, and it has been noted the First Nations has requested an electronic copy. 230627_Fallon Birch indicated that COTTFN could be removed from the mailing list and further engagement is required.		
D - Indigenous	Chippewas of the Thames First Nation	Consultation Coordinator	Rochelle Smith (interim)	rsmith@cottfn.com	Y	Y	Y	Y		Removed. June 27/23 email requesting to be removed from the project mailing list.	220323_Email. An email from NationsConnects was received confirming project had been loaded.		
D - Indigenous	Caldwell First Nation	Consultation Coordinator	Michelle McCormack	consultation.coordinator@caldwellfirstnation.ca; eco@caldwellfirstnation.ca	Y	Y				Removed. Replaced by Zack Hamm			
D - Indigenous	Caldwell First Nation	Consultation Coordinator	Zack Hamm	consultation.coordinator@caldwellfirstnation.ca	Y	Y	Y	Y		Removed. Email failed. June 9/23	220321_Email. Z. Hamm responded to NOCm, noting that the Environment and Consultation department will be primary contact, with Zack Hamm/ Michelle McCormack being first point of contact for correspondence (prior to the involvement of Chief & Council). Z. Hamm requested that the CFN protocol be used as the official engagement method (at https://consultwithcaldwell.ca/). Fill in the CCT report to begin the consultation process and upload all relevant correspondence, files and information to system. 220322_Email. Burnside responded on behalf of County with appreciation for response to NOCm. Noting that the Project Team will correspond with Zack Hamm/ Michelle McCormack; and will review the CFN protocol and use the official engagement method, at https://consultwithcaldwell.ca/, to upload all relevant correspondence, files and information to the system and fill out CCT report, to begin consultation. 220324_Email. An email from (CFN Consultation Coordinator noreply@consultwithcaldwell.ca) was sent to Burnside noting that the initial submission had been received, and that before the project is ready for review, additional information marked as "unsure" must be completed. (Additional documents, Habitat or flight paths, Endangered species permit, Landscape agreement, Archaeological assessment). It was noted that Michelle McCormack, Consultation Coordinator and Zack Hamm, Consultation Coordinator, would be the contacts for the project. 220602_Email. Confirming receipt. 221117_Email. An email from (CFN Consultation Coordinator noreply@consultwithcaldwell.ca) was sent to Burnside noting receipt of the initial submission. It was noted that some additional questions remain marked as "unsure" (Habitat or flight paths, Endangered species permit, Landscape agreement, Archaeological assessment) and must be completed when available. 230811_Email. Zack Hamm agreed to review the Archaeological Assessment and future docs. Requested that the proponent sign an agreement to cover participation expenses. 230906_Letter from County of Oxford in response to funding request. Funding will not be provided but the County will continue to engage and consult. 230921_Email. Zack will relay the response to Chief and Council.		
D - Indigenous	Caldwell First Nation Consultation Portal					Y	Y	Y	Y	Uploaded to portal + email June 9/23			
D - Indigenous	Caldwell First Nation	Environmental & Consultation Coordinator	Brianna Sands	ecc@caldwellfirstnation.ca	Y	Y	Y	Y	Y	Uploaded to portal + email June 9/23			
D - Indigenous	Caldwell First Nation	Chief	Mary Duckworth	chief@caldwellfirstnation.ca;		Y				Removed. Email failed.	220317_Email. chief@caldwellfirstnation.ca (email) failed		

PROJECT NAME	Oxford Rd 19	BEGIN DATE	Mar-22	VERSION NUMBER	1															
CLIENT	County of Oxford	END DATE	Jul-23	DATE PREPARED	June 28/23															
POINT OF CONTACT		DURATION		AUTHOR	TR															
PROJECT DESCRIPTION																				
CATEGORY	STAKEHOLDER	TITLE/ROLE	CONTACT		Upload Info to Consultation Database	Received Notice of Commencement	Received Notice of PCC#1	Received Notice of PCC#2	Received Archaeological Assessment for Review	Removed From/Added to Mailing List	Phone Call Log/Email Notes									
			Name	Email																
D - Indigenous	Oneida Nation of the Thames ONYOTA'A:KA	Environmental Consultation Coordinator	Brandon-Doxatator-Janelle Cornelius	environment@oneida.on.ca		Y	Y	Y	Y	Sent June 9/23	220317_Email. Brandon.doxatator@oneida.on.ca email failed; however went through other general email. 220607_Call. Spoke with Sandra Doxatator, Executive Assistant, who noted that B. Doxatator does not work at the number dialed. It was noted that a message will be left to call Burnside back. 221205_call. Follow up call on the Notice of PCC2 by Avid B. Left a voice msg on general mail box 230801 - left voicemail requesting a call back. 230802 - Janelle has replaced Brandon as per conversation w Janelle, she has not received a copy and has requested another be sent to environment@oneida.on.ca 230803 - email resent to environment@oneida.on.ca 230808_phone call away at a conference until Mon Aug 14 - 230815_Secund follow up email sent									
D - Indigenous	Munsee-Delaware Nation		Stacey Phillips	stacey.phillips@munsee.ca				Y			Removed. 221117_Email. Stacey Phillips email failed. Added. 220607_Call. Spoke with staff person noted that notices should be sent to Stacey Phillips, Consultation Coordinator.	220607_Email. Burnside noted that through a recent phone call with a staff member at the community, it was recommended that the notices be forwarded to Stacey Phillips. Burnside noted that a new Chief was elected in March, however, have been unable to find any information on the website, and requested to be forward the information, and confirm receipt of the notice. 221117_Email. Stacey Phillips email failed, however notice was mailed to Chief. 221205_Call. Follow up call on the Notice of PCC2 by Avid B. Left a msg for the reception to pass to Stacey Phillips.								
D - Indigenous	Munsee-Delaware Nation	Chief				Y	Y	Y												
D - Indigenous	Munsee-Delaware Nation			reception@munsee.ca					Y	Sent June 9/23	Added. June 2023. All other contact emails failed.	230801_Phone call to confirm receipt of Archaeological Report- no answer voicemail full. 230802_Phone call. Reception could not find record of email, sent another copy by email to reception@munsee.ca. 230808_Follow up phone call and email sent to reception@munsee.ca. Follow-up with phone call- reception confirmed they had received Stage 1 AA 230811_email from Munsee Delaware confirming receipt of stage 1 AA and no comments at this time.								
D - Indigenous	Munsee-Delaware Nation	Chief	Mark Peters	chief.peters@munsee.ca		Y					Removed. No longer Chief.									
D - Indigenous	Delaware Nation	Chief	Denise Stonefish	denise.stonefish@delawarenation.on.ca		Y	Y	Y			Removed. Far from project location. Will not be commenting.	220317_Email. gcpeters@mnsi.net (email) failed 220419_Note. Burnside sent message through website page confirming contact. 220519_Email. gcpeters@mnsi.net (email) failed 220602_Update. Email revised 220602_Email. Burnside sent PCC #1 to new email address of Chief. 220602_Call. Burnside spoke with Reception, who confirmed new email address for Chief. 220607_Call. Spoke with reception, who noted the Chief was on holidays. Reception was not aware whether notice had been received. Burnside asked whether there was another staff person who should receive the notices, and it was noted that the notice should go directly to the Chief. 221122_Email. Denise Stonefish responded to PCC #2 notice that both the Munsee-Delaware Nation and the Delaware Nation are Lenape people, however, we are two separately different First Nations. Munsee-Delaware is located 40 miles upstream from Delaware Nation and Delaware Nation is located in Oxford Township within the Municipality of Chatham-Kent, which a fair distance from Oxford Road 19, therefor Delaware Nation will be commenting on the Oxford Road 19 Corridor Improvement.								
D - Indigenous	Mississaugas of the Credit First Nation		Abby LaForme	abby.laforme@mncfn.ca			Y	Y	Y	Sent June 9/23 Adam reviewed and had no questions or comments.	Added. Replaced Fawn Sault	220419_Call. Burnside spoke with A. LaForme. Burnside requested whether the community wished to have FLR present during field work (aquatic, terrestrial or archaeological). A. LaForme responded that all their FLR are at full capacity at this time, however wished to be involved in the archaeological assessment, and to contact staff. at the time (estimate several months). A. LaForme noted that Fawn Sault's replacement is Abby LaForme and in the future for A. LaForme to be the primary contact with A. LaForme to be coed. 221205_Call. Follow up call on the Notice of PCC2 by Avid B. Talked to Abby and resent the notice email. Confirmed the receipt. 221205_Email. Abby LaForme responded to the NoPCC #2 with appreciation for reaching out to MCFN DOCA for Consultation, and noted that at this time MCFN DOCA has no comments or concerns, and to contact MCFN DOCA if any changes or new information arises.								

PROJECT NAME	Oxford Rd 19	BEGIN DATE	Mar-22	VERSION NUMBER	1									
CLIENT	County of Oxford	END DATE	Jul-23	DATE PREPARED	June 28/23									
POINT OF CONTACT		DURATION		AUTHOR	TR									
PROJECT DESCRIPTIVE														
CATEGORY	STAKEHOLDER	TITLE/ROLE	CONTACT		Upload Info to Consultation Database	Received Notice of Commencement	Received Notice of PCC#1	Received Notice of PCC#2	Received Archaeological Assessment for Review	Removed From/Added to Mailing List	Phone Call Log/Email Notes			
			Name	Email										
D - Indigenous	Mississaugas of the Credit First Nation	Department of Consultation & Accommodation	Adam LaForme	Adam.LaForme@mcncf.ca					Y	Y	Adam reviewed report and had no questions or comments.	Added. Replaced Fawn Sault	220407_Letter. A. LaForme emailed letter dated April 7, 2022 noting that DOCA has been notified a project information file (PIF) was submitted to the MHSTCI, regarding Oxford Road 19. A. LaForme requested a summary of the history of project and current state of its associated environmental and archaeological fieldwork; DOCA requires that Field Liaison Representatives participate in all environmental and archaeological fieldwork within the MCFN treaty territory, including Stages 2-4; and that no fieldwork will take place without FLRs participation. Information was requested by 4 p.m. April 21, 2022. 220413_Email (Burnside)+Letter (Region). The County in letter dated April 13, 2022 acknowledged with appreciation A. LaForme's letter dated April 7, 2022 requesting information on a project within the MCFN treaty territory. The letter noted further information and clarification on Study Background. It was noted that a Stage 1 Archaeological Assessment (AA) and Cultural Heritage Resource Assessment is being completed, and the County noted to A. LaForme assurance that it is the County's intention that ensure MCFN will have the ability to provide comments on the draft AA, which will be prior to finalizing, and submitting to MHSTCI. County offered to meet to answer questions or respond to any concerns. 220419_Call. Burnside spoke with A. LaForme. Burnside requested whether the community wished to have FLR present during field work (aquatic, terrestrial or archaeological). A. LaForme responded that all their FLR are at full capacity at this time, however wished to be involved in the archaeological assessment, and to contact staff at the time (estimate several months). A. LaForme noted that Fawn Sault's replacement is Abby LaForme and in the future for Abby LaForme to be the primary contact with Adam LaForme to be coed. 220603_Email. Burnside. Burnside informed the community that environmental field work (terrestrial) was to occur on June 10, 2022, 8:30am-3:30pm, if the community would like to have representatives on-site, it can be coordinated. 230713_Email from Adrian Blake. He has reviewed the Stage 1 Archaeological Assessment and has no comments at this time.	
D - Indigenous	Mississaugas of the Credit First Nation	Consultation Manager	Fawn Sault	Fawn.Sault@mcncf.ca		Y					Removed. No longer in consultation position.			
D - Indigenous	Mississaugas of the Credit First Nation	Archaeological Operations Supervisor Department of Consultation and Accommodation (DOCA)	Megan DeVries	Megan.DeVries@mcncf.ca		Y					Removed. No longer with MCFN.			
D - Indigenous	Six Nations of the Grand River	Land Use Office, Lands and Research	Robbin Vanstone	rvanstone@sixnations.ca		Y	Y				Removed. No longer with Six Nations.	220317_Email. Out of the office until Monday March 21, 2022. 220607_Call. Burnside spoke with reception who indicated that R. Vanstone had just gone into a meeting. Burnside left a voice mail for R. Vanstone regarding confirmation of receipt of notice, interest in the project, comments, concerns, etc.		
D - Indigenous	Six Nations of the Grand River	Lands & Resource Director	Lonny Bomberry	lonnybomberry@sixnations.ca		Y	Y	Y	Y	Sent June 9, follow up June 30.			221205_call. Follow up call on the Notice of PCC2 by Avid B. Left a voice msg.	
D - Indigenous	Six Nations of the Grand River	Archaeology Coordinator	Tanya Hill-Mountour	tanyahill-mountour@sixnations.ca		Y			Y	Sent June 9			230609_Phone call to reception to follow-up on Archaeological Report. Was told she is on site visit call back tomorrow morning (suggested to resend and cc dlaforme@sixnations.ca) 230802_Phone call, was told by reception she is in meetings all day, asked for a better time to call back was told Friday afternoon or to email 230808_Phone call- was not in office told to follow up by email - follow up email sent.	
D - Indigenous	Haudenosaunee Confederacy	Environmental Supervisor	Raechelle (Janice) Williams	janicewilliams@hdi.land		Y	Y	Y	Y	Sent June 9			221205_Call. Follow up call on the NoPCC #2 by Avid B. Left a message with Shannon Hill to pass to Raechelle. 220607_Call. Burnside spoke with Janice Williams. It was noted that projects should have an engagement application submitted. This has not been pushed thus far, however it makes it easier for staff to review. Burnside noted that an email will be sent directly to her in order to reply back with information on how to begin the process. 220607_Email. Burnside sent email with NoPCC #1. As per our telephone conversation today please find attached the NoPCC #1. Burnside appreciates the processes established by the community for engagement, and awaits your instruction regarding the engagement application requested. 221110_Email. Burnside noted in this email that HDI was sent a letter on March 17, 2022 with attached, NOCM, Study Area map and Project Response Form, as a first step to initiate the consultation process. Burnside noted that then on May 19, 2022, in follow-up to the letter HDI was sent the NoPCC #1; which was then followed by receipt confirmation calls. Burnside then addressed HDI's email dated June 7, 2022 which highlighted concerns with the project, and with Oxford County not submitting an application and fee to HDI for project review. It was noted that at this time, Oxford County is not able to submit an application and provide a fee to HDI for this project; however, Oxford County is committed to continued consultation. An offer to meet was extended to HDI to discuss study findings, receive the communities' input on these findings to assist with the decision making, and answer any questions. 221207_Email. Janice Williams, responded to the NoPCC #2 that the notifications sent to HDI do not represent engagement or consent, and noted that HDI and our nation's have significant concerns with proposed project, and that an application and fees was not submitted to HDI for review of the project. It was noted that staff are unable to provide feedback and consider engagement without funds to review and/or comment on this proposed project. HDI requested that the proponent provide a completed application and fee, to analyze the effects, impairment, and interference within our treaty rights. HDI provided a link to the application. 230630_Phone call to follow up on Archaeological Report. 230801_Phone call- reception indicated she is not in the office, call back tomorrow. 230802_Was told not in office to follow up by email. 230808_Phone call, not in office again, followed up with email.	

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CLIENT	County of Oxford	END DATE	Jul-23	DATE PREPARED	June 28/23							
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			Name	Email								
D - Indigenous	Haudenosaunee Confederacy Chiefs Council	Secretary to Haudenosaunee Confederacy	Leroy Hill	communications@hdi.lan d; joeko@sixnationsns.com; info@hdi.lan; 1749resource@gmail.com;		Y	Y	Y	Y Sent June 9		230630_Follow up email to confirm receipt of Archaeological Report.	
D - Indigenous	Haudenosaunee Confederacy Chiefs Council			cced_hdi2@bellnet.ca		Y				Removed. Email failed.	220317_Email_hdi2@bellnet.ca (email) failed	



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H4-B

Caldwell FN

Sylvia Waters

From: CFN Consultation Coordinator <noreply@consultwithcaldwell.ca>
Sent: Thursday, March 24, 2022 5:44 PM
To: Sylvia Waters
Subject: consultwithcaldwell.ca - Project Submission

Michelle McCormack
Consultation Coordinator

Zack Hamm
Consultation Coordinator

Caldwell First Nation
14 Orange Street
Leamington | ON | N8H 1P5

We have received your initial submission

Please note that before the project is ready for review, **you must provide some additional information:**

The remaining information involves questions that were marked as "unsure"

- Additional documents
- Habitat or flight paths
- Endangered species permit
- Landscape agreement
- Archaeological assessment

When you have this information ready, you can return to this website and submit your information.

If you have questions, please contact CFN's Consultation Coordinator: ecc@caldwellfirstnation.ca or [519-322-1766 ext. #1243](tel:519-322-1766).

*Please note that simply sending an email to the Consultation Coordinator or to a Chief and Council member does not constitute consultation.

*Please be advised that an administrative fee will be charged for a meeting with Chief and Council.

Tricia Radburn

From: Tricia Radburn
Sent: Wednesday, September 06, 2023 4:39 PM
To: 'ecd.manager@caldwellfirstnation.ca'; Brianna Sands; 'ecd.assistant@caldwellfirstnation.ca'; 'landguardian@caldwellfirstnation.ca'
Cc: Henry Centen; Noah Brister; David Simpson; Melissa Abercrombie; Reuben Davis; Crystal Ferguson
Subject: FW: Oxford Rd 19 Stage 1 Archaeological Assessment
Attachments: CFN Draft Response Letter - Sept 1 2023 - NB.pdf; PCC #2 - Display Boards.pdf

Zach,

Thank you for your interest in the Oxford Rd. 19 Municipal Class EA. Please see the attached letter from the County of Oxford in response to your email, below.

I also note that the Stage 1 Archaeological Assessment was uploaded to the Caldwell Consultation Tool site on June 9. We will also provide a copy of the draft EA report for your review and will upload it to the site shortly.

Please reach out if you have any questions or comments or would like to schedule a meeting to discuss the project.

Kind Regards,



Tricia Radburn, MCIP, RPP
Senior Environmental Planner

R.J. Burnside & Associates Limited
292 Speedvale Ave. W, Unit 20 Guelph ON
Office: [800-265-9662](tel:800-265-9662) Direct: 226-486-1778 www.rjburnside.com

From: Zack Hamm <ecd.manager@caldwellfirstnation.ca>
Sent: Friday, August 11, 2023 4:02 PM
To: Crystal Ferguson <Crystal.Ferguson@rjburnside.com>
Cc: Michelle McCormack <ecc@caldwellfirstnation.ca>; Michael McMaster <ecd.assistant@caldwellfirstnation.ca>; Jenna Maidment <landguardian@caldwellfirstnation.ca>
Subject: Re: Oxford Rd 19 Stage 1 Archeological Assessment

Good afternoon Crystal,

We will be interested in reviewing your desktop survey, as well as the EA once it is drafted. Please see attached agreement templates. Fill them out according to your project specifications - I recommend including language for the overall project to avoid future paperwork regarding the EA or possible fieldwork in which CFN would like to participate.

Once we sign onto the agreements, we will commence review on the community's behalf and provide comments if necessary. For reference, an AA takes roughly 2 staff hours per 100 pages, while an EA typically takes 2-3 staff hours for a similar number; this is variable based on the report of course, but these estimates are for your budgeting purposes.

If you haven't yet, I also encourage you to upload your project and engagement requests directly to consultwithcaldwell.ca, which helps us manage volume and is our preferred method of initial contact.

Best,

Zack Hamm

Environment and Consultation Department Manager

Environment and Consultation Department (ECD)



Caldwell First Nation

14 Orange Street

Leamington | ON | N8H 1P5

Phone: 226-936-2940

ecd.manager@caldwellfirstnation.ca

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On Tue, Aug 8, 2023 at 9:26 AM Michelle McCormack <ecc@caldwellfirstnation.ca> wrote:

Information sharing, I am not sure if you have received this.

----- Forwarded message -----

From: **Crystal Ferguson** <Crystal.Ferguson@rjburnside.com>

Date: Thu, Aug 3, 2023 at 2:59 PM

Subject: Oxford Rd 19 Stage 1 Archeological Assessment

To: ecc@caldwellfirstnation.ca <ecc@caldwellfirstnation.ca>

Good afternoon,

I am writing to follow up with the new information that was provided to you regarding the County of Oxford's **County Rd. 19 Corridor Improvement Project**. A Notice of Commencement was issued in March of last year and since the Notice of Commencement was issued, various technical studies have been undertaken. **A draft Stage 1 Archaeological Assessment** is available for your review and was previously provided to you at the link below:

 [Stage 1 Archaeological Assessment](#)

A draft copy of the Municipal Class EA report is expected to be available later this spring/summer and will also be forwarded for your review and comment.

Please reach out if you have any questions or comments about the archaeological study or any other aspect of the project.

Please respond in kind to confirm receipt of the notice of Stage 1 Archeological Assessment.

Kind Regards,



Crystal Ferguson

Environmental Coordinator

R.J. Burnside & Associates

128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6

Office: 800-265-9662 Direct Line: +1 705-797-4352

www.rjburnside.com



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Thank you.

--
Miigwech,

Michelle McCormack

Consultation Coordinator

Caldwell First Nation

14 Orange St. Leamington, ON N8H 1P5

www.caldwellfirstnation.ca

Office: (519) 322-1766

Mobile: (519) 329-1946

ecc@caldwellfirstnation.ca

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September 1, 2023

Caldwell First Nation
Attn: Zack Hamm, Environment and Consultation Department Manager

**RE: Oxford Road 19 Corridor Improvements
Municipal Class Environmental Assessment Study – Schedule C
Response to CFN Email dated August 11, 2023**

Dear Mr. Hamm:

We are following up with Caldwell First Nation (CFN) regarding your email dated August 11, 2023 which highlighted concerns with respect to the Oxford Road 19 Corridor Improvements project.

On behalf of Oxford County, I would like to thank you for your interest in this project. Please be assured that the Project Team understands CFN's concerns on the potential impacts of this project. Oxford County has reviewed two (2) agreement templates (Fieldwork Participation Agreement & Technical Review Agreement) that were provided to our Engineering Consultant (RJ Burnside) via email correspondence dated August. At this time, we would like to inform CFN that the County will not be engaging in CFN's agreement process. However, the County would like to offer CFN technical resources on any areas of their concern, to assist with a thorough understanding of the project and any potential impacts of concern.

The County deeply believes in transparent and open flow of communication and information. In line with that, we would be happy to share the findings of our research on any of the study areas that CFN may be interested in; and as noted above, provide technical resources to ensure that these findings address your concerns, particularly in the areas of natural heritage, along with the mitigation measures identified in the stage 1 archaeological assessment. We are also happy to provide in depth review of the road corridor preliminary designs.

The County is also aiming to share the draft supporting technical reports and / or memorandums for natural heritage and stage 1 archaeological assessment (if requested), along with a high-level summary of the study findings, potential impacts, and proposed mitigation measures. In the meantime, we respectfully invite you to review the attached copy of Public Consultation Centre #2 (PCC#2) boards presented at the PCC#2 event held on December 6, 2022.

The Project Team would be pleased to meet with CFN at any time during the EA process to discuss the findings of the study to date, receive the communities' input on these findings and / or discuss how and when the County's technical resources can assist CFN with a full understanding of the findings of the project to date.

Thank you again for your interest in this project. Should you have any questions or require additional information, please contact the undersigned by phone 519-539-9800, ext. 3107, fax 519-421-4711 or email nbrister@oxfordcounty.ca

Sincerely,



Noah Brister, P.Eng.
Project Engineer
Oxford County Public Works

Encl. Public Consultation Centre #2 Boards

cc: Tricia Radburn, Senior Environmental Planner, RJ Burnside & Associates Ltd.
Tricia.Radburn@rjburnside.com
Henry Centen, Senior Transportation Engineer, RJ Burnside & Associates Ltd.
Henry.Centen@rjburnside.com
David Simpson, Director of Public Works, Oxford County Public Works
dsimpson@oxfordcounty.ca
Melissa Abercrombie, Manager of Engineering Services, Oxford County Public Works
mabercrombie@oxfordcounty.ca
Reuben Davis, Supervisor of Engineering Services, Oxford County Public Works
rdavis@oxfordcounty.ca

Tricia Radburn

From: Zack Hamm <ecd.manager@caldwellfirstnation.ca>
Sent: Thursday, September 21, 2023 9:38 AM
To: Tricia Radburn
Cc: Michael McMaster; Michelle McCormack; Henry Centen; Noah Brister; David Simpson; Melissa Abercrombie; Councillor Ian Duckworth; Allen Deleary
Subject: Re: FW: Oxford Rd 19 Stage 1 Archaeological Assessment
Attachments: ~WRD0001.jpg

Thanks for the update and relaying the correspondence, Tricia, and I understand that this isn't your decision. I will escalate this to my superior and the Chief and Council. As there is no reasoning behind failing to supply an Agreement and funding for our department, this is now clearly beyond technical staff. I will also note that this constitutes a failure to Accommodate Caldwell First Nation via the standards present by the Duty to Consult.

Best,

Zack Hamm

Environment and Consultation Department Manager

Environment and Consultation Department (ECD)



Caldwell First Nation

14 Orange Street

Leamington | ON | N8H 1P5

Phone: 226-936-2940

ecd.manager@caldwellfirstnation.ca

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On Fri, Sep 15, 2023 at 3:21 PM Tricia Radburn <Tricia.Radburn@rjburnside.com> wrote:

Zach,

The Stage 1 AA can be found at the link below. The EA is still being finalized but we will send a copy when it is ready for review.

 [Stage 1 Archaeological Assessment](#)

Unfortunately, the County will not be signing the agreements. I have re-attached the letter from the County that explains their typical process.

Please reach out if you would like to discuss this further. We are happy to have a meeting to review and discuss the archaeological report or the EA, once finalized.

Kind regards,



Tricia Radburn, MCIP, RPP
Senior Environmental Planner

R.J. Burnside & Associates Limited
292 Speedvale Ave. W, Unit 20 Guelph ON
Office: [800-265-9662](tel:800-265-9662) Direct: 226-486-1778 www.rjburnside.com

From: Zack Hamm <ecd.manager@caldwellfirstnation.ca>
Sent: Friday, September 15, 2023 1:31 PM
To: Tricia Radburn <Tricia.Radburn@rjburnside.com>

Cc: Michael McMaster <ecd.assistant@caldwellfirstnation.ca>; Michelle McCormack <ecc@caldwellfirstnation.ca>

Subject: Re: FW: Oxford Rd 19 Stage 1 Archaeological Assessment

Good afternoon Tricia,

Thanks for checking in. I'll bump our review up our queue in recognition that we've had it for some time.

Could you please provide a new cloud link for both the Stage 1 AA and the EA? Additionally, please confirm that you have signed off on both the agreements sent earlier in the summer.

Best,

Zack Hamm

Environment and Consultation Department Manager

Environment and Consultation Department (ECD)



Caldwell First Nation

14 Orange Street

Leamington | ON | N8H 1P5

Phone: 226-936-2940

ecd.manager@caldwellfirstnation.ca

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On Wed, Sep 6, 2023 at 4:39 PM Tricia Radburn <Tricia.Radburn@rjburnside.com> wrote:

Zach,

Thank you for your interest in the Oxford Rd. 19 Municipal Class EA. Please see the attached letter from the County of Oxford in response to your email, below.

I also note that the Stage 1 Archaeological Assessment was uploaded to the Caldwell Consultation Tool site on June 9. We will also provide a copy of the draft EA report for your review and will upload it to the site shortly.

Please reach out if you have any questions or comments or would like to schedule a meeting to discuss the project.

Kind Regards,

Error! Filename not specified.
Tricia Radburn, MCIP, RPP
Senior Environmental Planner

R.J. Burnside & Associates Limited
292 Speedvale Ave. W, Unit 20 Guelph ON
Office: [800-265-9662](tel:800-265-9662) Direct: 226-486-1778 www.rjburnside.com

From: Zack Hamm <ecd.manager@caldwellfirstnation.ca>
Sent: Friday, August 11, 2023 4:02 PM
To: Crystal Ferguson <Crystal.Ferguson@rjburnside.com>
Cc: Michelle McCormack <ecc@caldwellfirstnation.ca>; Michael McMaster <ecd.assistant@caldwellfirstnation.ca>; Jenna Maidment <landguardian@caldwellfirstnation.ca>
Subject: Re: Oxford Rd 19 Stage 1 Archeological Assessment

Good afternoon Crystal,

We will be interested in reviewing your desktop survey, as well as the EA once it is drafted. Please see attached agreement templates. Fill them out according to your project specifications - I recommend including language for the overall project to avoid future paperwork regarding the EA or possible fieldwork in which CFN would like to participate.

Once we sign onto the agreements, we will commence review on the community's behalf and provide comments if necessary. For reference, an AA takes roughly 2 staff hours per 100 pages, while an EA typically takes 2-3 staff hours for a similar number; this is variable based on the report of course, but these estimates are for your budgeting purposes.

If you haven't yet, I also encourage you to upload your project and engagement requests directly to consultwithcaldwell.ca, which helps us manage volume and is our preferred method of initial contact.

Best,

Zack Hamm

Environment and Consultation Department Manager

Environment and Consultation Department (ECD)

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14 Orange Street

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On Tue, Aug 8, 2023 at 9:26 AM Michelle McCormack <ecc@caldwellfirstnation.ca> wrote:

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From: **Crystal Ferguson** <Crystal.Ferguson@rjburnside.com>

Date: Thu, Aug 3, 2023 at 2:59 PM

Subject: Oxford Rd 19 Stage 1 Archeological Assessment

To: ecc@caldwellfirstnation.ca <ecc@caldwellfirstnation.ca>

Good afternoon,

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A draft copy of the Municipal Class EA report is expected to be available later this spring/summer and will also be forwarded for your review and comment.

Please reach out if you have any questions or comments about the archaeological study or any other aspect of the project.

Please respond in kind to confirm receipt of the notice of Stage 1 Archeological Assessment.

Kind Regards,

Error! Filename not specified.

Crystal Ferguson

Environmental Coordinator

R.J. Burnside & Associates

128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6

Office: 800-265-9662 Direct Line: +1 705-797-4352

www.rjburnside.com

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Thank you.

--

Miigwech,

Michelle McCormack

Consultation Coordinator

Caldwell First Nation

14 Orange St. Leamington, ON N8H 1P5

www.caldwellfirstnation.ca

Office: (519) 322-1766

Mobile: (519) 329-1946

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BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H4-C

Chippewas of the Thames FN

Sylvia Waters

From: Chippewas of the Thames First Nation <no-reply-cottfn@knowledgekeeper.ca>
Sent: Tuesday, March 29, 2022 4:13 PM
To: fburch@cottfn.com; jmills@cottfn.com; rsmith@cottfn.com; Sylvia Waters
Subject: Decision regarding consultation: 300053425 - Oxford Road 19 Corridor Improvement
Attachments: consultation-response-18987-300053425-20220329-1612.pdf; inv-0263-oxford-road-19-corridor-improvement-rj-burnside.pdf; Wiindmaagewin-CONSULTATION-PROTOCOL-website.pdf

Please see attached PDF.



CHIPPEWAS OF THE THAMES FIRST NATION

Project Name:

Oxford Road 19 Corridor Improvement

FN Consultation ID:

300053425

Consulting Org Contact:

Sylvia Waters

Consulting Organization:

[RJ Burnside & Associates Ltd.](#)

Date Received:

Wednesday, March 23, 2022

March 29, 2022

Dear: Sylvia

We have received information concerning Oxford Road 19 Corridor Improvement, submitted March 23, 2022.

In our screening of your project we have identified no concerns with your project or the information that you have presented to us at this time. We ask that if there are any changes to your project that are of a substantive nature that you keep us informed through NationsConnect.

We ask that you please engage First Nations in closer proximity to your project. e.g. Six Nations of the Grand River, Mississaugas of the Credit First Nation.

We look forward to continuing this open line of communication. To implement meaningful consultation, Chippewas of the Thames First Nation has developed its own protocol - a document and a process that will guide positive working relationships. As per Appendix 'D' of the Wiindaamaagewin, please find attached invoice 0263.

If you have any questions, please feel free to contact me.

Sincerely,

Original Signed

Fallon Burch

Consultation Coordinator

Treaties, Lands & Environment Department

Chippewas of the Thames First Nation

fburch@cottfn.com

Deshkan Ziibiing/Chippewas of the Thames First Nation

Wiindmaagewin

CONSULTATION PROTOCOL

Final

26 November 2016



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- A. Deshkan Ziibiing/Chippewas of the Thames Consultation Map
- B. Southwestern Ontario Treaty Map
- C. Consultation Flow Chart
- D. Consultation Service Fees

1. Introduction and purpose

The watersheds of southwestern Ontario have been the home of Anishinaabe people for millennia. Widespread archaeological evidence of the “Western Basin Late Woodland Tradition” confirms our traditional oral history teachers’ accounts of this lengthy Anishinaabe dwelling in our territory of Waawayaatanong, or “Round Lake.” This region is known as the third stopping place of the Water Drum on its sacred journey to Madeline Island, centuries before the era of colonization. We have continued to dwell here despite the disruptions stemming from conflicts with other Anishinaabe nations also dwelling near the Great Lakes, from the wars between various settler powers between 1757 and 1815, and from the imposition of Britain’s, then the United States’, and Canada’s colonial rule.

Deshkan Ziibiing edbendaagzijig, “those that belong to Antler River” (The Chippewas of the Thames First Nation) comprise one of the traditional Anishinaabe nations governing the territory of Waawayaatanong, collectively known now as the Waawayaatanong Anishnaabeg Southwest Treaty Council. As a governing body, Deshkan Ziibiing has lengthy experience in developing relations with other communities interested in the lands and waters of Waawayaatanong, as early French explorers recognized, and as our historic treaty-making with Britain demonstrates.

The purpose of this protocol is to ensure that our relationships with other communities develop in the future in ways that are fully respectful of the breadth of Deshkan Ziibiing’s responsibilities to these watersheds, and ways that are protective of the full range of our rights. This protocol shall serve to guide governments and third parties interested in pursuing healthy and mutually beneficial relationships with Deshkan Ziibiing.

2. Statement of reserved rights

The rights that Deshkan Ziibiing exercises in relation to our ancestral lands, treaty lands, reserve lands, and Addition to Reserve lands, are inherent, grounded most basically in the Creator’s gift of lands, waters, and way of life to ndodeminaanig, “our clans.” These rights are embodied in our historical and ongoing occupation of our territory, and in our practice of self-determination as a people. Our rights as a self-determining people are also recognized within, although they are certainly not created by, the formation of several treaties, the terms of constitutional documents, and international conventions, including Article three of the Jay Treaty (1794). Our historic treaty partner, Britain, recognized these rights, as seen within the joint context of the Royal Proclamation of 1763 and the Treaty of Niagara, 1764; and within the subsequent treaties formed between 1790 and 1827. Our traditional understanding of these treaties with Britain indicates that they in no way eliminate our own rightful control of, and enduring ability to benefit from, the lands and waters within our territory. Section 35(1) of *Canada’s Constitution Act, 1982*, also clearly recognizes these rights, as do the expressions of international customary law elaborated within the *United Nations Declaration on the Rights of Indigenous Peoples (2007)*. This present protocol neither contains nor implies anything that subtracts or derogates from the fullest understanding of the range of rights found within those legal instruments, or within our traditional and customary law.

3. Territory

Traditional Anishinaabe territory in southwestern Ontario north of the Thames River includes the 2.78 million acres marked on the treaty maps concerning the Longwoods (1822) and Huron (1827) tracts. In addition, south of the Thames River, traditional territory also includes the lands addressed in the McKee Treaty (1790), the London Township Treaty (1796), and the Sombra Township Treaty (1796). Deshkan Zibiing is party with other Anishinaabe nations to several of these treaties, but is the sole Anishinaabe party to the Longwoods Treaty.

As recognized in these treaties, the ancestral lands of Deshkan Zibiing thus include all the lands and waters between Lake Huron to the north and Lake Erie to the south, and stretching eastward from the eastern banks of the St. Clair and Detroit rivers to the Mississaugas of New Credit 1792 treaty lands, a line running northwards from Point Bruce on the Erie shore, to Point Clark on the Huron shore (see Appendix A for map). In addition, Deshkan Zibiing territory extended into what are now the American states of Michigan and Ohio. Historically, we managed portions of our territory in common with other Anishinaabe nations, and at times in partnership with the Haudenosaunee. Nevertheless, the lands bordering the northern bank of the Thames River have been solely in the stewardship and possession of Deshkan Zibiing since before the treaty era.

Upper Canada's settlement and development from the early nineteenth century certainly transformed much of this land from its pre-treaty state. Nevertheless, we who are Deshkan Zibiing edbendaagzijig continue to hold our lands, and to assert over the full extent of our treaty lands and traditional territory our historic commitment to the protection of the watersheds of the Thames River, Bear Creek, and the Au Sable River, and to the Erie and Huron lakeshores. For the purposes of this protocol, we regard all of our ancestral lands as part of our consultation territory. As well, our understanding from our elders, an understanding we share with many other Anishinaabe nations, is that our treaties did not "surrender" our lands, despite what Britain and Canada have presumed. As part of our ongoing commitment to these watersheds, the citizens of Deshkan Zibiing are currently engaged in aboriginal title research concerning the bed of the Thames River.

4. Community profile

As a community, Deshkan Zibiing has always welcomed and incorporated people from other nations. Our families have always shared a common world around the Great Lakes with the families of the Pottawatomis and the Odawas. Delawares and Oneidas were welcomed here between 1791 and 1840, and allowed to create their own communities on land adjacent to and within our homeland. At various times, other Haudenosaunee, Shawnees, Huron/Wendats, as well as Anglo or French traders and settlers marrying our people, have all been incorporated into our society. Today, our population is approximately 2,800 people, with 1,000 residing here at Deshkan Zibiing.

Institutions operating within our homeland territory today include a school system with an elected board; a midewigaan (mide lodge), a sundance lodge; the Southwest Ontario Aboriginal Health Access Centre and the Nimkee Nupigawagan Healing Centre; several businesses,

including retail, service and engineering consulting firms; the Chippewa Development Corporation, the Big Bear Creek Trust, and the Thunderbird Trust; as well as police and fire services, government offices, and two churches.

5. Historical Relationships

Deshkan Ziiibiing has long engaged with other nations, in both war and peace. Prior to European movement into the Great Lakes region, conflict with various Iroquoian-speaking peoples in southern Ontario was also balanced with periods of peace. The creation of the fur trade, however, led to increased warfare with the Haudenosaunee – the Iroquois Confederacy, and to various protective alliances with the Hurons/Wendats, and with other Anishinaabe peoples. Deshkan Ziiibiing allied with France in its war with Britain in the mid 18th century. Our warriors also fought with Pontiac to protect lands west of the Alleghenies after France withdrew from the Americas. Deshkan Ziiibiing later allied with Britain in war against the revolutionary American colonies, and then again in the War of 1812 against American expansion into the Great Lakes region. Our ancestors fought during that war with Tecumseh, the great Shawnee leader and advocate for Anishinaabe independence, in order to protect our lands. His descendants are among our citizens today. Following Britain’s retreat in 1815, our warriors were the sole defenders of Upper Canada from American incursions. As allies of Britain during the 20th century, our young men also fought and died in two world wars.

Deshkan Ziiibiing’s relationships with other nations have generally been pursued through the creation of treaty partnerships. The oldest of our partnerships, more recently called the Three Fires Confederacy, is one grounded in shared language and joint protection with the Odawa and Potawatomi nations. Gdoonaaganinaan “Our Dish” – formed with the Haudenosaunee; and the Treaty of Montreal – formed with the French and over thirty Anishinaabe nations; ended decades of war and competition over the fur trade, in 1701. These partnerships establishing the foundations of peace were all grounded in mutual respect, and a shared understanding that legal alliances between distinct political communities are created and maintained through ceremony, through appeals to the Creator to attest the sincerity of promises, and through an exchange of wampum belts. Our treaty alliances were most basically a mutual extension of our kinship loyalties and responsibilities, as our gimaag indicated by attaching their doodem symbols to the Longwoods Treaty, and to many others. This extension of kinship to create social and political bonds remains as significant for us today as it was for our ancestors during the tumultuous years of the 17th and 18th centuries.

The great Treaty of Niagara (1764) emerged from this long practice, and is the template for all subsequent treaties between Britain and the Chippewas of the Thames. Its Two Row Wampum, a belt the Haudenosaunee first used with the Dutch in 1613, provides a fuller account of the sort of relationship of equals established between Britain and Chippewas of the Thames than appears in the written texts of the Royal Proclamation (1763), the post-War of 1812 treaties, or within any legislation embodying Canada’s unilateral presumption of colonial rule over us.

Deshkan Ziiibiing made all of its treaties with Britain, and none with Canada, as our elders have emphasized. The legal certainty that Canada has derived from those treaties is weaker than it might be had it undertaken to achieve a common understanding with our people. Apart from

our gimaag having inscribed their doodem symbols on parchment and paper, the treaty texts, and most other documents pertaining to the formation of our treaties, are only available in English. The documentary record contains very little to indicate the fullness of our ancestors' understandings of the treaty process, or of the shameful nature of the post-War of 1812 relationship with Britain, and subsequently with Canada.

Britain's withdrawal from the Ohio valley, and gradual abandonment of its Anishinaabe allies to the colonial administration of Canada, has had a lasting effect on the people of Deshkan Ziiibiing. Colonial rule presumed the 'surrender' of well over 90% of our traditional territory. Efforts to 'civilize' our people, through Canada's unilateral imposition of the Indian Act (1876), confinement through the imposition of the 'Indian Pass' system, harsh policing of our harvesting and hunting practices, and not least, the creation of a residential school on our homeland (Mt. Elgin Industrial Institute, 1851-1946), certainly took a toll on our well-being, and constrained our control and use of our lands and waters. However, throughout the hundred and fifty years of Canada's assertions of sovereignty over our lands and nation, our people have remained insistent on our continued inherent rights. In 2013, Deshkan Ziiibiing resolved a Specific Land Claim dispute with Canada, over its taking of the Big Bear Creek lands in the 1830s. In 2015 the Supreme Court of Canada agreed to consider our objections to the failure of consultation surrounding Enbridge's reversal of flow for Line 9. As the city of London expands, and as the province undertakes a variety of energy development projects, Deshkan Ziiibiing remains intent on protecting our traditional territory. Our vision for the engagement formalized in this protocol remains that of Tecumseh, our treaty chiefs, and the Two Row Wampum. First, we are committed to self-determination regarding the preservation and restoration of our Anishinaabe jurisdiction and heritage. Second, we are committed to the formation of fair partnerships focused on the wise and respectful use of our traditional lands and waters.

6. Principles of intersocietal governance and communication

Our engagement with other communities stems from our recognition of several principles, which derive from our Creator's gifts to us of life and land, and from our Creator having placed us within a world full of relationships with others. Our responsibility to maintain these relationships, in accord with principles derived from our creation story, is central to our continued wellbeing as a people. These principles animated our ancestors in their treaty partnerships historically, and they remain alive today in our dealings with federal, provincial, and municipal bodies. They indicate our fundamental orientation towards all matters of discussion and consultation concerning our rights and responsibilities. They function in addition to, although not in conflict with, the well known principles that Canada's courts have constructed from the common law regarding "the duty to consult and where necessary accommodate" (Haida Nation 2004). The courts have had a difficult time explaining how common law principles serve the task of reconciliation between settlers and Anishinaabe peoples, for they leave parties at odds, and mandate an inherently adversarial process. Our principles, however, grounded in our creation story, do provide a basis for fruitful and healthy intersocietal development, governance, and communication.

a. Principles of governance

(1) Gdinawendimi: “We are all related.” A basic truth of our creation story is that we are related to everything that shares the world with us. Our original Anishinaabe doodem ancestors: Ajijaak “Crane,” Waabizhesh “Marten,” Bneshiinh “Bird,” Wawashkesh “Deer,” Maang “Loon,” Giigoonh “Fish,” Mko “Bear;” all demonstrate that we humans are related to, that is, are family with, beings who are other than human. That our ancestors shaped our treaties with Britain by inscribing many of those same doodemag on treaty texts indicates that they extended the web of kinship relations to include settlers. We expect that all consultation and discussion with governments and third parties will focus on how the proposed project will foster this relatedness.

(2) Mno-bmaadiziwin: “The good life.” We understand that the Creator placed us within our world’s web of spiritual and bio-physical relationships in order for life to flourish, for all to enjoy the world. Life flourishes when we base our relationships on the gifts of the Seven Grandfathers: Nbwaakaawin “wisdom,” Zaagidiwin “love,” chi “respect,” Aakde’ewin – or Zoongide’ewin “bravery,” Gwakwaadiziwin “honesty,” Dbaadendiziwin “humility,” Debwewin “truth.” We expect that all proposals from and discussions with governments and third parties will demonstrate how the proposed project enhances the good life for all our relations.

(3) Naaknigewin: “Law”. This measure for our decisions and determinations is the gift of the Creator. We expect that all consultation and discussion with governments and third parties will aim to respect and embody law as the measure for our decisions provided by the Creator.

(4) Anishinaabe dbendizawin: “Anishinaabe independence,” or “self-determination.” Some of our elders overcame their repressive years spent within the local residential school, and were able to play crucial roles in entrenching the recognition of our rights into sec. 35(1) of Canada’s Constitution Act, 1982. Their personal struggles have taught us that we were created to live as an independent people, and are therefore able to ally with, but not to become subject to, other independent peoples. Many British treaty negotiators failed to understand this. Canada’s unilateral imposition of regulations on our people, and its presumptuous administration of our lands, stems from its own consistent failure to understand this. Nevertheless, we have seen in some settler leaders, such as Sir William Johnson and his work at Niagara in 1764, the enduring possibility that our peoples might finally create a relationship of equality. William Johnson’s Two Row Wampum embodies this alliance of equals, each party free to follow its own way without interference, but each also attentive to the wellbeing of the other. We expect that all proposals from governments will respect this most basic tenet of the Two Row Wampum.

b. Principles of communication

(1) Zgaswediwin: “To smoke together.” This word combines two sorts of acts into one. When Anishinaabeg met in council, they began with the ceremony of smoking. In our stories, Nanabush provided our ancestors with the pipe of peace in order to help us foster the path of goodwill and reconciliation towards earth, plants, animals, and our fellow humans. Asemaa, “tobacco,” carries our thoughts and prayers to the Creator, and demonstrates our desire to speak the truth, and to build relationships that reflect gratitude in our dependence on the natural order, law, or policing naaknigewin. We expect that all consultation and communication regarding project proposals

reflects the willingness of governments and third parties to place their thoughts and words in the same context.

(2) Ginoondiwin: “talk to each other.” As our elders have said, and as many accounts of Anishinaabe councils have indicated, our practice has been to reach decisions in common, after full and satisfying discussion addressing the concerns of all involved. As Mississauga historian and chief Peter Jones (1802-56) noted in his rending of a council meeting during the 1850s, the practice of addressing the concerns of all greatly reduced the number of “warm discussions.” We expect federal, provincial and municipal governments to engage with us in consultation that is animated by their need to satisfy our concerns, and not by the needs of third parties, or by deadlines imposed outside of those we might mutually agree to within our processes of consultation. In addition, we expect that when governments attempt to justify project proposals likely to infringe upon our rights and responsibilities, that we will be the party that determines the adequacy of the justification.

(3) Gii-nenmaasiinaawaan: “they didn’t let them”. Anishinaabe participants in treaty talks with settler governments fully expected to be able to consent to or dissent from the proposed matter at hand, as the available written record in Anishinaabemowin makes clear. Our consent to proposals that might affect our rights and responsibilities to our lands, waters and wellbeing is basic to our status as a people possessing dibendizawin, or self- determination. In all matters of consultation and communication, we expect federal, provincial and municipal governments to honour this customary principle of international law, embodied also in article 32(2) of The United Nations Declaration on the Rights of Indigenous Peoples (2007), and finally adopted by Canada in 2016.

(4) Chi-dibaakinigewin: a “great judgement,” as in a treaty between nations. Our ancestors spoke solemnly with settler governments in order to reach agreements that would establish mutually beneficial relationships, which by their nature are on-going, and subject to changing needs and circumstances. However, governments have been one-sided in regard to changing needs in relation to our lands, waters and wellbeing. They have seen our agreements as open, but only as justifying their constant erosion of our control over those lands and waters, and of our wellbeing. We expect that governments interested in consultation will temper this apparently endless desire to consume our lands and waters to the ill effect of our own wellbeing. As well, we expect that when governments insist to the contrary on the legal certainty of treaties and agreements, they will demonstrate persuasively to us how that certainty of the treaty encourages the mutually beneficial relationships that treaties are supposed to establish.

c. Principles of co-existence and economy

(1) Gdoonaaganinaan: “Our Dish,” the agreement reached with the Haudenosaunee in 1701, enabled both our peoples to hunt and harvest in mutual safety, and for mutual wellbeing, within our ancestral lands. We expect federal, provincial and municipal governments to demonstrate clearly and persuasively how proposed projects will undertake to secure mutual safety and mutual wellbeing.

(2) Maatookiiwin: “sharing” Our agreements with settler governments concern our sharing of the lands that the Creator has shared with us. There are no Anishinabemowin transcriptions of treaties that use the word *adaawaage*, meaning “to sell.” Similarly, as Akiwenzii, gimaa of the Lac Courte Orielles Ojibwe, said in relation to the 1837 treaty he co-signed with a US delegation seeking Wisconsin lands: “Gaawiin wiin gimiiisinoon, anishaa ida wi’in,” that is, “I do not make a present of this, I merely lend it to you.” Or, in an 1864 petition to U.S. President Abraham Lincoln, several of those same Anishinaabe chiefs said “Gaawin wiin aki nimbagidinamawaasii,” that is, “I do not offer the land.” We expect that governments interested in projects affecting our lands, waters, and wellbeing will demonstrate how the proposed project embodies this same spirit of sharing of what the Creator has provided, and also charged us with protecting.

(3) Gnawenjigewin: “to take care of things.” Our use of the lands and waters of our territory is subject to Anishinaabe principles of stewardship, derived from our creation story, and instilled through the growth of traditional knowledge. We expect that all communication regarding project proposals will demonstrate how projects plan to incorporate *Deshkan Ziiibiing* participation in the tasks of co-management and governance, as well as employ conservation practices grounded in and consistent with our traditional knowledge.

(4) Niigaan-inaabiwin: “looking ahead”. Decision making that respects the full web of relationships within which the Creator has placed us aims to chart the impacts of our choices as far as possible into the future, in order to minimize the destructiveness of those choices. We expect that all government decisions and project proposals with potential to affect our lands, waters, air, health and wellbeing will demonstrate as concretely as possible the long-term implications of the proposal for *Deshkan Ziiibiing*. We expect that they will also concretely demonstrate the steps to be taken to ensure that they will uphold *Deshkan Ziiibiing*’s responsibilities to protect the web of relationships constituting our traditional territory.

7. Consultation process and requirements

a. Aims of consultation

Appropriate consultation between *Deshkan Ziiibiing* and federal, provincial and municipal authorities serves several purposes. At its heart, appropriate consultation is a dialogue between communities, a mutual engagement, rather than a mere notification of an external party’s intention. Our being fully informed about projects and decisions that may affect our nation protects our full range of rights and responsibilities, as recognized within

traditional Anishinaabe law, in Canada's Constitution, within the jurisprudence of Canada's courts, and in customary international law. It "burnishes the Covenant Chain," by fostering the treaty relationship that should, but does not yet, exist between our people and Canada. In addition to protecting our range of rights, appropriate accommodation also promotes and deepens the path of reconciliation that will ensure a healthier future for both settler and Anishinaabe communities. Appropriate consultation encourages the development of projects that are mutually beneficial to all parties, and it ensures that projects have wide legitimacy both within Deshkan Ziiibiing, and also within the larger network of Anishinaabe nations at home around the Great Lakes.

b. Responsibilities of all parties

(1) All parties engaged in consultation activities have responsibilities in common. Among these are the responsibilities to participate in good faith, and to treat each other with respect, transparency, and honesty. In addition, Deshkan Ziiibiing recognizes that each party has its own unique responsibilities to ensure that the outcome of consultation is a fair, respectful and mutually beneficial understanding of the matter under discussion. Our Anishinaabe understanding of our treaty relationships as the extension of family networks and the attribution of kinship terms encourages us to respect the range of responsibilities borne by parties to consultation.

(2) Crown responsibilities: Deshkan Ziiibiing expects that federal, provincial, and municipal governments will consistently embody the Crown's self-proclaimed obligations towards First Nations. These obligations include a) its fiduciary duties, b) its unyielding motivation to uphold the honour of the Crown, and c) its ongoing commitment to pursue reconciliation with First Nations. Deshkan Ziiibiing edbendaagzijig expect that all inquiries and proposals submitted by the Crown for our consideration, and all government actions undertaken during the course of consultation, are capable of upholding the highest standards of justification on the basis of these three obligations.

Specific Crown responsibilities include timely, effective, and engaged oversight and coordination of all consultation processes and activities involving Deshkan Ziiibiing, and consistent with the honour of the Crown. Competent oversight and coordination requires the Crown to "trigger" the consultation process, that is, as soon as it becomes aware of, or contemplates, activities and proposals that may affect us. Such oversight should include regular updates and dialogues on all consultation processes under way with Deshkan Ziiibiing, and on all Crown determinations of limits to consultation. Oversight also includes due diligence to insure that a project's range, depth, and timeframe of consultation activities are consistent with, and adequate to, Deshkan Ziiibiing's own expressed needs. Crown responsibility also includes timely and adequate provision of funding necessary for Deshkan Ziiibiing to participate in consultation. As well, Crown responsibility includes forthright commitment to the timely and effective accommodation of Deshkan Ziiibiing's full range of rights. In addition, Crown responsibility extends to its earnest incorporation of Deshkan Ziiibiing's partnership into the planning and decision-making process related to the project, as these affect Deshkan Ziiibiing lands, waters, air, health, and wellbeing.

Third party responsibilities: Deshkan Ziibiing acknowledges that third party entities may be delegated certain procedural aspects of the Crown's duty to consult with us, when they pursue commercial or developmental interests in accessing our lands and waters, or potentially affecting our air, health, and well-being. We expect that third parties are responsible for providing full, accurate, and up-to-date information about their projects. Such information should be provided as it becomes available, and should not be subject to explicit requests from Deshkan Ziibiing staff. We also expect that that a project proponent seeking to enter into a relationship with us will actively work to accommodate our concerns, and to view its responsibility to accommodate in a positive manner. To that end, we expect that proponents will be conscientious in adjusting their timelines in order to allow for Deshkan Ziibiing's full participation in any necessary consultation activities. We expect that project proponents will acknowledge their responsibility to consider alternative approaches with us, when implementing their proposal conflicts with our aboriginal and treaty rights, and our ability to protect our lands, waters, air, health, and wellbeing. We expect that proponents will also commit to explore with Deshkan Ziibiing opportunities to share meaningfully in the range of benefits that might result from implementation of their project.

Should consultation need to proceed past the initial stage, we expect project proponents to provide us with the following written acknowledgements:

- a) a statement fully acknowledging our inherent and treaty rights, and our responsibilities to our territory, as they relate to the project,
- b) a statement indicating that the proponent will share this acknowledgement of our rights and responsibilities in all subsequent communication about the project with shareholders, the public, government departments, lenders, and others,
- c) a statement instructing the proponent's subcontractors that they also function within the same framework of Deshkan Ziibiing rights and responsibilities,
- d) a statement disclosing all judgments made against the proponent in all jurisdictions, the involvement of all silent partners, and all agreements made with other First Nations, American Indian tribes, and Anishinaabe communities globally, and
- e) a statement acknowledging that work on the project will only follow the full process of consultation and determination of accommodation.

(4) Deshkan Ziibiing responsibilities: These are, first of all, those that our elders have conveyed from the Creator regarding our obligations to protect the land and waters of our traditional territory. Our responsibilities to our contemporary partners in consultation are similar to those we undertook together with our historic partner in treaty. They include our commitment to uphold the terms of this protocol, as well as of any subsequent agreements arising in regard to consultation and accommodation. We acknowledge the responsibility of staff to provide accurate, sufficient, and timely government or proponent information to Chief and Council, and to Deshkan Ziibiing edbendaagzijig, in order to ensure that our people engage in full discussion and informed decision making regarding proposed projects. We acknowledge our responsibility to explain to governments and proponents, clearly and fully, any concern, distrust, or discomfort

that we have with a project proposal, to state our rights clearly and forthrightly, and to offer suggestions about how proponents and governments might resolve our concerns in good faith.

c. Contact procedures

(1) All proposals for activities with potential to affect Deshkan Ziibiing lands, waters, air, health, and wellbeing must be submitted to the office of the Chief, at the earliest possible moment in the development of a project idea. Such proposed activities include projects directly impacting Deshkan Ziibiing lands, waters and air, as well as those concerning implementation of, or modifications to, regulations and statutes with potential to affect our lands, waters, air, health, and wellbeing. Upon receipt, proposals will be delegated to the proper Deshkan Ziibiing staff for initial processing.

(2) Submission of proposals describing government or third party projects is necessarily part of the government-to-government relationship conducted between Deshkan Ziibiing and appropriate federal, provincial and municipal authorities. Accordingly, all commercial and industrial proposals with potential to affect Deshkan Ziibiing lands, waters, air, health, and wellbeing must be submitted through the relevant government office. All inquiries and initial proposals should be sent via regular mail to:

Chief
 Deshkan Ziibiing/Chippewas of the Thames First Nation
 320 Chippewa Road
 Muncey, Ontario NOL1Y0
 Canada

Copy to:
 Director Lands and
 Environment
 Deshkan Ziibiing/Chippewas of the Thames First Nation
 77 Anishinaabeg Road
 Muncey, Ontario NOL1Y0
 Canada

(3) All inquiries and initial proposals should contain brief, plain language descriptions of projects, including as necessary:

- a) copies of all project proponent communication with Crown departments regarding the delegation of consultation activities and procedures,
- b) accurate contact information for senior, decision-capable, Crown staff members, as well as for senior level project proponents, when appropriate,
- c) early disclosure of all potential project segmentation or staging scenarios,
- d) precise indications of affected landscape boundaries,
- e) estimated forms of all impacts and risks – including climate change impacts, and all

impacts on biodiversity, air quality, and watershed integrity,

d) explication of potential benefits to Deshkan Ziiibiing,

e) preferred timeframes for all aspects of project implementation,

f) estimated time span for effects stemming from life of the project,

g) complete catalogs, with content descriptions, of all relevant studies, mappings, reports, memos, permit applications, motions, and other documents regarding the project proposal,

h) explanation of how the proposal embodies a partner relationship between Deshkan Ziiibiing and federal, provincial, and municipal governments,

i) up-to-date measures of corporate social responsibility, such as ISO26000-2010, or B Corp certification.

d. Deshkan Ziiibiing processing of inquiries and proposals

1) Designated Deshkan Ziiibiing staff will post through regular mail a first response to inquiries and initial proposals, within two weeks of their receipt. First responses will indicate the timeframe necessary for any subsequent Deshkan Ziiibiing follow-up to the initial inquiry or proposal, and will include a reasonable target date for beginning subsequent communication.

2) Deshkan Ziiibiing determines its participation in consultation on the basis of two variables. First, it assesses projects on a scale of a) *minimal* impact, b) *moderate* impact, and c) *extensive* impact. Examples of minimal impact projects include road repair and resurfacing, and replacement of existing structures. Examples of extensive impact projects include nuclear energy waste storage facilities, alternative energy developments, oil and gas pipelines or facilities, and landfills.

A. Minimal impact consultation

1. Information about a proposed project is received by Chief, and forwarded to the Director of Lands and Environment and the Consultation Coordinator,
2. The Consultation Coordinator screens the proposal, logs details, and scans the entire package into a database,
3. The Consultation Coordinator prepares a response, and forwards it to the appropriate party (government department or third party proponent). Typically, the response indicates that Deshkan Ziiibiing has no concerns, and requests continued updates about the proposal, should details change. Consultation service fees apply.

B. Moderate impact consultation

1. Information about a proposed project is received by Chief, and forwarded to the Director of Lands & Environment, and the Consultation Coordinator,
2. The Consultation Coordinator screens the proposal, logs details and scans entire package into database,
3. The Consultation Coordinator adds to the log of projects submitted monthly to the

Environment Committee. Time sensitive responses will be emailed to the Environment Committee, with recommendations and comments for quicker response. The Coordinator prepares a response, and posts it to the appropriate party.

Response 1: Deshkan Ziibiing requests consultation. The appropriate party will be invited to meet with the Lands & Environment department, and may be asked to provide capacity: requests for jobs, job training or a formal request for Capacity Funding Agreement.

Response 2: Deshkan Ziibiing requests additional information in order to determine the extent of concern.

Consultation service fees apply.

C. Extensive impact consultation

1. The process is similar to that of medium impact consultation,
2. With the completion of a necessary Capacity Funding Agreement, and where appropriate, such additional agreements as memoranda of understanding, or community benefit agreements.
3. As well as with the necessary addition of a Deshkan Ziibiing-determined community engagement process, and community ratification.

Second, in addition to the impact scale for required consultation, Deshkan Ziibiing distinguishes between two levels of the scope to consultation. *Landscape level* consultation concerns the evaluation of specific impacts on our lands, waters, air, health, and well-being. *Strategic level* consultation insures more broadly that Deshkan Ziibiing is fully engaged in all federal, provincial and municipal policy formation, planning, implementation, and evaluation, that may affect our traditional territory, immediate homelands, health, and well-being, both at present and in the future.

e. Deshkan Ziibiing-determined provision of required information

Deshkan Ziibiing expects governments and delegated third parties to provide all relevant information, as Deshkan Ziibiing Lands and Environment staff, or Chief and Council might determine is necessary, in order to complete Deshkan Ziibiing's careful evaluation of the impact of the project.

f. Government to government engagement

Although Deshkan Ziibiing may choose to participate in public reviews, studies, and assessments of projects mandated or offered by the Crown to stakeholders in the public sphere, Deshkan Ziibiing is committed to the proposition that it is *not* a stakeholder. The Crown's duty to consult and where necessary accommodate Deshkan Ziibiing requires a separate process on those occasions when we indicate the necessity of consultation. Such consultation, grounded in fiduciary obligations and government willingness to uphold the honour of the Crown, cannot be subsumed.

For those projects and proposals that have a direct impact on Deshkan Ziibiing, we expect that authorities will respect our role and input in providing any necessary terms of reference, or in determining the scope of any necessary review, and in recommending knowledgeable

individuals to serve on reviewing panels and coordinating committees.

Deshkan Ziibiing also expects that its longstanding relationship of alliance with the Crown means that government to government affairs at the minimum must be worked out together in joint dialogue. Accordingly, we expect that the Crown will determine together with us in order to weigh whether it might be appropriate in a given situation to delegate its consultation obligations to a third party.

g. Project-specific work plans

The extent of a project's impact may require us to produce a work plan detailing the steps necessary in order to successfully complete the consultation process. Such a work plan may be developed in conjunction with the relevant government department and the project proponent. Should a work plan need joint development, Deshkan Ziibiing will provide the other consultation parties with a suitable draft, generally within 30 days following our initial contact. Although specific projects may require additional components, in general, plans will include as necessary the following sorts of components:

1. Realistic timeframe,
2. Budget (either cumulative or phase-specific, depending upon the nature of the project),
3. Catalog of information required from each party,
4. List of experts for any necessary review of the project – including legal review, and to undertake any additional studies necessary for Deshkan Ziibiing's informed assessment of the project,
5. List of necessary research projects,
6. Appropriate settings for gathering input from elders,
7. List of proposed meetings between parties, with details of time, place, goals and attendees,
8. Internal consultation steps necessary to incorporate voices of Deshkan Ziibiing edbendaagzijig,
9. Work plan monitoring procedures,
10. Review and evaluation of information,
11. Community decision and notification letter to Crown and proponent,
12. Drafting of any relevant memoranda or agreements.

Work plans may need to be revised as consultation proceeds, and should be done in agreement with all parties. Should government departments and project proponents object to Deshkan Ziibiing's work plan requirements, and only in the aftermath of good faith efforts to reach an

agreement, the parties may turn to a neutral mediator in order to successfully complete an adequate work plan.

h. Elders, traditional knowledge, and confidentiality

As necessary, Deshkan Ziiibiing will indicate the specific ways in which traditional knowledge should inform our assessment of a project proposal's fit within the parameters of the Deshkan Ziiibiing rights and responsibilities laid out above. To that end, staff will develop provisions for insuring that elders play an appropriate role in assessing the project.

Given that much traditional knowledge refers to matters that are sensitive to members of families, or that might only be transmittable within appropriate relational contexts between individuals, we expect that government departments and project proponents will fully respect our judicious determinations of confidentiality, in regards to the gathering of information from elders and other recognized knowledge keepers.

i. Processes for Deshkan Ziiibiing internal consultation

Deshkan Ziiibiing's practice of governance reflects a long history of community-based decision making, one preceding the development of democratic governments by centuries. Depending upon the matter for consultation, government departments and project proponents will need to embrace our approach to internal consultation, and be willing to work with the processes necessary for Deshkan Ziiibiing edbendaagzijig to determine their level of trust and support for the project.

j. Conditions for providing consent

Decisions regarding a project may be achieved in two ways. Those projects seen to have little impact on Deshkan Ziiibiing lands, air, waters, health and wellbeing may be evaluated completely through the efforts of administrative staff, select committees of council, or of council and chief.

Those projects with significant potential to impact Deshkan Ziiibiing lands, air, waters, health and wellbeing, will require the scrutiny of the community as whole. Our traditions of governance charge our leaders with gathering and articulating the voices of the community as a whole. Thus, projects raising significant concerns for Deshkan Ziiibiing edbendaagzijig will need to be accepted by the community in order for Chief and Council to speak in favour of them.

k. Ongoing needs for consultation

Deshkan Ziiibiing expects that consultation on specific matters will likely vary in terms of its duration. Consultation partners may not need to continue discussion past a particular point in the life of a project. However, we expect to be able to determine when we need to continue consultation throughout the life of a project, or even afterwards – for instance, if decommissioning or cumulative effects raise continuing or additional concerns regarding our lands, air, waters, health and wellbeing.

Government or proponent changes to a project, such as its timeline, design, or implementation,

are not unilateral matters. We expect that we will continue the consultation relationship through such reconfigurations.

As well, consultation may need to continue in conjunction with the development of any co-management procedures.

8. Capacity requirements:

Project proponents may see their specific enterprises as unique and urgent efforts. For Deshkan Ziibiing, however, the reality is that our administrative offices receive many proposals, notifications, overviews, and pressing correspondence, on a daily basis. The office of Treaty, Lands and Environment is quite small, with staff workloads consistently focused on a variety of pressing tasks. Unlike other departments of Deshkan Ziibiing governance, Treaty, Lands and Environment's work is self-funded. Consequently, Deshkan Ziibiing requires a range of capacity funding in order to ensure that proposals are adequately, efficiently, and fairly considered within the consultation process.

a. Consultation Service Fees

A complete breakdown of consultation service fees for the varying levels of project impact is attached (refer to Appendix D).

The extent of any ongoing processing fees will depend upon the nature of the investment of time and staff necessary for Deshkan Ziibiing to reach an informed and thorough assessment of the project's implications for our lands, waters, air, health, and wellbeing. We would expect to determine these fees, when significant, in conjunction with government administrators and project proponents. We would also expect that such determination would focus on the matter of insuring Deshkan Ziibiing's ability to fully engage in the consultation matter at hand.

b. Deshkan Ziibiing's participation in research

Depending upon the nature of the project, Deshkan Ziibiing may find that its ability to make a full and informed decision about the proposed project's fit within the framework of principles outlined above requires the conduct of additional research. To the extent that proponents and government departments directly engage in research related to Deshkan Ziibiing, we expect that we will be involved in determining the purpose and scope of the research, the participants in the research, and their roles, and the extent to which the research will involve the work of Deshkan Ziibiing community members, and/or staff. Study methods may include but are not limited to:

- Agricultural Assessment
- Air Quality Assessment
- Cultural Heritage Assessment
- Archeology Assessment
- Ecology Assessment
- Groundwater/Surface Water Assessment
- Land Use Planning Forecast Assessment
- Noise/Vibration Assessment
- Social Assessment
- Traffic Assessment

- Visual/Landscape Assessment

We expect that any resulting staffing needs will be appropriately met by agreement with the Crown and the proponent.

c. Deshkan Ziibiing-initiated research

In addition to research organized in service of the proponent's project, it may also be necessary for Deshkan Ziibiing to initiate its own research projects in order to reach a successful determination about the project. Examples of such research needs include questions about the cumulative effects of a project; or assessments of cultural and archeological, biodiversity, endangered species habitat, or water quality impacts; or traditional land use and occupancy studies where the project is not suited to assessing in the light of existing studies; or competent and thorough assessments of the extent of community support, when a project appears to be especially contentious. We expect that the capacity to engage in such research will be supported by agreement with the Crown and the proponent.

d. Travel and/or hosting expenses

Depending upon the project, it may be necessary for staff, elders, or others, to travel to a project site, or to host meetings or gatherings with those who have historical knowledge and family memories significant to collect, in order to increase Deshkan Ziibiing's ability to provide a thorough assessment of a project. We expect that governments and proponents will bear these costs.

e. Honoraria for elders

Our long-standing practice is to acknowledge our dependence upon the wisdom and knowledge of our elders, a dependence that extends to those outside our community who also wish to draw upon their wisdom and knowledge. Such acknowledgment is appropriately made in terms of money and gifts. We expect governments and proponents to provide these costs, which can be determined in conjunction with staff.

f. Distribution of print materials

Depending upon the nature of the project, staff may need to circulate significant amounts of print materials to Deshkan Ziibiing edbendaagzijig. We expect governments and proponents to provide these costs, as well.

9. Accommodation, mitigation, and compensation plans

Projects with potential to affect our lands, waters, health, and wellbeing cannot proceed without our determining in advance with governments and proponents exactly how they intend to mitigate any impacts, accommodate the depth and extent of our concerns, and compensate any envisioned losses or harms to our lands, waters, air, health and wellbeing. In general, the protection of our inherent and treaty rights, and the respect for our obligations to preserve the lands and waters of Deshkan Ziibiing, are matters to resolve prior to any discussion of potential benefits that might be created through implementation of the project.

The following are non-exhaustive examples of provisions and separate instruments to work out, as necessary, in the process of consultation, in order for Deshkan Ziibiing edbendaagzijig to

embrace the legitimacy of a project:

1. conclusion of any resulting memoranda of understanding, terms of reference, or impact benefit agreements
2. formation of equity and partnership agreements
3. configuration of rents and royalties
4. determination of the extent and composition of intellectual property
5. securing of training, employment, and education opportunities for Deshkan Ziibiing edbendaagzijing within the lifespan of the project
6. development of any necessary co-management structure
7. evidence of serious incorporation of Deshkan Ziibiing concerns into the drafting and subsequent ratification or authorization of all legislation and regulations affecting our lands, air, waters, health and wellbeing

Deshkan Ziibiing expects that the Crown in fulfillment of its fiduciary duty and its diligent regard to uphold the honour of Crown, will assist as, and only as, we may request it to, in undertaking all aspects of negotiation or discussion regarding any agreement that we might reach with a project proponent.

In addition, we expect that all agreements regarding matters of accommodation, mitigation, and compensation are in place before work on a project begins, or if such work actually began prior to our learning of the project from the relevant government department, before the work continues any further.

10. Dispute resolution mechanisms

In the event that governments and third parties are not content with Deshkan Ziibiing's determinations regarding the requirements necessary for our assessing a project, or regarding our conclusion that a particular project does not fit within the framework of rights, responsibilities, and principles elaborated above, we acknowledge that all parties reserve the right to engage in various means of dispute resolution.

- a. Deshkan Ziibiing expects that its expressed and timely intention to pursue dispute resolution will be sufficient for the other parties to place a hold on project development until the dispute is resolved.
- b. Given our historic commitment to resolving disagreements without "warm discussion", the first step in resolving disputes must be honest, good faith discussion in which the Deshkan Ziibiing and the Crown acknowledge that they each have equal decision-making power with the other.
- c. Should agreement between representatives of the parties be unreachable, continuing discussion should take place between such senior-level

decision-makers as Chief, Ministers, deputy ministers, and executives.

- d. Should these discussions fail to yield agreement, the parties may call in the services of a neutral mediator, whose costs will be borne by agreement with the Crown and the proponent.
- e. If the dispute between Deshkan Ziibiing and the other parties is a matter of scientific, technical, historical, archeological, or other such knowledge, the parties may rely upon an assessment from a panel of experts, chosen in equal number by the parties, and whose expenses will be provided by agreement with the Crown and the proponent.
- f. Termination of any agreements or processes prior to completion of consultation should be subject to what the Supreme Court has referred to as “the duty of good faith and honest performance” (Bhasin v. Hrynew, 2014).
- g. Deshkan Ziibiing reserves all right to pursue such adjudication as may seem to it necessary, whether within Canada’s courts, or before international bodies, such as the Inter-American Court of Human Rights.

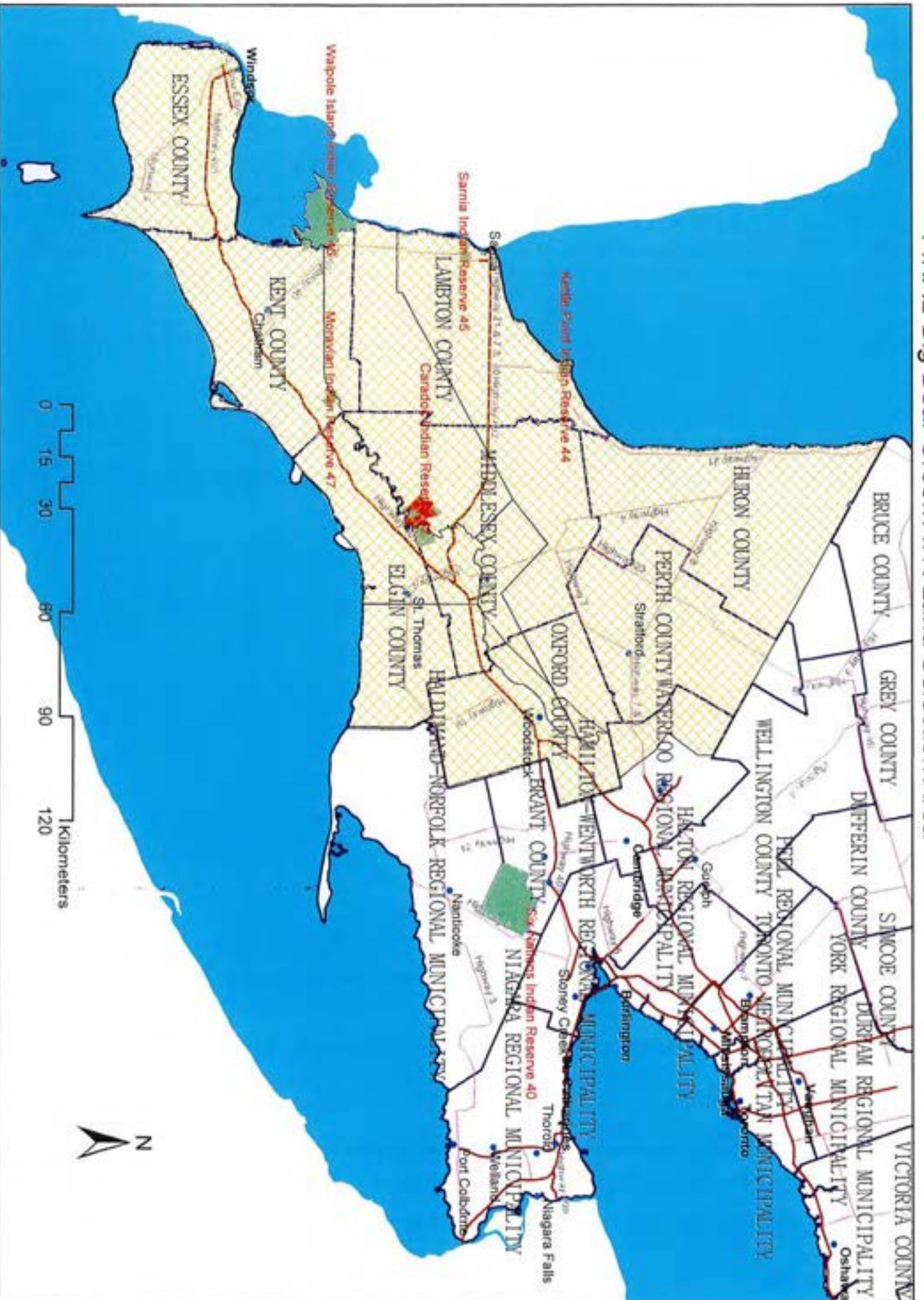
11. Appendices

- A. COTTFN Consultation Map
- B. Southwestern Ontario Treaty Map
- C. Consultation Flow Chart
- D. Consultation Service Fees

This protocol is subject to revision and further development, determinable by Chief and Council, and reflecting as needed the consent of Deshkan Ziibiing edbendaagzijig.

For use with permission of Deshkan Ziibiing/Chippewas of the Thames.

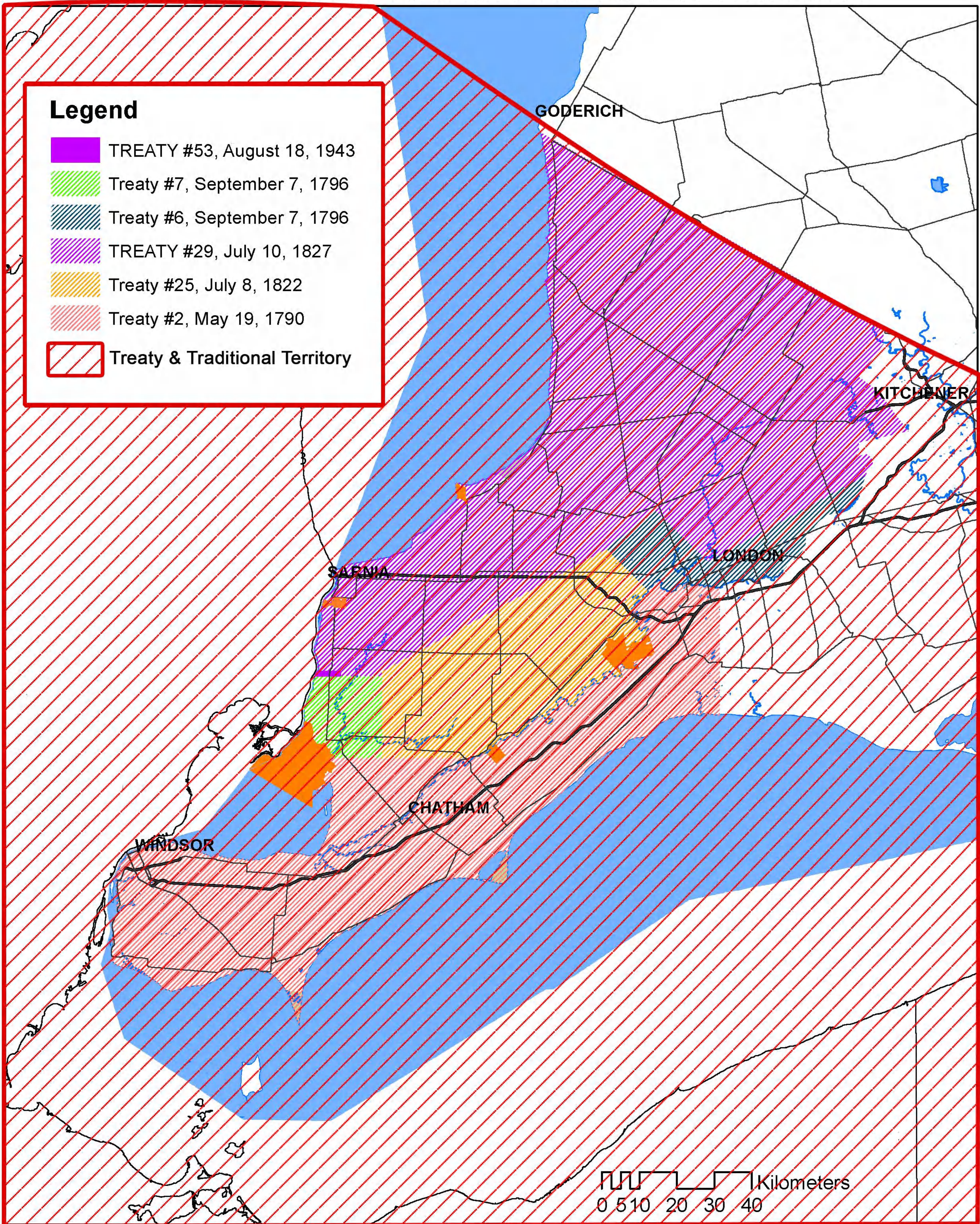
FINAL-Big Bear Creek ATR Land Selection Area



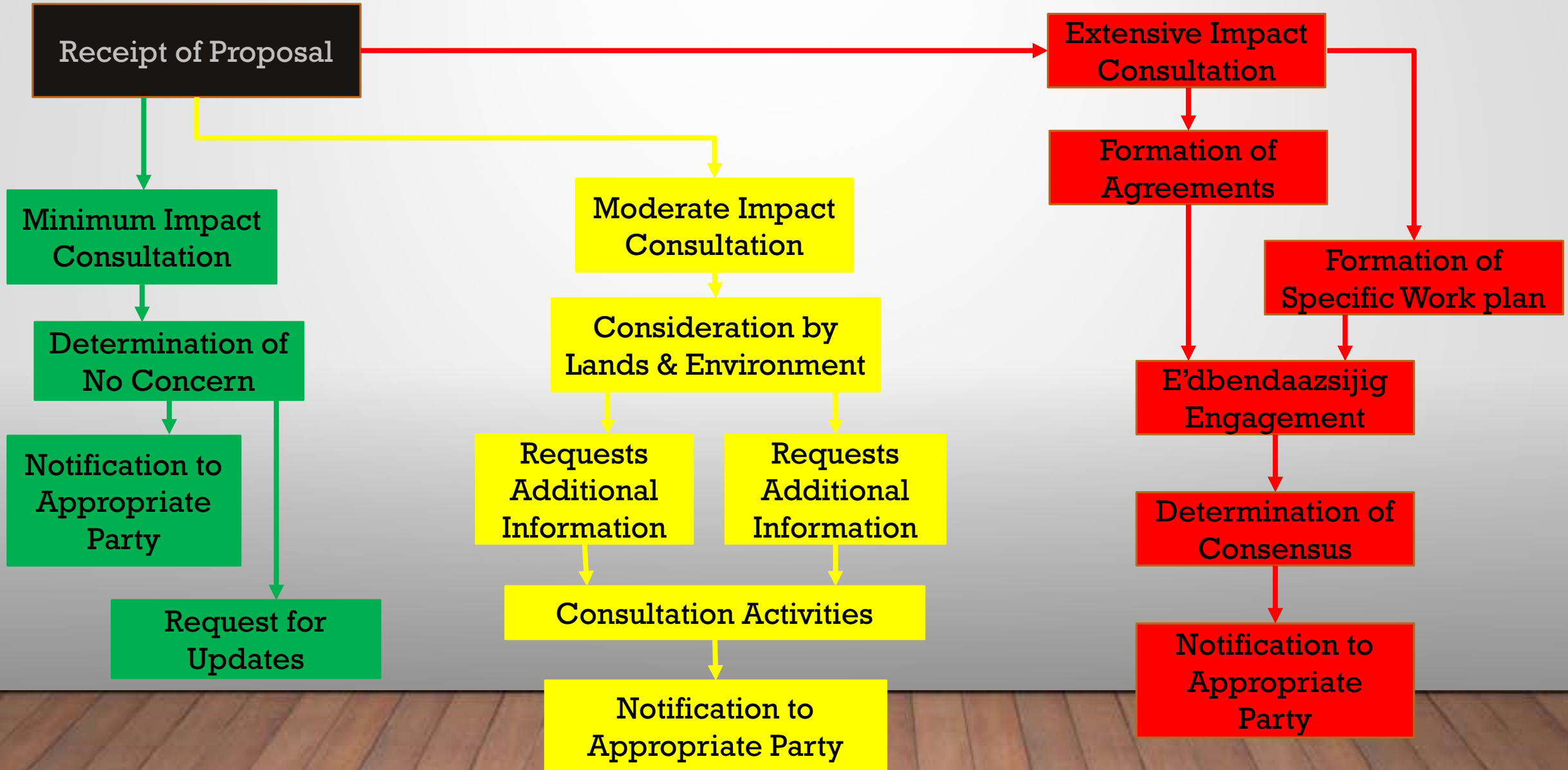
SOUTHERN FIRST NATION TREATY TERRITORY

Legend

- TRETTY #53, August 18, 1943
- Treaty #7, September 7, 1796
- Treaty #6, September 7, 1796
- TRETTY #29, July 10, 1827
- Treaty #25, July 8, 1822
- Treaty #2, May 19, 1790
- Treaty & Traditional Territory



Deshkan Ziibiing Consultation Flow Chart





TREATY, LANDS & ENVIRONMENT

Capacity Delivery Overview

CONSULTATION AND ACCOMMODATION

Chippewas of the Thames First Nation (COTTFN) has developed a general framework for the First Nation in all aspects of its treaty, lands and environmental issues.¹

Under this framework, the First Nation is able to provide a response to all incoming correspondence from project proponents under the *Duty to Consult* and Engagement. COTTFN uses an internal process for ranking and identifying both risks and opportunities when project information is received; and incorporates both environmental and *Haida*-spectrum analysis for the First Nation.

COST RECOVERY

As part of Chippewas of the Thames First Nation Administration the Treaties, Lands & Environment department is responsible for carrying out the environmental and land-related priorities of the nation. Our responsibilities extend to the Traditional Territory of our ancestors; the lands that were agreed to be shared through the Treaties between 1790 and 1822; and the lands that our ancestors chose to be reserved for us and future generations.

Our department is primarily funded through own-source revenues, reflecting a prioritization of lands and the environment based not only on our inherent responsibilities, but also influenced by a vision for the future. This vision utilizes both Traditional Knowledge and other environmental sciences.

Relatively recent developments in Canadian Law and policy, specifically the Duty to Consult, has introduced a dramatic increase in activity for our department. When we engage with a project proponent, it is important that the time and effort involved in receiving correspondence and identifying appropriate response levels is not to be underestimated.

COTTFN has developed three levels of response. These are based on the impacts the project may have on our Aboriginal and Treaty Rights. These levels are also based on factors that the COTTFN has identified in accordance with the responsibilities given to us by the Creator, and our responsibilities to future generations.

The following fee schedule reflects estimated compensation for time and resources that our office requires to actively engage with proponents for the Duty to Consult. These estimates do not include additional costs, such as Honorarium for Elders, Legal Fees, Hosting Fees, fees associated with study participation, i.e. Archaeology, Natural Heritage, Ecology, etc. (this is not an exhaustive list)

When such costs arise, they will be communicated prior to billing.

Colour Coded Ranking of Projects

Definition	Colour
Level 1: Minimal Impact	Costs Associated - Standard
Level 2: Moderate Impact	Costs Associated - Standard
Level 3: Extensive Impact	Costs Associated - Standard

LEVEL 1-ENGAGEMENT

ENGAGEMENT, CONSULTATION AND ACCOMMODATION					
Level 1					
Project					
Minimal					
Director	Activity	Daily	Hourly	Units Estimated	Projected Cost
	Review, high level response and issuing correspondence; and providing direction to staff on the First Nation response based on broad First Nation concerns.	\$ 550.00			NA
Senior Environment Officer	Activity	Daily	Hourly	Units Estimated	
	Environmental review for impacts to traditional territory and based on First Nation concerns, such as, but not limited to: noise, air, waste, contaminants, discharges, greenhouse gases, permitting required, cumulative effects. Development of Recommendations and support to the COTTFN Environment Committee.		\$ 85.00	4	\$ 340.00
Consultation Coordinator	Activity	Daily	Hourly	Units Estimated	
	Receipt of Information, Risk Identification, Internal Notification of Projects, Entry into Database, Issuing Correspondence, Maintenance of filing system, Library Services. Report production for the COTTFN Environment Committee and administrative support.		\$ 85.00	4	\$ 340.00
Treaty Research	Activity	Daily	Hourly	Units Estimated	
	Identification of project in relation to traditional territory, treaty areas, unceded areas, historical occupation	\$ 450.00			NA
COTTFN Environmental Committee	Activity	Daily	Hourly	Units Estimated	
	Reviews projects that have been submitted by the Treaty, Lands and Environment Department and approves recommendations by staff; and/or provides further direction on the project.	\$ 250.00		1	\$ 250.00
Filing Fee	fee charged to accept a document for processing and filing retention				\$ 125.00
TRAVEL	For proponents' meetings, workshops, open houses, public meetings.			0.525 / km	TBD
				SUB-TOTAL	\$ 1055.00
ADMINISTRATION CHARGE 15%	Central Mail, Financial Services: payroll, reimbursement procedures, accounts payable and receivable, production of financial statements, year-end audit. Photocopying, phone and internet service.			15%	\$ 158.25
				GRAND TOTAL:	\$ 1,213.25

***These prices are effective as of November 2018*

*** Prices subject to change, without notice*

LEVEL 2-CONSULTATION

ENGAGEMENT, CONSULTATION AND ACCOMMODATION					
Level 2					
Project					
Moderate					
Director	Activity	Daily	Hourly	Units Estimated	Projected Cost
	Review, high level response and issuing correspondence; and providing direction to staff on the First Nation response based on broad first Nation concerns.	\$ 550.00		1	\$ 550.00
Senior Environment Officer	Activity	Daily	Hourly	Units Estimated	
	Environmental review for impacts to traditional territory and based on First Nation concerns, such as, but not limited to: noise, air, waste, contaminants, discharges, greenhouse gases, permitting required, cumulative effects. Development of Recommendations and support to the COTTFN Environment Committee.		\$ 85.00	6	\$ 510.00
Consultation Coordinator	Activity	Daily	Hourly	Units Estimated	
	Receipt of Information, Risk Identification, Internal Notification of Projects, Entry into Database, Issuing Correspondence, Maintenance of filing system, Library Services. Report production for the COTTFN Environment Committee and administrative support.		\$ 85.00	8	\$ 680.00
Events & Promotions Coordinator	Activity	Daily	Hourly	Units Estimated	
	Development of internal community consultation communication website and social media update, and event planning		\$ 65.00	5	\$ 325.00
Treaty Research	Activity	Daily	Hourly	Units Estimated	
	Identification of project in relation to traditional territory, treaty areas, unceded areas, historical occupation.	\$ 450.00		1	\$ 450.00
COTTFN Environmental Committee	Activity	Daily	Hourly	Units Estimated	
	Reviews projects that have been submitted by the Treaty, Lands and Environment Department and approves recommendations by staff; and/or provides further direction on the project.	\$ 250.00		2	\$ 500.00
COTTFN Band Council	Activity	Daily	Hourly	Units Estimated	Projected Cost
	Reviews projects, deliberates, and provides overall direction to Treaty, Lands & Environment	\$ 350.00		2	\$ 700.00
COTTFN Development Corporation	Activity	Daily	Hourly	Units Estimated	
	The Development Corporation will be responsible for conducting the due diligence required to assess project feasibility, risk and benefit to the community. The information gathered will be presented to the Corporation's Board of Directors to determine the level of contribution and any future involvement based on economic viability and community consensus.	\$ 300.00		1	\$ 300.00
Filing Fee	Fee charged to accept a document for processing and filing retention				\$ 125.00
TRAVEL	For proponents' meetings, workshops, open houses, public meetings.			0.525 / km	TBD
SUB-TOTAL					\$ 4,140.00
ADMINISTRATION CHARGE 15%	Central Mail, Financial Services: payroll, reimbursement procedures, accounts payable and receivable, production of financial statements, year- end audit. Photocopying, phone and internet service.			15%	\$ 621.00
GRAND TOTAL:					\$ 4,761.00

**These prices are effective as of December 2018

** Prices subject to change, without notice

LEVEL 3- HIGH RIGHTS/HIGH IMPACTS

ENGAGEMENT, CONSULTATION AND ACCOMMODATION					
Level 3					
Negotiations and/or					
Extensive Impact					
Director	Activity	Daily	Hourly	Units Estimated	Projected Cost
	High level response and issuing correspondence; and providing direction to staff on the First Nation response based on broad first Nation concerns.	\$ 550.00		2	\$1,100.00
Senior Environment Officer	Activity	Daily	Hourly	Units Estimated	
	Environmental review for impacts to traditional territory and based on First Nation concerns, such as, but not limited to: noise, air, waste, contaminants, discharges, greenhouse gases, permitting required, cumulative effects. Development of Recommendations and support to the COTTFN Environment Committee.		\$ 85.00	6	\$ 510.00
Consultation Coordinator	Activity	Daily	Hourly	Units Estimated	
	Receipt of Information, Risk Identification, Internal Notification of Projects, Entry into Database, Issuing Correspondence, Maintenance of filing system, Library Services. Report production for the COTTFN Environment Committee and administrative support.		\$ 85.00	10	\$ 850.00
Events & Promotions Coordinator	Activity	Daily	Hourly	Units Estimated	
	Development of internal community consultation communication website and social media update, and event planning		\$ 65.00	5	\$ 325.00
Treaty Research	Activity	Daily	Hourly	Units Estimated	
	Identification of project in relation to traditional territory, treaty areas, unceded areas, historical occupation	\$ 450.00		1	\$ 450.00
COTTFN Environmental Committee	Activity	Daily	Hourly	Units Estimated	
	Reviews projects that have been submitted by the Treaty, Lands and Environment Department and approves recommendations by staff; and/or provides further direction on the project.	\$ 250.00		3	\$ 750.00
COTTFN Band Council	Activity	Daily	Hourly	Units Estimated	Projected Cost
	Reviews projects, deliberates, and provides overall direction to Treaty, Lands & Environment	\$ 350.00		2	\$ 700.00
COTTFN Development Corporation	Activity	Daily	Hourly	Units Estimated	
	The Development Corporation will be responsible for conducting the due diligence required to assess project feasibility, risk and benefit to the community. The information gathered will be presented to the Corporation's Board of Directors to determine the level of contribution and any future involvement based on economic viability and community consensus.	\$ 300.00		2	\$ 600.00
Filing Fee	Fee charged to accept a document for processing and filing retention				\$ 125.00
TRAVEL	For proponents' meetings, workshops, open houses, public meetings.			0.525 / km	TBD
				SUB-TOTAL	\$ 5,410.00
ADMINISTRATION CHARGE	Central Mail, Financial Services: payroll, reimbursement procedures, accounts payable and receivable, production of financial statements, year-end audit. Photocopying, phone and internet service.			15%	\$ 811.50
				GRAND TOTAL:	\$ 6,221.50

***These prices are effective as of December 2018

** Prices subject to change, without notice

April 8, 2022

Fallon Burch, Consultation Coordinator
Chippewas of the Thames First Nation
Treaties, Lands & Environment Department
fburch@cottfn.com

Sent via email to fburch@cottfn.com and through *NationsConnect* (nationsconnect.ca) portal

**RE: Oxford Road 19 Corridor Improvement
Municipal Class Environmental Assessment Study – Schedule C
Notice of Study Commencement**

Dear Fallon Burch:

Oxford County (the County) and our project consultant – R.J. Burnside & Associates Ltd. have received your March 29, 2022 letter correspondence associated with the Class Environmental Assessment (EA) Study to consider improvements to Oxford Road 19 corridor. Thank you for confirming that the Chippewas of the Thames First Nation (COTTFN) have identified no concerns with our project based on screening/review. We will keep your community informed if there are any changes to our project that are of a substantial nature. In reference to the aforementioned letter, we also hereby confirm that other First Nations, in closer proximity to our project (including Six Nations of the Grand River and Mississaugas of the Credit First Nation that you noted), have been engaged.

As mentioned in the County's correspondence submitted on March 23, 2022 (care of R.J. Burnside & Associates, through COTTFN's *NationsConnect* portal), consultation for this Class EA Study will comply with the mandatory guidelines developed by the Municipal Engineers Association (MEA) for First Nations and Aboriginal Peoples consultation as detailed in its *Municipal Class Environment Assessment* document (October 2000, amended 2007, 2011 & 2015) which is approved under the *Ontario Environmental Assessment Act*. The County also recognizes and follows the Ministry of Environment, Conservation and Parks (MECP) guidance protocol (Code of Practice for Consultation in Ontario's Environmental Assessment Process) for Aboriginal consultation under the *Ontario Environmental Assessment Act*.

Accordingly, the County willingly accepts its responsibility to conduct interest-based consultation with Indigenous Communities as part of the Environmental Assessment process. Oxford County is committed to the open flow of information and to ensuring that there are meaningful opportunities for the Chippewas of the Thames First Nation to provide input during this Class EA Study. As our neighbours in our community, we wish to build a strong and open relationship with your Nation.

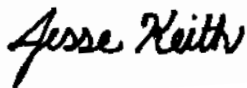
If potential adverse impacts of the project undertakings on asserted or established Aboriginal or treaty rights are anticipated or determined to exist, the Crown has a legal rights-based duty to consult Indigenous Communities. Where the Crown's rights-based duty to consult process may be triggered, the MECP Environmental Approvals Branch will assess the extent of any Crown duty to consult in such circumstances. In such cases, additional procedural aspects of the consultation process may be delegated from the MECP Environmental Approvals Branch to Oxford County (the Study proponent).

As noted in our previous correspondence, a Stage 1 Archaeological Assessment will be carried out to establish the archaeological significance of the Study area and identify any potential archaeological resources (including those of aboriginal descent) in order to minimize any potential impacts to the same prior to any future construction activities being undertaken. The Archaeological Assessment will be submitted to the Ministry of Tourism, Culture and Sport for approval to ensure that any concerns over any potential archaeological sites noted within the Study area are satisfied and/or are to be further addressed through additional archaeological study. Furthermore, the Archaeological Assessment can be made available upon request and will also form part of the draft Environmental Study Report that will be available during the mandatory public review period in the later stages of the Class EA Study project.

Unless advised otherwise by your community, we will continue to circulate you on all future Class EA Study project notifications (e.g. invite/notice of upcoming Public Consultation Centre) as required by the Municipal Class EA Study process. Any additional comments are welcome and will be taken into consideration. Our project team remains available to meet with you at any time during the Study to answer your questions or respond to any concerns you may have.

Thank you again for your participation in this study. Should you have any questions or require additional information, please contact the undersigned by phone 519-539-9800, ext. 3194, fax 519-421-4711 or email jkeith@oxfordcounty.ca

Sincerely,



Jesse Keith
Project Manager
Oxford County Public Works

Encl. *Chippewas of the Thames First Nation Response Letter dated March 29, 2022*

cc: Henry Centen, Project Manager, R.J. Burnside & Associates Ltd.
Sylvia Waters, Technical Administrator, R.J. Burnside & Associates Ltd.
Mark Badali, Regional Environmental Planner, MECP



CHIPPEWAS OF THE THAMES FIRST NATION

Project Name:

Oxford Road 19 Corridor Improvement

FN Consultation ID:

300053425

Consulting Org Contact:

Sylvia Waters

Consulting Organization:

[RJ Burnside & Associates Ltd.](#)

Date Received:

Wednesday, March 23, 2022

March 29, 2022

Dear: Sylvia

We have received information concerning Oxford Road 19 Corridor Improvement, submitted March 23, 2022.

In our screening of your project we have identified no concerns with your project or the information that you have presented to us at this time. We ask that if there are any changes to your project that are of a substantive nature that you keep us informed through NationsConnect.

We ask that you please engage First Nations in closer proximity to your project. e.g. Six Nations of the Grand River, Mississaugas of the Credit First Nation.

We look forward to continuing this open line of communication. To implement meaningful consultation, Chippewas of the Thames First Nation has developed its own protocol - a document and a process that will guide positive working relationships. As per Appendix 'D' of the Wiindaamaagewin, please find attached invoice 0263.

If you have any questions, please feel free to contact me.

Sincerely,

Original Signed

Fallon Burch

Consultation Coordinator

Treaties, Lands & Environment Department

Chippewas of the Thames First Nation

fburch@cottfn.com

^ From: External system 

Created: 10-Jun-2022

Good afternoon,

Thank you for the Notice of the Public Consultation Centre held on June 9, 2022. Unfortunately, a representative from our First Nation was unable to attend. Is this information available electronically or on website?

Thank you,

Fallon Burch
Consultation Coordinator
Chippewas of the Thames First Nation

”

^ From: Sylvia Waters 

Created: 7 minutes ago

Hello Fallon Burch

Thank you for your acknowledgement of receipt of Notice of Public Consultation Centre #1. The Stage 1 Archaeological Assessment will be completed and be part of the Environmental Study Report (ESR). It has been noted that your First Nations has requested an electronic copy of the Stage 1 Archaeological Assessment.

”

Tricia Radburn

From: Fallon Burch <fburch@cottfn.com>
Sent: Tuesday, June 27, 2023 10:03 PM
To: Tricia Radburn
Subject: Oxford Road 19 Corridor Improvement

Hi Tricia,

Thank you for your follow-up and providing the Stage 1 AA report. I have reviewed the file and our previous correspondence sent March 29, 2022. Please remove us from the project list considering that other First Nations are engaging or have been engaged and responded to the project.

If you have any questions, please feel free to contact me.

Thank you,

Fallon



Fallon Burch
Consultation Coordinator
Chippewas of the Thames First Nation
Email: fburch@cottfn.com
519-289-5555 Ex: 251
320 Chippewa Road, Muncey, Ontario



[Visit us online at cottfn.com](http://cottfn.com)

This communication is intended for the use of the recipient to whom it is addressed and may contain confidential and or privileged information. If you are not the intended recipient of this communication any information received should be deleted or destroyed.

From: Chippewas of the Thames First Nation <no-reply-cottfn@knowledgekeeper.ca>
Sent: Friday, June 9, 2023 3:47 PM
To: Fallon Burch <fburch@cottfn.com>
Subject: New message posted on consultation from: Tricia Radburn

A message has been posted on a consultation you are assigned to.

Consultation: [300053425 - Oxford Road 19 Corridor Improvement](#)

Message author: Tricia Radburn

Message:

Fallon, I am following up on your message from April 22, 2022. You had indicated that the project was unlikely to impact treaty and traditional aboriginal rights. We had intended to take you off the project contact list unless anything changed substantially with the project. Unfortunately, you were inadvertently sent additional information about a public meeting. I see that, in response to that, you had requested additional information. Information about the project can be found here: <https://www.oxfordcounty.ca/en/news/oxford-road-19-corridor-improvements...> I have also uploaded the Stage 1 Archaeological Assessment for this project.

Please let me know if you wish to be taken off the project contact list.

Kind Regards,
Tricia Radburn
R.J. Burnside & Associates Limited
226-486-1778

Caution: This email came from someone outside CHIPPEWA OF THE THAMES Do not open attachments or click on links if you do not recognize the sender.



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H4-D

Delaware Nation

Sylvia Waters

From: Denise Stonefish <denise.stonefish@delawarenation.on.ca>
Sent: Tuesday, November 22, 2022 4:43 PM
To: Sylvia Waters
Cc: jkeith@oxfordcounty.ca; Henry Centen; Avid Banihashemi; mark.badali1@ontario.ca; Badali, Mark (MECP)
Subject: RE: 053425-Munsee Nation - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Good afternoon Ms. Waters

Just an FYI, both the Munsee-Delaware Nation and the Delaware Nation are Lenape people, however, we are two separately different First Nations. Munsee-Delaware is located 40 miles upstream from us (Delaware Nation) and we are located in Orford Township within the Municipality of Chatham-Kent, which a fair distance from Oxford Road 19.

So I don't think we will be commenting on the Oxford Road 19 Corridor Improvement.

Anúshiik,

Chief Denise Stonefish

Eelünaapéewi Lahkéewiit

(Delaware Nation)

"OUR VISION IS A COMMUNITY WHERE PEOPLE CAN DEPEND ON EACH OTHER, ARE RESPECTFUL, AND HAS THE COURAGE TO SHAPE OUR OWN FUTURE"

CONFIDENTIALITY WARNING

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BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H4-E

Kettle and Stony Point

Sylvia Waters

From: Avid Banihashemi
Sent: Wednesday, August 03, 2022 10:48 AM
To: consultation@kettlepoint.org
Cc: Sylvia Waters; Jesse Keith; Henry Centen
Subject: RE: 053425 - Chippewas of Kettle and Stony Point First Nation - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hello Emily,

Thank you for your email regarding the Oxford Road 19 Improvements EA Study. We are currently in the process of reviewing the draft stage 1 archaeological assessment report and we would be happy to share the draft report with you for review before it is finalized. The natural environment supporting studies as part of this EA study are ongoing and the results will be included in the final ESR report.

I believe information provided at the [Public Consultation Centre #1](#) (PCC#1) would also be able to provide more details on the findings of the Study to date. The Project Team would be happy to discuss any additional questions or concerns you may have.

Kind regards,
Avid

Avid Banihashemi

Environmental Project Manager

R.J. Burnside & Associates Limited | www.rjburnside.com

Office: +1 800-265-9662 Direct: +1 226-486-1562

From: Consultation <consultation@kettlepoint.org>

Sent: July 8, 2022 9:40 AM

To: Jesse Keith <jkeith@oxfordcounty.ca>; henry.centen@rjburnside.com

Subject: Fw: 053425 - Chippewas of Kettle and Stony Point First Nation - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or on clicking links from unknown senders.

Good morning Jesse and Henry,

Please see the forwarded email below. I just received a bounce back from Sylvia's email.

Miigwetch,

Emily Ferguson

Consultation Advisor, CKSPFN

From: Consultation

Sent: Friday, July 8, 2022 9:37 AM

To: Sylvia.Waters@rjburnside.com

Subject: Fw: 053425 - Chippewas of Kettle and Stony Point First Nation - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Sylvia,

Apologies for the delay getting back to you. We've had some high profile projects we've had to prioritize and have not opened the Oxford Road 19 file yet.

Could you please provide an update on the project to date? CKSPFN is interested in the environmental impact and archaeological studies associated with the project. Please send along any documentation that is now available.

Miigwetch,

Emily Ferguson,

Consultation Advisor, CKSPFN

From: Consultation

Sent: Wednesday, June 15, 2022 9:23 AM

To: Claire Sault; Sylvia.Waters@rjburnside.com

Subject: Re: 053425 - Chippewas of Kettle and Stony Point First Nation - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Claire,

Thank you for passing this along.

Sylvia - We will review the documentation and get back to you shortly.

Miigwetch,

Emily Ferguson

Consultation Advisor, CKSPFN

From: Claire Sault

Sent: Tuesday, June 14, 2022 5:14:43 PM

To: Sylvia.Waters@rjburnside.com

Cc: Consultation

Subject: FW: 053425 - Chippewas of Kettle and Stony Point First Nation - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hi Sylvia,

I got your recent voicemail and have cc'd our Consultation department and will ask them to follow up with you.

Thanks.

Claire Sault

First Nation Manager/CEO

Chippewas of Kettle and Stony Point First Nation

6247 Indian Lane

Kettle and Stony Point FN, ON NON 1J1

Office: 519-786-2125

Cell: 519-209-1518

From: Sylvia Waters [<mailto:Sylvia.Waters@rjburnside.com>]

Sent: June 7, 2022 10:57 AM

To: Claire Sault <Claire.Sault@kettlepoint.org>

Cc: Avid Banihashemi <Avid.Banihashemi@rjburnside.com>; Henry Centen <Henry.Centen@rjburnside.com>

Subject: FW: 053425 - Chippewas of Kettle and Stony Point First Nation - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hello Claire

Further to my earlier voicemail, please see the attached Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study. It is our understanding that Valerie George is no longer working there and the position has not been filled. It was recommended that notices be forwarded to yourself at this time. If you could please confirm receipt of the notice that would be great. Thank you.

Sylvia Waters

Technical Administrator, EPA

R.J. Burnside & Associates Limited | www.rjburnside.com

Office: +1 800-265-9662 Direct: +1 705-797-4379

From: Sylvia Waters

Sent: Thursday, May 19, 2022 11:58 AM

To: valerie.george@kettlepoint.org; Jason.Henry@kettlepoint.org

Cc: jkeith@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>; Badali, Mark (MECP) <mark.badali1@ontario.ca>

Subject: 053425 - Chippewas of Kettle and Stony Point First Nation - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Valerie George, Consultation Coordinator

On behalf of Oxford County, please see attached a letter and the Notice of Public Consultation Centre (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred alternative solution. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC are as follows:

Date: Thursday, June 9, 2022

Time: 5:00 p.m. - 7:00 p.m.

Location: Springford Community Hall

429 Main St. W, Springford, Ontario

The Project Team would be pleased to meet with your community at any time during the EA Study to answer your questions or to discuss any concerns you may have. If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager

Oxford County Public Works

519-539-9800 ext.3194

jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,

Project Manager

R.J. Burnside & Associates Limited

519-340-2003

henry.centen@rjburnside.com

May 19, 2022

Chippewas of Kettle and Stony Point First Nation

**RE: Oxford Road 19 Corridor Improvement
Municipal Class Environmental Assessment Study – Schedule C
Notice of Public Consultation Centre #1**

Dear Ms. George,

On March 17, 2022, your community was sent a letter (*R.J. Burnside & Associated*, on behalf of Oxford County), regarding Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment (Class EA) Study with three attachments, Notice of Commencement, Study Area map and Project Response Form, as the first step to initiate the consultation process for this project.

In follow-up to that letter please find attached the Notice of Public Consultation Centre #1 (NoPCC #1), for the Oxford Road 19 Corridor Improvement Class EA Study, to be held on June 9, 2022 (see notice for details).

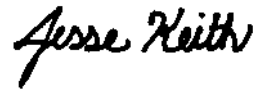
The PCC will be a drop-in format to provide residents/interested parties with an opportunity to review display materials. The display material will consist of the initial findings of the Supporting Studies such as Transportation Study, Natural Environment Assessment, Stage 1 Archaeological Assessment, Cultural Heritage Resource Assessment, and the recommended preferred alternative solution. Representatives from the County and its Consultant will be present to answer questions and discuss next steps in the study.

Consultation will comply with the mandatory guidelines developed by the Municipal Engineers Association for First Nations and Aboriginal Peoples consultation as detailed in its Municipal Class Environmental Assessment document. Oxford County also recognizes and follows the Ministry of Environment, Conservation and Parks' "Areas of Interest: guidance protocol for Aboriginal consultation" under the Ontario *Environmental Assessment Act*. Through this process, Oxford County is committed to the open flow of information and to ensuring that there are meaningful opportunities for Indigenous communities to provide input during the Study. In line with this commitment, the Project Team would be pleased to meet with your community at any time during the EA Study to answer your questions or to discuss any concerns you may have.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

Thank you in advance for your participation. Should you have any questions or require additional information, please contact the undersigned by phone 519-539-9800, ext. 3194, fax 519-421-4711 or email jkeith@oxfordcounty.ca

Sincerely,

A handwritten signature in black ink that reads "Jesse Keith". The signature is written in a cursive, slightly slanted style.

Jesse Keith
Project Manager
Oxford County Public Works

Encl. *Notice of Public Consultation Centre #1*

cc: Henry Centen, Project Manager, R.J. Burnside & Associates,
 henry.centen@rjburnside.com
 Mark Badali, Environmental Resource Planner and EA Coordinator, MECP



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H4-F

Mississaugas of the Credit FN



April 7th, 2022

To Whom It May Concern,

I am writing on behalf of the Department of Consultation and Accommodation [DOCA], requesting information on a project within the Mississaugas of the Credit First Nation's [MCFN] treaty territory.

MCFN are an Aboriginal people within the meaning of section 35 of the *Constitution Act, 1982*. We have signed numerous treaties with the Crown, reaffirming our rights as the original owners of the lands in our territory and establishing Treaty rights over the same. Furthermore, we have un-surrendered Aboriginal title to the waters, beds of water, and foreshore within our territory. Our constitutionally protected rights give rise to specific legal obligations and duties which supersede policies and guidelines.

We are an Indigenous community as understood by the United Nations and our rights include those referenced in the United Nations Declaration on the Rights of Indigenous Peoples ("UNDRIP"). Article 11 of UNDRIP states that Indigenous peoples have "the right to maintain, protect and develop the past, present and future manifestations of their cultures, such as archaeological and historical sites, artefacts..." In May, 2016, the Federal Government committed to adopting and implementing UNDRIP; therefore, the rights of Indigenous peoples outlined in it deserve renewed consideration and respect.

These lands have been the territory and home of MCFN and our ancestors for many generations. As such, there is significant potential for archaeological and other cultural resources of our people to be located during the archaeological fieldwork required for projects or development. Such resources are of critical importance to MCFN given the increasing urbanization and development of our territory that effectively whitewashes our past. Without our active participation and monitoring during archaeological fieldwork, our history stands to be lost forever. As the original stewards of these lands – and continuing owners of the waters – we have ongoing obligations to ensure the protection of our cultural and natural resources for future generations. This is our responsibility and our right.



DOCA has been notified that in a project information file was submitted to the Ministry of Heritage, Sport, Tourism, and Culture Industries for the following project:

PIF ID	116837
Project Name	Oxford Road 19 (21EA-182)
Proponent Identified	R.J. Burnside & Associates Limited (Guelph)
Stage of Assessment	Stage 1
Licensee Name and Number	Johanna Kelly P1017

Please provide a summary of the history of this project and the current state of its associated environmental and archaeological fieldwork. If it is complete, please provide a summary of the preliminary results, followed by the draft report when available. If it is not yet complete, please provide the anticipated start date of fieldwork.

Please be aware that the development may have impacts on MCFN's treaty and aboriginal rights and MCFN has not been properly consulted on this project. Until a reasonable understanding has been reached between MCFN and the proponent regarding the project and our participation in it to ensure that the fieldwork is conducted in a respectful manner that protects our rights, we are of the opinion that any duty to consult over the project has not been met and all subsequent approvals relating to the project are subject to challenge on this basis.

Finally, we would like to take this opportunity to remind you that MCFN has its own *Standards and Guidelines for Archaeology*, which we expect that will be followed in our Territory. Additionally, DOCA requires that our Field Liaison Representatives participate in all environmental and archaeological fieldwork within the MCFN treaty territory, including Stages 2 through 4. **It is our expectation that no fieldwork will take place without the participation of our FLRs.** MCFN has an Aboriginal and Treaty Right to protect the environmental and our archaeological heritage and our FLRs are our boots on the ground to ensure our interests are protected. MCFN considers it disrespectful to our rights as Indigenous peoples if our natural and cultural heritage is interfered with without our involvement.

It is my hope that in light of the above considerations and with a renewed focus on reconciliation, we can navigate through these issues towards a relationship of respect, partnership, and mutual benefit. **Please provide the requested information by 4pm on April 21st, 2022.**



Thank you.



Adam LaForme,
Archaeological Operations Supervisor

CC Mark LaForme, MCFN-DOCA (Mark.LaForme@mncfn.ca)
Ministry of Heritage, Sport, Tourism, and Culture Industries
(archaeology@ontario.ca)



April 13, 2022

Adam LaForme, Archaeological Operations Supervisor
Mississaugas of the Credit First Nation
Department of Consultation and Accommodation
Adam.LaForme@mncfn.ca

Sent via email to Adam.LaForme@mncfn.ca

**RE: Oxford Road 19 Corridor Improvement
Municipal Class Environmental Assessment Study – Schedule C
Response to MCFN Letter dated April 7, 2022**

Dear Mr. LaForme,

Thank you for your letter dated April 7, 2022 on behalf of the Department of Consultation and Accommodation, requesting information on a project within the Mississaugas of the Credit First Nation's (MCFN) treaty territory. This letter provides further information and clarification on the items noted in your letter.

Study Background

Oxford Road 19 is an existing 2-lane road, generally of rural cross-section with shoulders and ditches, owned & maintained by Oxford County (i.e., project location is developed/within disturbed area). Following completion of the County of Oxford's 2019 Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), to support the safe and efficient movement of people and goods.

Accordingly, Oxford County has retained R.J. Burnside & Associates (Burnside) to undertake a Municipal Class Environment Assessment (EA) Study to assess a range of design alternatives to address Oxford Road 19 corridor improvements (including road/road allowance widening considerations). Any potential impact of the project alternatives on social, cultural, economic, and natural environments will be evaluated and assessed during the Study. Archaeological Services Inc (ASI) is acting as a specialist subconsultant to Burnside to study the archaeological and cultural heritage impacts of this proposed undertaking.

In order to initiate engagement with this Study, we have notified your First Nation of the project through a letter dated 17 March 2022, along with the project's Notice of Study Commencement, Response Form and Study Area map, to get your input and to determine if your community may hold an interest in this project. This information is also attached to this follow-up letter.

Archaeological Assessment and Cultural Heritage Resource Assessment

A Stage 1 Archaeological Assessment and Cultural Heritage Resource Assessment is being completed by ASI as part of the project, with reporting anticipated early summer 2022. A copy of the draft Stage 1 report will be provided to you for your review when available.

We acknowledge that ASI's work was commenced before the formal Notice of Study Commencement was issued and that a project information file may have subsequently been submitted to the Ministry of Heritage, Sport, Tourism, and Culture Industries as part of the initiation of the Stage 1 work. Please be assured that it is our intention to maintain all required consultation requirements for this project and that this initial submission to the Ministry does not indicate otherwise.

With respect to MCFN's interest in the County's Archaeological Assessment (stage 1 desktop survey) which is yet to be undertaken for this project, MCFN will have the ability to provide comments on the findings for consideration in the draft Archaeological Assessment report. The project team will review and consider MCFN's comments prior to finalizing the Archaeological Assessment report. The Archaeological Assessment report will then be submitted to the Ministry of Heritage, Sport, Tourism and Culture Industries for approval to ensure that any concerns over any potential archaeological sites noted within the Study area are satisfied and/or are to be further addressed through additional archaeological study.

The need for a Stage 2 Archaeological Assessment will be determined based on the results of the Stage 1 and will be completed at the detailed design stage, if the preliminary preferred solution selected has the potential to impact identified areas of archaeological potential within the study area, due to earthworks and ground disturbance.

Assessment of Terrestrial and Aquatic Conditions

An assessment of the existing terrestrial and aquatic conditions will be completed in support of this EA. The assessment consists of a database information review along with a field assessment. The field assessments are tentatively planned for April 2022. The findings will be summarized in a Natural Environment Technical Memo. A copy of this memo, and/or any other technical reports completed as part of this project, can be provided to you upon request.

With respect to MCFN's interest in the County's Natural Heritage Assessment, the project team is tentatively scheduling environmental field work to occur in the next 2 weeks within the study area, given the narrow timeline during which such field studies could be conducted. We understand that MCFN's preferred method to engage in this project is to participate (observe) in field studies and review natural heritage assessment findings.

Further, the MCFN will have the ability to provide comments on the findings for consideration in the draft Natural Heritage report. The project team will review and consider MCFN's comments prior to finalizing the Natural Heritage report. This natural heritage information will then be submitted to the Ministry of Environment, Conservation and Parks, Ministry of Natural Resources and Forestry and area Conservation Authorities, for review to ensure that any potential environmental concerns with the proposed future undertakings are sufficiently mitigated.

Consultation Requirements

This Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

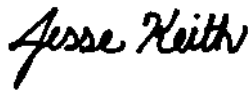
Consultation for this Study will comply with the mandatory guidelines developed by the Municipal Engineers Association for First Nations and Aboriginal Peoples consultation as detailed in its Municipal Class Environmental Assessment document. Oxford County also recognizes and follows the Ministry of Environment, Conservation and Parks' "Areas of Interest: guidance protocol for Aboriginal consultation" under the Ontario *Environmental Assessment Act*. Through this process, Oxford County is committed to the open flow of information and to ensuring that there are meaningful opportunities for Indigenous communities to provide input during the Study.

All of the above information will form part of the draft Class EA Environment Study Report that will be further available during the mandatory public review period in the later stages of the Class EA Study project. The draft Environmental Study Report will include all completed environmental work, cultural heritage assessment and archaeological assessment undertaken within the Study area, including the associated findings and/or recommendations.

Our project team would be pleased to meet with you at any time during the Study to answer your questions or respond to any concerns you may have. Since initial field study work is tentatively scheduled to occur as early as next week, please contact Burnside's project manager (Henry Centen, P. Eng., 519-340-2003, henry.centen@rjburnside.com) to coordinate any voluntary involvement by MCFN in this work.

Thank you in advance for your participation. Should you have any questions or require additional information, please contact the undersigned by phone 519-539-9800, ext. 3194, fax 519-421-4711 or email jkeith@oxfordcounty.ca

Sincerely,



Jesse Keith
Project Manager
Oxford County Public Works

Encl. *Letter from Mississaugas of the Credit First Nation (MCFN) dated April 7, 2022*
Notice of Commencement Package to MCFN dated March 17, 2022

cc: Henry Centen, Project Manager, R.J. Burnside & Associates,
(henry.centen@rjburnside.com)
Mark Badali, Environmental Resource Planner and EA Coordinator, MECP
(Mark.Badali1@ontario.ca)
Mark LaForme, MCFN-DOCA (Mark.LaForme@mncfn.ca)
Ministry of Heritage, Sport, Tourism, and Culture Industries (archaeology@ontario.ca)
Eliza Brandy, ASI (ebrandy@asiheritage.ca)

Sylvia Waters

From: Abby LaForme <Abby.LaForme@mncfn.ca>
Sent: Monday, December 05, 2022 4:19 PM
To: Avid Banihashemi
Cc: Sylvia Waters; jkeith@oxfordcounty.ca
Subject: RE: 053425-Mississaugas of the Credit First Nation - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Good Afternoon,

Thank you for reaching out to MCFN DOCA for Consultation. At this time MCFN DOCA has no comments or concerns regarding MCEA Study for Oxford Road 19 Corridor Improvements.

Please contact MCFN DOCA if any changes or new information arises about said above project.

Thank you

Abby (LaForme) Lee
Acting Consultation Coordinator



Mississaugas of the Credit First Nation (MCFN)
Department of Consultation & Accommodation (DOCA)
4065 Highway 6, Hagersville, ON N0A 1H0
Ph: (905) 768 – 4260
Email: Abby.LaForme@mncfn.ca

From: Avid Banihashemi <Avid.Banihashemi@rjburnside.com>
Sent: Monday, December 5, 2022 4:13 PM
To: Abby LaForme <Abby.LaForme@mncfn.ca>
Subject: FW: 053425-Mississaugas of the Credit First Nation - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Avid Banihashemi
Environmental Project Manager

R.J. Burnside & Associates Limited | www.rjburnside.com
Office: +1 800-265-9662 Direct: +1 226-486-1562

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>
Sent: Thursday, November 17, 2022 12:11 PM
To: abby.laforme@mcfn.ca
Cc: jkeith@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>; Avid Banihashemi <Avid.Banihashemi@rjburnside.com>; mark.badali1@ontario.ca; Badali, Mark (MECP) <mark.badali1@ontario.ca>; adam.laforme@mncfn.ca
Subject: 053425-Mississaugas of the Credit First Nation - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Hello Abby

On behalf of Oxford County, please see attached Notice of Public Consultation Centre # 2 (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC #2 will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC #2 are as follows:

Date: Tuesday December 6, 2022
Time: 5:00 p.m. - 7:00 p.m.
Location: Springford Community Hall, 429 Main St. W., Springford, Ontario

The Project Team would be pleased to meet with your community at any time during the EA Study to answer our questions or to discuss any concerns you may have. If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com


Sylvia Waters
Technical Administrator, EPA

R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-9662 Direct: +1 705-797-4379
www.rjburnside.com

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Thank you.

Tricia Radburn

From: Adrian Blake <Adrian.Blake@mncfn.ca>
Sent: Thursday, July 13, 2023 11:10 AM
To: Tricia Radburn; 053425 Oxford Road 19 Class EA; abby.laforme@mcfnc.ca
Cc: rdavis@oxfordcounty.ca; Henry Centen
Subject: RE: 053425-(21EA-182) Stage 1 Archaeological Assessment for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

Good morning Tricia,

Thank you for sharing this Stage 1-2 archaeological assessment with us at MCFN-DOCA. I have reviewed the report on behalf of the Nation and wanted you to know that we do not have any additional comments, questions or concerns about this archaeological assessment or its recommendations.

Regards,

Adrian Blake, MSc. (he/him)
Field Archaeologist



Department of Consultation and Accommodation (DOCA)

Mississaugas of the Credit First Nation (MCFN)

4065 Highway 6 North, Hagersville, ON NOA 1H0

M: 905-979-3862

<http://www.mncfn.ca>

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From: Tricia Radburn <Tricia.Radburn@rjburnside.com>

Sent: Friday, June 9, 2023 2:55 PM

To: MCFN.Consultation <MCFN.Consultation@mncfn.ca>; 053425 Oxford Road 19 Class EA

<053425OxfordRoad19ClassEA@rjburnside.com>; abby.laforme@mcfnc.ca; Adam LaForme <Adam.LaForme@mncfn.ca>

Cc: rdavis@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>

Subject: Re: 053425-(21EA-182) Stage 1 Archaeological Assessment for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

Good afternoon,

I am writing to provide you with new information regarding the County of Oxford's **County Rd. 19 Corridor Improvement Project**. A Notice of Commencement was issued in March of last year (see email below).

Since the Notice of Commencement was issued, various technical studies have been undertaken. **A draft Stage 1 Archaeological Assessment** is available for your review at the link below.



[Stage 1 Archaeological Assessment](#)

A draft copy of the Municipal Class EA report is expected to be available later this spring/summer and will also be forwarded for your review and comment.

Please reach out if you have any questions or comments about the archaeological study or any other aspect of the project.

Kind Regards,

Tricia Radburn, MCIP, RPP
Senior Environmental Planner

R.J. Burnside & Associates Limited
292 Speedvale Ave. W, Unit 20 Guelph ON
Office: 800-265-9662 Direct: 226-486-1778 www.rjburnside.com

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>
Sent: March 17, 2022 8:58 AM
To: MCFN.Consultation@mncfn.ca <MCFN.Consultation@mncfn.ca>; DOCA.Admin@mncfn.ca <DOCA.Admin@mncfn.ca>; doca@mncfn.ca <doca@mncfn.ca>
Cc: jkeith@oxfordcounty.ca <jkeith@oxfordcounty.ca>
Subject: 053425-Mississaugas of the Credit First Nation - Notice of Commencement for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

Hello Chief LaForme

On behalf of the County of Oxford, please see the attached Notification Letter from the County of Oxford, with attached Notice of Commencement, Project Response Form and Study Area Map, for the Oxford Road 19 Corridor Improvement Project. Project details are summarized below:

Project Lead:	County of Oxford
Project Location:	Oxford Rd. 19 between Hwy 19 (Plank Line) and the boundary of Norfolk County (Windham Rd. 19). The settlements of Springford and Otterville are excluded. Refer to the Notice of Commencement for a Study Area figure, or the larger Project Location/Study Area Map provided.
Approval Process:	Municipal Class Environmental Assessment, Schedule C
Duty to Consult:	The province has delegated the responsibility for consultation to the County of Oxford
Consultant:	R.J. Burnside & Associates Limited has been retained to support the County
Project Description:	The County has identified the need to improve Oxford Rd. 19 to support the safe and efficient movement of goods and people. Options to improve the

	road within the existing road right-of-way (ROW) or within a new widened ROW are being considered.
Field Inventories to be Completed:	<ul style="list-style-type: none"> The need for field inventories may be identified, subject to initial background review and need to expand the ROW, however no ecological field inventories or Stage 2 archaeological work is currently planned. Fieldwork requirements will be identified as the project progresses and as consultation continues.
Documents to be Prepared:	<ul style="list-style-type: none"> Stage 1 Archaeological Assessment Transportation Needs Analysis Municipal Class EA Environmental Study Report
Potential Impacts to Treaty or Indigenous Rights:	<p>To be determined through consultation with Indigenous communities but may include:</p> <ul style="list-style-type: none"> Removal of trees, subject to need for widened ROW Potential in-water work related to the need for the lengthening/replacement of culverts along Spittler Creek, Plumb Creek and Big Otter Creek, subject to the need to widen the ROW Potential impacts to archaeological resources

Please complete the attached response form with any questions or comments you may have, or contact:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext. 3194 | jkeith@oxfordcounty.ca

Henry Centen, P.Eng., Project Manager
R.J. Burnside & Associates Limited
519-340-2003 | henry.centen@rjburnside.com



R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-9662 Direct Line: +1 705-797-4379
www.rjburnside.com



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Appendix H4-G

HDI

Avid Banihashemi

From: Avid Banihashemi
Sent: Friday, January 13, 2023 2:28 PM
To: Philip Rowe
Subject: Email 2: FW: 053425 - Haudenosaunee Confederacy Chiefs Council - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study
Attachments: Notice of PCC#1 - OR19 Corridor Improvement FINAL.pdf; Haudenosaunee Development Institute .pdf

From: Janice Williams <janicewilliams@hdi.land>
Sent: Tuesday, June 07, 2022 7:45 PM
To: Sylvia Waters <Sylvia.Waters@rjburnside.com>
Cc: Tracey General <traceyghdi@gmail.com>; Todd Williams <williams.todde@gmail.com>; Aaron Detlor <Aarondetlor@gmail.com>; Brian Doolittle <ganowa@me.com>; jkeith@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>; mark.badali1@ontario.ca
Subject: Fwd: 053425 - Haudenosaunee Confederacy Chiefs Council - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Sge:no/Hello Sylvia,

It was a pleasure speaking with you over the phone today and I want to provide my feedback towards the project and explain why we have not spoken with the county or consultants.

Nya:weh/Thank-you for the notification sent to Haudenosaunee Development Institute (HDI) regarding the proposed project. At this time, we have significant concerns with respect to the Oxford Road 19 Corridor Improvement project. Particularly with the Oxford County and RJ Burnside for not submitting an application and fee with HDI so that we may review the project. How are we to provide feedback and consider engagement when we have no funds to review and/or comment on this proposed project?

It is necessary that Oxford County and RJ Burnside provide a completed application so we can participate meaningfully on this project which is going to impair and interfere with our rights. Please see the provided instructions to our application process. Again, this application process provides initial funding for our team to begin to review the documents internally and recognize how this project impacts and interferes within our treaty rights.

As for the application process, you are able to access this information on the link below:
[Development - Haudenosaunee Confederacy](#)

Click on the PDF file download and complete the application. Once this is completed, please mail off to:
Haudenosaunee Development Institute
16 Sunrise Court - Suite 600
P.O.Box 714
Ohsweken, Ontario
N0A 1M0

Once the appropriate measures have been followed through, we will discuss how and when we can participate meaningfully. Until then, we ask this proposed project to halt any further.

Nya:weh/Thank-you,

Raechelle Williams
HDI Environmental Supervisor
Haudenosaunee Development Institute
16 Sunrise Court, Suite 402B Ohsweken, ON
P.O. Box 714
Ph: 519-445-4222
(Direct): 519-802-9402



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----- Forwarded message -----

From: **Tracey General** <info@hdi.land>

Date: Thu, May 19, 2022 at 11:41 AM

Subject: Fwd: 053425 - Haudenosaunee Confederacy Chiefs Council - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

To: <ganowa@me.com>, <aarondetlor@gmail.com>, <williams.todde@gmail.com>, Janice Williams <janicewilliams@hdi.land>, Kahsenniyo Williams <kahsennyowilliams@gmail.com>

----- Forwarded message -----

From: **Sylvia Waters** <Sylvia.Waters@rjburnside.com>

Date: Thu, May 19, 2022 at 12:07 PM

Subject: 053425 - Haudenosaunee Confederacy Chiefs Council - Notice of Public Consultation Centre #1 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

To: <jocko@sixnationsns.com> <jocko@sixnationsns.com>, <info@hdi.land> <info@hdi.land>, <1749resource@gmail.com> <1749resource@gmail.com>

Cc: <jkeith@oxfordcounty.ca> <jkeith@oxfordcounty.ca>, Henry Centen <Henry.Centen@rjburnside.com>, Badali, Mark (MECP) <mark.badali1@ontario.ca>

Leroy Hill, Secretary to Haudensaunee Confederacy

On behalf of Oxford County, please see attached a letter and the Notice of Public Consultation Centre (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred alternative solution. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC are as follows:

Date: Thursday, June 9, 2022

Time: 5:00 p.m. - 7:00 p.m.

Location: Springford Community Hall

429 Main St. W, Springford, Ontario

The Project Team would be pleased to meet with your community at any time during the EA Study to answer your questions or to discuss any concerns you may have. If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager

Oxford County Public Works

519-539-9800 ext.3194

jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,

Project Manager

R.J. Burnside & Associates Limited

519-340-2003

henry.centen@rjburnside.com



R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-9662 Direct Line: +1 705-797-4379
www.rjburnside.com



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Thank you.

Sylvia Waters

From: Janice Williams <janicewilliams@hdi.land>
Sent: Wednesday, December 07, 2022 10:27 AM
To: Avid Banihashemi
Cc: Mark.Badali1@ontario.ca; Henry Centen; jkeith@oxfordcounty.ca; Melissa Abercrombie; Frank Gross; Shawn Vanacker; Reuben Davis; 053425 Oxford Road 19 Class EA; Sylvia Waters; Tracey General; Todd Williams; Aaron Detlor
Subject: Re: Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

Sge:no/Hello Avid Banihashemi,

Currently, the HDI and our nation's do not hold these notifications as engagement or consent. Again I address HDI and our nation's significant concerns with respect to the proposed project, explicitly towards the Oxford County and RJ Burnside not applying an application and fee with HDI to review the project. How are we to provide feedback and consider engagement when we have no funds to review and/or comment on this proposed project?

It is required that the proponent of the project provide a completed application and fee so we can analyze the effects, impairment, and interference within our treaty rights. Once we complete our evaluation, we can begin participation in a meaningful way. You may access the application on the link below and see the provided instructions to complete our application process.

[Development - Haudenosaunee Confederacy](#)

Click on the PDF file download and complete the application. Once this is completed, please mail to:

Haudenosaunee Development Institute

44 Sixth Line

Caledonia, Ontario

N3W 1Y9

Payment can be made as a cheque and mailed to HDI P.O.Box number:

P.O.Box office: P.O.Box 714 Ohsweken ON. N0A 1M0

Again, this application process provides initial funding for our team to internally review the documents and recognize how this project impacts and interferes within our treaty rights. Once the appropriate measures have been followed through, we will reach out and discuss HDI participation and impacts in a meaningful way. Until then, we ask this proposed project to halt any further.

Nya:weh/Thank-you,

Raechelle Williams
HDI Environmental Supervisor
Haudenosaunee Development Institute
16 Sunrise Court, Suite 402B Ohsweken, ON
P.O. Box 714
Ph: 519-445-4222
(Direct): 519-802-9402

Sylvia Waters

From: Sylvia Waters
Sent: Thursday, November 17, 2022 12:12 PM
To: janicewilliams@hdi.land; communications@hdi.land; jocko@sixnationsns.com; info@hdi.land; 1749resource@gmail.com
Cc: jkeith@oxfordcounty.ca; Henry Centen; Avid Banihashemi; mark.badali1@ontario.ca; Badali, Mark (MECP); Tracey General; Todd Williams; Aaron Detlor
Subject: 053425-Haudenosaunee Confederacy - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study
Attachments: Notice of Public Consultation Centre #2.pdf

Hello Raechelle Williams of the Haudensaunee Confederacy and Leroy Hill, Secretary to Haudenosaunee Confederacy Chiefs Council

On behalf of Oxford County, please see attached Notice of Public Consultation Centre # 2 (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC #2 will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC #2 are as follows:

Date: Tuesday December 6, 2022
Time: 5:00 p.m. - 7:00 p.m.
Location: Springford Community Hall, 429 Main St. W., Springford, Ontario

The Project Team would be pleased to meet with your community at any time during the EA Study to answer our questions or to discuss any concerns you may have. If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com



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On Thu, Nov 10, 2022 at 3:23 PM Avid Banihashemi <Avid.Banihashemi@rjburnside.com> wrote:

Dear Ms. Williams,

On March 17, 2022, your community was sent a letter from R.J. Burnside & Associates, on behalf of Oxford County, regarding Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment (Class EA) Study with three attachments, Notice of Commencement, Study Area map and Project Response Form, as the first step to initiate the consultation process for this project. On May 19, 2022, in follow-up to that letter, your community was sent the Notice of Public Consultation Centre #1 (NoPCC #1), for the Oxford Road 19 Corridor Improvement Class EA Study, held on June 9, 2022. The notices above were followed by receipt confirmation calls.

We are following up with HDI regarding your email dated June 7, 2022 which highlighted concerns with respect to the Oxford Road 19 Corridor Improvement project, particularly with Oxford County not submitting an application and fee to HDI for project review. At this time, Oxford County is not able to submit an application and provide a fee to HDI for this project. However, Oxford County is committed to the open flow of information and to ensuring that there are opportunities for Indigenous communities to provide input during the Study. In line with this commitment, the County would be happy to share the draft Stage I Archaeological Assessment Report and the draft Natural Heritage Report that are underway as part of this study, once they become available, or any other findings of the study that HDI may be interested in as the study progresses. The Project Team would also be pleased to meet with your community at any time during the EA process to discuss the findings of the study to date, receive the communities' input on these findings to assist with the decision making, answer any questions you may have or to discuss any community concerns with the project.

Thank you in advance for your participation. Should you have any questions or require additional information, please contact the undersigned by phone 519-539-9800, ext. 3194, fax 519-421-4711 or email jkeith@oxfordcounty.ca

Best regards,

Avid Banihashemi (on behalf of Oxford County's Oxford Road 19 Improvements EA Study Team)

 **BURNSIDE**
Avid Banihashemi
Environmental Project Manager

R.J. Burnside & Associates Limited
292 Speedvale Avenue West, Unit 20, Guelph, Ontario N1H 1C4
Office: +1 800-265-9662 Direct: +1 226-486-1562
www.rjburnside.com

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Thank you.

February 6, 2023

Haudenosaunee Development Institute

**RE: Oxford Road 19 Corridor Improvements
Municipal Class Environmental Assessment Study – Schedule C
Response Letter to December 7, 2022 Communication**

Dear Ms. Williams,

We are following up with HDI regarding your email dated December 7, 2022 which highlighted concerns with respect to the Oxford Road 19 Corridor Improvements project.

On behalf of Oxford County, I would like to thank you for your interest in this project. Please rest assured that the Project Team understands HDI's concerns on the potential impacts of this project. At this time, Oxford County will not be engaging in HDI's application process. However, the County would like to offer HDI technical resources on any areas of their concern, to assist with a thorough understanding of the project and any potential impacts of concern.

The County deeply believes in transparent and open flow of communication and information. In line with that, we would be happy to share the findings of our research on any of the study areas that HDI may be interested in; and as noted above, provide technical resources to ensure that these findings address your concerns, particularly in the areas of natural heritage, along with the mitigation measures identified in the stage 1 archaeological assessment. We are also happy to provide in depth review of the road corridor preliminary designs.

The County is also aiming to share the draft supporting technical reports and / or memorandums for natural heritage and stage 1 archaeological assessment as soon as they become available, along with a high-level summary of the study findings, potential impacts, and proposed mitigation measures. In the meantime, we respectfully invite you to review the attached copy of Public Consultation Centre #2 (PCC#2) boards presented at the PCC#2 event held on December 6, 2022.

The Project Team would be pleased to meet with HDI at any time during the EA process to discuss the findings of the study to date, receive the communities' input on these findings and / or discuss how and when the County's technical resources can assist HDI with a full understanding of the findings of the project to date.

Thank you again for your interest in this project. Should you have any questions or require additional information, please contact the undersigned by phone 519-539-9800, ext. 3194, fax 519-421-4711 or email jkeith@oxfordcounty.ca



Sincerely,

A handwritten signature in black ink that reads "Jesse Keith".

Jesse Keith
Project Manager
Oxford County Public Works

Encl. *Public Consultation Centre #2 Boards*

cc: Henry Centen, Project Manager, R.J. Burnside & Associates,
henry.centen@rjburnside.com

Reuben Davis, Supervisor of Engineering Services, Oxford County Public Works
rdavis@oxfordcounty.ca



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H5

Consultation with Agencies

Appendix H5

Sylvia Waters

From: Tricia Radburn
Sent: Thursday, March 17, 2022 11:02 AM
To: Sylvia Waters
Subject: FW: Township of South-West Oxford and Township of Norwich, MEA Class EA, Oxford Road 19 Corridor Improvement
Attachments: Streamlined_ea_project_information_form_2.xlsx; Notice of Study Commencement - OR19 Corridor Improvement.pdf

From: Jesse Keith <jkeith@oxfordcounty.ca>
Sent: Friday, March 11, 2022 3:17 PM
To: 'eanotification.swregion@ontario.ca' <eanotification.swregion@ontario.ca>
Cc: Henry Centen <Henry.Centen@rjburnside.com>
Subject: Township of South-West Oxford and Township of Norwich, MEA Class EA, Oxford Road 19 Corridor Improvement

Good Afternoon,

Please find notification documents attached for the subject EA Study.

Best Regards,

JESSE KEITH, P. ENG. (HE / HIM) | Project Engineer, Public Works
OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3
WWW.OXFORDCOUNTY.CA | T 519.539.9800 / 1-800-755-0394, ext 3194

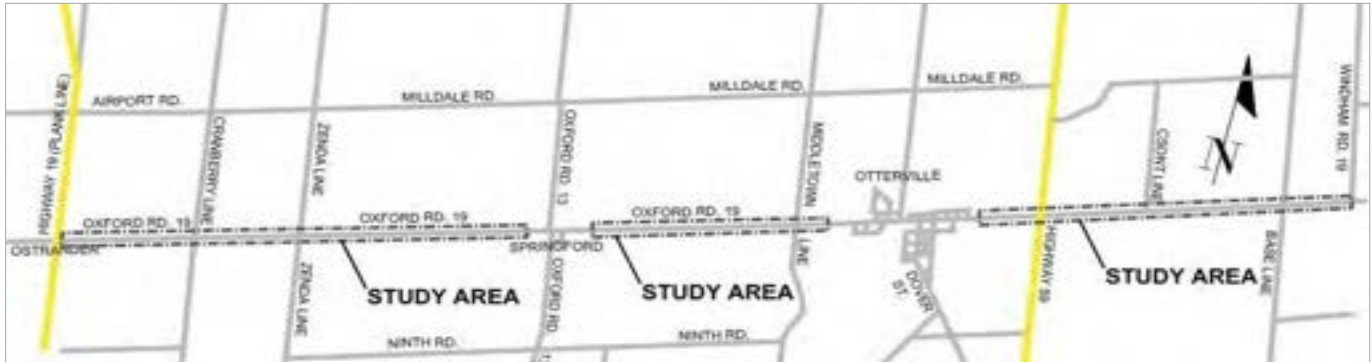


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 Think about our environment. Print only if necessary.

Notice of Study Commencement - Municipal Class Environmental Assessment Study Oxford Road 19 Corridor Improvement

Oxford County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of goods and people.



About the Study

Following completion of Oxford County's 2019 Transportation Master Plan, the County has identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods.

Accordingly, the County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond. The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville - refer to the map herein.

The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the *Municipal Class Environmental Assessment* (2000, as amended in 2007, 2011 & 2015), approved under the *Ontario Environmental Assessment Act*.

We want to hear from you

This notice signals the commencement of the Class EA Study, a study that will identify and evaluate alternative solutions and alternative design concepts in consultation with adjacent property owners, regulatory agencies, indigenous communities, members of the local business community and the public.

Public consultation will occur during the course of the Study to present and receive comments on the project, alternative solutions and alternative designs for the corridor. Consultation with stakeholders is a key component of the Study process and input will be sought throughout the Study, including at a future public consultation centre which will be announced via a subsequent notice and posted to the County's project webpage: www.oxfordcounty.ca/Projects-studies.

An Environmental Study Report will be prepared and made available for final public review and comment upon completion.

Contacts for information

If you have questions or comments regarding the Study or wish to be added to the Project contact list, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194 | jkeith@oxfordcounty.ca

Henry Centen, P.Eng., Project Manager
R.J. Burnside & Associates Limited
519-340-2003 | henry.centen@rjburnside.com

Comments received during the Study will be considered and documented in the Environmental Study Report.

Information will be collected in accordance with the Municipal Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

This notice first issued on March 17, 2022

**Ministry of the Environment,
Conservation and Parks**

**Ministère de l'Environnement,
de la Protection de la nature
et des Parcs**

Environmental Assessment
Branch

Direction des évaluations
environnementales

1st Floor
135 St. Clair Avenue W
Toronto ON M4V 1P5
Tel.: 416 314-8001
Fax.: 416 314-8452

Rez-de-chaussée
135, avenue St. Clair Ouest
Toronto ON M4V 1P5
Tél. : 416 314-8001
Télééc. : 416 314-8452

April 4, 2022

Jesse Keith
Project Manager
Oxford County
jkeith@oxfordcounty.ca

**Re: Oxford Road 19 Corridor Improvement
Oxford County
Municipal Class EA
Response to Notice of Commencement**

Dear Jesse Keith,

This letter is in response to the Notice of Commencement for the above noted project. The Ministry of the Environment, Conservation and Parks (MECP) acknowledges that Oxford County (proponent) has indicated that the study is following the approved environmental planning process for a Schedule C project under the Municipal Class Environmental Assessment (Class EA).

The **updated (February 2021)** attached “Areas of Interest” document provides guidance regarding the ministry’s interests with respect to the Class EA process. Please address all areas of interest in the EA documentation at an appropriate level for the EA study. Proponents who address all the applicable areas of interest can minimize potential delays to the project schedule. **Further information is provided at the end of the Areas of Interest document relating to recent changes to the Environmental Assessment Act through Bill 197, Covid-19 Economic Recovery Act 2020.**

Considering that this project is a Schedule C Municipal Class EA for approximately 16 kilometres of roadway that may be close to sensitive receptors, the proponent should consider whether an Air Quality Impact Assessment (AQIA) is required as part of the decision-making process for the preferred alternative to address all potential air quality impacts to sensitive receptors. An AQIA would include at a minimum the predicted traffic flows and the current and future emissions estimates, as well as any required mitigation measures.

The Crown has a legal duty to consult Aboriginal communities when it has knowledge, real or constructive, of the existence or potential existence of an Aboriginal or treaty right and contemplates conduct that may adversely impact that right. Before authorizing this project, the Crown must ensure that its duty to consult has been fulfilled, where such a duty is triggered. Although the duty to consult with Aboriginal peoples is a duty of the Crown, the Crown may delegate procedural aspects of this duty to project proponents while retaining oversight of the consultation process.

The proposed project may have the potential to affect Aboriginal or treaty rights protected under Section 35 of Canada's *Constitution Act* 1982. Where the Crown's duty to consult is triggered in relation to the proposed project, **the MECP is delegating the procedural aspects of rights-based consultation to the proponent through this letter.** The Crown intends to rely on the delegated consultation process in discharging its duty to consult and maintains the right to participate in the consultation process as it sees fit.

Based on information provided to date and the Crown's preliminary assessment the proponent is required to consult with the following communities who have been identified as potentially affected by the proposed project:

- Mississaugas of the Credit First Nation
- Six Nations of the Grand River (both Elected Council and Haudenosaunee Confederacy Chiefs Council)
- Aamjiwnaang First Nation
- Bkejwanong (Walpole Island)
- Caldwell First Nation
- Chippewas of Kettle and Stony Point
- Chippewas of the Thames First Nation
- Oneida Nation of the Thames
- Eelūnaapèewii Lahkèewiit (Delaware Nation or Moravian of the Thames)
- Munsee-Delaware Nation

Steps that the proponent may need to take in relation to Aboriginal consultation for the proposed project are outlined in the "[Code of Practice for Consultation in Ontario's Environmental Assessment Process](#)". Additional information related to Ontario's Environmental Assessment Act is available online at: www.ontario.ca/environmentalassessments.

Please also refer to the attached document “A Proponent’s Introduction to the Delegation of Procedural Aspects of consultation with Aboriginal Communities” for further information, including the MECP’s expectations for EA report documentation related to consultation with communities.

The proponent must contact the Director of Environmental Assessment Branch (EABDirector@ontario.ca) under the following circumstances subsequent to initial discussions with the communities identified by the MECP:

- Aboriginal or treaty rights impacts are identified to you by the communities
- You have reason to believe that your proposed project may adversely affect an Aboriginal or treaty right
- Consultation with Indigenous communities or other stakeholders has reached an impasse
- A Part II Order request is expected on the basis of impacts to Aboriginal or treaty rights

The MECP will then assess the extent of any Crown duty to consult for the circumstances and will consider whether additional steps should be taken, including what role you will be asked to play should additional steps and activities be required.

A draft copy of the report should be sent directly to me prior to the filing of the final report, allowing a minimum of 30 days for the ministry’s technical reviewers to provide comments.

Please also ensure a copy of the final notice is sent to the ministry’s Southwest Region EA notification email account (eanotification.swregion@ontario.ca) after the draft report is reviewed and finalized.

Should you or any members of your project team have any questions regarding the material above, please contact me at mark.badali1@ontario.ca.

Yours truly,



Mark Badali
Regional Environmental Planner – Southwest Region

Cc: Rob Wrigley, Manager, London District Office, MECP
Henry Centen, Project Manager, R.J. Burnside & Associates Limited

Encl. Areas of Interest
A Proponent’s Introduction to the Delegation of Procedural Aspects of Consultation with
Aboriginal Communities

AREAS OF INTEREST (v. February 2021)

It is suggested that you check off each section after you have considered / addressed it.

Planning and Policy

- Projects located in MECP Central Region are subject to [A Place to Grow: Growth Plan for the Greater Golden Horseshoe \(2020\)](#). Parts of the study area may also be subject to the [Oak Ridges Moraine Conservation Plan \(2017\)](#), [Niagara Escarpment Plan \(2017\)](#), [Greenbelt Plan \(2017\)](#) or [Lake Simcoe Protection Plan \(2014\)](#). Applicable plans and the applicable policies should be identified in the report, and the proponent should describe how the proposed project adheres to the relevant policies in these plans.
- The [Provincial Policy Statement \(2020\)](#) contains policies that protect Ontario's natural heritage and water resources. Applicable policies should be referenced in the report, and the proponent should describe how the proposed project is consistent with these policies.
- In addition to the provincial planning and policy level, the report should also discuss the planning context at the municipal and federal levels, as appropriate.

Source Water Protection

The *Clean Water Act, 2006 (CWA)* aims to protect existing and future sources of drinking water. To achieve this, several types of vulnerable areas have been delineated around surface water intakes and wellheads for every municipal residential drinking water system that is located in a source protection area. These vulnerable areas are known as a Wellhead Protection Areas (WHPAs) and surface water Intake Protection Zones (IPZs). Other vulnerable areas that have been delineated under the CWA include Highly Vulnerable Aquifers (HVAs), Significant Groundwater Recharge Areas (SGRAs), Event-based modelling areas (EBAs), and Issues Contributing Areas (ICAs). Source protection plans have been developed that include policies to address existing and future risks to sources of municipal drinking water within these vulnerable areas.

Projects that are subject to the Environmental Assessment Act that fall under a Class EA, or one of the Regulations, have the potential to impact sources of drinking water if they occur in designated vulnerable areas or in the vicinity of other at-risk drinking water systems (i.e. systems that are not municipal residential systems). MEA Class EA projects may include activities that, if located in a vulnerable area, could be a threat to sources of drinking water (i.e. have the potential to adversely affect the quality or quantity of drinking water sources) and the activity could therefore be subject to policies in a source protection plan. Where an activity poses a risk to drinking water, policies in the local source protection plan may impact how or where that activity is undertaken. Policies may prohibit certain activities, or they may require risk management measures for these activities. Municipal Official Plans, planning decisions,

Class EA projects (where the project includes an activity that is a threat to drinking water) and prescribed instruments must conform with policies that address significant risks to drinking water and must have regard for policies that address moderate or low risks.

- In October 2015, the MEA Parent Class EA document was amended to include reference to the Clean Water Act (Section A.2.10.6) and indicates that proponents undertaking a Municipal Class EA project must identify early in their process whether a project is or could potentially be occurring with a vulnerable area. **Given this requirement, please include a section in the report on source water protection.**
 - The proponent should identify the source protection area and should clearly document how the proximity of the project to sources of drinking water (municipal or other) and any delineated vulnerable areas was considered and assessed. Specifically, the report should discuss whether or not the project is located in a vulnerable area and provide applicable details about the area.
 - If located in a vulnerable area, proponents should document whether any project activities are prescribed drinking water threats and thus pose a risk to drinking water (this should be consulted on with the appropriate Source Protection Authority). Where an activity poses a risk to drinking water, the proponent must document and discuss in the report how the project adheres to or has regard to applicable policies in the local source protection plan. This section should then be used to inform and be reflected in other sections of the report, such as the identification of net positive/negative effects of alternatives, mitigation measures, evaluation of alternatives etc.
- While most source protection plans focused on including policies for significant drinking water threats in the WHPAs and IPZs it should be noted that even though source protection plan policies may not apply in HVAs, these are areas where aquifers are sensitive and at risk to impacts and within these areas, activities may impact the quality of sources of drinking water for systems other than municipal residential systems.
- In order to determine if this project is occurring within a vulnerable area, proponents can use this mapping tool: <http://www.applications.ene.gov.on.ca/swp/en/index.php>. Note that various layers (including WHPAs, WHPA-Q1 and WHPA-Q2, IPZs, HVAs, SGRAs, EBAs, ICAs) can be turned on through the “Map Legend” bar on the left. The mapping tool will also provide a link to the appropriate source protection plan in order to identify what policies may be applicable in the vulnerable area.
- For further information on the maps or source protection plan policies which may relate to their project, proponents must contact the appropriate source protection authority. **Please consult with the local source protection authority to discuss potential impacts on drinking water. Please document the results of that consultation within the report and include all communication documents/correspondence.**

More Information

For more information on the *Clean Water Act*, source protection areas and plans, including specific information on the vulnerable areas and drinking water threats, please refer to [Conservation Ontario's website](#) where you will also find links to the local source protection plan/assessment report.

A list of the prescribed drinking water threats can be found in [section 1.1 of Ontario Regulation 287/07](#) made under the *Clean Water Act*. In addition to prescribed drinking water threats, some source protection plans may include policies to address additional "local" threat activities, as approved by the MECP.

Climate Change

The document "[Considering Climate Change in the Environmental Assessment Process](#)" (Guide) is now a part of the Environmental Assessment program's Guides and Codes of Practice. The Guide sets out the MECP's expectation for considering climate change in the preparation, execution and documentation of environmental assessment studies and processes. The guide provides examples, approaches, resources, and references to assist proponents with consideration of climate change in EA. Proponents should review this Guide in detail.

• **The MECP expects proponents of Class EA projects to:**

1. Consider during the assessment of alternative solutions and alternative designs, the following:
 - a. the project's expected production of greenhouse gas emissions and impacts on carbon sinks (climate change mitigation); and
 - b. resilience or vulnerability of the undertaking to changing climatic conditions (climate change adaptation).
2. Include a discrete section in the report detailing how climate change was considered in the EA.

How climate change is considered can be qualitative or quantitative in nature and should be scaled to the project's level of environmental effect. In all instances, both a project's impacts on climate change (mitigation) and impacts of climate change on a project (adaptation) should be considered.

- The MECP has also prepared another guide to support provincial land use planning direction related to the completion of energy and emission plans. The "[Community Emissions Reduction Planning: A Guide for Municipalities](#)" document is designed to educate stakeholders on the municipal opportunities to reduce energy and greenhouse gas emissions, and to provide guidance on methods and techniques to incorporate consideration of energy and greenhouse gas emissions into municipal activities of all types. We encourage you to review the Guide for information.

□ Air Quality, Dust and Noise

- If there are sensitive receptors in the surrounding area of this project, a quantitative air quality/odour impact assessment will be useful to evaluate alternatives, determine impacts and identify appropriate mitigation measures. The scope of the assessment can be determined based on the potential effects of the proposed alternatives, and typically includes source and receptor characterization and a quantification of local air quality impacts on the sensitive receptors and the environment in the study area. The assessment will compare to all applicable standards or guidelines for all contaminants of concern. **Please contact this office for further consultation on the level of Air Quality Impact Assessment required for this project if not already advised.**
- If a quantitative Air Quality Impact Assessment is not required for the project, the MECP expects that the report contain a qualitative assessment which includes:
 - A discussion of local air quality including existing activities/sources that significantly impact local air quality and how the project may impact existing conditions;
 - A discussion of the nearby sensitive receptors and the project's potential air quality impacts on present and future sensitive receptors;
 - A discussion of local air quality impacts that could arise from this project during both construction and operation; and
 - A discussion of potential mitigation measures.
- As a common practice, "air quality" should be used as an evaluation criterion for all road projects.
- Dust and noise control measures should be addressed and included in the construction plans to ensure that nearby residential and other sensitive land uses within the study area are not adversely affected during construction activities.
- The MECP recommends that non-chloride dust-suppressants be applied. For a comprehensive list of fugitive dust prevention and control measures that could be applied, refer to [Cheminfo Services Inc. Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities](#) report prepared for Environment Canada. March 2005.
- The report should consider the potential impacts of increased noise levels during the operation of the completed project. The proponent should explore all potential measures to mitigate significant noise impacts during the assessment of alternatives.

□ **Ecosystem Protection and Restoration**

- Any impacts to ecosystem form and function must be avoided where possible. The report should describe any proposed mitigation measures and how project planning will protect and enhance the local ecosystem.
- Natural heritage and hydrologic features should be identified and described in detail to assess potential impacts and to develop appropriate mitigation measures. The following sensitive environmental features may be located within or adjacent to the study area:
 - Key Natural Heritage Features: Habitat of endangered species and threatened species, fish habitat, wetlands, areas of natural and scientific interest (ANSIs), significant valleylands, significant woodlands; significant wildlife habitat (including habitat of special concern species); sand barrens, savannahs, and tallgrass prairies; and alvars.
 - Key Hydrologic Features: Permanent streams, intermittent streams, inland lakes and their littoral zones, seepage areas and springs, and wetlands.
 - Other natural heritage features and areas such as: vegetation communities, rare species of flora or fauna, Environmentally Sensitive Areas, Environmentally Sensitive Policy Areas, federal and provincial parks and conservation reserves, Greenland systems etc.

We recommend consulting with the Ministry of Natural Resources and Forestry (MNRF), Fisheries and Oceans Canada (DFO) and your local conservation authority to determine if special measures or additional studies will be necessary to preserve and protect these sensitive features. In addition, you may consider the provisions of the Rouge Park Management Plan if applicable.

□ **Species at Risk**

- The Ministry of the Environment, Conservation and Parks has now assumed responsibility of Ontario's Species at Risk program. Information, standards, guidelines, reference materials and technical resources to assist you are found at <https://www.ontario.ca/page/species-risk>.
- The Client's Guide to Preliminary Screening for Species at Risk (Draft May 2019) has been attached to the covering email for your reference and use. Please review this document for next steps.
- For any questions related to subsequent permit requirements, please contact SAROntario@ontario.ca.

□ Surface Water

- The report must include enough information to demonstrate that there will be no negative impacts on the natural features or ecological functions of any watercourses within the study area. Measures should be included in the planning and design process to ensure that any impacts to watercourses from construction or operational activities (e.g. spills, erosion, pollution) are mitigated as part of the proposed undertaking.
- Additional stormwater runoff from new pavement can impact receiving watercourses and flood conditions. Quality and quantity control measures to treat stormwater runoff should be considered for all new impervious areas and, where possible, existing surfaces. The ministry's [Stormwater Management Planning and Design Manual \(2003\)](#) should be referenced in the report and utilized when designing stormwater control methods. **A Stormwater Management Plan should be prepared as part of the Class EA process** that includes:
 - Strategies to address potential water quantity and erosion impacts related to stormwater draining into streams or other sensitive environmental features, and to ensure that adequate (enhanced) water quality is maintained
 - Watershed information, drainage conditions, and other relevant background information
 - Future drainage conditions, stormwater management options, information on erosion and sediment control during construction, and other details of the proposed works
 - Information on maintenance and monitoring commitments.
- Ontario Regulation 60/08 under the *Ontario Water Resources Act* (OWRA) applies to the Lake Simcoe Basin, which encompasses Lake Simcoe and the lands from which surface water drains into Lake Simcoe. If the proposed sewage treatment plant is listed in Table 1 of the regulation, the report should describe how the proposed project and its mitigation measures are consistent with the requirements of this regulation and the OWRA.
- Any potential approval requirements for surface water taking or discharge should be identified in the report. A Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 L/day, except for certain water taking activities that have been prescribed by the Water Taking EASR Regulation – *O. Reg. 63/16*. These prescribed water-taking activities require registration in the EASR instead of a PTTW. Please review the [Water Taking User Guide for EASR](#) for more information. Additionally, an Environmental Compliance Approval under the OWRA is required for municipal stormwater management works.

□ **Groundwater**

- The status of, and potential impacts to any well water supplies should be addressed. If the project involves groundwater takings or changes to drainage patterns, the quantity and quality of groundwater may be affected due to drawdown effects or the redirection of existing contamination flows. In addition, project activities may infringe on existing wells such that they must be reconstructed or sealed and abandoned. Appropriate information to define existing groundwater conditions should be included in the report.
- If the potential construction or decommissioning of water wells is identified as an issue, the report should refer to Ontario Regulation 903, Wells, under the OWRA.
- Potential impacts to groundwater-dependent natural features should be addressed. Any changes to groundwater flow or quality from groundwater taking may interfere with the ecological processes of streams, wetlands or other surficial features. In addition, discharging contaminated or high volumes of groundwater to these features may have direct impacts on their function. Any potential effects should be identified, and appropriate mitigation measures should be recommended. The level of detail required will be dependent on the significance of the potential impacts.
- Any potential approval requirements for groundwater taking or discharge should be identified in the report. A Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 L/day, with the exception of certain water taking activities that have been prescribed by the Water Taking EASR Regulation – *O. Reg. 63/16*. These prescribed water-taking activities require registration in the EASR instead of a PTTW. Please review the [Water Taking User Guide for EASR](#) for more information.
- Consultation with the railroad authorities is necessary wherever there is a plan to use construction dewatering in the vicinity of railroad lines or where the zone of influence of the construction dewatering potentially intercepts railroad lines.

□ **Excess Materials Management**

- In December 2019, MECP released a new regulation under the Environmental Protection Act, titled “On-Site and Excess Soil Management” (O. Reg. 406/19) to support improved management of excess construction soil. This regulation is a key step to support proper management of excess soils, ensuring valuable resources don’t go to waste and to provide clear rules on managing and reusing excess soil. New risk-based standards referenced by this regulation help to facilitate local beneficial reuse which in turn will reduce greenhouse gas emissions from soil transportation, while ensuring strong protection of human health and the environment. The new regulation is being phased in over time, with the first phase

in effect on January 1, 2021. For more information, please visit <https://www.ontario.ca/page/handling-excess-soil>.

- The report should reference that activities involving the management of excess soil should be completed in accordance with O. Reg. 406/19 and the MECP's current guidance document titled "[Management of Excess Soil – A Guide for Best Management Practices](#)" (2014).
- All waste generated during construction must be disposed of in accordance with ministry requirements

Contaminated Sites

- Any current or historical waste disposal sites should be identified in the report. The status of these sites should be determined to confirm whether approval pursuant to Section 46 of the EPA may be required for land uses on former disposal sites. We recommend referring to the [MECP's D-4 guideline](#) for land use considerations near landfills and dumps.
 - Resources available may include regional/local municipal official plans and data; provincial data on [large landfill sites](#) and [small landfill sites](#); Environmental Compliance Approval information for waste disposal sites on [Access Environment](#).
- Other known contaminated sites (local, provincial, federal) in the study area should also be identified in the report (Note – information on federal contaminated sites is found on the Government of Canada's [website](#)).
- The location of any underground storage tanks should be investigated in the report. Measures should be identified to ensure the integrity of these tanks and to ensure an appropriate response in the event of a spill. The ministry's Spills Action Centre must be contacted in such an event.
- Since the removal or movement of soils may be required, appropriate tests to determine contaminant levels from previous land uses or dumping should be undertaken. If the soils are contaminated, you must determine how and where they are to be disposed of, consistent with *Part XV.1 of the Environmental Protection Act* (EPA) and Ontario Regulation 153/04, Records of Site Condition, which details the new requirements related to site assessment and clean up. Please contact the appropriate MECP District Office for further consultation if contaminated sites are present.

Servicing, Utilities and Facilities

- The report should identify any above or underground utilities in the study area such as transmission lines, telephone/internet, oil/gas etc. The owners should be consulted to discuss impacts to this infrastructure, including potential spills.
- The report should identify any servicing infrastructure in the study area such as wastewater, water, stormwater that may potentially be impacted by the project.
- Any facility that releases emissions to the atmosphere, discharges contaminants to ground or surface water, provides potable water supplies, or stores, transports or disposes of waste must have an Environmental Compliance Approval (ECA) before it can operate lawfully. Please consult with MECP's Environmental Permissions Branch to determine whether a new or amended ECA will be required for any proposed infrastructure.
- We recommend referring to the ministry's [environmental land use planning guides](#) to ensure that any potential land use conflicts are considered when planning for any infrastructure or facilities related to wastewater, pipelines, landfills or industrial uses.

Mitigation and Monitoring

- Contractors must be made aware of all environmental considerations so that all environmental standards and commitments for both construction and operation are met. Mitigation measures should be clearly referenced in the report and regularly monitored during the construction stage of the project. In addition, we encourage proponents to conduct post-construction monitoring to ensure all mitigation measures have been effective and are functioning properly.
- Design and construction reports and plans should be based on a best management approach that centres on the prevention of impacts, protection of the existing environment, and opportunities for rehabilitation and enhancement of any impacted areas.
- The proponent's construction and post-construction monitoring plans must be documented in the report, as outlined in Section A.2.5 and A.4.1 of the MEA Class EA parent document.

Consultation

- The report must demonstrate how the consultation provisions of the Class EA have been fulfilled, including documentation of all stakeholder consultation efforts undertaken during the planning process. This includes a discussion in the report that identifies concerns that were raised and **describes how they have been addressed by the proponent** throughout

the planning process. The report should also include copies of comments submitted on the project by interested stakeholders, and the proponent's responses to these comments (as directed by the Class EA to include full documentation).

- Please include the full stakeholder distribution/consultation list in the documentation.

□ **Class EA Process**

- If this project is a Master Plan: there are several different approaches that can be used to conduct a Master Plan, examples of which are outlined in Appendix 4 of the Class EA. **The Master Plan should clearly indicate the selected approach for conducting the plan**, by identifying whether the levels of assessment, consultation and documentation are sufficient to fulfill the requirements for Schedule B or C projects. Please note that any Schedule B or C projects identified in the plan would be subject to Part II Order Requests under the Environmental Assessment Act, although the plan itself would not be. **Please include a description of the approach being undertaken (use Appendix 4 as a reference).**
- If this project is a Master Plan: Any identified projects should also include information on the MCEA schedule associated with the project.
- The report should provide clear and complete documentation of the planning process in order to allow for transparency in decision-making.
- The Class EA requires the consideration of the effects of each alternative on all aspects of the environment (including planning, natural, social, cultural, economic, technical). The report should include a level of detail (e.g. hydrogeological investigations, terrestrial and aquatic assessments, cultural heritage assessments) such that all potential impacts can be identified, and appropriate mitigation measures can be developed. Any supporting studies conducted during the Class EA process should be referenced and included as part of the report.
- Please include in the report a list of all subsequent permits or approvals that may be required for the implementation of the preferred alternative, including but not limited to, MECP's PTTW, EASR Registrations and ECAs, conservation authority permits, species at risk permits, MTO permits and approvals under the *Impact Assessment Act*, 2019.
- Ministry guidelines and other information related to the issues above are available at <http://www.ontario.ca/environment-and-energy/environment-and-energy>. We encourage you to review all the available guides and to reference any relevant information in the report.

Amendments to the EAA through the Covid-19 Economic Recovery Act, 2020

Once the EA Report is finalized, the proponent must issue a Notice of Completion providing a minimum 30-day period during which documentation may be reviewed and comment and input can be submitted to the proponent. The Notice of Completion must be sent to the appropriate MECP Regional Office email address (for projects in MECP Southwest Region, the email is eanotification.swregion@ontario.ca).

The public has the ability to request a higher level of assessment on a project if they are concerned about potential adverse impacts to constitutionally protected Aboriginal and treaty rights. In addition, the Minister may issue an order on his or her own initiative within a specified time period. The Director (of the Environmental Assessment Branch) will issue a Notice of Proposed Order to the proponent if the Minister is considering an order for the project within 30 days after the conclusion of the comment period on the Notice of Completion. At this time, the Director may request additional information from the proponent. Once the requested information has been received, the Minister will have 30 days within which to make a decision or impose conditions on your project.

Therefore, the proponent cannot proceed with the project until at least 30 days after the end of the comment period provided for in the Notice of Completion. Further, the proponent may not proceed after this time if:

- a Part II Order request has been submitted to the ministry regarding potential adverse impacts to constitutionally protected Aboriginal and treaty rights, or
- the Director has issued a Notice of Proposed order regarding the project.

Please ensure that the Notice of Completion advises that outstanding concerns are to be directed to the proponent for a response, and that in the event there are outstanding concerns regarding potential adverse impacts to constitutionally protected Aboriginal and treaty rights, Part II Order requests on those matters should be addressed in writing to:

Minister Jeff Yurek
Ministry of Environment, Conservation and Parks
777 Bay Street, 5th Floor
Toronto ON M7A 2J3
minister.mecp@ontario.ca

and

Director, Environmental Assessment Branch
Ministry of Environment, Conservation and Parks
135 St. Clair Ave. W, 1st Floor
Toronto ON, M4V 1P5
EABDirector@ontario.ca

A PROPONENT'S INTRODUCTION TO THE DELEGATION OF PROCEDURAL ASPECTS OF CONSULTATION WITH ABORIGINAL COMMUNITIES

DEFINITIONS

The following definitions are specific to this document and may not apply in other contexts:

Aboriginal communities – the First Nation or Métis communities identified by the Crown for the purpose of consultation.

Consultation – the Crown's legal obligation to consult when the Crown has knowledge of an established or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. This is the type of consultation required pursuant to s. 35 of the *Constitution Act, 1982*. Note that this definition does not include consultation with Aboriginal communities for other reasons, such as regulatory requirements.

Crown – the Ontario Crown, acting through a particular ministry or ministries.

Procedural aspects of consultation – those portions of consultation related to the process of consultation, such as notifying an Aboriginal community about a project, providing information about the potential impacts of a project, responding to concerns raised by an Aboriginal community and proposing changes to the project to avoid negative impacts.

Proponent – the person or entity that wants to undertake a project and requires an Ontario Crown decision or approval for the project.

I. PURPOSE

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that may adversely impact that right. In outlining a framework for the duty to consult, the Supreme Court of Canada has stated that the Crown may delegate procedural aspects of consultation to third parties. This document provides general information about the Ontario Crown's approach to delegation of the procedural aspects of consultation to proponents.

This document is not intended to instruct a proponent about an individual project, and it does not constitute legal advice.

II. WHY IS IT NECESSARY TO CONSULT WITH ABORIGINAL COMMUNITIES?

The objective of the modern law of Aboriginal and treaty rights is the *reconciliation* of Aboriginal peoples and non-Aboriginal peoples and their respective rights, claims and interests. Consultation is an important component of the reconciliation process.

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. For example, the Crown's duty to consult is triggered when it considers

issuing a permit, authorization or approval for a project which has the potential to adversely impact an Aboriginal right, such as the right to hunt, fish, or trap in a particular area.

The scope of consultation required in particular circumstances ranges across a spectrum depending on both the nature of the asserted or established right and the seriousness of the potential adverse impacts on that right.

Depending on the particular circumstances, the Crown may also need to take steps to accommodate the potentially impacted Aboriginal or treaty right. For example, the Crown may be required to avoid or minimize the potential adverse impacts of the project.

III. THE CROWN'S ROLE AND RESPONSIBILITIES IN THE DELEGATED CONSULTATION PROCESS

The Crown has the responsibility for ensuring that the duty to consult, and accommodate where appropriate, is met. However, the Crown may delegate the procedural aspects of consultation to a proponent.

There are different ways in which the Crown may delegate the procedural aspects of consultation to a proponent, including through a letter, a memorandum of understanding, legislation, regulation, policy and codes of practice.

If the Crown decides to delegate procedural aspects of consultation, the Crown will generally:

- Ensure that the delegation of procedural aspects of consultation and the responsibilities of the proponent are clearly communicated to the proponent;
- Identify which Aboriginal communities must be consulted;
- Provide contact information for the Aboriginal communities;
- Revise, as necessary, the list of Aboriginal communities to be consulted as new information becomes available and is assessed by the Crown;
- Assess the scope of consultation owed to the Aboriginal communities;
- Maintain appropriate oversight of the actions taken by the proponent in fulfilling the procedural aspects of consultation;
- Assess the adequacy of consultation that is undertaken and any accommodation that may be required;
- Provide a contact within any responsible ministry in case issues arise that require direction from the Crown; and
- Participate in the consultation process as necessary and as determined by the Crown.

IV. THE PROPONENT'S ROLE AND RESPONSIBILITIES IN THE DELEGATED CONSULTATION PROCESS

Where aspects of the consultation process have been delegated to a proponent, the Crown, in meeting its duty to consult, will rely on the proponent's consultation activities and documentation of those activities. The consultation process informs the Crown's decision of whether or not to approve a proposed project or activity.

A proponent's role and responsibilities will vary depending on a variety of factors including the extent of consultation required in the circumstance and the procedural aspects of consultation the Crown has delegated to it. Proponents are often in a better position than the Crown to discuss a project and its potential impacts with Aboriginal communities and to determine ways to avoid or minimize the adverse impacts of a project.

A proponent can raise issues or questions with the Crown at any time during the consultation process. If issues or concerns arise during the consultation that cannot be addressed by the proponent, the proponent should contact the Crown.

a) What might a proponent be required to do in carrying out the procedural aspects of consultation?

Where the Crown delegates procedural aspects of consultation, it is often the proponent's responsibility to provide notice of the proposed project to the identified Aboriginal communities. The notice should indicate that the Crown has delegated the procedural aspects of consultation to the proponent and should include the following information:

- a description of the proposed project or activity;
- mapping;
- proposed timelines;
- details regarding anticipated environmental and other impacts;
- details regarding opportunities to comment; and
- any changes to the proposed project that have been made for seasonal conditions or other factors, where relevant.

Proponents should provide enough information and time to allow Aboriginal communities to provide meaningful feedback regarding the potential impacts of the project. Depending on the nature of consultation required for a project, a proponent also may be required to:

- provide the Crown with copies of any consultation plans prepared and an opportunity to review and comment;
- ensure that any necessary follow-up discussions with Aboriginal communities take place in a timely manner, including to confirm receipt of information, share and update information and to address questions or concerns that may arise;

- as appropriate, discuss with Aboriginal communities potential mitigation measures and/or changes to the project in response to concerns raised by Aboriginal communities;
- use language that is accessible and not overly technical, and translate material into Aboriginal languages where requested or appropriate;
- bear the reasonable costs associated with the consultation process such as, but not limited to, meeting hall rental, meal costs, document translation(s), or to address technical & capacity issues;
- provide the Crown with all the details about potential impacts on established or asserted Aboriginal or treaty rights, how these concerns have been considered and addressed by the proponent and the Aboriginal communities and any steps taken to mitigate the potential impacts;
- provide the Crown with complete and accurate documentation from these meetings and communications; and
- notify the Crown immediately if an Aboriginal community not identified by the Crown approaches the proponent seeking consultation opportunities.

b) What documentation and reporting does the Crown need from the proponent?

Proponents should keep records of all communications with the Aboriginal communities involved in the consultation process and any information provided to these Aboriginal communities.

As the Crown is required to assess the adequacy of consultation, it needs documentation to satisfy itself that the proponent has fulfilled the procedural aspects of consultation delegated to it. The documentation required would typically include:

- the date of meetings, the agendas, any materials distributed, those in attendance and copies of any minutes prepared;
- the description of the proposed project that was shared at the meeting;
- any and all concerns or other feedback provided by the communities;
- any information that was shared by a community in relation to its asserted or established Aboriginal or treaty rights and any potential adverse impacts of the proposed activity, approval or disposition on such rights;
- any proposed project changes or mitigation measures that were discussed, and feedback from Aboriginal communities about the proposed changes and measures;
- any commitments made by the proponent in response to any concerns raised, and feedback from Aboriginal communities on those commitments;
- copies of correspondence to or from Aboriginal communities, and any materials distributed electronically or by mail;

- information regarding any financial assistance provided by the proponent to enable participation by Aboriginal communities in the consultation;
- periodic consultation progress reports or copies of meeting notes if requested by the Crown;
- a summary of how the delegated aspects of consultation were carried out and the results; and
- a summary of issues raised by the Aboriginal communities, how the issues were addressed and any outstanding issues.

In certain circumstances, the Crown may share and discuss the proponent's consultation record with an Aboriginal community to ensure that it is an accurate reflection of the consultation process.

c) Will the Crown require a proponent to provide information about its commercial arrangements with Aboriginal communities?

The Crown may require a proponent to share information about aspects of commercial arrangements between the proponent and Aboriginal communities where the arrangements:

- include elements that are directed at mitigating or otherwise addressing impacts of the project;
- include securing an Aboriginal community's support for the project; or
- may potentially affect the obligations of the Crown to the Aboriginal communities.

The proponent should make every reasonable effort to exempt the Crown from confidentiality provisions in commercial arrangements with Aboriginal communities to the extent necessary to allow this information to be shared with the Crown.

The Crown cannot guarantee that information shared with the Crown will remain confidential. Confidential commercial information should not be provided to the Crown as part of the consultation record if it is not relevant to the duty to consult or otherwise required to be submitted to the Crown as part of the regulatory process.

V. WHAT ARE THE ROLES AND RESPONSIBILITIES OF ABORIGINAL COMMUNITIES' IN THE CONSULTATION PROCESS?

Like the Crown, Aboriginal communities are expected to engage in consultation in good faith. This includes:

- responding to the consultation notice;
- engaging in the proposed consultation process;
- providing relevant documentation;

- clearly articulating the potential impacts of the proposed project on Aboriginal or treaty rights; and
- discussing ways to mitigate any adverse impacts.

Some Aboriginal communities have developed tools, such as consultation protocols, policies or processes that provide guidance on how they would prefer to be consulted. Although not legally binding, proponents are encouraged to respect these community processes where it is reasonable to do so. Please note that there is no obligation for a proponent to pay a fee to an Aboriginal community in order to enter into a consultation process.

To ensure that the Crown is aware of existing community consultation protocols, proponents should contact the relevant Crown ministry when presented with a consultation protocol by an Aboriginal community or anyone purporting to be a representative of an Aboriginal community.

VI. WHAT IF MORE THAN ONE PROVINCIAL CROWN MINISTRY IS INVOLVED IN APPROVING A PROPONENT'S PROJECT?

Depending on the project and the required permits or approvals, one or more ministries may delegate procedural aspects of the Crown's duty to consult to the proponent. The proponent may contact individual ministries for guidance related to the delegation of procedural aspects of consultation for ministry-specific permits/approvals required for the project in question. Proponents are encouraged to seek input from all involved Crown ministries sooner rather than later.

April 7, 2022

EMAIL ONLY

Henry Centen, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
henry.centen@rjburnside.com

MHSTCI File : 0016257
Proponent : County of Oxford
Subject : Notice of Study Commencement – MCEA Schedule C
Project : Oxford Road 19 Corridor Improvement
Location : County of Oxford

Dear Henry Centen:

Thank you for providing the Ministry of Heritage, Sport, Tourism and Culture Industries (MHSTCI) with the Notice of Commencement for the above-referenced project. MHSTCI's interest in this Environmental Assessment (EA) project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- archaeological resources, including land and marine;
- built heritage resources, including bridges and monuments; and
- cultural heritage landscapes.

Under the EA process, the proponent is required to determine a project's potential impact on known (previously recognized) and potential cultural heritage resources.

Project Summary

The County is undertaking a Municipal Class Environmental Assessment (Class EA) Study to consider improvement options for the Oxford Road 19 corridor. The Study area includes approximately 16 kilometres of Oxford Road 19, between Highway 19 (Plank Line) and the boundary of Norfolk County (Windham Road 19), which excludes the Settlements of Springford and Otterville. The Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (2000, as amended in 2007, 2011 & 2015), approved under the Ontario Environmental Assessment Act.

Identifying Cultural Heritage Resources

While some cultural heritage resources may have already been formally identified, others may be identified through screening and evaluation.

Archaeological Resources

This EA project may impact archaeological resources and should be screened using the MHSTCI [Criteria for Evaluating Archaeological Potential](#) to determine if an archaeological assessment is needed. MHSTCI archaeological sites data are available at archaeology@ontario.ca. If the EA project area exhibits archaeological potential, then an archaeological assessment (AA) should be undertaken by an archaeologist licenced under the *Ontario Heritage Act (OHA)*, who is responsible for submitting the report directly to MHSTCI for review.

Built Heritage Resources and Cultural Heritage Landscapes

A Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment will be undertaken for the entire study area during the planning phase and will be summarized in the EA Report. This study will:

1. Describe the existing baseline cultural heritage conditions within the study area by identifying all known or potential built heritage resources and cultural heritage landscapes, including a historical summary of the study area. MHSTCI has developed screening criteria that may assist with this exercise: [Criteria for Evaluating for Potential Built Heritage Resources and Cultural Heritage Landscapes](#).
2. Identify preliminary potential project-specific impacts on the known and potential built heritage resources and cultural heritage landscapes that have been identified. The report should include a description of the anticipated impact to each known or potential built heritage resource or cultural heritage landscape that has been identified.
3. Recommend measures to avoid or mitigate potential negative impacts to known or potential built heritage resources and cultural heritage landscapes. The proposed mitigation measures are to inform the next steps of project planning and design.

Given that this project covers a large study area, MHSTCI recommends that the Cultural Heritage Report is carried out so that step 1 described above is undertaken early in the planning process. Then, steps 2 and 3 can be undertaken once the preferred alternatives have been selected.

Cultural Heritage Reports will be undertaken by a qualified person who has expertise, recent experience, and knowledge relevant to the type of cultural heritage resources being considered and the nature of the activity being proposed.

Community input should be sought to identify locally recognized and potential cultural heritage resources. Sources include, but are not limited to, municipal heritage committees, historical societies and other local heritage organizations.

Cultural heritage resources are often of critical importance to Indigenous communities. Indigenous communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Indigenous communities includes a discussion about known or potential cultural heritage resources that are of value to them.

Environmental Assessment Reporting

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects. Please advise MHSTCI whether any technical cultural heritage studies will be completed for this EA project, and provide them to MHSTCI before issuing a Notice of Completion or commencing any work on the site. If screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file.

Thank you for consulting MHSTCI on this project and please continue to do so throughout the EA process. If you have any questions or require clarification, do not hesitate to contact me.

Sincerely,

Joseph Harvey
Heritage Planner
joseph.harvey@Ontario.ca

Copied to: Jesse Keith, Project Manager, Oxford County Public Works
Sylvie Waters, Technical Administrator, R.J. Burnside & Associates Limited

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. MHSTCI makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MHSTCI be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Please notify MHSTCI (at archaeology@ontario.ca) if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out an archaeological assessment in accordance with the Ontario Heritage Act and the Standards and Guidelines for Consultant Archaeologists.

If human remains are encountered, all activities must cease immediately, and the local police and coroner must be contacted. In situations where human remains are associated with archaeological resources, MHSTCI should also be notified (at archaeology@ontario.ca) to ensure that the site is not subject to unlicensed alterations which would be a contravention of the Ontario Heritage Act.

Sylvia Waters

From: Jesse Keith <jkeith@oxfordcounty.ca>
Sent: Friday, March 18, 2022 2:23 PM
To: 'MNRF Ayl Planners (NDMNRF)'; Henry Centen
Cc: Sylvia Waters; Tricia Radburn
Subject: RE: 053425-Agency - Notice of Commencement for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

Hi Karina,

Thanks kindly for this information. We will review further on our end and reach out to you with any questions or concerns.



Best Regards,

JESSE KEITH, P. ENG. (HE / HIM) | Project Engineer, Public Works
OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3
WWW.OXFORDCOUNTY.CA | T 519.539.9800 / 1-800-755-0394, ext 3194



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Think about our environment. Print only if necessary.

From: MNRF Ayl Planners (NDMNRF) <MNRF.Ayl.Planners@ontario.ca>
Sent: March 18, 2022 10:54 AM
To: Jesse Keith <jkeith@oxfordcounty.ca>; henry.centen@rjburnside.com
Cc: Sylvia.Waters@rjburnside.com
Subject: RE: 053425-Agency - Notice of Commencement for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

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**Ministry of Northern Development,
Mines, Natural Resources
and Forestry**

**Ministère du Développement du Nord,
des Mines, des Richesses naturelles
et des Forêts**

March 18, 2022

Subject: 053425-Agency - Notice of Commencement for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

The Ministry of Northern Development, Mines, Natural Resources and Forestry (NDMNRF) received the notice for the Oxford Road 19 Corridor Improvement project. Thank you for circulating this information to our office, however, please note that we have not completed a screening of natural heritage or other resource values for the project at this time. Please also note that it is your

responsibility to be aware of and comply with all relevant federal or provincial legislation, municipal by-laws or other agency approvals.

This response provides information to guide you in identifying and assessing natural features and resources as required by applicable policies and legislation, and engaging with the Ministry for advice as needed.

Natural Heritage & Endangered Species Act

In order to provide the most efficient service possible, the attached Natural Heritage Information Request Guide has been developed to assist you with accessing natural heritage data and values from convenient online sources.

It remains the proponent's responsibility to complete a preliminary screening for each project, to obtain available information from multiple sources, to conduct any necessary field studies, and to consider any potential environmental impacts that may result from an activity. We wish to emphasize the need for the proponents of development activities to complete screenings prior to contacting the Ministry or other agencies for more detailed technical information and advice.

The Ministry continues to work on updating data housed by Land Information Ontario and the Natural Heritage Information Centre, and ensuring this information is accessible through online resources. Species at risk data is regularly being updated. To ensure access to reliable and up to date information, please contact the Ministry of the Environment, Conservation and Parks at SAROntario@ontario.ca.

Petroleum Wells & Oil, Gas and Salt Resource Act

There may be petroleum wells within the proposed project area. Please consult the Ontario Oil, Gas and Salt Resources Library website (www.ogsrlibrary.com) for the best known data on any wells recorded by NDMNRF. Please reference the 'Definitions and Terminology Guide' listed in the publications on the Library website in order to better understand the well information available. Any oil and gas wells in your project area are regulated by the *Oil, Gas and Salt Resource Act*, and the supporting regulations and operating standards. If any unanticipated wells are encountered during development of the project, or if the proponent has questions regarding petroleum operations, the proponent should contact the Petroleum Operations Section at POSRecords@ontario.ca or 519-873-4634.

Public Lands Act & Lakes and Rivers Improvement Act

Some projects may be subject to the provisions of the *Public Lands Act* or the *Lakes and Rivers Improvement Act*. Please review the information on NDMNRF's web pages provided below regarding when an approval is required or not. Please note that many of the authorizations issued under the *Lakes and Rivers Improvement Act* are administered by the local Conservation Authority.

- For more information about the *Public Lands Act*: <https://www.ontario.ca/page/crown-land-work-permits>
- For more information about the *Lakes and Rivers Improvement Act*: <https://www.ontario.ca/document/lakes-and-rivers-improvement-act-administrative-guide>

After reviewing the information provided, if you have not identified any of NDMNRF's interests stated above, there is no need to circulate any subsequent notices to our office.

If you have any questions or concerns, please feel free to contact me.

Sincerely,
Karina

Karina Černiavskaja | District Planner

Ministry of Northern Development, Mines, Natural Resources and Forestry

MNRF.Ayl.Planners@ontario.ca



As part of providing [accessible customer service](#), please let me know if you have any accommodation needs or require communication supports or alternate formats.

From: Sylvia Waters <Sylvia.Waters@rjburnside.com>

Sent: March-17-22 8:54 AM

To: FisheriesProtection@dfo-mpo.gc.ca; neil.mazey@canadapost.postescanada.ca; Ormsby, Jennifer (OPP) <Jennifer.Ormsby@opp.ca>; Crinklaw, Drew (OMAFRA) <Drew.Crinklaw@ontario.ca>; Barboza, Karla (MHSTCI) <Karla.Barboza@ontario.ca>; Dupuy, Damian (MEDJCT) <Damian.Dupuy@ontario.ca>; Boyd, Erick (MMAH) <Erick.Boyd@ontario.ca>; Graham Harkness, Jennifer (MTO) <Jennifer.GrahamHarkness@ontario.ca>; Hodgins, Allan (MTO) <Allan.Hodgins@ontario.ca>; EA Notices to SWRegion (MECP) <eanotification.swregion@ontario.ca>; Badali, Mark (MECP) <Mark.Badali1@ontario.ca>; Černiavskaja, Karina (NDMNRF) <Karina.Cerniavskaja@ontario.ca>; MNRF Ayl Planners (NDMNRF) <MNRF.Ayl.Planners@ontario.ca>; mduben@oxfordcounty.ca; cseior@oxfordcounty.ca; clerk@swox.org; al.meneses@norfolkcounty.ca; teresa.olsen@norfolkcounty.ca; karmstrong@norwich.ca; CentralFBCPlanning@HydroOne.com; SecondaryLandUse@HydroOne.com; jeff.soetemans@execulink.com; swo@tdlcanada.ca; nordel@nor-del.com; scott.moon@bell.ca; rowcentre@bell.ca; Rogers.MOC@telecon.ca; azocco@uniongas.com; Kevin.Schimus@enbridge.com; vince.cina@enbridge.com; notifications@enbridge.com; mark-ups@enbridge.com; dpresley@mhbcpplan.com; cleo@tnpi.ca; crossingrequesteast@tnpi.ca; azocco@uniongas.com; Utility.Circulations@zayo.com; stefan.linder@cn.ca; michael.vallins@cn.ca; jeff.willsie@ontsouthland.com; brad.jolliffe@ontsouthland.com; greg.rankin@ontsouthland.com; Brian_Costigan@cpr.ca

Cc: jkeith@oxfordcounty.ca

Subject: 053425-Agency - Notice of Commencement for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

CAUTION -- EXTERNAL E-MAIL - Do not click links or open attachments unless you recognize the sender.

Oxford County (County) is initiating a Schedule C Municipal Class Environmental Assessment Study for **Oxford Road 19 Corridor Improvement** in Oxford County. Following the completion of the Oxford County's 2019 Transportation Master Plan, the County identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods. Please see the attached Notice of Commencement.

 **BURNSIDE**
Sylvia Waters
Technical Administrator, EPA

R.J. Burnside & Associates Limited
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Office: +1 800-265-9662 Direct Line: +1 705-797-4379
www.rjburnside.com





Minutes of Meeting

Meeting Date: May 4, 2022 **Project No.:** 300053425.0000
Project Name: Oxford Rd. 19 Environmental Assessment
Meeting Subject: InTAC Meeting #1
Meeting Location: Video Conference
Date Prepared: May 12, 2022

Those in attendance were:

Jesse Keith	Oxford County (County)	jkeith@oxfordcounty.ca
Shawn Vanacker	Oxford County	svanacker@oxfordcounty.ca
Heather St. Claire	Oxford County	hstclair@oxfordcounty.ca
Melissa Abercrombie	Oxford County	mabercrombie@oxfordcounty.ca
Henry Centen	R.J. Burnside and Associates Ltd. (Burnside)	henry.centen@rjburnside.com
Avid Banihashemi	Burnside	Avid.Banihashemi@rjburnside.com
Jeremy Taylor	Burnside	Jeremy.taylor@rjburnside.com
Chris Pfohl	Burnside	Chris.pfohl@rjburnside.com

The following items were discussed	Action by
<p>1. Project Background</p> <p>1.1 Henry Centen (HC), R.J. Burnside (Burnside) started the meeting with welcoming everyone for attending. The County staff in attendance included representatives from both planning and public works. Burnside staff in attendance are: Avid Banihashemi (AB) taking over for Tricia Radburn, Chris Pfohl (CP), aquatics and ecology, and Jeremy Taylor (JT), to prepare the regional design.</p> <p>HC noted that this meeting is part of the EA consultation efforts per County's consultation policy. Burnside has met with the County twice earlier, and some technical constraints has been reviewed through these meetings. The External Technical Committee (ExTAC) to be held the following week.</p> <p>The Project has started last November. Survey work and preliminary technical analysis were completed, and a Technical Memo was prepared on technical constraints in January. The Notice of</p>	

The following items were discussed	Action by
<p>Commencement (NOcm) was then released to agencies, Indigenous communities and the public.</p> <p>HC reviewed the purpose of the InTAC meeting and listed the agenda items. He then walked through the draft Public Consultation Centre #1 (PCC#1) slides, highlighting key points on: the project background for Oxford Road 19 improvements, the Study Approach, Existing Conditions, Alternatives and Preliminary Preferred Solution, additional information on the technical constraints and opportunities and a summary of consultation to date. The staff were then invited to provide feedback and comments</p> <p>The draft PIC#1 slides to be sent to County staff by the end of day for their review for any further input, by Wednesday following week, prior to PCC #1.</p>	<p>Burnside</p>
<p>2. Comments and Responses regarding Draft PCC Slides</p>	
<p>2.1 Stage 1 Archaeological Assessment and the Cultural Heritage Resource</p> <p>The Stage 1 Archeological assessment is currently under way which will provide information if a Stage 2 Archaeological Assessment will be required at the detail Design stage of the project. A cultural heritage assessment study has just recently started.</p> <p>Additional information on the preliminary findings of Stage 1 Archaeological Assessment and the Cultural Heritage Resource Assessment to be added to the slides prior to the EXTAC meeting.</p>	<p>Burnside</p>
<p>2.2 Burial Site</p> <p>HC noted that there is some preliminary information regarding an out-of-record cemetery and some discussion has occurred; JK confirmed that the burial site thought to be in the Township of Norwich. Now it is said to be in SW Oxford near the 4th Line is not in Norwich.</p>	
<p>2.3 Existing Conditions (pavement)</p> <p>JT commented on the Existing Road Condition slide whether it should be mentioned about the deteriorating condition of the asphalt or should the slide be more high level. JK notes that the base is noted as being 60 years old which would generally support the need to improve its condition. It was also noted that some surface treatment work will be completed this year on the asphalt as part of</p>	

The following items were discussed

Action by

normal preventive maintenance, however this would only have a minimal impact on the overall road condition.

2.4 Consultation

AB gave a summary of input to date from agencies, Indigenous communities and the public.

It was noted that there had been no concerns from *agencies* thus far, and no comments from any *utilities*.

Indigenous communities have not sent any specific comments or concerns at this time. Mississauga's of the Credit First Nation would like to be involved with Archaeological Assessment field work (if any during the EA) and the Natural Environment field work, however, they do not have capacity at this time. Chippewas of the Thames would like be kept updated on the project.

The Mississauga's of the Credit First Nation have requested to be contacted and involved in the fieldwork, If a Stage 2 Archaeological Assessment is warranted for the Detailed Design.

Public Comments have been of a general nature, such as concern for repeated issues from previous construction projects, and general lack of trust; speed issues; truck movement and whether the improvements will cause addition traffic and noise impact; and that a stronger justification is required for the project.

HC commented on the concern regarding truck movements and noted that the road currently carries truck volumes that are typical of County roads.

3. Ecology Perspective of the Project

CP noted that the road is a standard road from an ecological perspective and there are some impacts to the natural environment to be considered, such as the removal of trees and the potential impacts to bat habitat. No significant bridge works have been identified at this time that would indicate involve the Department of Fisheries and Oceans (DFO), however this will be verified as part of the next phase of the MCEA in the consideration of alternative designs. Staff expect to receive correspondence from the Long Point Region Conservation Authority (LPRCA) regarding flood regulation; Ministry of the Environment and Conservation Parks (MECP).

The following items were discussed	Action by
<p>4. Alternatives Cost</p> <p>JK requested that Burnside prepare a costing of alternatives, as the public at the PCC may enquire. JT noted that Burnside has completed some initial cost estimates and requested at what level of detail the County would like to see. JK noted that a ballpark cost would be adequate at this point.</p> <p>Shawn Vanacker (SV) recommended a cost per metre, not a lump sum, as the public may have an issue with a large dollar figure. JK also noted that this would not be something that would be displayed on a PCC board. JK also requested confirmation from Public Works and if the staff are satisfied with Alternative #5 at all costs. SV noted that staff do need to consider all options, ultimately Alternative 5 is what the staff prefer as an outcome, but dollars do factor in.</p> <p>HC noted Alternative #5 provides the County flexibility in the amount of widening of the shoulders, which would affect the costs, however the ROW is proposed to expand to 30.5 metres to meet the County's Official Plan requirements. JT costed acquisition of additional ROW at \$20 K a hectare. It was noted that recently rural lands have been paid at \$30 K, and that this should be taken into consideration. JK requested that Burnside send estimate and staff will review offline.</p> <p>Heather St. Claire (HSC) asked why the settlement areas have not been included. JK noted that because improvements have already been done in the settlement areas.</p>	Burnside
<p>5. ROW to meet Official Plan</p> <p>HC noted that the project should take into consideration the shoulder requirements to accommodate agricultural traffic, horse and buggy traffic and active transportation needs. This will be a factor in the ongoing consultation with the public. JK noted that this will be reviewed. JK noted that a comment had been received regarding road curvature and if speed is an issue this could impact horses drawn vehicles.</p>	
<p>6. Next Steps</p> <p>HC noted that at the EXTAC meeting on Tuesday attendees will be asked to review PCC slides by the end of the following week. County staff noted that there will be no need to have drainage staff at the EXTAC as the County reviews drainage. JK recommended</p>	Burnside

The following items were discussed

Action by

that the PCC be sent to the drainage staff (Daniel LeDuc) for review.
Burnside to extend the EXTAC invite to drainage staff.

The preceding are the minutes of the meeting as observed by the undersigned. Should there be a need for revision, please advise Burnside within seven days of issuance. In the absence of notification to the contrary, these minutes will be deemed to be an accurate record of the meeting.

Minutes prepared by:

R.J. Burnside & Associates Limited



Henry Centen, P. Eng.
Senior Transportation Engineer
HC:ba/js

Enclosure(s): Public Consultation PowerPoint Presentation

Distribution:
All Attendees

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5/17/2022 9:36 AM



Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements

PUBLIC CONSULTATION CENTRE / OXFORD COUNTY ADMINISTRATION
BUILDING, 21 REEVE STREET, WOODSTOCK, ON, N4S 7Y3
JUNE 9, 2022
5:00 P.M. - 7:00 P.M.

WELCOME

to the **Public Consultation Centre for the
Oxford Road 19 Corridor Improvements**

Please Sign In

- Meet with Study Team Members
- Review the display materials and discuss your questions and ideas with the Study Team
- Please fill out a comment sheet and return it to the Study Team in person, by email or fax by **June 23, 2022**

STUDY AREA

The County is undertaking a Class EA study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study Area includes approximately 16 kilometres of Oxford Road 19 between Highway 19 (Plank Line), and the Norfolk County boundary (Windham Road 19), which excludes the Settlements of Springford and Otterville.



The site location and approximate extent of the Study Area are shown on the map.

PROBLEM/OPPORTUNITY STATEMENT

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

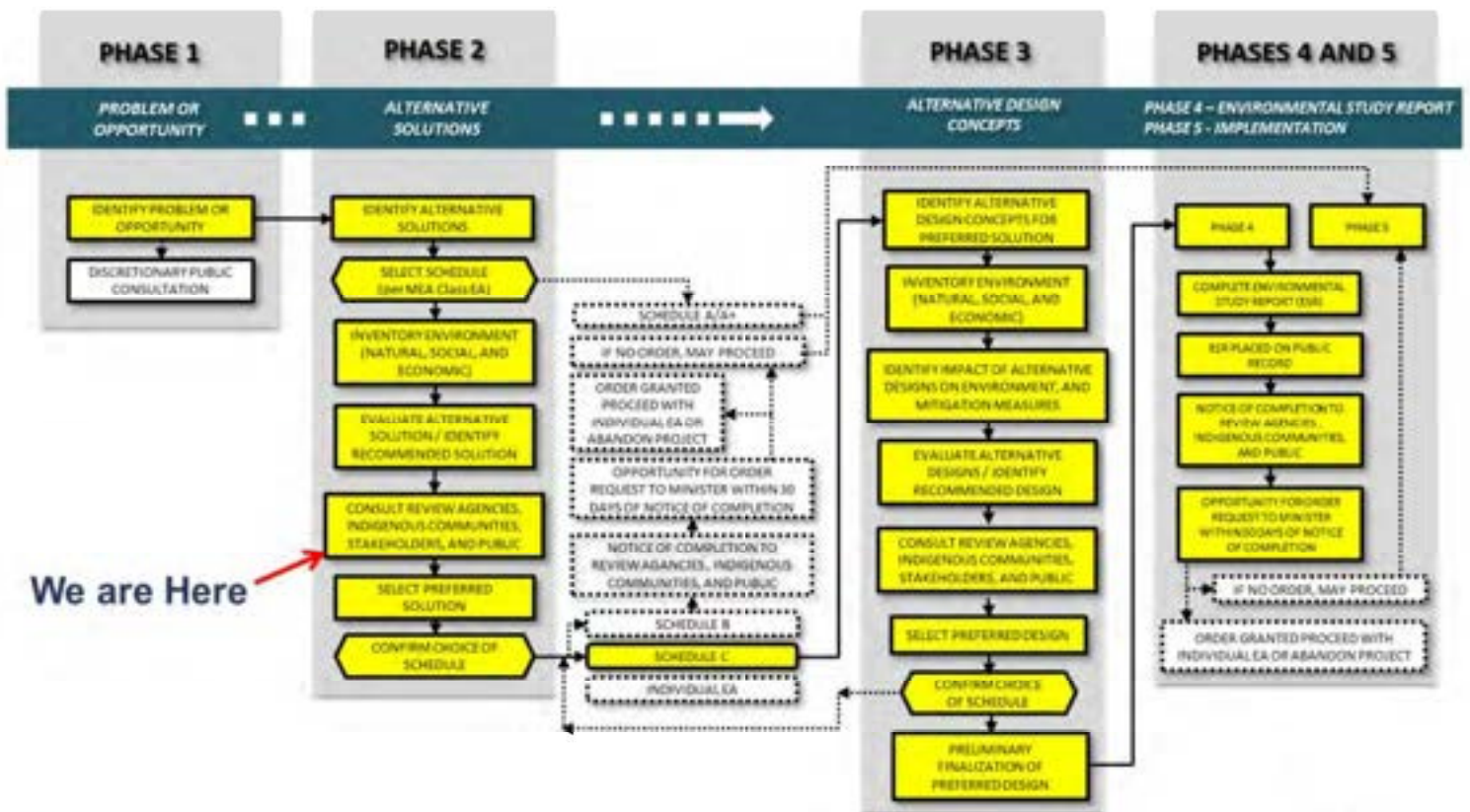


THE EA PROCESS

The Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act.

Nearing completion of the study, an Environmental Study Report (ESR) will be prepared and made available for final public review and comment.

THE EA PROCESS



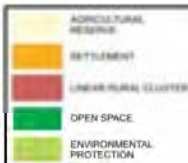
PLANNING CONTEXT

- Provincial Policy Statement, 2020
- County of Oxford Official Plan (consolidation of Official Plan amendments, as of March 31, 2021)
- Oxford County Transportation Master Plan (2019)
- Phase One Comprehensive Review Oxford County (2020)
- Draft Cycling Master Plan (2021)
- TAC Design Guidelines (2017)
- Guidelines from Ontario Traffic Manual – Book 18 (Cycling Facilities)



EXISTING LAND USE

- The two communities of Springford and Otterville within Norwich are zoned as Settlements by the County of Oxford Official Plan.
- Outside of these communities, most land is Agricultural Reserve with lands surrounding Ostrander is a Rural Cluster. All other land in South-West Oxford is Agricultural Reserve.
- Between the villages of Springford and Otterville is Spittler Creek which is zoned as Environmental Protection. This designation falls within the County's Natural Heritage System.
- Within Otterville, the Otter Creek floodplain crosses Oxford Rd 19. As floodplain falls within the Open Space designation, this land also falls within the County's Natural Heritage System.
- Below the floodplain to the south of Oxford Road 19, the Environmental Protection designation begins.



SUPPORTING STUDIES

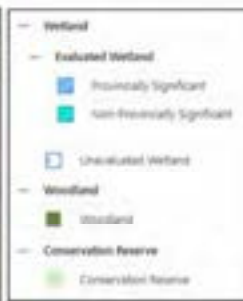
- Transportation Study
- Natural Environment Assessment
 - Terrestrial Habitat Assessment
 - Aquatic Habitat Assessment
- Stage 1 Archaeological Assessment
- Cultural Heritage Resource Assessment
- Air and Noise Impact Assessments
- Stormwater Management Assessment



NATURAL HERITAGE RESOURCES

Designated Features within the Study Area

- Significant valleylands and locally significant natural heritage features
- Watercourses: Spittler Creek, Plumb Creek, and Big Otter Creek.
- Spittler Creek and Big Otter Creek are both associated with significant valleylands
- Otterville Provincially Significant Wetland (PSW) Complex and other wetlands across Spittler Creek
- Woodlands



NATURAL HERITAGE RESOURCES

Aquatic habitat

No Species at Risk (SAR) were found through the desktop review. Field work will be conducted in the coming field season to assess aquatic habitat in the Study Area.

- Big Otter Creek is a warm water creek with spring-spawning species such as Largemouth Bass and Yellow Perch.
- Spittler Creek is a cool water creek with observed spring-spawning species.
- Plumb Creek is a cool water creek with fall and spring-spawning species such as Brown Trout, Creek Chub, and Blacknose Dace.



NATURAL HERITAGE RESOURCES

Terrestrial Habitat

The desktop review found several potential Species At Risk (SAR) such as:

Snapping Turtle (Special Concern), Wood Thrush (Special Concern), American Badger (Endangered), American Chestnut (Endangered)

Field work will be conducted in the coming field season to assess ecological communities present in the Study Area.

Bats

The desktop review found records of 4 bat species: Eastern Small-footed Myotis, Little Brown Myotis, Northern Myotis, and Tricoloured Bat

Impact to potential habitat is not expected. Direct impact is to be avoided with timing vegetation removal to be completed outside of the active season for bats.



SOURCE WATER PROTECTION

The Study Area falls under the Long Point Region Source Water Protection Area and Lake Erie Source Water Protection Area.

Wellhead Protection Areas

Three portions of the study area have been identified to fall under Wellhead Protect Areas (i.e., part of Ostrander, north part of Springford and east of Otterville).



Issue Contributing Area

The area west of Otterville is also considered an Issue Contributing Area (ICA), for its potential to contribute elevated concentrations of particular substances to the drinking water source (e.g., chloride, sodium, nitrate).

Highly Vulnerable Aquifer

A significant part of the Oxford Road corridor (i.e., from west of Otterville to east of Oxford Road 59) is also located in a Highly Vulnerable Aquifer (HVA) area.

Significant Groundwater Recharge Areas

A significant part of the study area is also within Significant Groundwater Recharge Areas (SGRAs)

ARCHAEOLOGICAL RESOURCES

Text

To be inserted once
finalized

CULTURAL HERITAGE RESOURCES

Text

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finalized

FUTURE GROWTH

- By 2046, Oxford County is expected to grow by about 47,000 people and employment is expected to grow by 21,000 jobs. While much of this growth will be in Woodstock, Tillsonburg and Ingersoll, Oxford Road 19 provides an important linkage for the movement of goods and people within the County.
- Traffic growth on Oxford Road 19 is impacted by growth in the Township of Norwich and the Township of South-West Oxford, which has historically been low.
- A small amount of subdivision development is forecasted to occur in the Villages of Otterville and Springford.
- Industrial lands in the Town of Norwich have the potential for development or redevelopment, with Oxford Road 19 providing a linkage to this area and to the County of Norfolk.



Oxford Road 19 provides an important east-west link for the movement of goods and people within the County and supports growth and development in this area.

FUTURE GROWTH: DEVELOPMENTS

- A 9-lot subdivision was constructed in Springford over the last few years and 4 lots currently have draft approval.
- About 42 lots have been developed in Otterville over the last several years, with a 34-lot subdivision currently being built. A further 30-lot subdivision has draft approval in the village.
- In 2019, the Township of Norwich had 172 units approved for residential development. Most of the development in Norwich Township is to occur in the Town of Norwich.



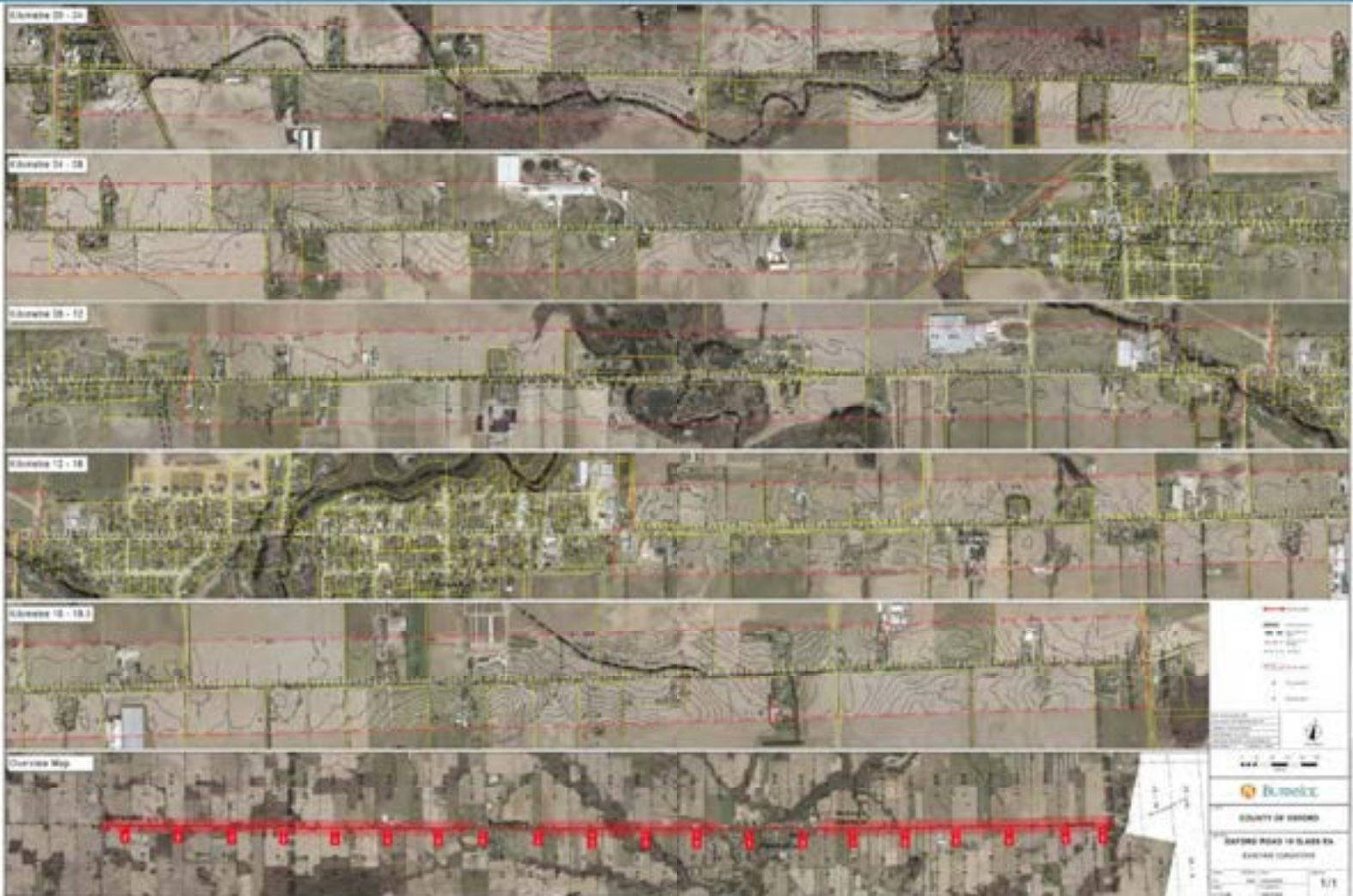
EXISTING ROAD NETWORK AND TRAFFIC CONDITION

- Oxford Road 19 is a significant east-west County road serving the southeast part of the County, connecting the villages of Springford and Otterville and the rural cluster of Ostrander, with connections to Highway 19, Oxford Road 13, Oxford Road 59 and the County of Norfolk.



- Traffic on Oxford Road 19 operates with free flow condition at most intersections within the study area and is only stop controlled at Highway 19 and Oxford Road 59.
- The existing ROW widths, in the rural area of Oxford Road 19 varies from about 20 metres to 30 metres along the corridor, with travel lanes being about 3.35 metres and the existing gravel shoulder widths varying between about 0.6 metres to 2.3 metres.

PROFILE OF EXISTING ROAD ROW CONFIGURATION



TRAFFIC VOLUMES AND TRAFFIC SAFETY

- Annual Average Daily Traffic (AADT) volumes on Oxford Road 19 are forecasted to grow by over 20%, to a total of 4400 vehicles per day (vpd), by 2046. Traffic volumes to the east of Oxford Road 59 are lower (i.e., about 2300 vpd by 2046). Based on these AADT forecasts, no additional travel lanes are required.
- Intersections along Oxford Road 19 corridor are forecasted to have good operations through horizon year 2046, with the exception of the westbound approach to Highway 19, which may warrant the edition of a westbound left-turn lane in this horizon.
- Collision rates along the corridor are in the typical range for arterial County roads, with no significant collision hot spots or patterns.
- The provision of paved shoulders will improve safety for both motor vehicle travel and other users (e.g., pedestrians, cyclists, farm equipment).

EXISTING ROAD CONDITION

STRUCTURAL ADEQUACY AND GEOMETRIC DEFICIENCIES

- Given the typical age of the pavement profile on Oxford Road 19 it is expected that full depth replacement will be required to improve the road structure.
- Geotechnical study indicates the existing pavement structure has an average Granular Base Equivalency (GBE) of about 450 mm, while a GBE of 650 to 750 is recommended.
- The road base is generally over 60 years old and therefore may be near the end of a typical lifecycle for such infrastructure.
- No significant horizontal curves are along Oxford Road 19. A number of vertical sightline deficiencies were identified.



REDUCED LOAD LIMITS ON ROAD NETWORK

- Oxford Road 19 is currently subject to a 5-tonne load limit restriction in the Spring, with the exception of the section from Otterville to Oxford Road 59.
- Improvements to the east-west network of unrestricted roads will improve the connectivity for goods movements to Springford, Otterville, Town of Norwich and north-south unrestricted roads (Highway 19, Oxford Road 13 and Oxford Road 59), as well as to Norfolk County.



ACTIVE TRANSPORTATION

- West section of Oxford Road 19 is within the 5-kilometre Bikeshed Area of Tillsonburg.

- Cycling recreational destination points exist along Oxford Road 19.



- Oxford Road 19 is currently not part of the Primary or Secondary Cycling Networks proposed for the County, although it does provide connectivity to north-south cycling networks (Primary, Secondary and Off-road Trail).

EVALUATION CRITERIA

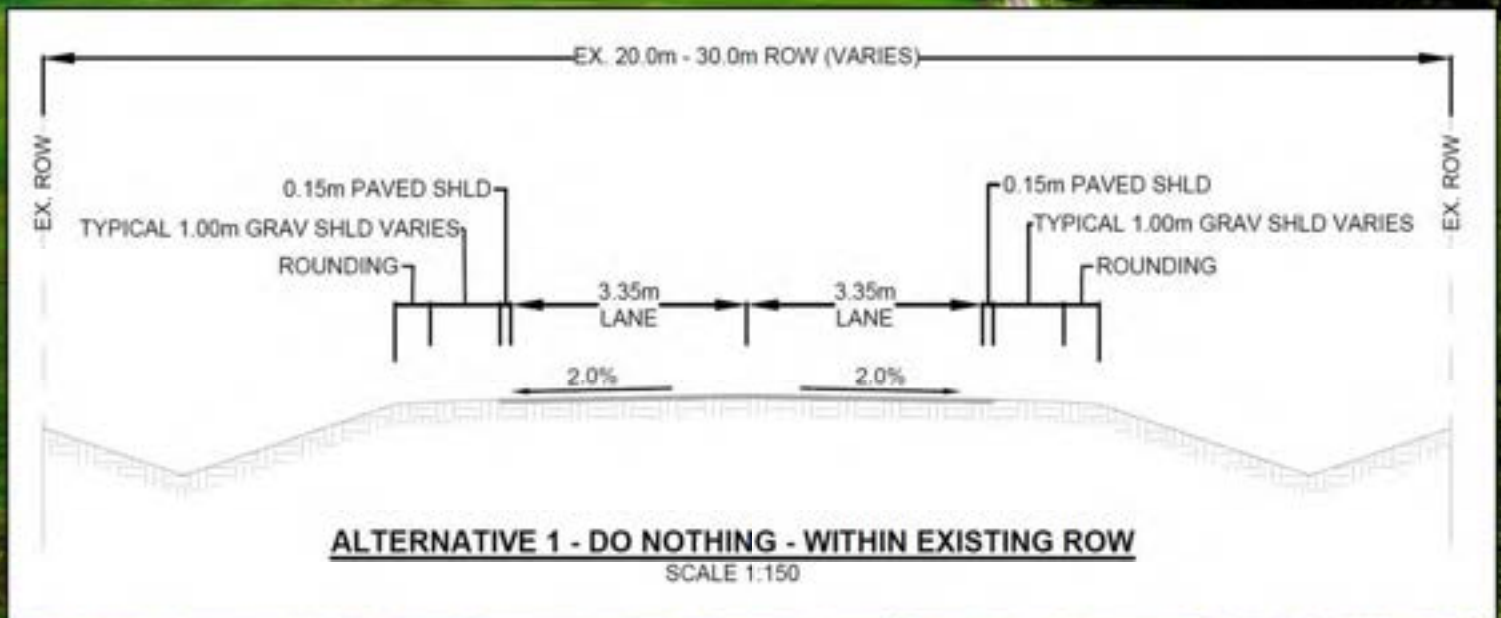
- **Natural Environment**
 - Impacts to vegetation and vegetation communities
 - Impacts to fisheries and aquatic habitat
 - Impacts to terrestrial habitat
 - Impacts to trees
 - Impacts to Woodlots, Wetlands and Designated Features (including PSWs, ESAs, ANSIs, Regional NHS)
 - Impacts to surface water and drainage
 - Impacts to groundwater and source water protection
 - Climate change and natural hazard impacts (erosion, soil stability, flooding)
- **Technical Environment**
 - Accommodation of all types of traffic and modes of travel
 - Improvement to operational safety
 - Road maintenance requirements
 - Impacts to utilities and drainage work/structures
- **Cultural Environment**
 - Impacts to cultural heritage resources
 - Impacts to archaeological resources
- **Socio-Economic Environment**
 - Impacts to private property
 - Compatibility with existing and future land uses
 - Compatibility with active transportation plans or needs
 - Conformity to municipal and agency plans and policies
 - Impacts to air quality and noise levels
 - Impacts to farms and business operations
 - Provision of safe access to private properties and businesses
- **Financial**
 - Capital and operation/maintenance costs
 - Property acquisition cost



Alternative Solutions

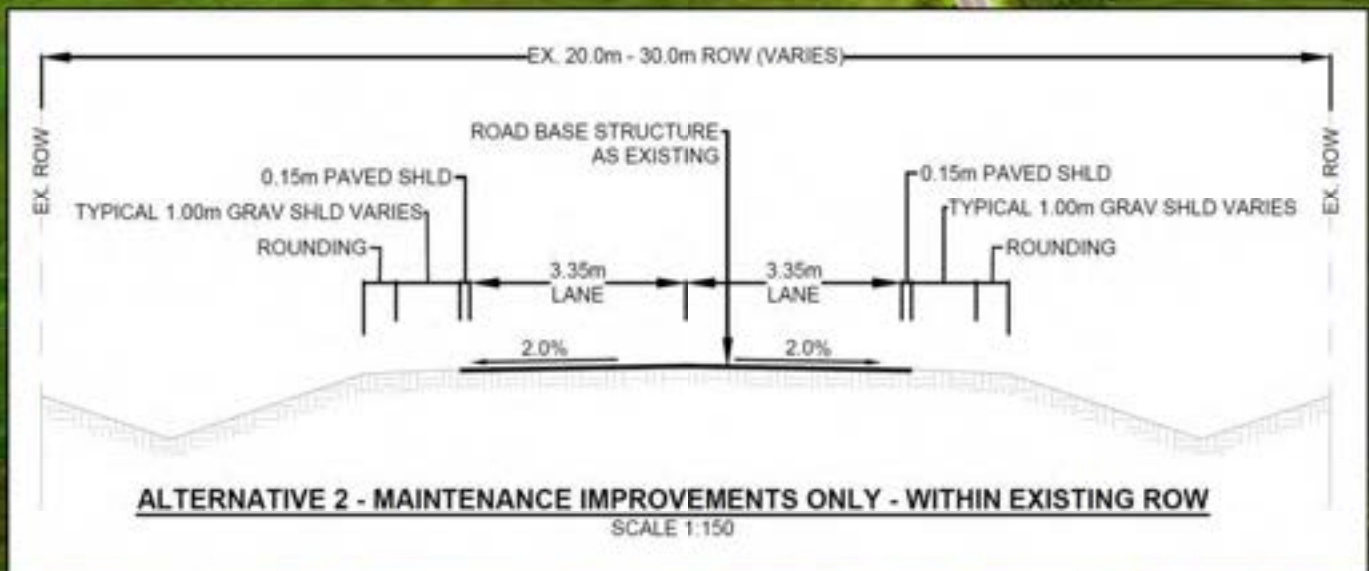
Alternative 1 - Do Nothing.

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.



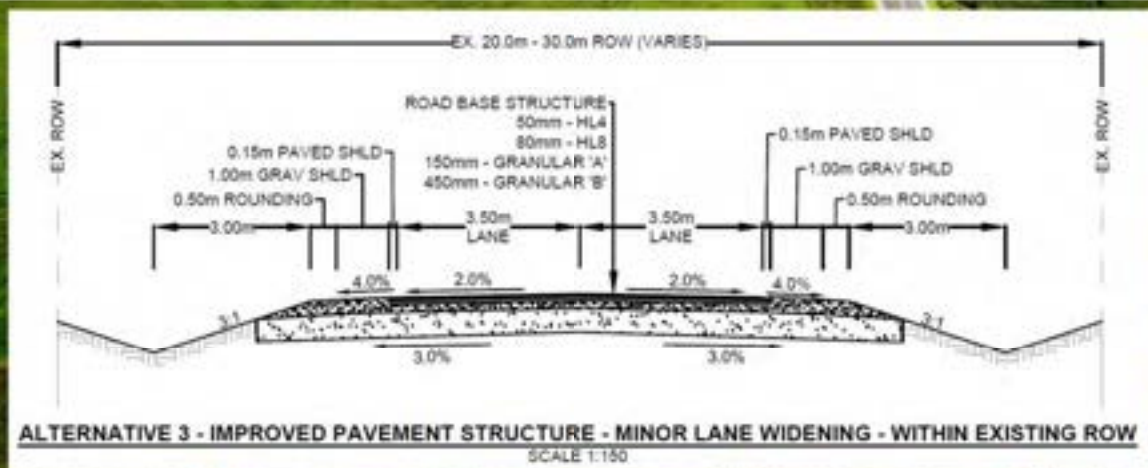
Alternative 2 – Regular Maintenance and Surface Treatments

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.
- All regular maintenance will be performed, as required.



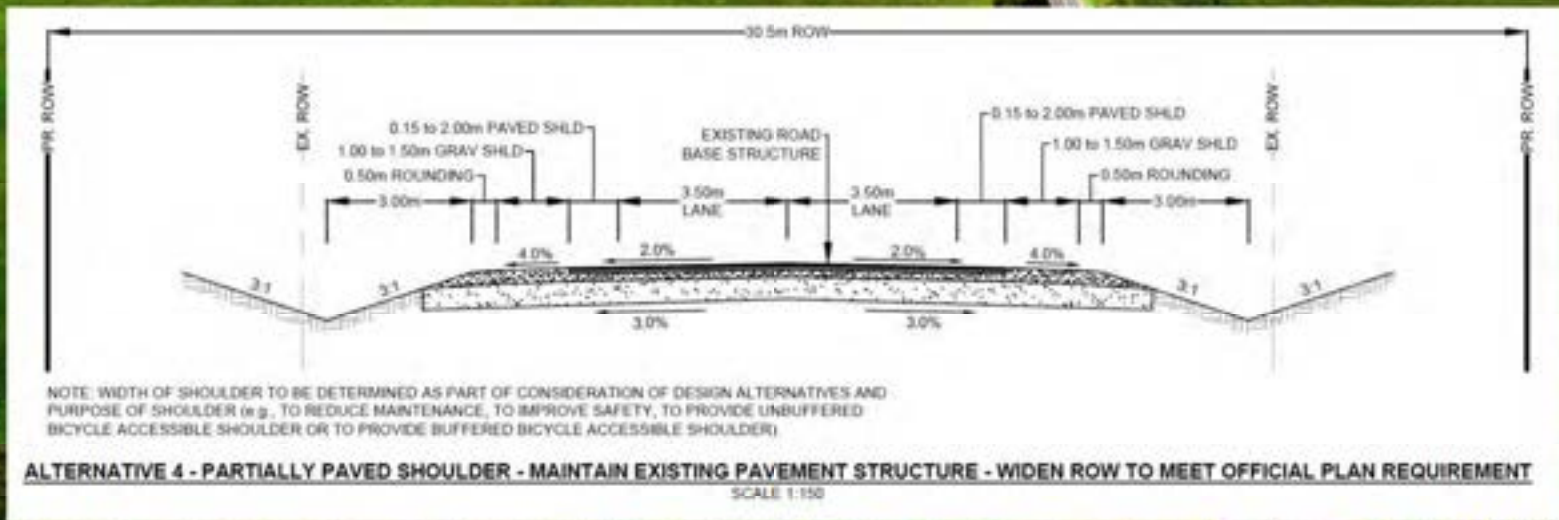
Alternative 3 – Structural Improvements to Road Within Existing ROW

- Provide a two-lane road to County requirement with a gravel shoulder.
- Widening of travel lanes to improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.
- No opportunities for active transportation facilities.
- Addresses the structural condition issues and allows for the Spring half load restriction on the road to be removed.



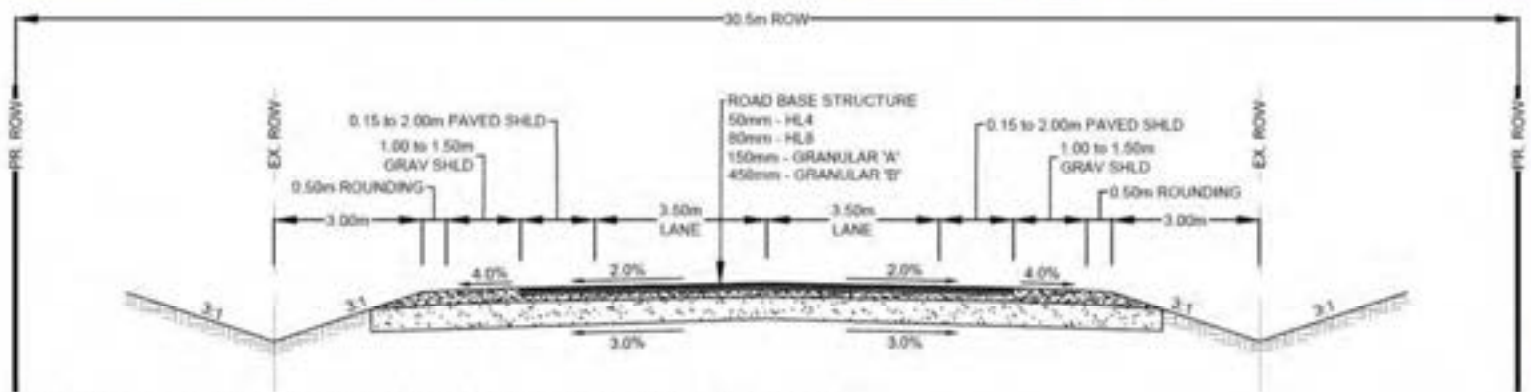
Alternative 4 – Widened Lanes and Shoulders within a Widened ROW- No Structural Improvements to the Road

- Provide a two-lane road to County requirement with paved shoulder wide.
- Widening of travel lanes to improve safety.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.



Alternative 5 – Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW































- Provide a two-lane road to County requirements with paved shoulder.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.







NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 5 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

Evaluation Criteria	Alternative 1: Do Nothing	Alternative 2: Preventive Maintenance Improvements Only	Alternative 3: Structural Improvements to Road Within Existing ROW	Alternative 4: Widening Lanes and Shoulders within a Widened ROW	Alternative 5: Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW
Natural Environment	 <p>No general impact over existing conditions. Increased frequency and severity of adverse climatological events will impact aging infrastructure.</p>	 <p>Minor potential impacts to woodlands, surface and ground water. Risks to be mitigated.</p>	 <p>Minor potential impacts to woodlands and vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.</p>	 <p>Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.</p>	 <p>Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.</p>
Cultural Environment	 <p>No impact over existing conditions.</p>	 <p>No impact over existing conditions.</p>	 <p>No impact over existing conditions.</p>	 <p>There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.</p>	 <p>There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.</p>
Socio-Economic Environment	 <p>No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.</p>	 <p>No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.</p>	 <p>Does not meet the County's Official Plan ROW requirements. Provide benefits to local businesses, allowing effective transport for goods.</p>	 <p>Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses.</p>	 <p>Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses. Provide benefits to local businesses, allowing effective transport for goods.</p>
Technical Environment	 <p>Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.</p>	 <p>Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.</p>	 <p>Facilitates the road's ability to accommodate additional truck traffic. Does not improve the road's ability to accommodate all modes of travel. Some improvements to road safety and maintenance requirements. May impact utilities.</p>	 <p>Facilitates the road's ability to accommodate additional modes of travel. Does not improve the road's ability to accommodate trucks. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.</p>	 <p>Improves the road's ability to accommodate all types of traffic and modes of travel. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.</p>
Financial Environment	 <p>No capital cost. Relatively high O&M cost due to road aging. No property acquisition is required.</p>	 <p>Lowest low capital cost. Moderate to high O&M cost. No property acquisition is required.</p>	 <p>Moderate capital cost. Moderate to high O&M cost. No property acquisition is required.</p>	 <p>High capital cost. Moderate O&M cost. Some property acquisition will be required.</p>	 <p>Highest capital cost. Moderate O&M cost. Some property acquisition will be required.</p>
Adherence to POS	 <p>Does not meet POS</p>	 <p>Partially meet POS</p>	 <p>Partially meet POS</p>	 <p>Partially meet POS</p>	 <p>Fully meets POS</p>
Overall Summary	Not Carried Forward	Not Carried Forward	Not Carried Forward	Not Carried Forward	Carried Forward

Level of Preference: Least Preferred  Less Preferred  Moderately Preferred  More Preferred  Most Preferred 



Next Steps

- Complete additional supporting studies (May 2022)
- Confirm Preferred Solution (June 2022)
- Complete Transportation Study (June 2022)
- Develop and Evaluate Alternative Design Concepts (August 2022)
- Public Information Centre # 2 (September 2022)
- Environmental Study Report (October 2022)
- File Environmental Study Report for Schedule C Class EA Study (November 2022)

Invitation for Participation

Public input is an important component of the decision-making process.

You are invited to provide comments by completing the forms provided and submitting forms to the Study Team members below on or before June 23, 2022.

**Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca**

**Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com**

THANK-YOU FOR ATTENDING



Minutes of Meeting

Meeting Date: May 4, 2022 **Project No.:** 300053425.0000
Project Name: Oxford Rd. 19 Environmental Assessment
Meeting Subject: InTAC Meeting #1
Meeting Location: Video Conference
Date Prepared: May 12, 2022

Those in attendance were:

Jesse Keith	Oxford County (County)	jkeith@oxfordcounty.ca
Shawn Vanacker	Oxford County	svanacker@oxfordcounty.ca
Heather St. Claire	Oxford County	hstclair@oxfordcounty.ca
Melissa Abercrombie	Oxford County	mabercrombie@oxfordcounty.ca
Henry Centen	R.J. Burnside and Associates Ltd. (Burnside)	henry.centen@rjburnside.com
Avid Banihashemi	Burnside	Avid.Banihashemi@rjburnside.com
Jeremy Taylor	Burnside	Jeremy.taylor@rjburnside.com
Chris Pfohl	Burnside	Chris.pfohl@rjburnside.com

The following items were discussed	Action by
<p>1. Project Background</p> <p>1.1 Henry Centen (HC), R.J. Burnside (Burnside) started the meeting with welcoming everyone for attending. The County staff in attendance included representatives from both planning and public works. Burnside staff in attendance are: Avid Banihashemi (AB) taking over for Tricia Radburn, Chris Pfohl (CP), aquatics and ecology, and Jeremy Taylor (JT), to prepare the regional design.</p> <p>HC noted that this meeting is part of the EA consultation efforts per County’s consultation policy. Burnside has met with the County twice earlier, and some technical constraints has been reviewed through these meetings. The External Technical Committee (ExTAC) to be held the following week.</p> <p>The Project has started last November. Survey work and preliminary technical analysis were completed, and a Technical Memo was prepared on technical constraints in January. The Notice of</p>	

The following items were discussed	Action by
<p>Commencement (NOCm) was then released to agencies, Indigenous communities and the public.</p> <p>HC reviewed the purpose of the InTAC meeting and listed the agenda items. He then walked through the draft Public Consultation Centre #1 (PCC#1) slides, highlighting key points on: the project background for Oxford Road 19 improvements, the Study Approach, Existing Conditions, Alternatives and Preliminary Preferred Solution, additional information on the technical constraints and opportunities and a summary of consultation to date. The staff were then invited to provide feedback and comments</p> <p>The draft PIC#1 slides to be sent to County staff by the end of day for their review for any further input, by Wednesday following week, prior to PCC #1.</p>	<p>Burnside</p>
<p>2. Comments and Responses regarding Draft PCC Slides</p>	
<p>2.1 Stage 1 Archaeological Assessment and the Cultural Heritage Resource</p> <p>The Stage 1 Archeological assessment is currently under way which will provide information if a Stage 2 Archaeological Assessment will be required at the detail Design stage of the project. A cultural heritage assessment study has just recently started.</p> <p>Additional information on the preliminary findings of Stage 1 Archaeological Assessment and the Cultural Heritage Resource Assessment to be added to the slides prior to the ExTAC meeting.</p>	<p>Burnside</p>
<p>2.2 Burial Site</p> <p>HC noted that there is some preliminary information regarding an out-of-record cemetery and some discussion has occurred; JK confirmed that the burial site thought to be in the Township of Norwich. Now it is said to be in SW Oxford near the 4th Line is not in Norwich.</p>	
<p>2.3 Existing Conditions (pavement)</p> <p>JT commented on the Existing Road Condition slide whether it should be mentioned about the deteriorating condition of the asphalt or should the slide be more high level. JK notes that the base is noted as being 60 years old which would generally support the need to improve its condition. It was also noted that some surface treatment work will be completed this year on the asphalt as part of</p>	

The following items were discussed

Action by

normal preventive maintenance, however this would only have a minimal impact on the overall road condition.

2.4 Consultation

AB gave a summary of input to date from agencies, Indigenous communities and the public.

It was noted that there had been no concerns from *agencies* thus far, and no comments from any *utilities*.

Indigenous communities have not sent any specific comments or concerns at this time. Mississauga's of the Credit First Nation would like to be involved with Archaeological Assessment field work (if any during the EA) and the Natural Environment field work, however, they do not have capacity at this time. Chippewas of the Thames would like be kept updated on the project.

The Mississauga's of the Credit First Nation have requested to be contacted and involved in the fieldwork, If a Stage 2 Archaeological Assessment is warranted for the Detailed Design.

Public Comments have been of a general nature, such as concern for repeated issues from previous construction projects, and general lack of trust; speed issues; truck movement and whether the improvements will cause addition traffic and noise impact; and that a stronger justification is required for the project.

HC commented on the concern regarding truck movements and noted that the road currently carries truck volumes that are typical of County roads.

3. Ecology Perspective of the Project

CP noted that the road is a standard road from an ecological perspective and there are some impacts to the natural environment to be considered, such as the removal of trees and the potential impacts to bat habitat. No significant bridge works have been identified at this time that would indicate involve the Department of Fisheries and Oceans (DFO), however this will be verified as part of the next phase of the MCEA in the consideration of alternative designs. Staff expect to receive correspondence from the Long Point Region Conservation Authority (LPRCA) regarding flood regulation; Ministry of the Environment and Conservation Parks (MECP).

The following items were discussed	Action by
<p>4. Alternatives Cost</p> <p>JK requested that Burnside prepare a costing of alternatives, as the public at the PCC may enquire. JT noted that Burnside has completed some initial cost estimates and requested at what level of detail the County would like to see. JK noted that a ballpark cost would be adequate at this point.</p> <p>Shawn Vanacker (SV) recommended a cost per metre, not a lump sum, as the public may have an issue with a large dollar figure. JK also noted that this would not be something that would be displayed on a PCC board. JK also requested confirmation from Public Works and if the staff are satisfied with Alternative #5 at all costs. SV noted that staff do need to consider all options, ultimately Alternative 5 is what the staff prefer as an outcome, but dollars do factor in.</p> <p>HC noted Alternative #5 provides the County flexibility in the amount of widening of the shoulders, which would affect the costs, however the ROW is proposed to expand to 30.5 metres to meet the County's Official Plan requirements. JT costed acquisition of additional ROW at \$20 K a hectare. It was noted that recently rural lands have been paid at \$30 K, and that this should be taken into consideration. JK requested that Burnside send estimate and staff will review offline.</p> <p>Heather St. Claire (HSC) asked why the settlement areas have not been included. JK noted that because improvements have already been done in the settlement areas.</p>	Burnside
<p>5. ROW to meet Official Plan</p> <p>HC noted that the project should take into consideration the shoulder requirements to accommodate agricultural traffic, horse and buggy traffic and active transportation needs. This will be a factor in the ongoing consultation with the public. JK noted that this will be reviewed. JK noted that a comment had been received regarding road curvature and if speed is an issue this could impact horses drawn vehicles.</p>	
<p>6. Next Steps</p> <p>HC noted that at the ExTAC meeting on Tuesday attendees will be asked to review PCC slides by the end of the following week. County staff noted that there will be no need to have drainage staff at the ExTAC as the County reviews drainage. JK recommended</p>	Burnside

The following items were discussed


Action by

that the PCC be sent to the drainage staff (Daniel LeDuc) for review.
Burnside to extend the EXTAC invite to drainage staff.

The preceding are the minutes of the meeting as observed by the undersigned. Should there be a need for revision, please advise Burnside within seven days of issuance. In the absence of notification to the contrary, these minutes will be deemed to be an accurate record of the meeting.

Minutes prepared by:

R.J. Burnside & Associates Limited



Henry Centen, P. Eng.
Senior Transportation Engineer
HC:ba/js

Enclosure(s): Public Consultation PowerPoint Presentation

Distribution:
All Attendees

Other than by the addressee, copying or distribution of this document, in whole or in part, is not permitted without the express written consent of R.J. Burnside & Associates Limited.

5/17/2022 9:36 AM



Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements

PUBLIC CONSULTATION CENTRE / OXFORD COUNTY ADMINISTRATION
BUILDING, 21 REEVE STREET, WOODSTOCK, ON, N4S 7Y3
JUNE 9, 2022
5:00 P.M. - 7:00 P.M.

WELCOME

to the Public Consultation Centre for the Oxford Road 19 Corridor Improvements

Please Sign In

- Meet with Study Team Members
- Review the display materials and discuss your questions and ideas with the Study Team
- Please fill out a comment sheet and return it to the Study Team in person, by email or fax by **June 23, 2022**

STUDY AREA

The County is undertaking a Class EA study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study Area includes approximately 16 kilometres of Oxford Road 19 between Highway 19 (Plank Line), and the Norfolk County boundary (Windham Road 19), which excludes the Settlements of Springford and Otterville.



The site location and approximate extent of the Study Area are shown on the map.

PROBLEM/OPPORTUNITY STATEMENT

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

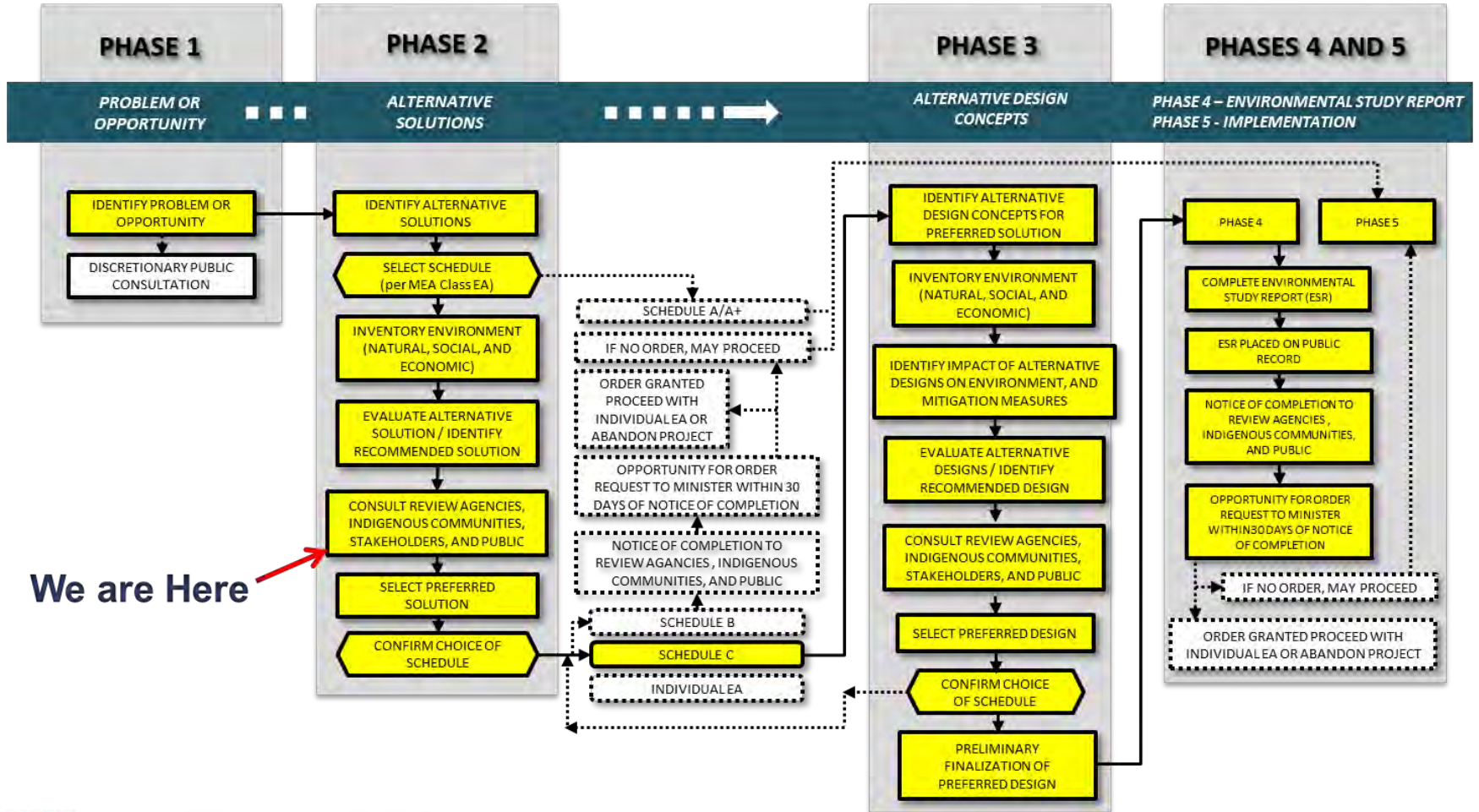


THE EA PROCESS

The Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act.

Nearing completion of the study, an Environmental Study Report (ESR) will be prepared and made available for final public review and comment.

THE EA PROCESS



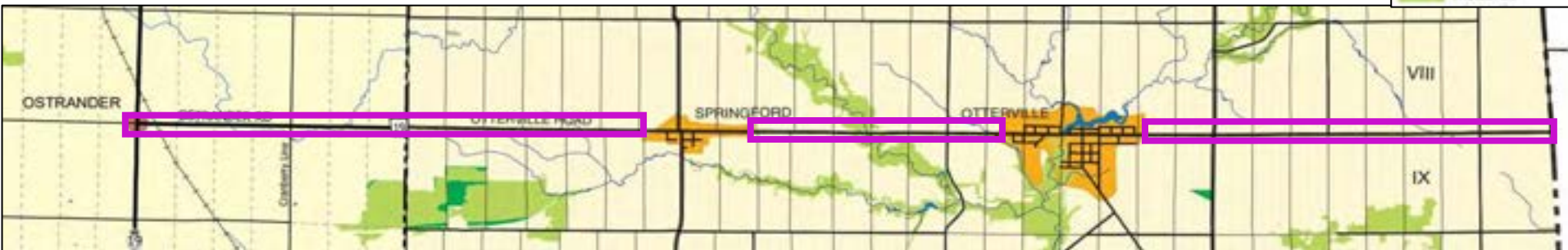
PLANNING CONTEXT

- Provincial Policy Statement, 2020
- County of Oxford Official Plan (consolidation of Official Plan amendments, as of March 31, 2021)
- Oxford County Transportation Master Plan (2019)
- Phase One Comprehensive Review Oxford County (2020)
- Draft Cycling Master Plan (2021)
- TAC Design Guidelines (2017)
- Guidelines from Ontario Traffic Manual – Book 18 (Cycling Facilities)



EXISTING LAND USE

- The two communities of Springford and Otterville within Norwich are zoned as Settlements by the County of Oxford Official Plan.
- Outside of these communities, most land is Agricultural Reserve with lands surrounding Ostrander is a Rural Cluster. All other land in South-West Oxford is Agricultural Reserve.
- Between the villages of Springford and Otterville is Spittler Creek which is zoned as Environmental Protection. This designation falls within the County's Natural Heritage System.
- Within Otterville, the Otter Creek floodplain crosses Oxford Rd 19. As floodplain falls within the Open Space designation, this land also falls within the County's Natural Heritage System.
- Below the floodplain to the south of Oxford Road 19, the Environmental Protection designation begins.



SUPPORTING STUDIES

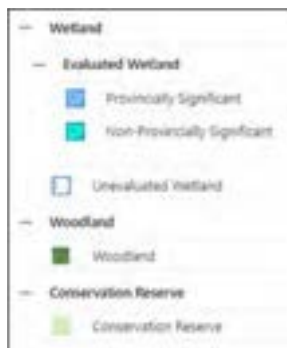
- Transportation Study
- Natural Environment Assessment
 - Terrestrial Habitat Assessment
 - Aquatic Habitat Assessment
- Stage 1 Archaeological Assessment
- Cultural Heritage Resource Assessment
- Air and Noise Impact Assessments
- Stormwater Management Assessment



NATURAL HERITAGE RESOURCES

Designated Features within the Study Area

- Significant valleylands and locally significant natural heritage features
- Watercourses: Spittler Creek, Plumb Creek, and Big Otter Creek.
- Spittler Creek and Big Otter Creek are both associated with significant valleylands
- Otterville Provincially Significant Wetland (PSW) Complex and other wetlands across Spittler Creek
- Woodlands



NATURAL HERITAGE RESOURCES

Aquatic habitat

No Species at Risk (SAR) were found through the desktop review. Field work will be conducted in the coming field season to assess aquatic habitat in the Study Area.

- Big Otter Creek is a warm water creek with spring-spawning species such as Largemouth Bass and Yellow Perch.
- Spittler Creek is a cool water creek with observed spring-spawning species.
- Plumb Creek is a cool water creek with fall and spring-spawning species such as Brown Trout, Creek Chub, and Blacknose Dace.



NATURAL HERITAGE RESOURCES

Terrestrial Habitat

The desktop review found several potential Species At Risk (SAR) such as:

Snapping Turtle (Special Concern), Wood Thrush (Special Concern), American Badger (Endangered), American Chestnut (Endangered)

Field work will be conducted in the coming field season to assess ecological communities present in the Study Area.

Bats

The desktop review found records of 4 bat species: Eastern Small-footed Myotis, Little Brown Myotis, Northern Myotis, and Tricoloured Bat

Impact to potential habitat is not expected. Direct impact is to be avoided with timing vegetation removal to be completed outside of the active season for bats.



SOURCE WATER PROTECTION

The Study Area falls under the Long Point Region Source Water Protection Area and Lake Erie Source Water Protection Area.

Wellhead Protection Areas

Three portions of the study area have been identified to fall under Wellhead Protect Areas (i.e., part of Ostrander, north part of Springford and east of Otterville).



Issue Contributing Area

The area west of Otterville is also considered an Issue Contributing Area (ICA), for its potential to contribute elevated concentrations of particular substances to the drinking water source (e.g., chloride, sodium, nitrate).

Highly Vulnerable Aquifer

A significant part of the Oxford Road corridor (i.e., from west of Otterville to east of Oxford Road 59) is also located in a Highly Vulnerable Aquifer (HVA) area.

Significant Groundwater Recharge Areas

A significant part of the study area is also within Significant Groundwater Recharge Areas (SGRAs)

ARCHAEOLOGICAL RESOURCES

Text

To be inserted once
finalized

CULTURAL HERITAGE RESOURCES

Text

To be inserted once
finalized

FUTURE GROWTH

- By 2046, Oxford County is expected to grow by about 47,000 people and employment is expected to grow by 21,000 jobs. While much of this growth will be in Woodstock, Tillsonburg and Ingersoll, Oxford Road 19 provides an important linkage for the movement of goods and people within the County.
- Traffic growth on Oxford Road 19 is impacted by growth in the Township of Norwich and the Township of South-West Oxford, which has historically been low.
- A small amount of subdivision development is forecasted to occur in the Villages of Otterville and Springford.
- Industrial lands in the Town of Norwich have the potential for development or redevelopment, with Oxford Road 19 providing a linkage to this area and to the County of Norfolk.



Oxford Road 19 provides an important east-west link for the movement of goods and people within the County and supports growth and development in this area.

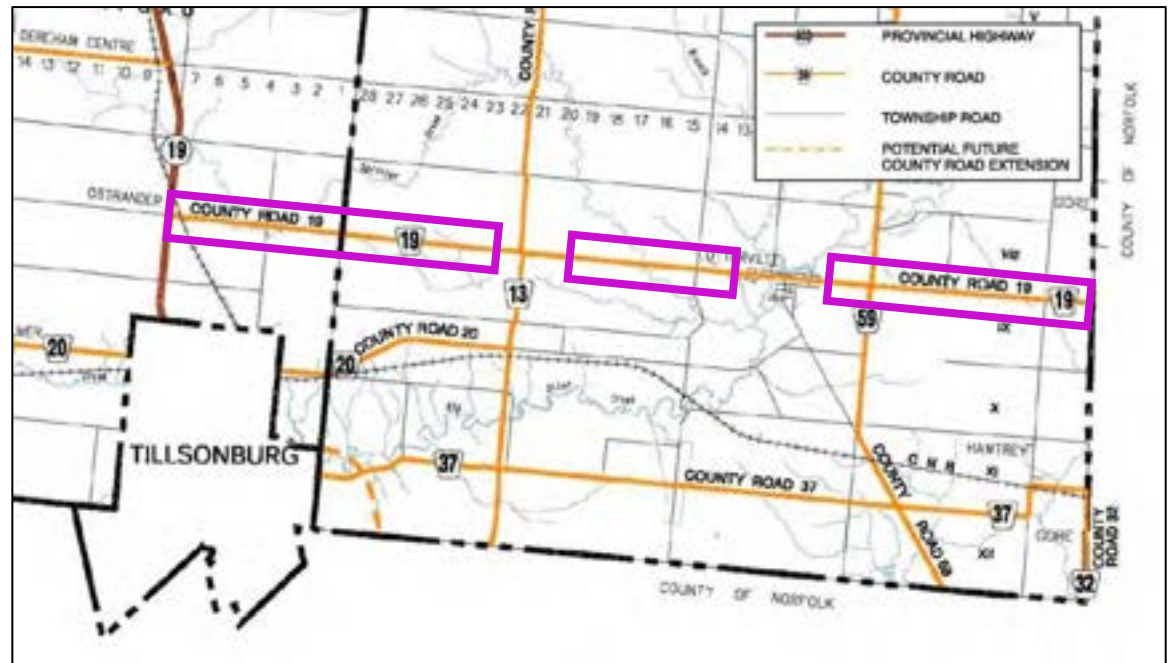
FUTURE GROWTH: DEVELOPMENTS

- A 9-lot subdivision was constructed in Springford over the last few years and 4 lots currently have draft approval.
- About 42 lots have been developed in Otterville over the last several years, with a 34-lot subdivision currently being built. A further 30-lot subdivision has draft approval in the village.
- In 2019, the Township of Norwich had 172 units approved for residential development. Most of the development in Norwich Township is to occur in the Town of Norwich.



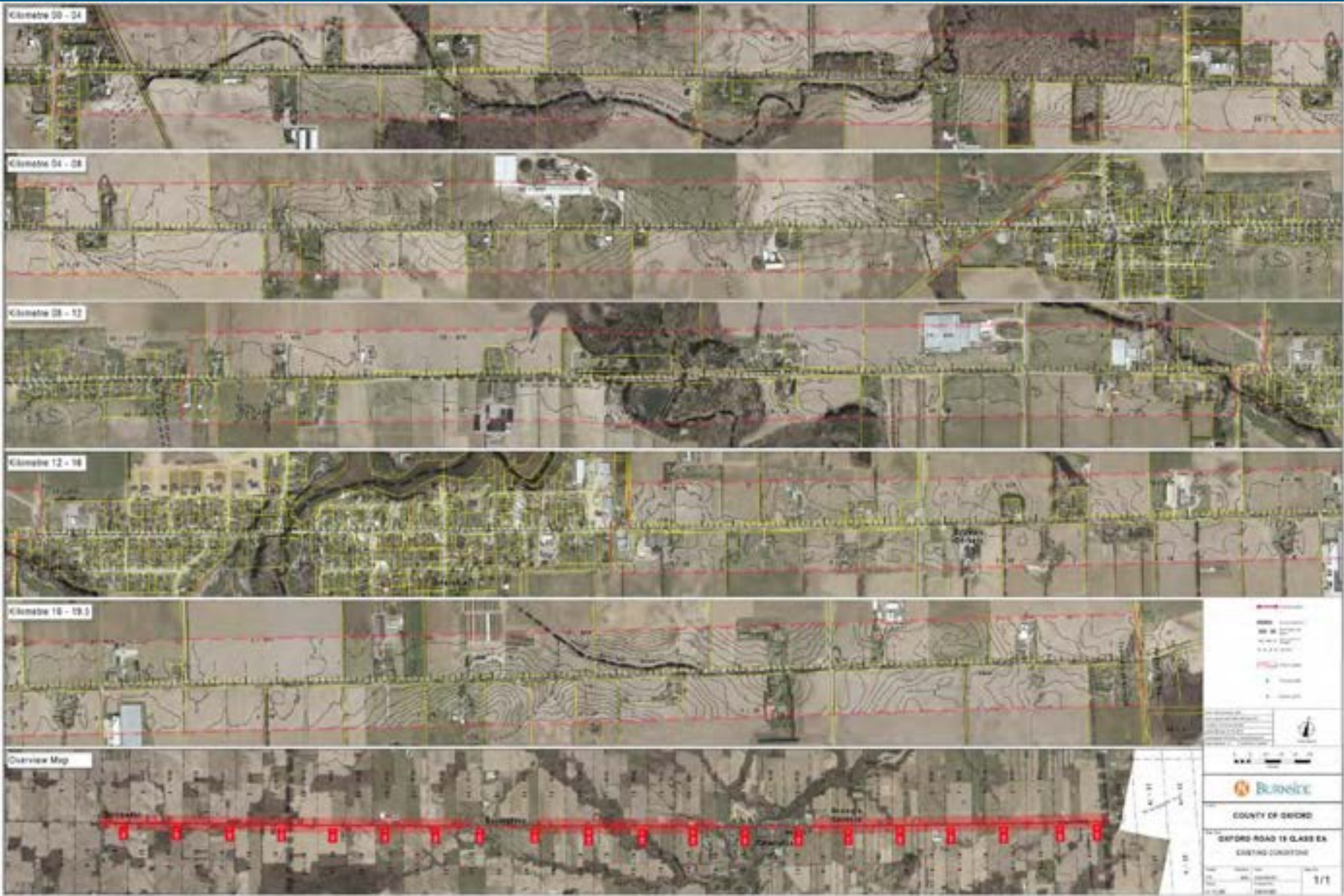
EXISTING ROAD NETWORK AND TRAFFIC CONDITION

- Oxford Road 19 is a significant east-west County road serving the southeast part of the County, connecting the villages of Springford and Otterville and the rural cluster of Ostrander, with connections to Highway 19, Oxford Road 13, Oxford Road 59 and the County of Norfolk.



- Traffic on Oxford Road 19 operates with free flow condition at most intersections within the study area and is only stop controlled at Highway 19 and Oxford Road 59.
- The existing ROW widths, in the rural area of Oxford Road 19 varies from about 20 metres to 30 metres along the corridor, with travel lanes being about 3.35 metres and the existing gravel shoulder widths varying between about 0.6 metres to 2.3 metres.

PROFILE OF EXISTING ROAD ROW CONFIGURATION



TRAFFIC VOLUMES AND TRAFFIC SAFETY

- Annual Average Daily Traffic (AADT) volumes on Oxford Road 19 are forecasted to grow by over 20%, to a total of 4400 vehicles per day (vpd), by 2046. Traffic volumes to the east of Oxford Road 59 are lower (i.e., about 2300 vpd by 2046). Based on these AADT forecasts, no additional travel lanes are required.
- Intersections along Oxford Road 19 corridor are forecasted to have good operations through horizon year 2046, with the exception of the westbound approach to Highway 19, which may warrant the edition of a westbound left-turn lane in this horizon.
- Collision rates along the corridor are in the typical range for arterial County roads, with no significant collision hot spots or patterns.
- The provision of paved shoulders will improve safety for both motor vehicle travel and other users (e.g., pedestrians, cyclists, farm equipment).

EXISTING ROAD CONDITION

STRUCTURAL ADEQUACY AND GEOMETRIC DEFICIENCIES

- Given the typical age of the pavement profile on Oxford Road 19 it is expected that full depth replacement will be required to improve the road structure.
- Geotechnical study indicates the existing pavement structure has an average Granular Base Equivalency (GBE) of about 450 mm, while a GBE of 650 to 750 is recommended.
- The road base is generally over 60 years old and therefore may be near the end of a typical lifecycle for such infrastructure.
- No significant horizontal curves are along Oxford Road 19. A number of vertical sightline deficiencies were identified.



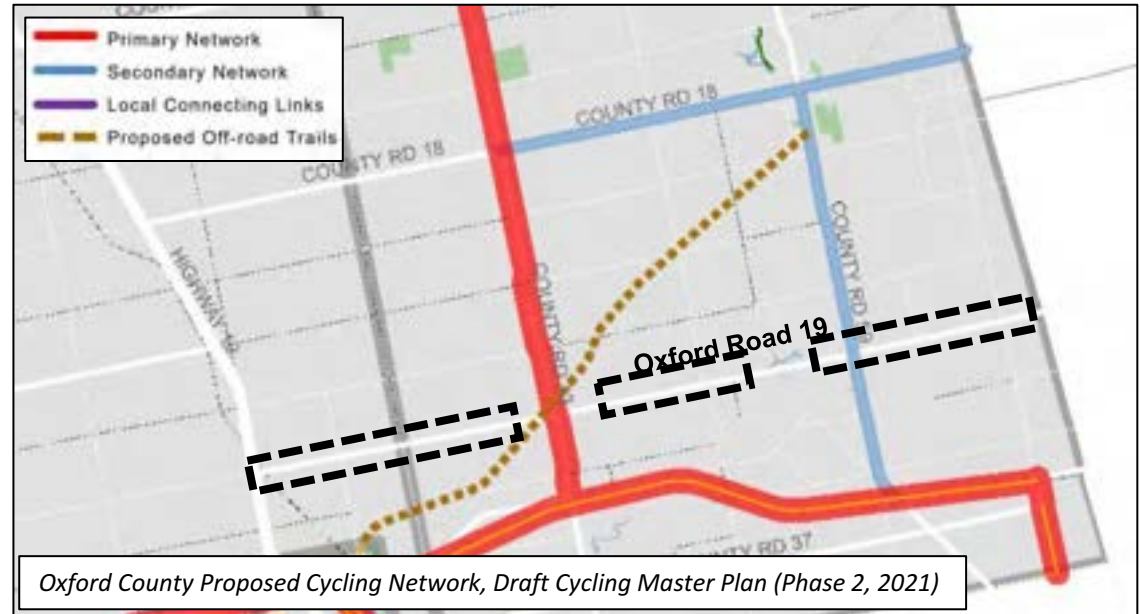
REDUCED LOAD LIMITS ON ROAD NETWORK

- Oxford Road 19 is currently subject to a 5-tonne load limit restriction in the Spring, with the exception of the section from Otterville to Oxford Road 59.
- Improvements to the east-west network of unrestricted roads will improve the connectivity for goods movements to Springford, Otterville, Town of Norwich and north-south unrestricted roads (Highway 19, Oxford Road 13 and Oxford Road 59), as well as to Norfolk County.



ACTIVE TRANSPORTATION

- West section of Oxford Road 19 is within the 5-kilometre Bikeshed Area of Tillsonburg.
- Cycling recreational destination points exist along Oxford Road 19.
- Oxford Road 19 is currently not part of the Primary or Secondary Cycling Networks proposed for the County, although it does provide connectivity to north-south cycling networks (Primary, Secondary and Off-road Trail).



EVALUATION CRITERIA

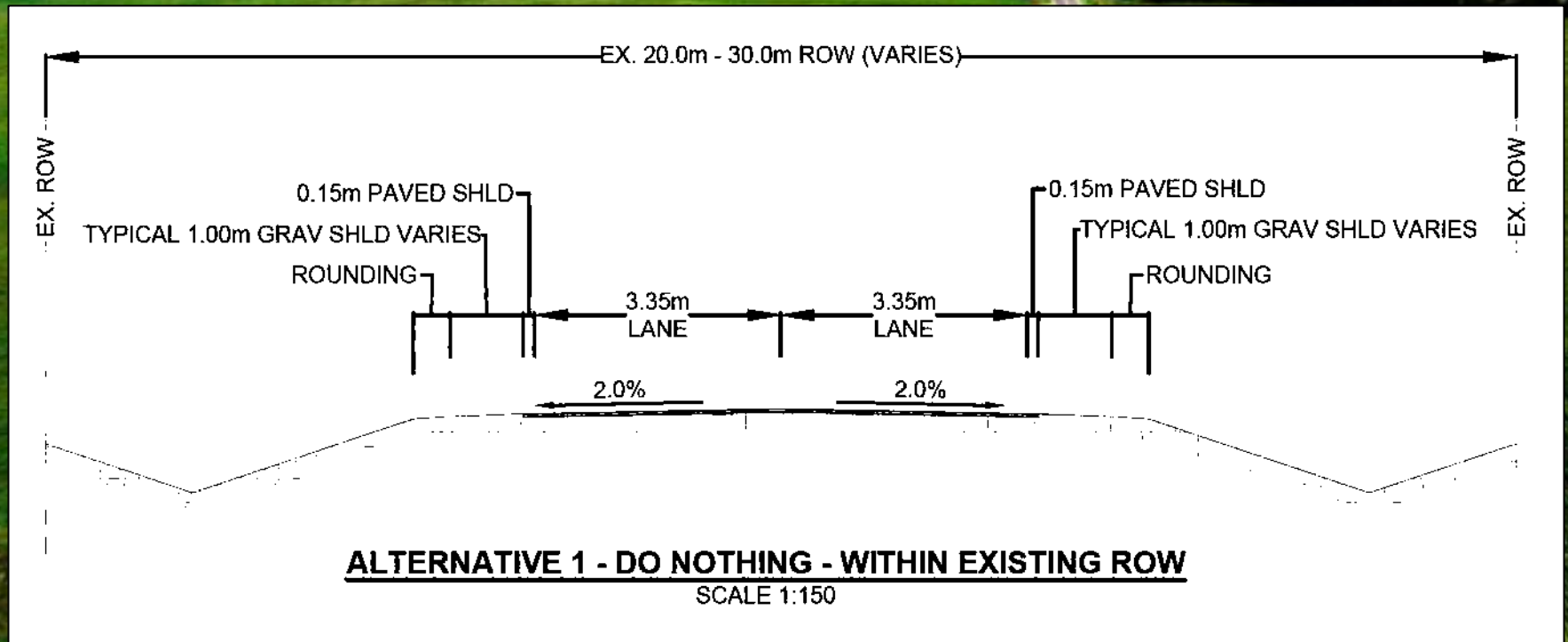
- **Natural Environment**
 - Impacts to vegetation and vegetation communities
 - Impacts to fisheries and aquatic habitat
 - Impacts to terrestrial habitat
 - Impacts to trees
 - Impacts to Woodlots, Wetlands and Designated Features (including PSWs, ESAs, ANSIs, Regional NHS)
 - Impacts to surface water and drainage
 - Impacts to groundwater and source water protection
 - Climate change and natural hazard impacts (erosion, soil stability, flooding)
- **Technical Environment**
 - Accommodation of all types of traffic and modes of travel
 - Improvement to operational safety
 - Road maintenance requirements
 - Impacts to utilities and drainage work/structures
- **Cultural Environment**
 - Impacts to cultural heritage resources
 - Impacts to archaeological resources
- **Socio-Economic Environment**
 - Impacts to private property
 - Compatibility with existing and future land uses
 - Compatibility with active transportation plans or needs
 - Conformity to municipal and agency plans and policies
 - Impacts to air quality and noise levels
 - Impacts to farms and business operations
 - Provision of safe access to private properties and businesses
- **Financial**
 - Capital and operation/maintenance costs
 - Property acquisition cost

An aerial photograph of a rural landscape. A paved road runs diagonally from the bottom right towards the center of the frame. The surrounding area is filled with vibrant green fields, some of which appear to be planted in rows, possibly for agriculture. In the distance, there are clusters of trees and a few buildings, including what looks like a farm or a small village. The sky is a clear, bright blue with a few wispy clouds near the horizon.

Alternative Solutions

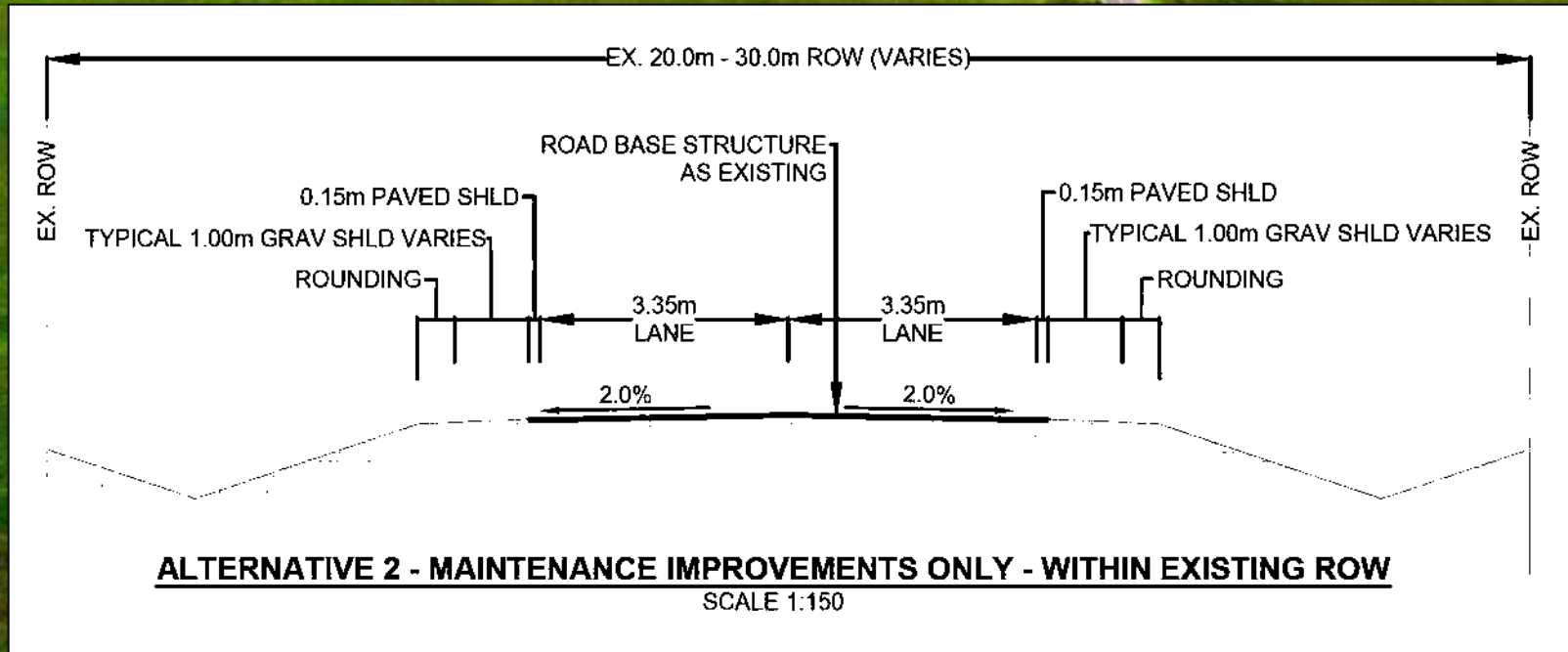
Alternative 1 - Do Nothing.

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.



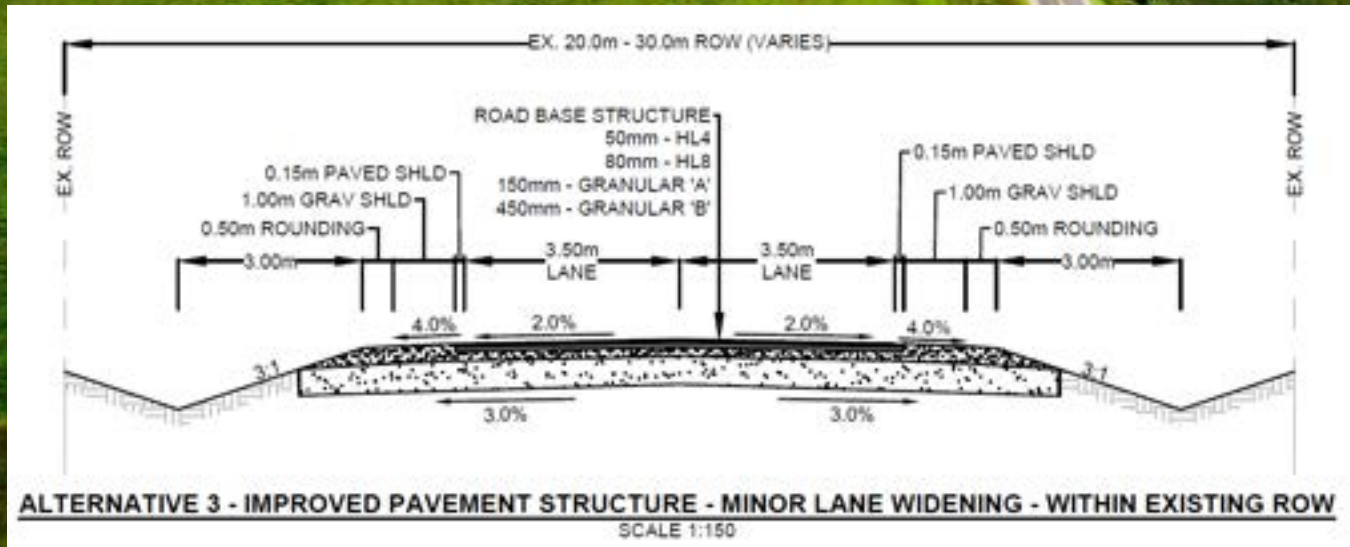
Alternative 2 – Regular Maintenance and Surface Treatments

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.
- All regular maintenance will be performed, as required.



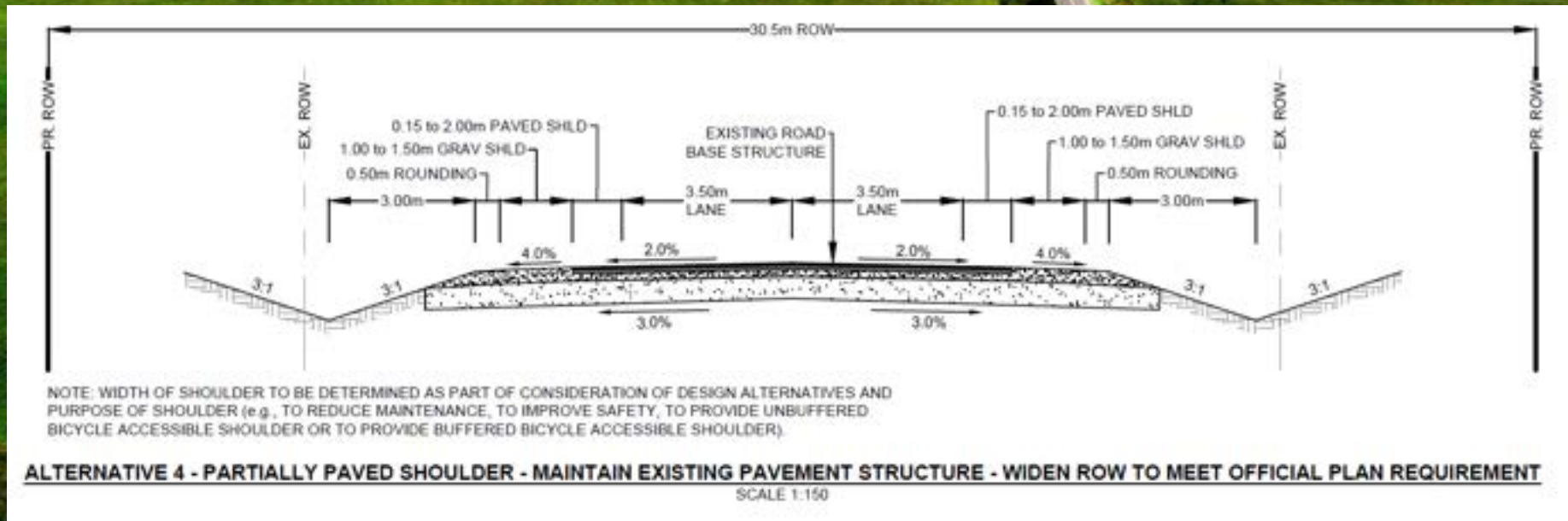
Alternative 3 – Structural Improvements to Road Within Existing ROW

- Provide a two-lane road to County requirement with a gravel shoulder.
- Widening of travel lanes to improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.
- No opportunities for active transportation facilities.
- Addresses the structural condition issues and allows for the Spring half load restriction on the road to be removed.



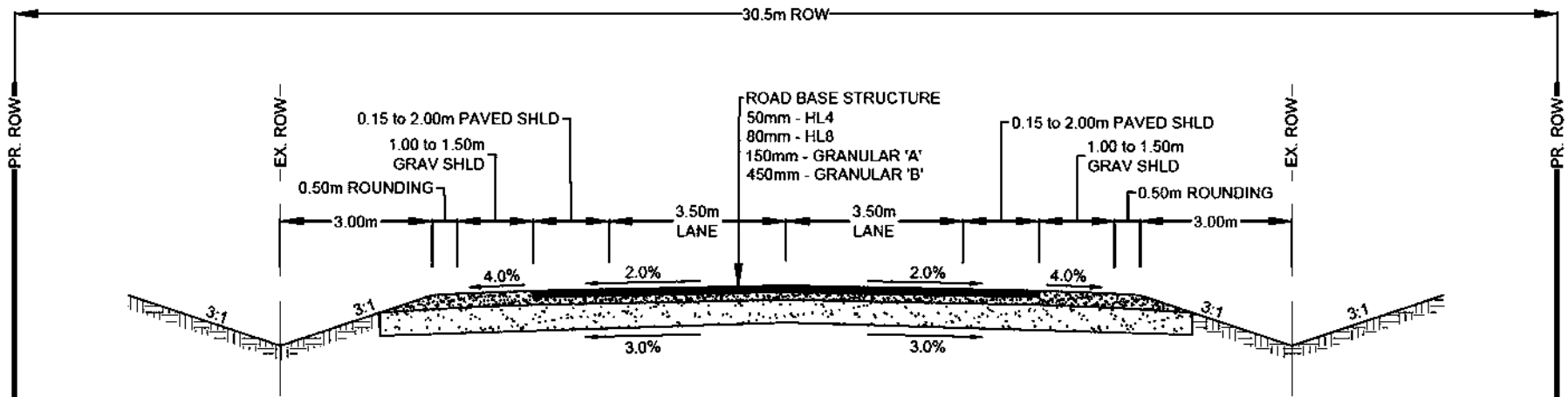
Alternative 4 – Widened Lanes and Shoulders within a Widened ROW- No Structural Improvements to the Road

- Provide a two-lane road to County requirement with paved shoulder wide.
- Widening of travel lanes to improve safety.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.



Alternative 5 – Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW

- Provide a two-lane road to County requirements with paved shoulder.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.



NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 5 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

Evaluation Criteria	Alternative 1: Do Nothing		Alternative 2: Preventive Maintenance Improvements Only		Alternative 3: Structural Improvements to Road Within Existing ROW		Alternative 4: Widening Lanes and Shoulders within a Widened ROW		Alternative 5: Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW	
Natural Environment		No general impact over existing conditions. Increased frequency and severity of adverse climatological events will impact aging infrastructure.		Minor potential impacts to woodlands, surface and ground water. Risks to be mitigated.		Minor potential impacts to woodlands and vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.		Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.		Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.
Cultural Environment		No impact over existing conditions.		No impact over existing conditions.		No impact over existing conditions.		There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.		There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.
Socio-Economic Environment		No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.		No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.		Does not meet the County's Official Plan ROW requirements. provide benefits to local businesses, allowing effective transport for goods.		Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses		Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses. Provide benefits to local businesses, allowing effective transport for goods.
Technical Environment		Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.		Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.		Facilitates the road's ability to accommodate additional truck traffic. Does not improve the road's ability to accommodate all modes of travel. Some improvements to road safety and maintenance requirements. May impact utilities		Facilitates the road's ability to accommodate additional modes of travel. Does not improve the road's ability to accommodate trucks. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.		Improves the road's ability to accommodate all types of traffic and modes of travel. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.
Financial Environment		No capital cost. Relatively high O&M cost due to road aging. No property acquisition is required.		Lowest low capital cost. Moderate to high O&M cost. No property acquisition is required.		Moderate capital cost. Moderate to high O&M cost. No property acquisition is required.		High capital cost. Moderate O&M cost. Some property acquisition will be required.		Highest capital cost. Moderate O&M cost. Some property acquisition will be required.
Adherence to POS		Does not meet POS		Partially meet POS		Partially meet POS		Partially meet POS		Fully meets POS
Overall Summary	Not Carried Forward		Not Carried Forward		Not Carried Forward		Not Carried Forward		Carried Forward	

Level of Preference: Least Preferred Less Preferred Moderately Preferred More Preferred Most Preferred

Next Steps

- Complete additional supporting studies (May 2022)
- Confirm Preferred Solution (June 2022)
- Complete Transportation Study (June 2022)
- Develop and Evaluate Alternative Design Concepts (August 2022)
- Public Information Centre # 2 (September 2022)
- Environmental Study Report (October 2022)
- File Environmental Study Report for Schedule C Class EA Study (November 2022)

Invitation for Participation

Public input is an important component of the decision-making process.

You are invited to provide comments by completing the forms provided and submitting forms to the Study Team members below on or before June 23, 2022.

**Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca**

**Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com**

THANK-YOU FOR ATTENDING



Minutes of Meeting

Meeting Date: May 10, 2022 **Project No.:** 300053425.0000
Project Name: Oxford Rd. 19 Environmental Assessment
Meeting Subject: ExTAC Meeting #1
Meeting Location: Video Conference
Date Prepared: May 13, 2022

Those in attendance were:

Mark Badali	MECP	Mark.Badali1@ontario.ca
Isabel Johnson	LPRCA	ijohnson@lprca.on.ca
Martin Leyten	MTO	Martin.Leyten@ontario.ca
Jesse Keith	Oxford County (County)	jkeith@oxfordcounty.ca
Shawn Vanacker	County	svanacker@oxfordcounty.ca
Melissa Abercrombie	County	mabercrombie@oxfordcounty.ca
Henry Centen	R.J. Burnside and Associates Ltd. (Burnside)	henry.centen@rjburnside.com
Avid Banihashemi	Burnside	Avid.Banihashemi@rjburnside.com
Jeremy Taylor	Burnside	Jeremy.taylor@rjburnside.com
Amy Boddy	Burnside	Amy.Boddy@rjburnside.com
Chris Pfohl	Burnside	Chris.pfohl@rjburnside.com

The following items were discussed

Action by

1. Project Background

1.1 The meeting started by welcoming everyone and a round of introduction.

Henry Centen (HC), R.J. Burnside (Burnside) reviewed the purpose of the ExTAC meeting and walked through the listed items on the agenda. The agenda included the following items: project background for Oxford Road 19 improvements, summary of the Study Approach and existing conditions, a list of Alternatives and Preliminary Preferred Solution, additional information on the technical constraints and opportunities, and a summary of consultation to date.

The following items were discussed

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Project started last November, survey work, technical analysis and completed Technical Memo in January of the technical constraints. Following the release of Technical Memo Burnside met with County and the Notice of Commencement (NOCm) was released to agencies, Indigenous communities and the public.

HC noted that he will review with EXTAC the slides which would form the basis of the material that will be presented at the upcoming Public Consultation Centre (PCC #1). The presentation will be provided to the ExTAC members along with the meeting minutes for further review.

1.2 Consultation

Avid Banihashemi (AB) gave a summary of input to date from agencies, Indigenous communities and the public.

It was noted that there had been no concerns from *agencies* thus far, and no comments from any *utilities*.

Indigenous communities have not sent any specific comments or concerns at this time. However, correspondence from both Chippewas of the Thames and Mississauga's of the Credit First Nation would like to be involved, with Archaeological Assessment and the Natural Environment work, however, have no capacity at this time.

Stage 1 Archaeological Assessment is a desk top exercise and if a Stage 2 is indicated it will be done at detailed design. If a Stage 2 is warranted the Mississauga's of the Credit First Nation have requested to be contacted and involved in the field work.

Public Comments have been of a general nature, such as concern for repeated issues from previous construction projects and how they impacted local farms and businesses, speed issues, truck movement and whether the improvements will cause addition traffic and noise impact, some road visibility issues (road vertical alignment), and some drainage issues along the road.

2. EXTAC Members Comments

2.1 LPCRA Comments

Isabel Johnson (IJ) noted that the CA's preliminary comment would be to general maintaining the same morphology and have sediment and erosion control measures in place during construction. Also, would like to be circulate on the storm water management

The following items were discussed

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assessment for the study when it becomes available to get the opportunity to review the aspects regarding the natural hazards and natural heritage.

Chris Pfohl (CP) noted that the aquatic team will be going out in the field later in the week to review the conditions related to terrestrial, SAR, and aquatic conditions to confirm the background desktop review findings.

HC noted that there is some preliminary information regarding a cemetery and some discussion has occurred; as well as some Indigenous communities have been notified and provided input. If there is a cemetery confirmed a Stage 2 Archaeological Assessment will be required.

Cultural Heritage slide information will not be available for PCC #1.

County staff enquired whether text should be added to the slides regarding the Stage 1 Archaeological Assessment and the Cultural Heritage Resource; and that the text be added prior to the ExTAC meeting.

Jesse Keith (JK) agreed that some text should be added prior to ExTAC meeting. JT noted that Burnside would add in text of preliminary findings.

2.2 MTO Comments

Martin Leyten (ML) noted that as the project moves along any improvements to the intersection of Oxford Road 19 and Plank Line (Road 19) needs to be reviewed by MTO.

2.3 MECP Comments

Mark Badali (MB) noted that at this stage MECP does not have any major comments. In the future stages one the preferred alternative is confirmed, mitigation measures can be discussed with MECP.

In terms of the consultation with Indigenous Communities, MECO encourages you to reach out to each and every community as early in the process as feasible and continue to follow up as best as you can to form a meaningful engagement.

The Ministry encourages the Study Team to submit a draft ESR to the Ministry for review (with a minimal of 30-day review period), just to tackle any major risks, including the risk and mitigation of the potential of receiving a Section 16 Order.

The following items were discussed	Action by
AB noted that as part of the consultation process follow up contacts are made (calls or emails) after each notice send-out to each of the indigenous communities to confirm that they received the notice and if they have any questions or concerns.	
3. Adjournment The meeting was ajourned by HC and the county thanking everyone for attending.	

The preceding are the minutes of the meeting as observed by the undersigned. Should there be a need for revision, please advise Burnside within seven days of issuance. In the absence of notification to the contrary, these minutes will be deemed to be an accurate record of the meeting.

Minutes prepared by:

R.J. Burnside & Associates Limited



Henry Centen, P. Eng.

Senior Transportation Engineer

HC:ba/js

Attachments:

Meeting Agenda

Draft PCC#1 Slides

Distribution:

All Attendees

Other than by the addressee, copying or distribution of this document, in whole or in part, is not permitted without the express written consent of R.J. Burnside & Associates Limited.



Meeting Agenda

Date and Time: May 10, 2022 11:00am-12:00pm **Project No.:** 53425.0000

Project Name: Oxford Road 19 Environmental Assessment

Meeting Subject: Ex-TAC Meeting #1

Meeting Location: Teams Teleconference

Items

- Project background
- Study approach
- Existing conditions
- List alternatives and preliminary preferred solution
- Input on technical constraints and opportunities
- Summary of consultation efforts and feedback to date



Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements

PUBLIC CONSULTATION CENTRE / OXFORD COUNTY ADMINISTRATION
BUILDING, 21 REEVE STREET, WOODSTOCK, ON, N4S 7Y3
JUNE 9, 2022
5:00 P.M. - 7:00 P.M.

WELCOME

to the **Public Consultation Centre for the
Oxford Road 19 Corridor Improvements**

Please Sign In

- Meet with Study Team Members
- Review the display materials and discuss your questions and ideas with the Study Team
- Please fill out a comment sheet and return it to the Study Team in person, by email or fax by **June 23, 2022**

STUDY AREA

The County is undertaking a Class EA study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study Area includes approximately 16 kilometres of Oxford Road 19 between Highway 19 (Plank Line), and the Norfolk County boundary (Windham Road 19), which excludes the Settlements of Springford and Otterville.



The site location and approximate extent of the Study Area are shown on the map.

PROBLEM/OPPORTUNITY STATEMENT

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

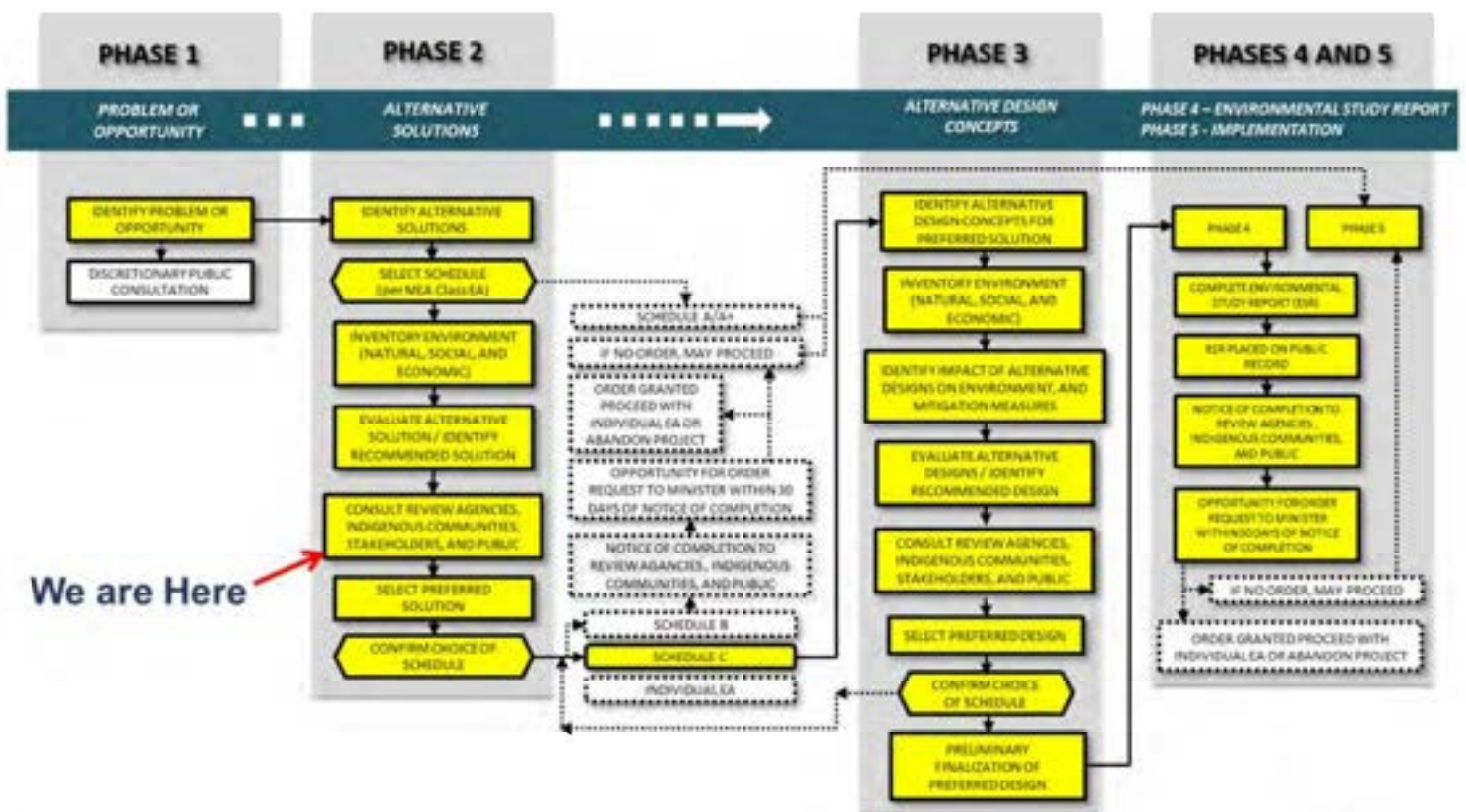


THE EA PROCESS

The Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act.

Nearing completion of the study, an Environmental Study Report (ESR) will be prepared and made available for final public review and comment.

THE EA PROCESS



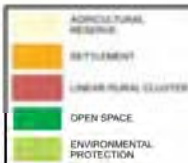
PLANNING CONTEXT

- Provincial Policy Statement, 2020
- County of Oxford Official Plan (consolidation of Official Plan amendments, as of March 31, 2021)
- Oxford County Transportation Master Plan (2019)
- Phase One Comprehensive Review Oxford County (2020)
- Draft Cycling Master Plan (2021)
- TAC Design Guidelines (2017)
- Guidelines from Ontario Traffic Manual – Book 18 (Cycling Facilities)



EXISTING LAND USE

- The two communities of Springford and Otterville within Norwich are zoned as Settlements by the County of Oxford Official Plan.
- Outside of these communities, most land is Agricultural Reserve with lands surrounding Ostrander is a Rural Cluster. All other land in South-West Oxford is Agricultural Reserve.
- Between the villages of Springford and Otterville is Spittler Creek which is zoned as Environmental Protection. This designation falls within the County's Natural Heritage System.
- Within Otterville, the Otter Creek floodplain crosses Oxford Rd 19. As floodplain falls within the Open Space designation, this land also falls within the County's Natural Heritage System.
- Below the floodplain to the south of Oxford Road 19, the Environmental Protection designation begins.



SUPPORTING STUDIES

- Transportation Study
- Natural Environment Assessment
 - Terrestrial Habitat Assessment
 - Aquatic Habitat Assessment
- Stage 1 Archaeological Assessment
- Cultural Heritage Resource Assessment
- Air and Noise Impact Assessments
- Stormwater Management Assessment



NATURAL HERITAGE RESOURCES

Designated Features within the Study Area

- Significant valleylands and locally significant natural heritage features
- Watercourses: Spittler Creek, Plumb Creek, and Big Otter Creek.
- Spittler Creek and Big Otter Creek are both associated with significant valleylands
- Otterville Provincially Significant Wetland (PSW) Complex and other wetlands across Spittler Creek
- Woodlands



NATURAL HERITAGE RESOURCES

Aquatic habitat

No Species at Risk (SAR) were found through the desktop review. Field work will be conducted in the coming field season to assess aquatic habitat in the Study Area.

- Big Otter Creek is a warm water creek with spring-spawning species such as Largemouth Bass and Yellow Perch.
- Spittler Creek is a cool water creek with observed spring-spawning species.
- Plumb Creek is a cool water creek with fall and spring-spawning species such as Brown Trout, Creek Chub, and Blacknose Dace.



NATURAL HERITAGE RESOURCES

Terrestrial Habitat

The desktop review found several potential Species At Risk (SAR) such as:

Snapping Turtle (Special Concern), Wood Thrush (Special Concern), American Badger (Endangered), American Chestnut (Endangered)

Field work will be conducted in the coming field season to assess ecological communities present in the Study Area.

Bats

The desktop review found records of 4 bat species: Eastern Small-footed Myotis, Little Brown Myotis, Northern Myotis, and Tricoloured Bat

Impact to potential habitat is not expected. Direct impact is to be avoided with timing vegetation removal to be completed outside of the active season for bats.



SOURCE WATER PROTECTION

The Study Area falls under the Long Point Region Source Water Protection Area and Lake Erie Source Water Protection Area.

Wellhead Protection Areas

Three portions of the study area have been identified to fall under Wellhead Protect Areas (i.e., part of Ostrander, north part of Springford and east of Otterville).



Issue Contributing Area

The area west of Otterville is also considered an Issue Contributing Area (ICA), for its potential to contribute elevated concentrations of particular substances to the drinking water source (e.g., chloride, sodium, nitrate).

Highly Vulnerable Aquifer

A significant part of the Oxford Road corridor (i.e., from west of Otterville to east of Oxford Road 59) is also located in a Highly Vulnerable Aquifer (HVA) area.

Significant Groundwater Recharge Areas

A significant part of the study area is also within Significant Groundwater Recharge Areas (SGRAs)

ARCHAEOLOGICAL RESOURCES

Text

To be inserted once finalized

CULTURAL HERITAGE RESOURCES

Text

To be inserted once
finalized

FUTURE GROWTH

- By 2046, Oxford County is expected to grow by about 47,000 people and employment is expected to grow by 21,000 jobs. While much of this growth will be in Woodstock, Tillsonburg and Ingersoll, Oxford Road 19 provides an important linkage for the movement of goods and people within the County.
- Traffic growth on Oxford Road 19 is impacted by growth in the Township of Norwich and the Township of South-West Oxford, which has historically been low.
- A small amount of subdivision development is forecasted to occur in the Villages of Otterville and Springford.
- Industrial lands in the Town of Norwich have the potential for development or redevelopment, with Oxford Road 19 providing a linkage to this area and to the County of Norfolk.



Oxford Road 19 provides an important east-west link for the movement of goods and people within the County and supports growth and development in this area.

FUTURE GROWTH: DEVELOPMENTS

- A 9-lot subdivision was constructed in Springford over the last few years and 4 lots currently have draft approval.
- About 42 lots have been developed in Otterville over the last several years, with a 34-lot subdivision currently being built. A further 30-lot subdivision has draft approval in the village.
- In 2019, the Township of Norwich had 172 units approved for residential development. Most of the development in Norwich Township is to occur in the Town of Norwich.



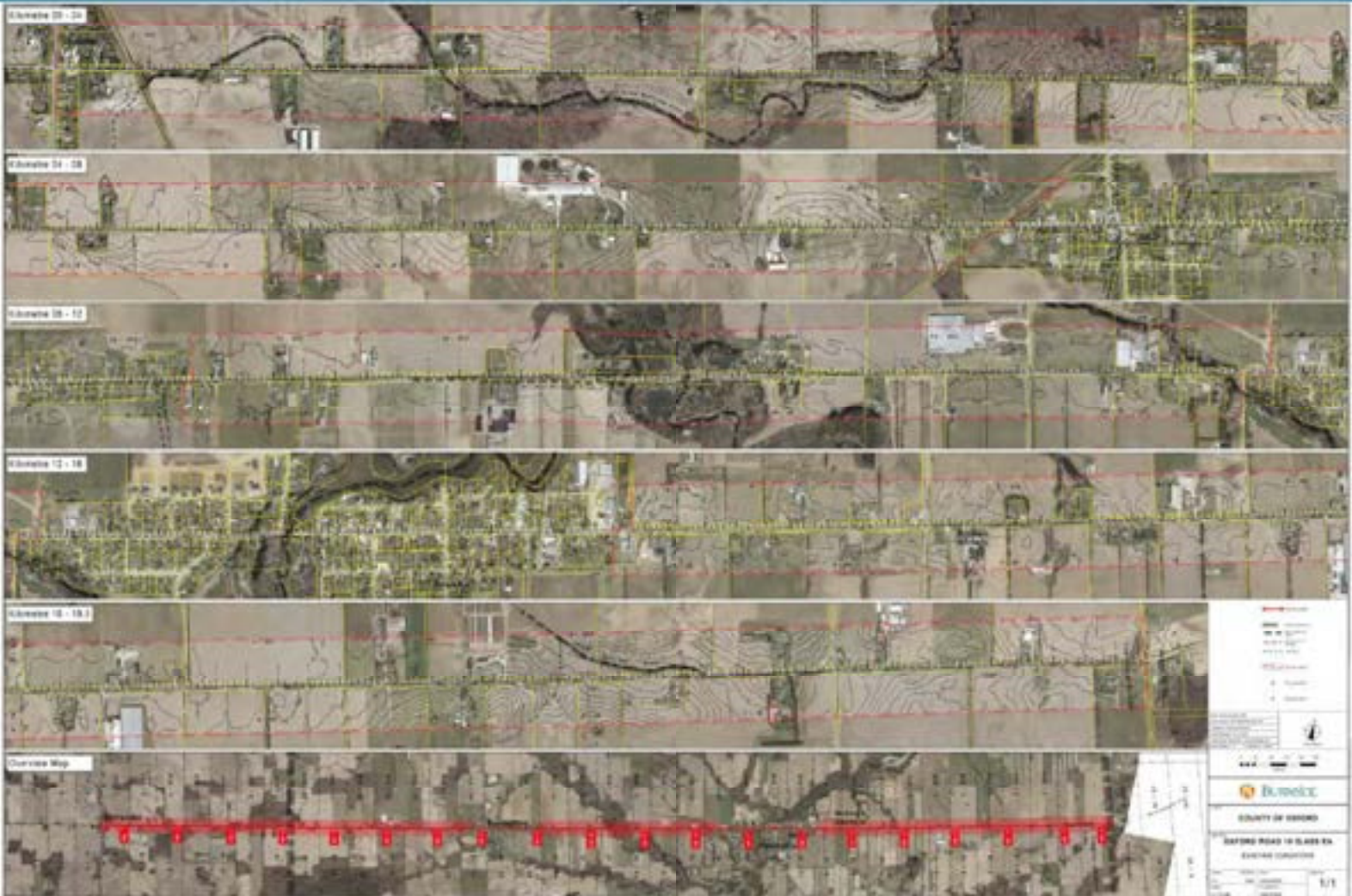
EXISTING ROAD NETWORK AND TRAFFIC CONDITION

- Oxford Road 19 is a significant east-west County road serving the southeast part of the County, connecting the villages of Springford and Otterville and the rural cluster of Ostrander, with connections to Highway 19, Oxford Road 13, Oxford Road 59 and the County of Norfolk.



- Traffic on Oxford Road 19 operates with free flow condition at most intersections within the study area and is only stop controlled at Highway 19 and Oxford Road 59.
- The existing ROW widths, in the rural area of Oxford Road 19 varies from about 20 metres to 30 metres along the corridor, with travel lanes being about 3.35 metres and the existing gravel shoulder widths varying between about 0.6 metres to 2.3 metres.

PROFILE OF EXISTING ROAD ROW CONFIGURATION



TRAFFIC VOLUMES AND TRAFFIC SAFETY

- Annual Average Daily Traffic (AADT) volumes on Oxford Road 19 are forecasted to grow by over 20%, to a total of 4400 vehicles per day (vpd), by 2046. Traffic volumes to the east of Oxford Road 59 are lower (i.e., about 2300 vpd by 2046). Based on these AADT forecasts, no additional travel lanes are required.
- Intersections along Oxford Road 19 corridor are forecasted to have good operations through horizon year 2046, with the exception of the westbound approach to Highway 19, which may warrant the edition of a westbound left-turn lane in this horizon.
- Collision rates along the corridor are in the typical range for arterial County roads, with no significant collision hot spots or patterns.
- The provision of paved shoulders will improve safety for both motor vehicle travel and other users (e.g., pedestrians, cyclists, farm equipment).

EXISTING ROAD CONDITION

STRUCTURAL ADEQUACY AND GEOMETRIC DEFICIENCIES

- Given the typical age of the pavement profile on Oxford Road 19 it is expected that full depth replacement will be required to improve the road structure.
- Geotechnical study indicates the existing pavement structure has an average Granular Base Equivalency (GBE) of about 450 mm, while a GBE of 650 to 750 is recommended.
- The road base is generally over 60 years old and therefore may be near the end of a typical lifecycle for such infrastructure.
- No significant horizontal curves are along Oxford Road 19. A number of vertical sightline deficiencies were identified.



REDUCED LOAD LIMITS ON ROAD NETWORK

- Oxford Road 19 is currently subject to a 5-tonne load limit restriction in the Spring, with the exception of the section from Otterville to Oxford Road 59.
- Improvements to the east-west network of unrestricted roads will improve the connectivity for goods movements to Springford, Otterville, Town of Norwich and north-south unrestricted roads (Highway 19, Oxford Road 13 and Oxford Road 59), as well as to Norfolk County.



ACTIVE TRANSPORTATION

- West section of Oxford Road 19 is within the 5-kilometre Bikeshed Area of Tillsonburg.

- Cycling recreational destination points exist along Oxford Road 19.



- Oxford Road 19 is currently not part of the Primary or Secondary Cycling Networks proposed for the County, although it does provide connectivity to north-south cycling networks (Primary, Secondary and Off-road Trail).

EVALUATION CRITERIA

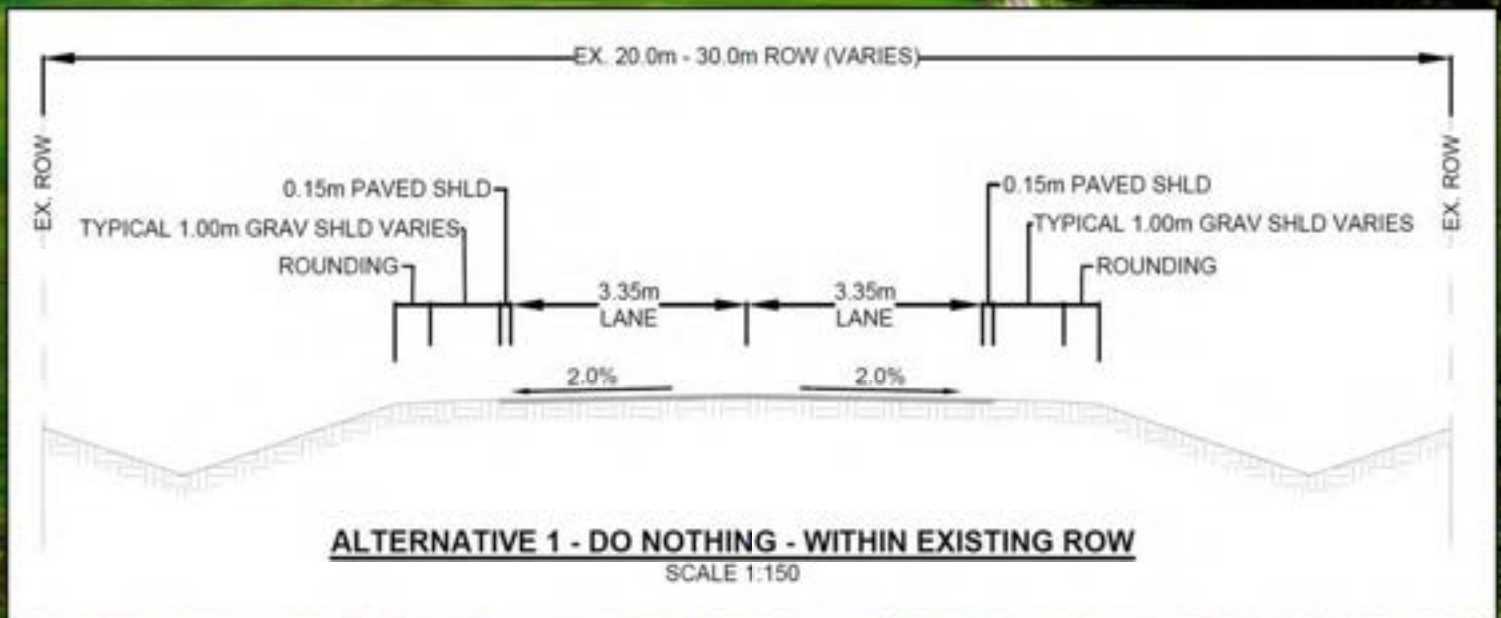
- **Natural Environment**
 - Impacts to vegetation and vegetation communities
 - Impacts to fisheries and aquatic habitat
 - Impacts to terrestrial habitat
 - Impacts to trees
 - Impacts to Woodlots, Wetlands and Designated Features (including PSWs, ESAs, ANSIs, Regional NHS)
 - Impacts to surface water and drainage
 - Impacts to groundwater and source water protection
 - Climate change and natural hazard impacts (erosion, soil stability, flooding)
- **Technical Environment**
 - Accommodation of all types of traffic and modes of travel
 - Improvement to operational safety
 - Road maintenance requirements
 - Impacts to utilities and drainage work/structures
- **Cultural Environment**
 - Impacts to cultural heritage resources
 - Impacts to archaeological resources
- **Socio-Economic Environment**
 - Impacts to private property
 - Compatibility with existing and future land uses
 - Compatibility with active transportation plans or needs
 - Conformity to municipal and agency plans and policies
 - Impacts to air quality and noise levels
 - Impacts to farms and business operations
 - Provision of safe access to private properties and businesses
- **Financial**
 - Capital and operation/maintenance costs
 - Property acquisition cost



Alternative Solutions

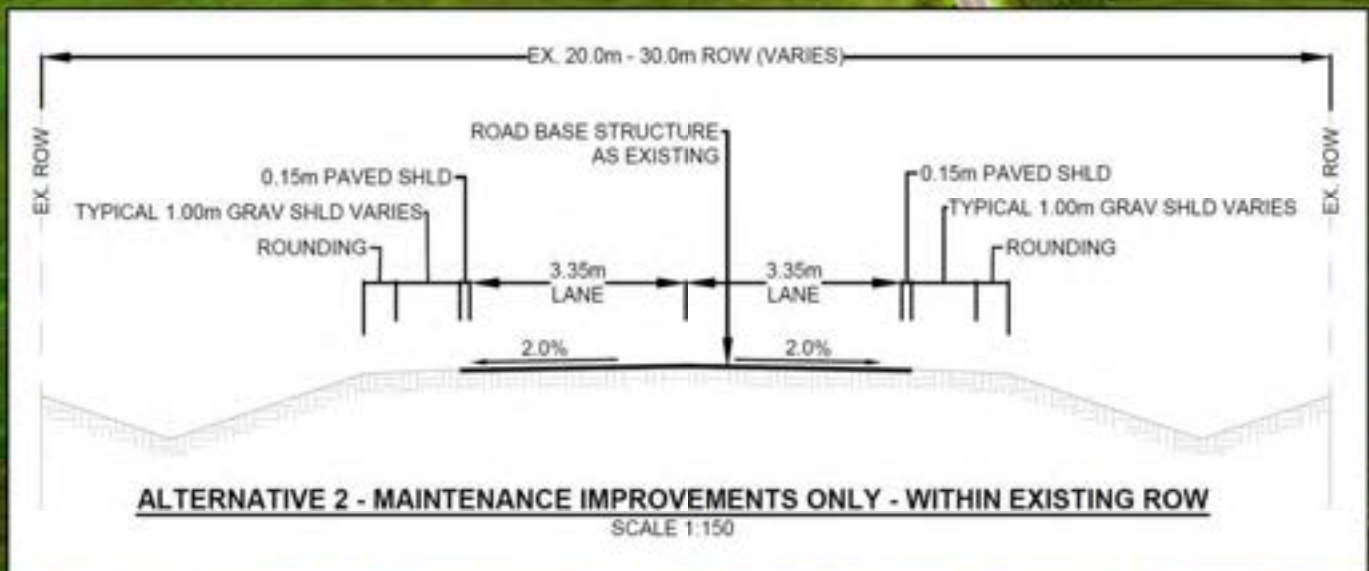
Alternative 1 - Do Nothing.

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.



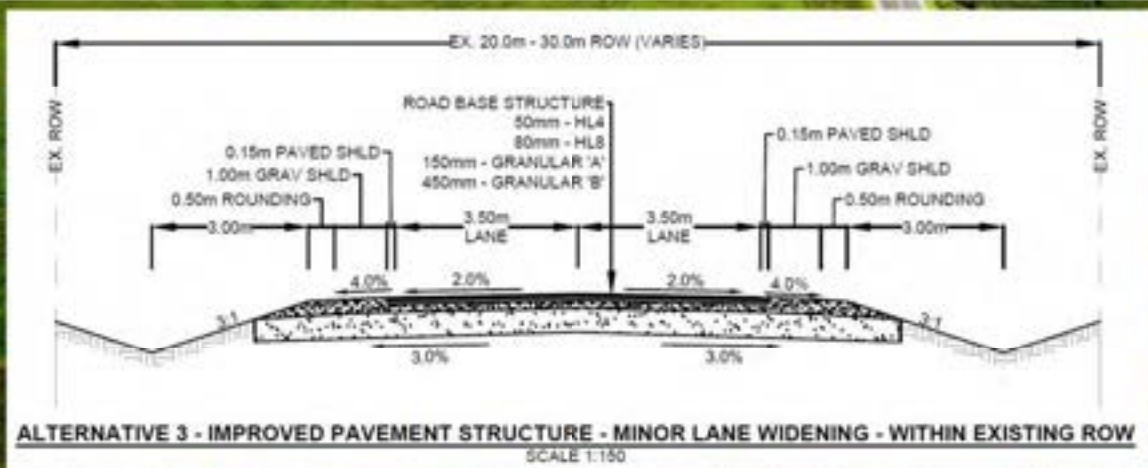
Alternative 2 – Regular Maintenance and Surface Treatments

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.
- All regular maintenance will be performed, as required.



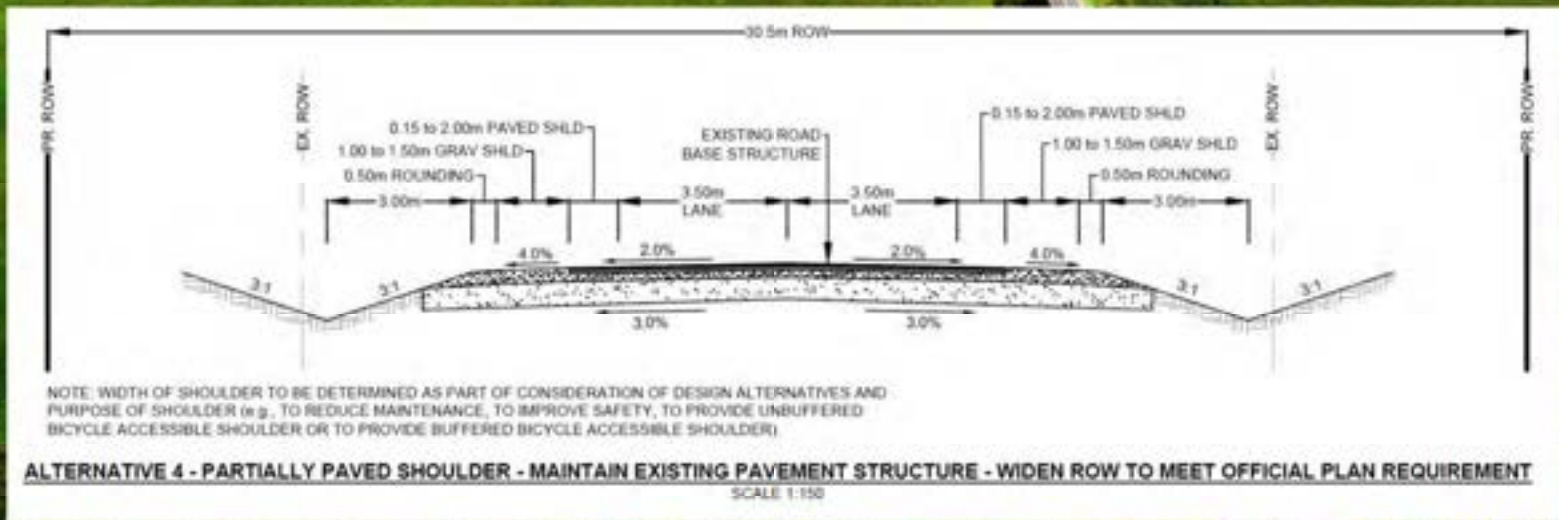
Alternative 3 – Structural Improvements to Road Within Existing ROW

- Provide a two-lane road to County requirement with a gravel shoulder.
- Widening of travel lanes to improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.
- No opportunities for active transportation facilities.
- Addresses the structural condition issues and allows for the Spring half load restriction on the road to be removed.



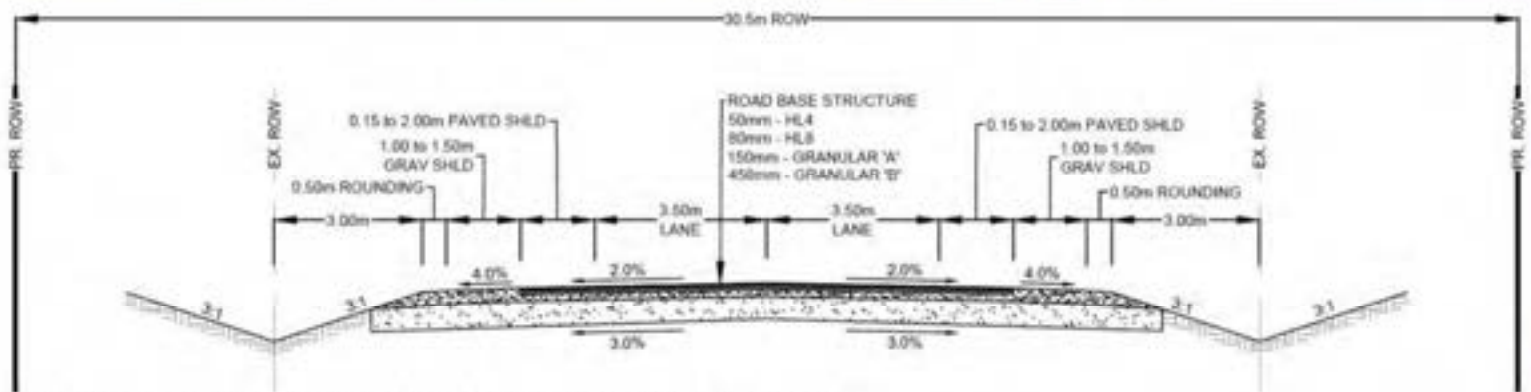
Alternative 4 – Widened Lanes and Shoulders within a Widened ROW- No Structural Improvements to the Road

- Provide a two-lane road to County requirement with paved shoulder wide.
- Widening of travel lanes to improve safety.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.



Alternative 5 – Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW































- Provide a two-lane road to County requirements with paved shoulder.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.






NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 5 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

Evaluation Criteria	Alternative 1: Do Nothing	Alternative 2: Preventive Maintenance Improvements Only	Alternative 3: Structural Improvements to Road Within Existing ROW	Alternative 4: Widening Lanes and Shoulders within a Widened ROW	Alternative 5: Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW
Natural Environment	 No general impact over existing conditions. Increased frequency and severity of adverse climatological events will impact aging infrastructure.	 Minor potential impacts to woodlands, surface and ground water. Risks to be mitigated.	 Minor potential impacts to woodlands and vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.	 Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.	 Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.
Cultural Environment	 No impact over existing conditions.	 No impact over existing conditions.	 No impact over existing conditions.	 There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.	 There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.
Socio-Economic Environment	 No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.	 No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.	 Does not meet the County's Official Plan ROW requirements. Provide benefits to local businesses, allowing effective transport for goods.	 Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses.	 Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses. Provide benefits to local businesses, allowing effective transport for goods.
Technical Environment	 Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.	 Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.	 Facilitates the road's ability to accommodate additional truck traffic. Does not improve the road's ability to accommodate all modes of travel. Some improvements to road safety and maintenance requirements. May impact utilities.	 Facilitates the road's ability to accommodate additional modes of travel. Does not improve the road's ability to accommodate trucks. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.	 Improves the road's ability to accommodate all types of traffic and modes of travel. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.
Financial Environment	 No capital cost. Relatively high O&M cost due to road aging. No property acquisition is required.	 Lowest low capital cost. Moderate to high O&M cost. No property acquisition is required.	 Moderate capital cost. Moderate to high O&M cost. No property acquisition is required.	 High capital cost. Moderate O&M cost. Some property acquisition will be required.	 Highest capital cost. Moderate O&M cost. Some property acquisition will be required.
Adherence to POS	 Does not meet POS	 Partially meet POS	 Partially meet POS	 Partially meet POS	 Fully meets POS
Overall Summary	Not Carried Forward	Not Carried Forward	Not Carried Forward	Not Carried Forward	Carried Forward

Level of Preference: Least Preferred  Less Preferred  Moderately Preferred  More Preferred  Most Preferred 



Next Steps

- Complete additional supporting studies (May 2022)
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HC noted that there is some preliminary information regarding a cemetery and some discussion has occurred; as well as some Indigenous communities have been notified and provided input. If there is a cemetery confirmed a Stage 2 Archaeological Assessment will be required.

Cultural Heritage slide information will not be available for PCC #1.

County staff enquired whether text should be added to the slides regarding the Stage 1 Archaeological Assessment and the Cultural Heritage Resource; and that the text be added prior to the ExTAC meeting.

Jesse Keith (JK) agreed that some text should be added prior to ExTAC meeting. JT noted that Burnside would add in text of preliminary findings.

2.2 *MTO Comments*

Martin Leyten (ML) noted that as the project moves along any improvements to the intersection of Oxford Road 19 and Plank Line (Road 19) needs to be reviewed by MTO.

2.3 *MECP Comments*

Mark Badali (MB) noted that at this stage MECP does not have any major comments. In the future stages one the preferred alternative is confirmed, mitigation measures can be discussed with MECP.

In terms of the consultation with Indigenous Communities, MECO encourages you to reach out to each and every community as early in the process as feasible and continue to follow up as best as you can to form a meaningful engagement.

The Ministry encourages the Study Team to submit a draft ESR to the Ministry for review (with a minimal of 30-day review period), just to tackle any major risks, including the risk and mitigation of the potential of receiving a Section 16 Order.

The following items were discussed	Action by
AB noted that as part of the consultation process follow up contacts are made (calls or emails) after each notice send-out to each of the indigenous communities to confirm that they received the notice and if they have any questions or concerns.	
3. Adjournment The meeting was ajourned by HC and the county thanking everyone for attending.	

The preceding are the minutes of the meeting as observed by the undersigned. Should there be a need for revision, please advise Burnside within seven days of issuance. In the absence of notification to the contrary, these minutes will be deemed to be an accurate record of the meeting.

Minutes prepared by:

R.J. Burnside & Associates Limited



Henry Centen, P. Eng.

Senior Transportation Engineer

HC:ba/js

Attachments:

Meeting Agenda

Draft PCC#1 Slides

Distribution:

All Attendees

Other than by the addressee, copying or distribution of this document, in whole or in part, is not permitted without the express written consent of R.J. Burnside & Associates Limited.



Meeting Agenda

Date and Time: May 10, 2022 11:00am-12:00pm **Project No.:** 53425.0000

Project Name: Oxford Road 19 Environmental Assessment

Meeting Subject: Ex-TAC Meeting #1

Meeting Location: Teams Teleconference

Items

- Project background
- Study approach
- Existing conditions
- List alternatives and preliminary preferred solution
- Input on technical constraints and opportunities
- Summary of consultation efforts and feedback to date



Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements

PUBLIC CONSULTATION CENTRE / OXFORD COUNTY ADMINISTRATION
BUILDING, 21 REEVE STREET, WOODSTOCK, ON, N4S 7Y3
JUNE 9, 2022
5:00 P.M. - 7:00 P.M.

WELCOME

to the Public Consultation Centre for the Oxford Road 19 Corridor Improvements

Please Sign In

- Meet with Study Team Members
- Review the display materials and discuss your questions and ideas with the Study Team
- Please fill out a comment sheet and return it to the Study Team in person, by email or fax by **June 23, 2022**

STUDY AREA

The County is undertaking a Class EA study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study Area includes approximately 16 kilometres of Oxford Road 19 between Highway 19 (Plank Line), and the Norfolk County boundary (Windham Road 19), which excludes the Settlements of Springford and Otterville.



The site location and approximate extent of the Study Area are shown on the map.

PROBLEM/OPPORTUNITY STATEMENT

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

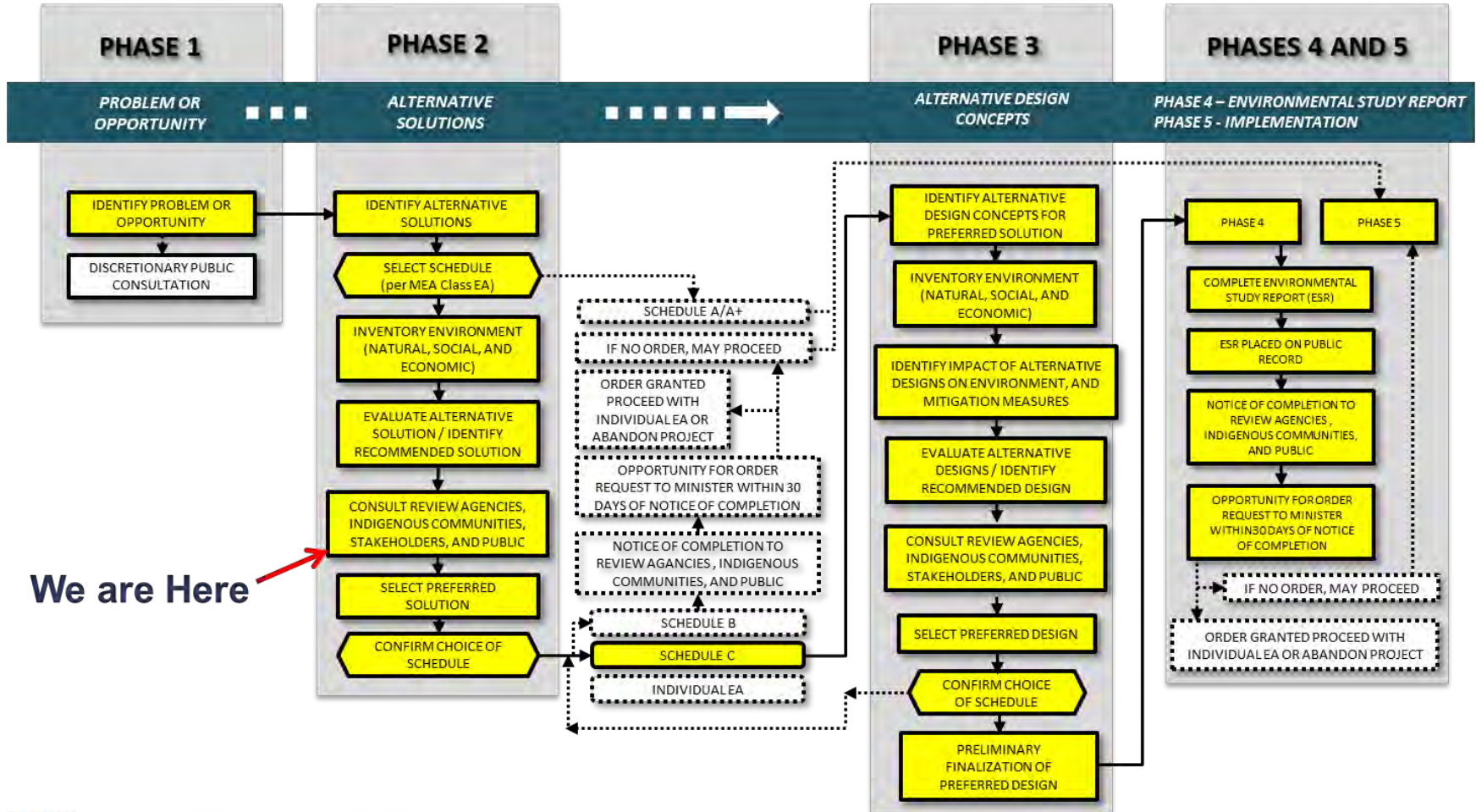


THE EA PROCESS

The Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act.

Nearing completion of the study, an Environmental Study Report (ESR) will be prepared and made available for final public review and comment.

THE EA PROCESS



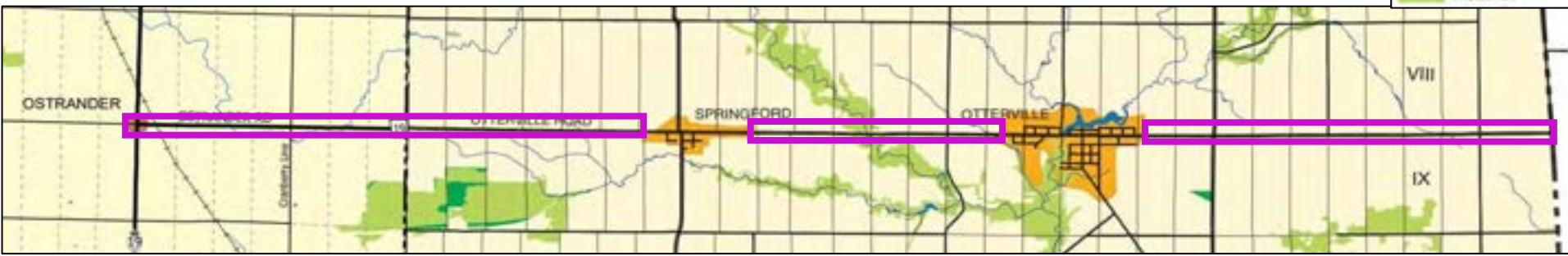
PLANNING CONTEXT

- Provincial Policy Statement, 2020
- County of Oxford Official Plan (consolidation of Official Plan amendments, as of March 31, 2021)
- Oxford County Transportation Master Plan (2019)
- Phase One Comprehensive Review Oxford County (2020)
- Draft Cycling Master Plan (2021)
- TAC Design Guidelines (2017)
- Guidelines from Ontario Traffic Manual – Book 18 (Cycling Facilities)



EXISTING LAND USE

- The two communities of Springford and Otterville within Norwich are zoned as Settlements by the County of Oxford Official Plan.
- Outside of these communities, most land is Agricultural Reserve with lands surrounding Ostrander is a Rural Cluster. All other land in South-West Oxford is Agricultural Reserve.
- Between the villages of Springford and Otterville is Spittler Creek which is zoned as Environmental Protection. This designation falls within the County's Natural Heritage System.
- Within Otterville, the Otter Creek floodplain crosses Oxford Rd 19. As floodplain falls within the Open Space designation, this land also falls within the County's Natural Heritage System.
- Below the floodplain to the south of Oxford Road 19, the Environmental Protection designation begins.



SUPPORTING STUDIES

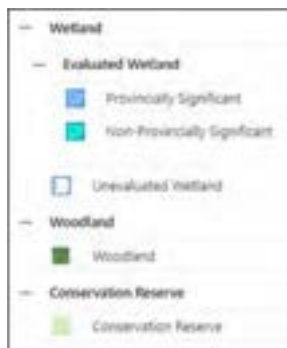
- Transportation Study
- Natural Environment Assessment
 - Terrestrial Habitat Assessment
 - Aquatic Habitat Assessment
- Stage 1 Archaeological Assessment
- Cultural Heritage Resource Assessment
- Air and Noise Impact Assessments
- Stormwater Management Assessment



NATURAL HERITAGE RESOURCES

Designated Features within the Study Area

- Significant valleylands and locally significant natural heritage features
- Watercourses: Spittler Creek, Plumb Creek, and Big Otter Creek.
- Spittler Creek and Big Otter Creek are both associated with significant valleylands
- Otterville Provincially Significant Wetland (PSW) Complex and other wetlands across Spittler Creek
- Woodlands



NATURAL HERITAGE RESOURCES

Aquatic habitat

No Species at Risk (SAR) were found through the desktop review. Field work will be conducted in the coming field season to assess aquatic habitat in the Study Area.

- Big Otter Creek is a warm water creek with spring-spawning species such as Largemouth Bass and Yellow Perch.
- Spittler Creek is a cool water creek with observed spring-spawning species.
- Plumb Creek is a cool water creek with fall and spring-spawning species such as Brown Trout, Creek Chub, and Blacknose Dace.



NATURAL HERITAGE RESOURCES

Terrestrial Habitat

The desktop review found several potential Species At Risk (SAR) such as:

Snapping Turtle (Special Concern), Wood Thrush (Special Concern), American Badger (Endangered), American Chestnut (Endangered)

Field work will be conducted in the coming field season to assess ecological communities present in the Study Area.

Bats

The desktop review found records of 4 bat species: Eastern Small-footed Myotis, Little Brown Myotis, Northern Myotis, and Tricoloured Bat

Impact to potential habitat is not expected. Direct impact is to be avoided with timing vegetation removal to be completed outside of the active season for bats.



SOURCE WATER PROTECTION

The Study Area falls under the Long Point Region Source Water Protection Area and Lake Erie Source Water Protection Area.

Wellhead Protection Areas

Three portions of the study area have been identified to fall under Wellhead Protect Areas (i.e., part of Ostrander, north part of Springford and east of Otterville).



Issue Contributing Area

The area west of Otterville is also considered an Issue Contributing Area (ICA), for its potential to contribute elevated concentrations of particular substances to the drinking water source (e.g., chloride, sodium, nitrate).

Highly Vulnerable Aquifer

A significant part of the Oxford Road corridor (i.e., from west of Otterville to east of Oxford Road 59) is also located in a Highly Vulnerable Aquifer (HVA) area.

Significant Groundwater Recharge Areas

A significant part of the study area is also within Significant Groundwater Recharge Areas (SGRAs)

ARCHAEOLOGICAL RESOURCES

Text

To be inserted once
finalized

CULTURAL HERITAGE RESOURCES

Text

To be inserted once
finalized

FUTURE GROWTH

- By 2046, Oxford County is expected to grow by about 47,000 people and employment is expected to grow by 21,000 jobs. While much of this growth will be in Woodstock, Tillsonburg and Ingersoll, Oxford Road 19 provides an important linkage for the movement of goods and people within the County.
- Traffic growth on Oxford Road 19 is impacted by growth in the Township of Norwich and the Township of South-West Oxford, which has historically been low.
- A small amount of subdivision development is forecasted to occur in the Villages of Otterville and Springford.
- Industrial lands in the Town of Norwich have the potential for development or redevelopment, with Oxford Road 19 providing a linkage to this area and to the County of Norfolk.



Oxford Road 19 provides an important east-west link for the movement of goods and people within the County and supports growth and development in this area.

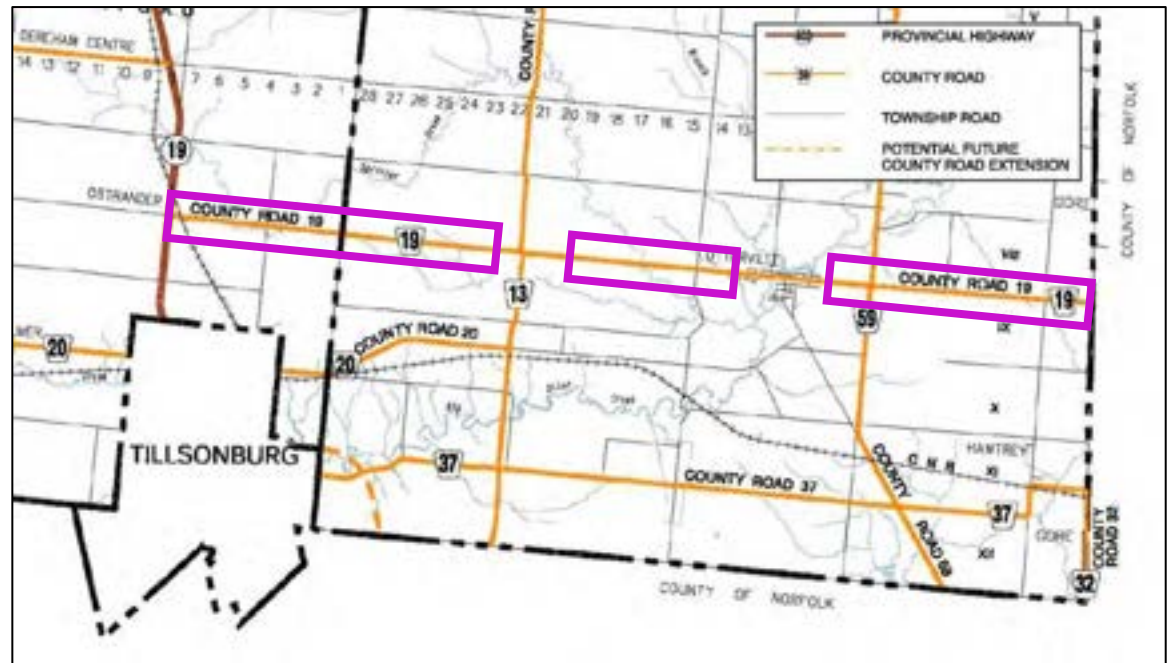
FUTURE GROWTH: DEVELOPMENTS

- A 9-lot subdivision was constructed in Springford over the last few years and 4 lots currently have draft approval.
- About 42 lots have been developed in Otterville over the last several years, with a 34-lot subdivision currently being built. A further 30-lot subdivision has draft approval in the village.
- In 2019, the Township of Norwich had 172 units approved for residential development. Most of the development in Norwich Township is to occur in the Town of Norwich.



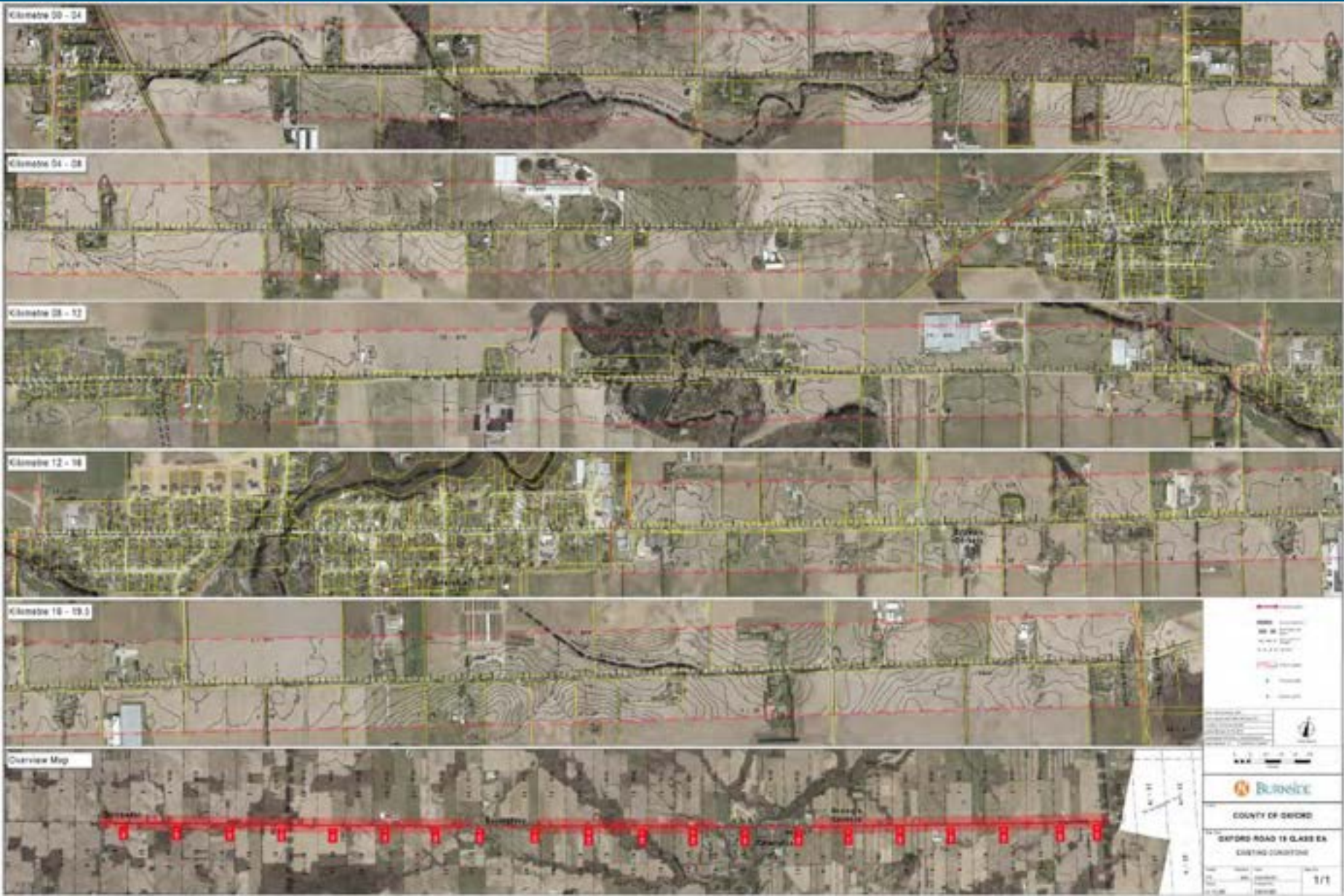
EXISTING ROAD NETWORK AND TRAFFIC CONDITION

- Oxford Road 19 is a significant east-west County road serving the southeast part of the County, connecting the villages of Springford and Otterville and the rural cluster of Ostrander, with connections to Highway 19, Oxford Road 13, Oxford Road 59 and the County of Norfolk.



- Traffic on Oxford Road 19 operates with free flow condition at most intersections within the study area and is only stop controlled at Highway 19 and Oxford Road 59.
- The existing ROW widths, in the rural area of Oxford Road 19 varies from about 20 metres to 30 metres along the corridor, with travel lanes being about 3.35 metres and the existing gravel shoulder widths varying between about 0.6 metres to 2.3 metres.

PROFILE OF EXISTING ROAD ROW CONFIGURATION



TRAFFIC VOLUMES AND TRAFFIC SAFETY

- Annual Average Daily Traffic (AADT) volumes on Oxford Road 19 are forecasted to grow by over 20%, to a total of 4400 vehicles per day (vpd), by 2046. Traffic volumes to the east of Oxford Road 59 are lower (i.e., about 2300 vpd by 2046). Based on these AADT forecasts, no additional travel lanes are required.
- Intersections along Oxford Road 19 corridor are forecasted to have good operations through horizon year 2046, with the exception of the westbound approach to Highway 19, which may warrant the edition of a westbound left-turn lane in this horizon.
- Collision rates along the corridor are in the typical range for arterial County roads, with no significant collision hot spots or patterns.
- The provision of paved shoulders will improve safety for both motor vehicle travel and other users (e.g., pedestrians, cyclists, farm equipment).

EXISTING ROAD CONDITION

STRUCTURAL ADEQUACY AND GEOMETRIC DEFICIENCIES

- Given the typical age of the pavement profile on Oxford Road 19 it is expected that full depth replacement will be required to improve the road structure.
- Geotechnical study indicates the existing pavement structure has an average Granular Base Equivalency (GBE) of about 450 mm, while a GBE of 650 to 750 is recommended.
- The road base is generally over 60 years old and therefore may be near the end of a typical lifecycle for such infrastructure.
- No significant horizontal curves are along Oxford Road 19. A number of vertical sightline deficiencies were identified.



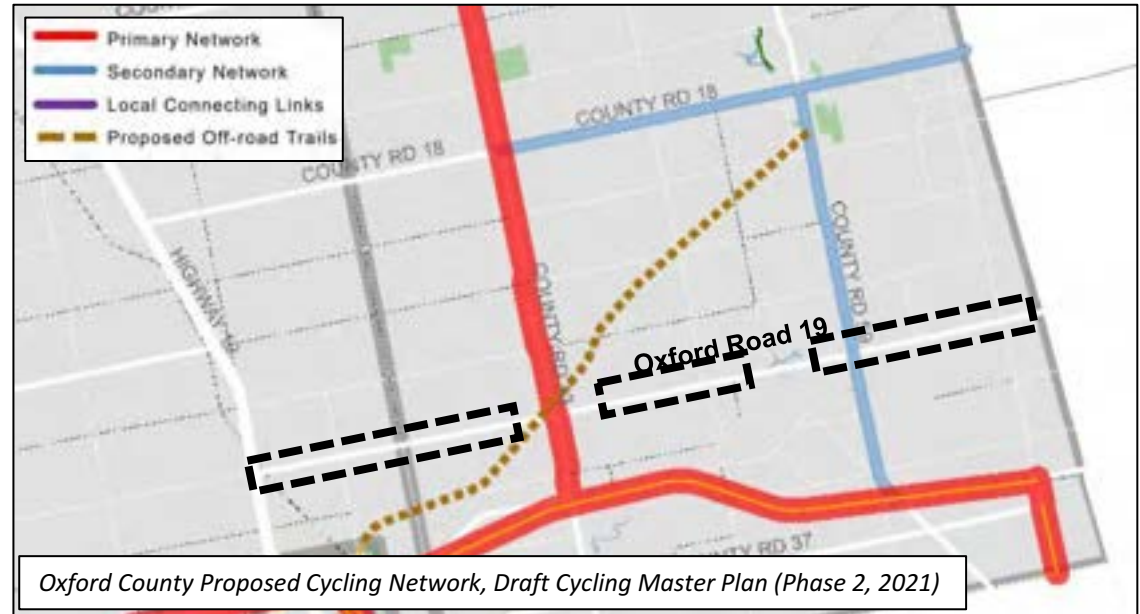
REDUCED LOAD LIMITS ON ROAD NETWORK

- Oxford Road 19 is currently subject to a 5-tonne load limit restriction in the Spring, with the exception of the section from Otterville to Oxford Road 59.
- Improvements to the east-west network of unrestricted roads will improve the connectivity for goods movements to Springford, Otterville, Town of Norwich and north-south unrestricted roads (Highway 19, Oxford Road 13 and Oxford Road 59), as well as to Norfolk County.



ACTIVE TRANSPORTATION

- West section of Oxford Road 19 is within the 5-kilometre Bikeshed Area of Tillsonburg.
- Cycling recreational destination points exist along Oxford Road 19.
- Oxford Road 19 is currently not part of the Primary or Secondary Cycling Networks proposed for the County, although it does provide connectivity to north-south cycling networks (Primary, Secondary and Off-road Trail).



EVALUATION CRITERIA

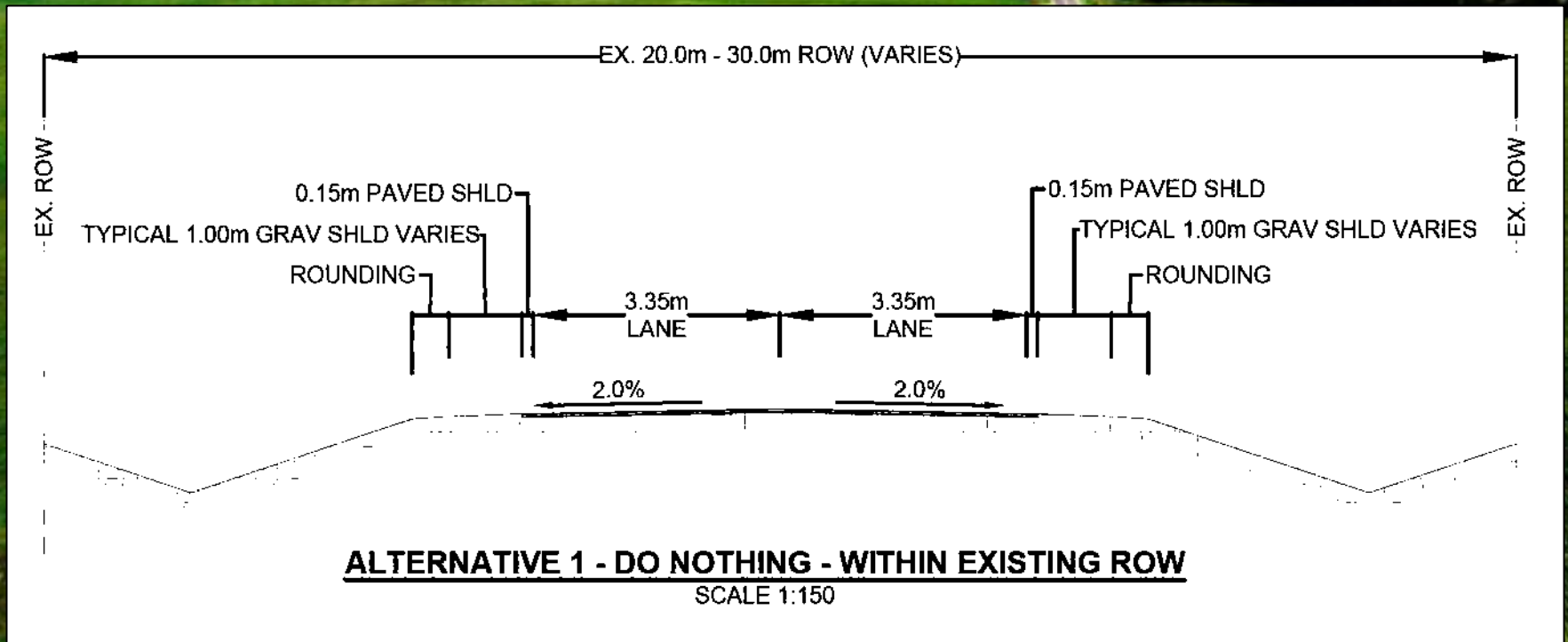
- **Natural Environment**
 - Impacts to vegetation and vegetation communities
 - Impacts to fisheries and aquatic habitat
 - Impacts to terrestrial habitat
 - Impacts to trees
 - Impacts to Woodlots, Wetlands and Designated Features (including PSWs, ESAs, ANSIs, Regional NHS)
 - Impacts to surface water and drainage
 - Impacts to groundwater and source water protection
 - Climate change and natural hazard impacts (erosion, soil stability, flooding)
- **Technical Environment**
 - Accommodation of all types of traffic and modes of travel
 - Improvement to operational safety
 - Road maintenance requirements
 - Impacts to utilities and drainage work/structures
- **Cultural Environment**
 - Impacts to cultural heritage resources
 - Impacts to archaeological resources
- **Socio-Economic Environment**
 - Impacts to private property
 - Compatibility with existing and future land uses
 - Compatibility with active transportation plans or needs
 - Conformity to municipal and agency plans and policies
 - Impacts to air quality and noise levels
 - Impacts to farms and business operations
 - Provision of safe access to private properties and businesses
- **Financial**
 - Capital and operation/maintenance costs
 - Property acquisition cost

An aerial photograph of a rural landscape. A paved road runs diagonally from the bottom right towards the center of the image. The surrounding area is filled with vibrant green fields, some of which appear to be agricultural crops. In the distance, there are clusters of trees and a few buildings, possibly a farm or a small village. The sky is a clear, bright blue.

Alternative Solutions

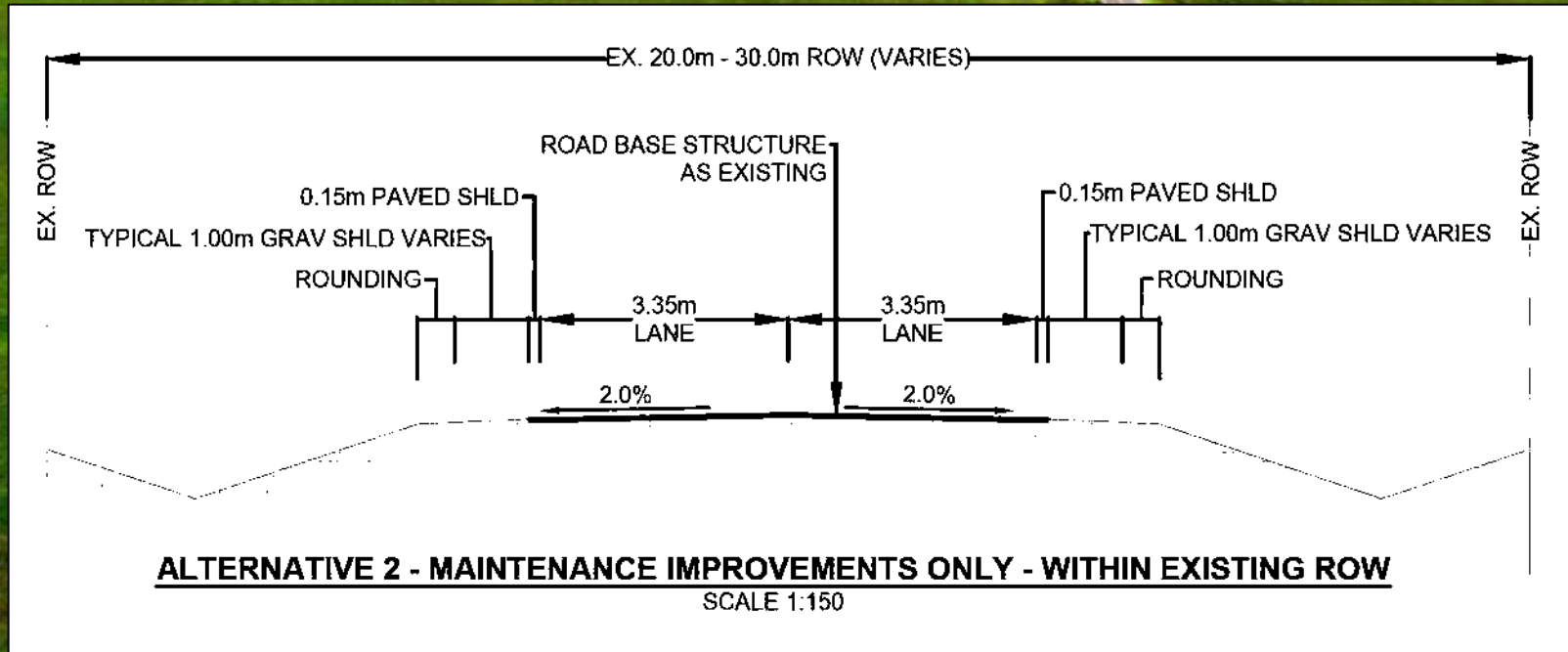
Alternative 1 - Do Nothing.

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.



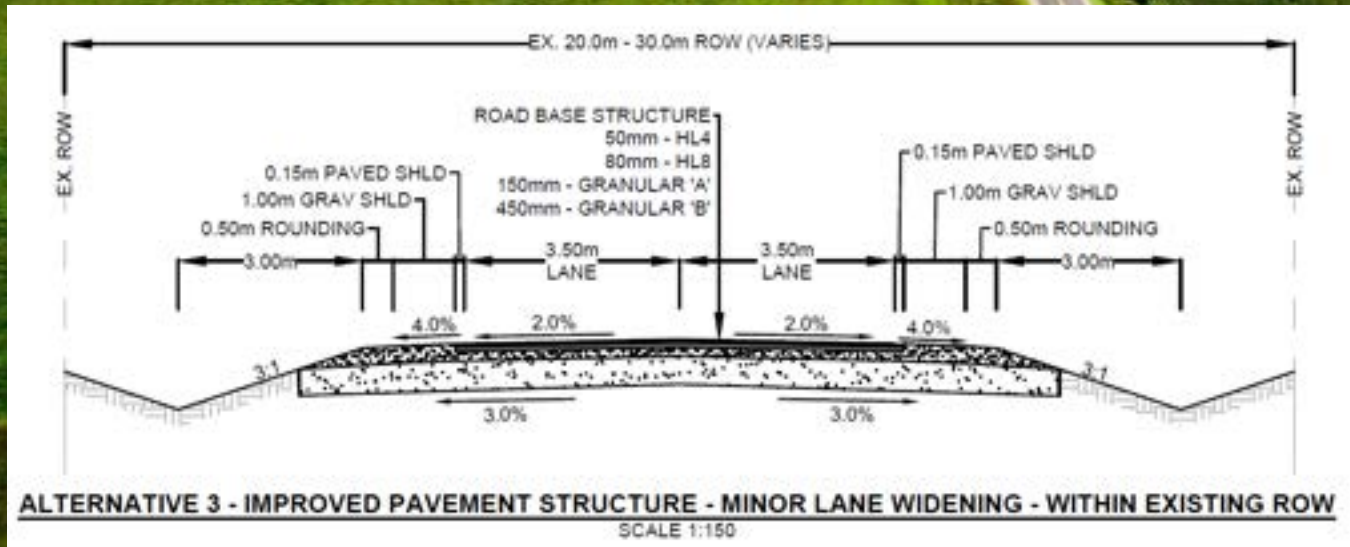
Alternative 2 – Regular Maintenance and Surface Treatments

- The road will continue to operate as a two-way road fully open to the public.
- Half-load restrictions will continue to be applied in the Spring.
- No construction or widening will occur.
- All regular maintenance will be performed, as required.



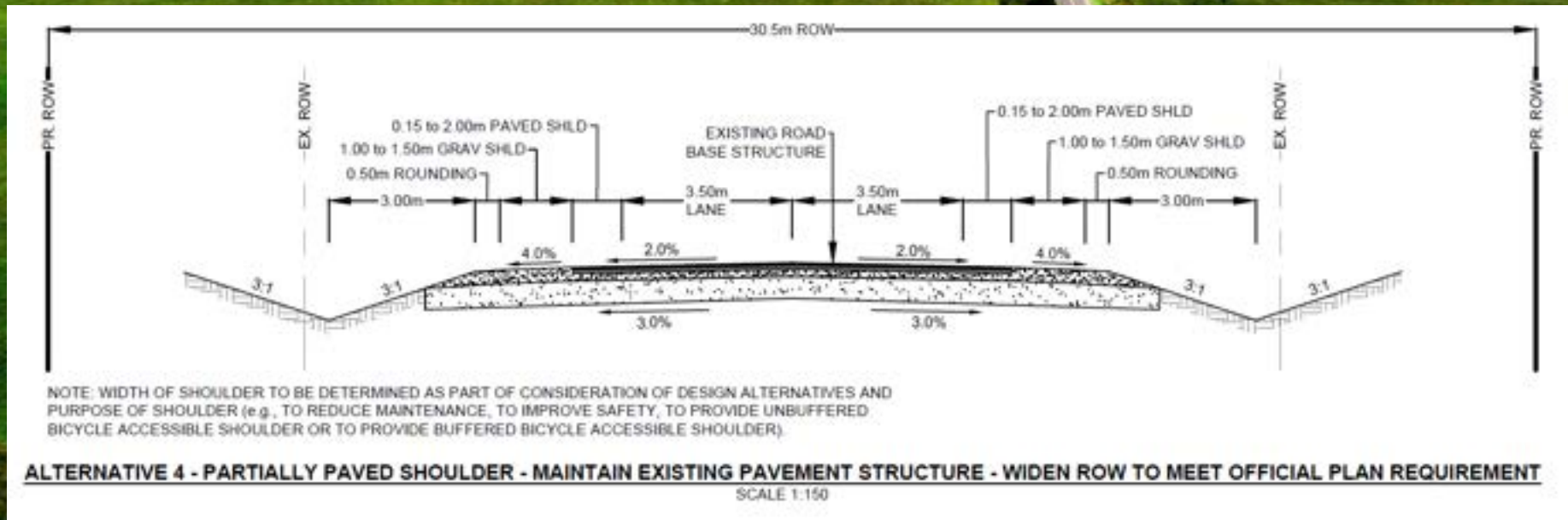
Alternative 3 – Structural Improvements to Road Within Existing ROW

- Provide a two-lane road to County requirement with a gravel shoulder.
- Widening of travel lanes to improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.
- No opportunities for active transportation facilities.
- Addresses the structural condition issues and allows for the Spring half load restriction on the road to be removed.



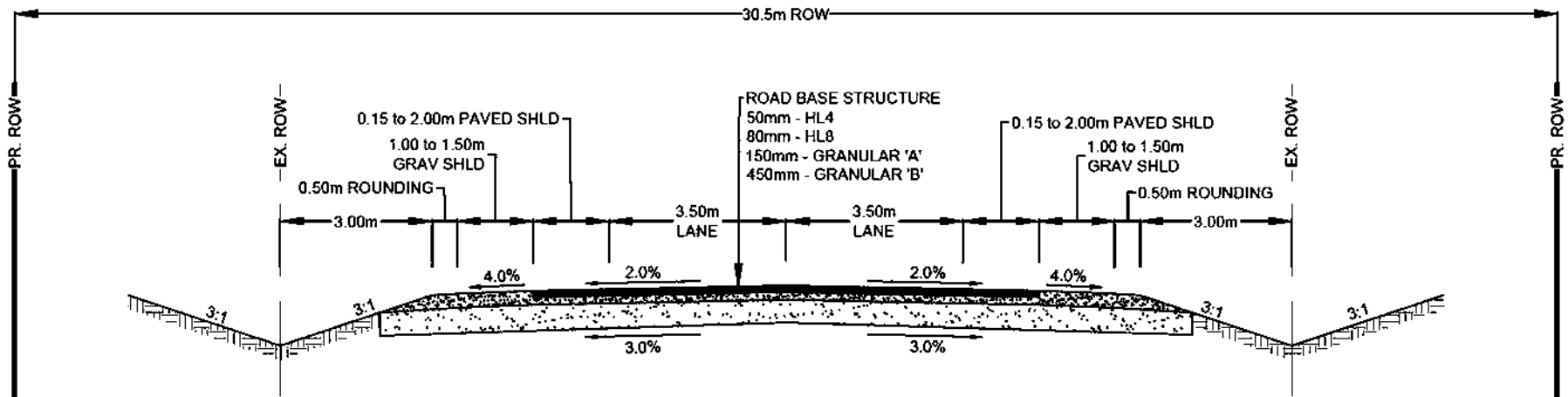
Alternative 4 – Widened Lanes and Shoulders within a Widened ROW- No Structural Improvements to the Road

- Provide a two-lane road to County requirement with paved shoulder wide.
- Widening of travel lanes to improve safety.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.



Alternative 5 – Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW

- Provide a two-lane road to County requirements with paved shoulder.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.



NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 5 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

Evaluation Criteria	Alternative 1: Do Nothing		Alternative 2: Preventive Maintenance Improvements Only		Alternative 3: Structural Improvements to Road Within Existing ROW		Alternative 4: Widening Lanes and Shoulders within a Widened ROW		Alternative 5: Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW	
Natural Environment		No general impact over existing conditions. Increased frequency and severity of adverse climatological events will impact aging infrastructure.		Minor potential impacts to woodlands, surface and ground water. Risks to be mitigated.		Minor potential impacts to woodlands and vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.		Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction. Risks to be mitigated.		Some potential loss of woodlands and potential impacts to vegetation and terrestrial habitat. Potential impacts to surface and ground water during construction Risks to be mitigated.
Cultural Environment		No impact over existing conditions.		No impact over existing conditions.		No impact over existing conditions.		There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.		There may be an impact to archaeological resources however this is not likely as most of the area to be widened has been previously disturbed.
Socio-Economic Environment		No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.		No impact over existing conditions. Does not meet the County's Official Plan ROW requirements.		Does not meet the County's Official Plan ROW requirements. provide benefits to local businesses, allowing effective transport for goods.		Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses		Meets the Official Plan ROW requirements. Will impact properties within the widened ROW. Offers opportunities for active transportation. Improves the safety for access to private properties and businesses. Provide benefits to local businesses, allowing effective transport for goods.
Technical Environment		Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.		Does not improve the road's ability to accommodate all types of traffic and modes of travel, road safety, or maintenance requirements. No impact to utilities/drainage structure.		Facilitates the road's ability to accommodate additional truck traffic. Does not improve the road's ability to accommodate all modes of travel. Some improvements to road safety and maintenance requirements. May impact utilities		Facilitates the road's ability to accommodate additional modes of travel. Does not improve the road's ability to accommodate trucks. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.		Improves the road's ability to accommodate all types of traffic and modes of travel. Some improvements to road maintenance requirements. May impact Hydro One transmission poles and/or drainage structure.
Financial Environment		No capital cost. Relatively high O&M cost due to road aging. No property acquisition is required.		Lowest low capital cost. Moderate to high O&M cost. No property acquisition is required.		Moderate capital cost. Moderate to high O&M cost. No property acquisition is required.		High capital cost. Moderate O&M cost. Some property acquisition will be required.		Highest capital cost. Moderate O&M cost. Some property acquisition will be required.
Adherence to POS		Does not meet POS		Partially meet POS		Partially meet POS		Partially meet POS		Fully meets POS
Overall Summary	Not Carried Forward		Not Carried Forward		Not Carried Forward		Not Carried Forward		Carried Forward	

Level of Preference: Least Preferred Less Preferred Moderately Preferred More Preferred Most Preferred

Next Steps

- Complete additional supporting studies (May 2022)
- Confirm Preferred Solution (June 2022)
- Complete Transportation Study (June 2022)
- Develop and Evaluate Alternative Design Concepts (August 2022)
- Public Information Centre # 2 (September 2022)
- Environmental Study Report (October 2022)
- File Environmental Study Report for Schedule C Class EA Study (November 2022)

Invitation for Participation

Public input is an important component of the decision-making process.

You are invited to provide comments by completing the forms provided and submitting forms to the Study Team members below on or before June 23, 2022.

Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com

THANK-YOU FOR ATTENDING



Minutes of Meeting

Meeting Date: November 17 2022 **Project No.:** 300053425.0000
Project Name: Oxford Rd. 19 Environmental Assessment
Meeting Subject: TAC Meeting #2
Meeting Location: Video Conference
Date Prepared: November 28, 2022

Those in attendance were:

Mark Badali	MECP	Mark.Badali1@ontario.ca
Isabel Johnson	LPRCA	ijohnson@lprca.on.ca
Allan Hodgins	MTO	Allan.Hodgins@ontario.ca
Daniel Leduc	Township of Southwest Oxford (Drainage)	Drainage@swox.ca
Jesse Keith	Oxford County (County)	jkeith@oxfordcounty.ca
Reuben Davis	County	rdavis@oxfordcounty.ca
Frank Gross	County	fgross@oxfordcounty.ca
Heather St. Clair	County	hstclair@oxfordcounty.ca
Henry Centen	R.J. Burnside and Associates Ltd. (Burnside)	Henry.Centen@rjburnside.com
Avid Banihashemi	Burnside	Avid.Banihashemi@rjburnside.com
Jeremy Taylor	Burnside	Jeremy.taylor@rjburnside.com
Amy Boddy	Burnside	Amy.Boddy@rjburnside.com
Michael Siemon	Burnside	Michael.Siemon@rjburnside.com

The following items were discussed

Action by

1. Project Background

1.1 The meeting started by welcoming everyone and a round of introduction.

Henry Centen (HC), R.J. Burnside (Burnside) reviewed the purpose of the TAC meeting and walked through the listed items on the agenda. The agenda included the following items:

- Minutes of ExTAC Meeting #1 (May 10, 2022)
- Preliminary preferred solution

The following items were discussed	Action by
<ul style="list-style-type: none">• Public Consultation Centre #1 (June 9, 2022) and Draft Summary Report (August 2022)• Status of ongoing studies (Natural Environment, Archaeological, Cultural Heritage)• Identification of conceptual design issues and identification of preliminary preferred design• Review of draft slide deck for Public Consultation Centre #2 (December 6, 2022)• Other matters <p>HC noted that he will review with TAC the slides which would form the basis of the material that will be presented at the upcoming Public Consultation Centre (PCC #2). The presentation will be provided to the TAC members along with the meeting minutes for further review.</p>	
1.2 Review of ExTAC Meeting #1	
<p>1.2.1 HC reviewed the discussions from ExTAC#1:</p> <p>The comments from MTO in the first ExTAC meeting were related to Highway 19, and any intersection improvements at 19 that MTO would have to review and be involved in approving for those improvements.</p> <p>The conservation authority at the ExTAC#1 indicated that they would be involved in reviewing, especially during construction or during the detailed design on erosion, sediment control and maintaining the morphology of the watercourses in the area, and any mitigation work.</p> <p>MECP at the ExTAC#1 indicated that they would be reviewing the ESR and the mitigation, in particular the mitigation measures that are being proposed.</p>	
1.3 Preliminary Preferred Solution	
<p>1.3.1 The preliminary preferred solution was the hybrid solution, which was to remove the half load designation by improving the structural capacity of the road and also widening the shoulders.</p> <p>A 30 m meter right-of-way (ROW) (approximately equivalent of the 100 foot right of way within the Official Plan (OP)) was anticipated. The project team proceeded with that in the rural area where the 30 meter ROW is desirable to accommodate all of the drainage in the widened cross section. A subsequent discussion with the</p>	

The following items were discussed	Action by
<p>County had indicated that in the area of Ostrander, which is an area adjacent to highway 19 there is strip residential development and that a 26 m ROW be applied in that area (consistent with the OP requirements for settlement areas).</p>	
<p>1.4 Public Consultation Centre #1 Input Summary</p>	
<p>1.4.1 Avid Banihashemi (AB) gave a summary of input to date from agencies, Indigenous communities and the public.</p> <p>It was noted that there had been no concerns from <i>agencies</i> thus far, and no comments from any <i>utilities</i>.</p> <p><i>Indigenous communities</i> have not sent any specific comments or concerns at this time. However, correspondence from both Chippewas of the Thames and Mississauga's of the Credit First Nation would like to be involved, with Archaeological Assessment and the Natural Environment work, however, have no capacity at this time.</p> <p>Stage 1 Archaeological Assessment is a desk top exercise and if a Stage 2 Archaeological Assessment is indicated it will be done at <i>detailed design</i>. If a Stage 2 is warranted the Mississauga's of the Credit First Nation have requested to be contacted and involved in the field work.</p> <p><i>Public Comments</i> have been of a general nature, such as concern for repeated issues from previous construction projects and how they impacted local farms and businesses, speed issues, truck movement and whether the improvements will cause additional traffic and noise impact, some road visibility issues i.e., the (road's horizontal alignment and vertical alignment), and some drainage issues along the road.</p>	
<p>1.5 Status of Ongoing Studies</p>	
<p>1.5.1 Natural Environment: desktop and fieldwork is complete: ongoing impact assessment and identification of mitigation measures</p> <p>Archaeological and Cultural Heritage: Existing conditions assessment is complete, ongoing impact assessment and identification of mitigation measures</p> <p>Transportation study (also includes illumination review): Ongoing, generally complete for identifying the preliminary preferred design concept.</p>	

The following items were discussed

Action by

Stormwater: ongoing, review of the corridor and the potential for the impact is complete.

Noise and Air Quality Impact Assessment: The potential impacts assessment along the corridor is complete.

AB noted that as the project moves forward, the team is producing mitigation measures of proposed design concepts on different areas including natural environment and on archaeology and cultural heritage resources, stormwater, air and noise, and other areas. These mitigation measures will be included in a complete table in the environmental study report, which will be available for review.

1.6 Identification of conceptual design issues and identification of preliminary preferred design

1.6.1 HC reviewed the conceptual design considerations:

- No collision hotspots were noted along the corridor; however it is anticipated that the proposed work will improve the safety of traffic operations along the corridor overall.
- There's a need for only two lanes along Oxford Roads 19, given the volumes of traffic up to beyond 2046.
- For the corridor east of Otterville between Otterville and County Road 59, there were no significant improvements identified, since it already has a 30 meter ROW and was previously improved with works in Otterville. This segment also has shoulders that are partly paved, Drainage might be the one item that the municipality may want to consider upgrading.
- On other segments of the corridor, there is a benefit to having some pavement on the shoulders to accommodate cycling and to make it safer for cycling, but also for movement of agricultural equipment along the corridor.
- Vertical and horizontal alignments were considered along the corridor. 20 locations of potential alignment improvements were identified to be reviewed further at the detailed design.
- The 30 m ROW is being recommended to address the Official Plan requirement, but also to accommodate the flexibility and the needs associated with drainage (e.g. open ditch drainage, etc.) along the corridor. 26 m ROW in Ostrander is being recommended since it is a lower speed environment and has the potential for adjusting the cross sections in that area, also in order to accommodate the strip residential development in that area.

The following items were discussed	Action by
1.7 Review of draft slide deck for Public Consultation Centre #2	
1.7.1 HC and AB went through the PCC#2 draft slides (attached to these minutes).	
1.8 ExTAC Members Comments	
1.8.1 <i>MTO Comments</i> <p>Allan Hodgins (AH) noted that as the project moves along any improvements to the intersection of Oxford Road 19 (Plank Line) needs to be reviewed by MTO. He specified further that for anything within 180 meters east or west of highway 19 (MTO permit control for the highway) an early consultation is appreciated to ensure that the permits are issued in a timely manner, if any are required. Also coordination of should ensure that the timing fits with any adjacent MTO works. Once concept plans are available, they can be shared with MTO to identify what's needed from MTO's perspective (illumination, stormwater work, etc.).</p> <p>On illumination, AH noted that MTO will need to verify the locations and how the proposed illumination impacts them. He added that a lot of MTO's provincial highways aren't signalized intersections so it's hard to get a warrant after lighting is already in place. So, it is important to make sure we're all on the same page before it gets too far along. At the detailed design, any works associated with either a turning lane or illumination or even just the connection itself would be subject to MTO approvals and review.</p> <p>HC noted that the ESR will identify the concept designs for illumination, for review and comment by the MTO.</p>	
1.8.2 <i>MECP Comments</i> <p>Mark Badali (MB) asked if the air quality impact assessment that was completed, if it was qualitative or quantitative. HC responded that a quantitative assessment was completed, considering the traffic volumes and the truck volumes etc, and how that would impact the overall air quality in that area.</p> <p>MB noted that the ministry is looking to have some consideration of climate change. In the ESR we'll be looking to have some sort of discussion notes. AB confirmed that a Climate Change section will be included in the ESR, covering both potential impacts of the project on climate change, and the potential impacts of climate change on the project.</p>	

The following items were discussed

Action by

MB asked about the noise study and whether MECP criteria were applied. HC responded that MTO noise guidelines have been applied and further comparisons on MECP noise criteria will be made in the ESR

MB noted that the project has identified potential habitat for Barn Swallow and butternut. MECP Species at Risk branch tends to get more involved at the detailed design stage, when location-specific impacts are clearly identified. AB clarified that continuing the consultation with MECP regarding SAR and SAR habitat and the potential impacts, will be included as a commitment in the ESR, to be part of the detailed design work

MB noted that the locations of vulnerability of the areas groundwater and surface water resources were identified in the PCC slides. The ESR will identify the potential magnitude of impacts to these resources and recommend commitments to mitigate impacts.

MB asked if the Do Nothing alternative was included, as part of the evaluation of alternatives. AB clarified that Do Nothing was included as part of the evaluation of alternative solutions in Phase 2 of the study.

MB noted that since there will be some property acquisition for the project, has there been any concerns raised by the property owners so far? AB clarified that the information presented at the PCC#1 did not yet show the areas of potential property impacts, it is expected that the PCC#2 information will result in additional response from the public.

AB noted that as part of the consultation process, follow up contacts are made (calls or emails) after each notice is sent out, to each of the indigenous communities, to confirm that they received the notice and enquire if they have any questions or concerns. MB noted that as discussed with David Simpson, back in June regarding requests for capacity funding, the proponent is not obligated to provide such funding. However, if there are reasonable requests for funding, MECP strongly encourages the proponent to consider such request, to allow for a reasonable and adequate level of effective engagement by indigenous communities.

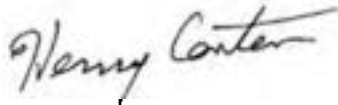
2. Adjournment

The meeting was adjourned by HC and the County thanking everyone for attending.

The preceding are the minutes of the meeting as observed by the undersigned. Should there be a need for revision, please advise Burnside within seven days of issuance. In the absence of notification to the contrary, these minutes will be deemed to be an accurate record of the meeting.

Minutes prepared by:

R.J. Burnside & Associates Limited



Henry Centen, P. Eng.

Senior Transportation Engineer

HC:js

Attachments:

Meeting Agenda

Draft PCC#2 Slides

Draft Corridor Improvement Plans

Distribution:

All Attendees

Other than by the addressee, copying or distribution of this document, in whole or in part, is not permitted without the express written consent of R.J. Burnside & Associates Limited.

TAC2 Minutes November 2022.docx
11/28/2022 1:50 PM



Meeting Agenda

Date and Time: November 17, 10:30am-12:00pm **Project No.:** 53425.0000

Project Name: Oxford Road 19 Environmental Assessment

Meeting Subject: Ex-TAC Meeting #2

Meeting Location: Teams Teleconference

Items

- Minutes of ExTAC Meeting #1 (May 10, 2022)
- Preliminary preferred solution
- Public Consultation Centre #1 (June 9, 2022) and Draft Summary Report (August 2022)
- Status of ongoing studies (Natural Environment, Archaeological, Cultural Heritage)
- Identification of conceptual design issues and identification of preliminary preferred design
- Review of draft slide deck for Public Consultation Centre #2 (December 6, 2022)
- Other matters



Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements

PUBLIC CONSULTATION CENTRE 2 / SPRINGFORD HALL
429 MAIN ST W SPRINGFORD, ON
DECEMBER 6, 2022
5:00 P.M. - 7:00 P.M.

WELCOME

to the **Public Consultation Centre 2** for the
Oxford Road 19 Corridor Improvements

Please Sign In

- Meet with Study Team Members
- Review the display materials and discuss your questions and ideas with the Study Team
- Please fill out a comment sheet and return it to the Study Team in person, by email or fax by **January 13, 2023**

STUDY AREA

The County is undertaking a Class EA study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study Area includes approximately 16 kilometres of Oxford Road 19 between Highway 19 (Plank Line), and the Norfolk County boundary (Windham Road 19), which excludes the Settlements of Springford and Otterville.



The site location and approximate extent of the Study Area are shown on the map.

PROBLEM/OPPORTUNITY STATEMENT

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

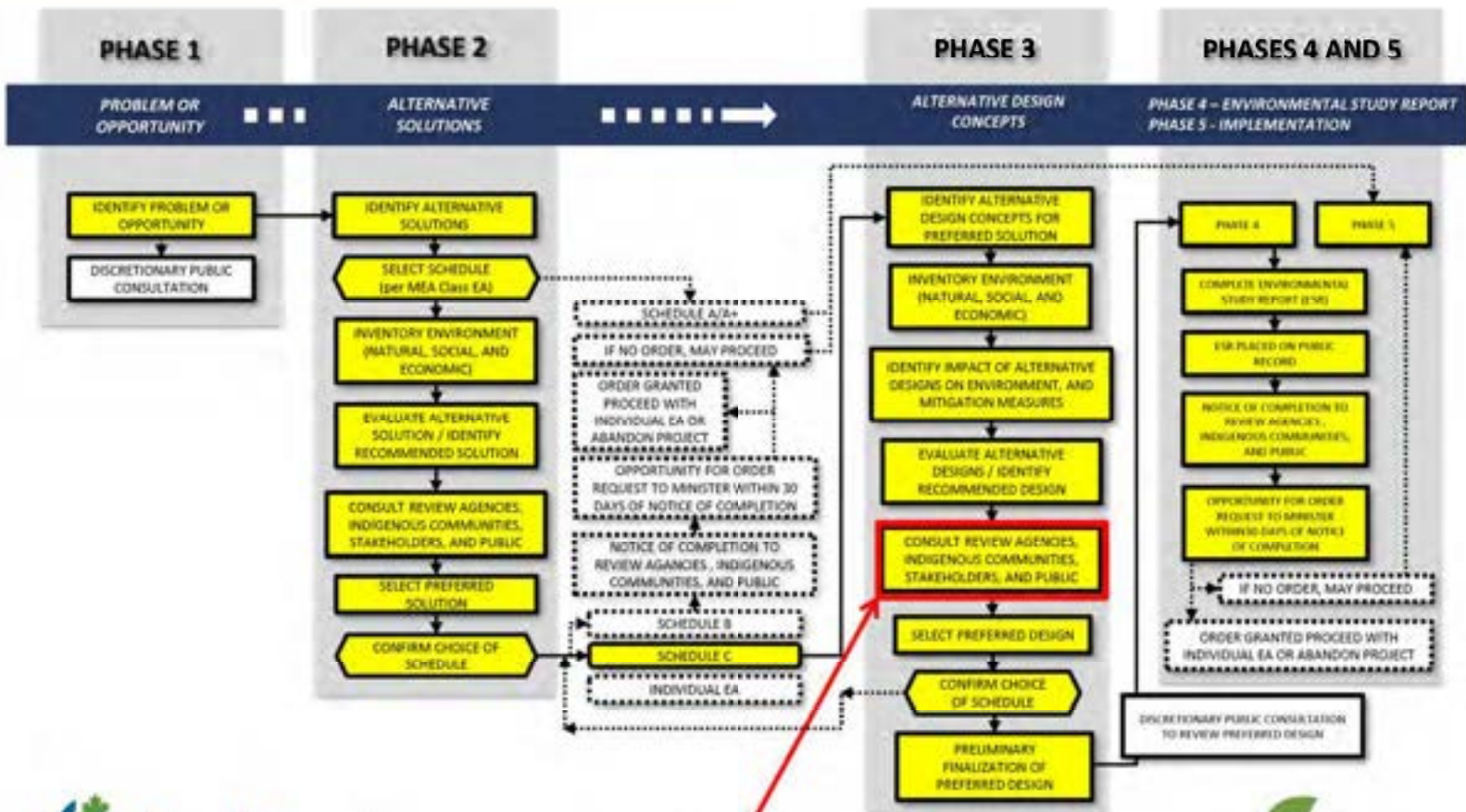


THE EA PROCESS

The Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act.

Nearing completion of the study, an Environmental Study Report (ESR) will be prepared and made available for final public review and comment.

THE EA PROCESS



SUPPORTING STUDIES

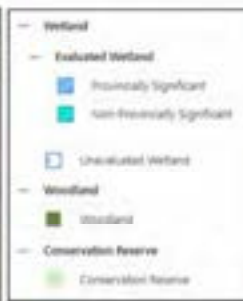
- Transportation Study (including Illumination Assessment)
- Natural Environment Assessment
 - Terrestrial Habitat Assessment
 - Aquatic Habitat Assessment
- Stage 1 Archaeological Assessment
- Cultural Heritage Resource Assessment
- Air and Noise Impact Assessments
- Stormwater Management Assessment



NATURAL HERITAGE RESOURCES

Designated Features within the Study Area

- Significant valleylands and locally significant natural heritage features
- Watercourses: Spittler Creek, Plumb Creek, and Big Otter Creek.
- Spittler Creek and Big Otter Creek are both associated with significant valleylands
- Otterville Provincially Significant Wetland (PSW) Complex and other wetlands across Spittler Creek
- Woodlands



NATURAL HERITAGE RESOURCES

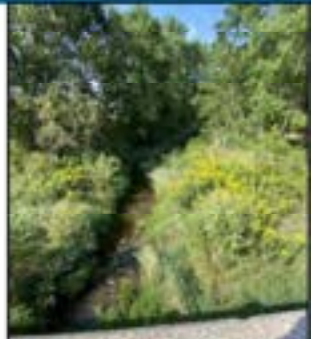
Aquatic habitat

No Species at Risk (SAR) were found through the desktop review.

- Big Otter Creek is a warm water creek with spring-spawning species such as Largemouth Bass and Yellow Perch.
- Spittler Creek is a cool water creek with observed spring-spawning species.
- Plumb Creek is a cool water creek with fall and spring-spawning species such as Brown Trout, Creek Chub, and Blacknose Dace.

Field work was conducted in the field season to assess aquatic habitat in the Study Area:

- Fish habitat was noted at all six crossings. All crossings has adequate, flow / depth, riparian life, and substrate providing suitable fish habitat.
- Large schools of bait fish were noted throughout both the upstream and downstream reaches of the crossings.
- Spawning Hornyhead Chub (*Nocomius biguttatus*) were observed in the downstream reach and underneath the clear span bridge at two of the crossings.



NATURAL HERITAGE RESOURCES

Terrestrial Habitat

The desktop review found several potential Species At Risk (SAR):

Snapping Turtle (Special Concern), Wood Thrush (Special Concern), American Badger (Endangered), American Chestnut (Endangered)

Field work was conducted in the field season to assess ecological communities present in the Study Area:

- Barn Swallows were observed foraging within agricultural fields. Potential habitat for Barn Swallow is restricted to crossing structures within the Study Area. Barn Swallow nests were not present on crossing structures at the time of field investigations. Consideration should be provided for this species during the detailed design phase as this species is known to nest within bridges and culverts.

A single Butternut was observed on the south side of the ROW. Additional surveys including a Butternut Health Assessment may be required at the detailed design phase if it is anticipated that the specimen will be impacted by road improvements.

Bats

The desktop review found records of 4 bat species: Eastern Small-footed Myotis, Little Brown Myotis, Northern Myotis, and Tricoloured Bat

Impact to potential habitat is not expected. Direct impact is to be avoided with timing vegetation removal to be completed outside of the active season for bats.



SOURCE WATER PROTECTION

The Study Area falls under the Long Point Region Source Water Protection Area and Lake Erie Source Water Protection Area.

Wellhead Protection Areas

Three portions of the study area have been identified to fall under Wellhead Protect Areas (i.e., part of Ostrander, north part of Springford and east of Otterville).



Issue Contributing Area

The area west of Otterville is also considered an Issue Contributing Area (ICA), for its potential to contribute elevated concentrations of particular substances to the drinking water source (e.g., chloride, sodium, nitrate).

Highly Vulnerable Aquifer

A significant part of the Oxford Road corridor (i.e., from west of Otterville to east of Oxford Road 59) is also located in a Highly Vulnerable Aquifer (HVA) area.

Significant Groundwater Recharge Areas

A significant part of the study area is also within Significant Groundwater Recharge Areas (SGRAs)

ARCHAEOLOGICAL RESOURCES

The Ministry of Citizenship and Multiculturalism (MCM) Standards & Guidelines lists criteria that are indicative of archaeological potential. The Stage 1 archaeological assessment showed that:

- 27 Previously identified archaeological sites within 1 km of the Study Area, 2 sites of which are located within the Study Area, and six of which are within 50 m of the Study Area;
- Springford Community Cemetery and Pine Street Burial Ground are located within 20 m of the Study Area. The legal cemetery boundaries shall be avoided by project designs.
- The Pettman Cemetery is approximately 100 m north of the Study Area, however the historic limits are unknown and therefore the lands within the Study Area require Stage 2 archaeological assessment.
- Stage 2 archaeological assessment is recommended in any other areas identified as exhibiting archaeological potential within the impact area of the project.
- Stage 2 archaeological assessment, if required, will be completed during the detailed design.



CULTURAL HERITAGE RESOURCES

Two known heritage properties, designated under Part IV of the Ontario Heritage Act, were identified within the project study area. A total of 5 Potential Built Heritage Resources and 23 Potential Cultural Heritage Landscapes were identified within the project study area.



Built Heritage Resources and Cultural Heritage Landscapes throughout the corridor

TRANSPORTATION STUDY DESIGN NEED ASSESSMENT

- Collision rates along the corridor in the study area (0.71 per 1 million vehicle km's travelled [MVKT] over the last 8 years) as compared to the provincial average collision rate of 1.53 per MVKT (2019), with no significant collision hot spots or patterns.
- No additional travel lanes are required on Oxford Road 19 to beyond year 2046
- A westbound left turn lane may be required at Highway 19 by 2046
- The existing pavement structure should be improved to address the deterioration and to allow for removal of half-load restrictions
- Oxford Road 19 is not part of the County's planned primary or secondary cycling networks, however it does provide connectivity to such cycling networks and trails
- Improvements to vertical or horizontal alignments of the road to be considered at 20 identified locations in the detailed designs, to improve traffic operations
- Right-of-way is recommended to be expanded to a minimum width of 30 metres in the rural area and 26 metres in Ostrander
- Improved illumination is warranted at the intersections with Highway 19 and Oxford Road 59.

AIR AND NOISE ASSESSMENT

Noise

- Traffic noise is forecasted to be 1 dBA over the daytime threshold level of 65 dBA (according to the Ministry of Transportation [MTO] Noise Guide) at two houses along the corridor, which triggers further investigation of noise impacts.

Further investigation of noise levels were completed at the two houses noted above, however no noise mitigation is warranted based on noise levels estimated within the various living spaces.

Air Quality

There are no houses on the road that will be exposed to a level of air contaminants that exceed criteria, except for the annual benzene impact.

- Annual benzene impact is unaffected by the local road and people would be impacted by the same level anywhere in the area, regardless of how close they were to this or any other road. Therefore, No significant impacts to air quality are forecasted to result from the proposed road improvements.

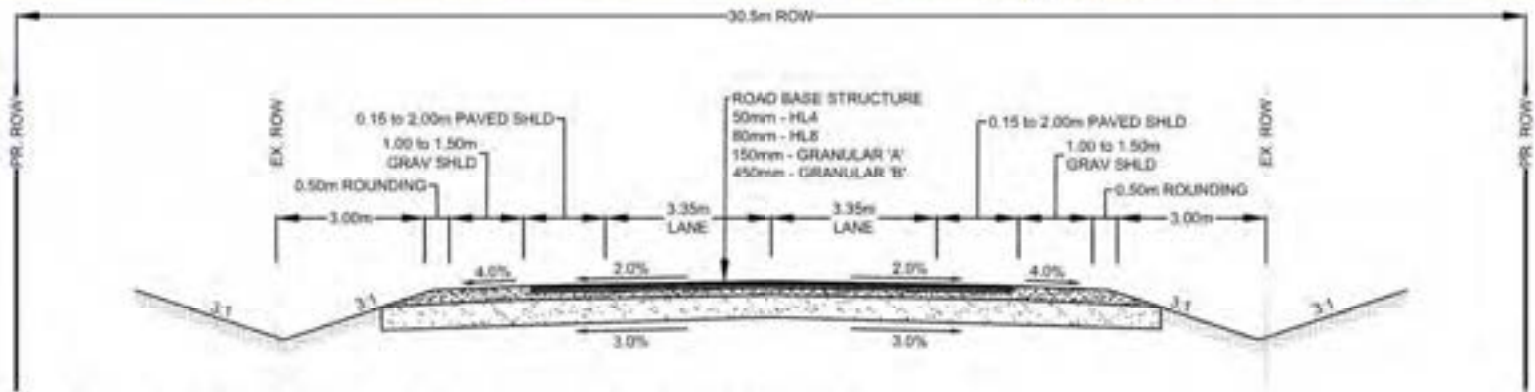
STORMWATER ASSESSMENT

- One structure was identified to have an existing width that will require widening to accommodate wider paved shoulders.
- Vegetated ditches will continue to provide roadside drainage requirements
- Impacts to surface water quantity or quality are expected to be minimal from the proposed project.
- Erosion and sediment control measures will be implemented during the construction phase.

Preferred Alternative Solution

Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW

- Provide a two-lane road to County requirements with paved shoulder.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.



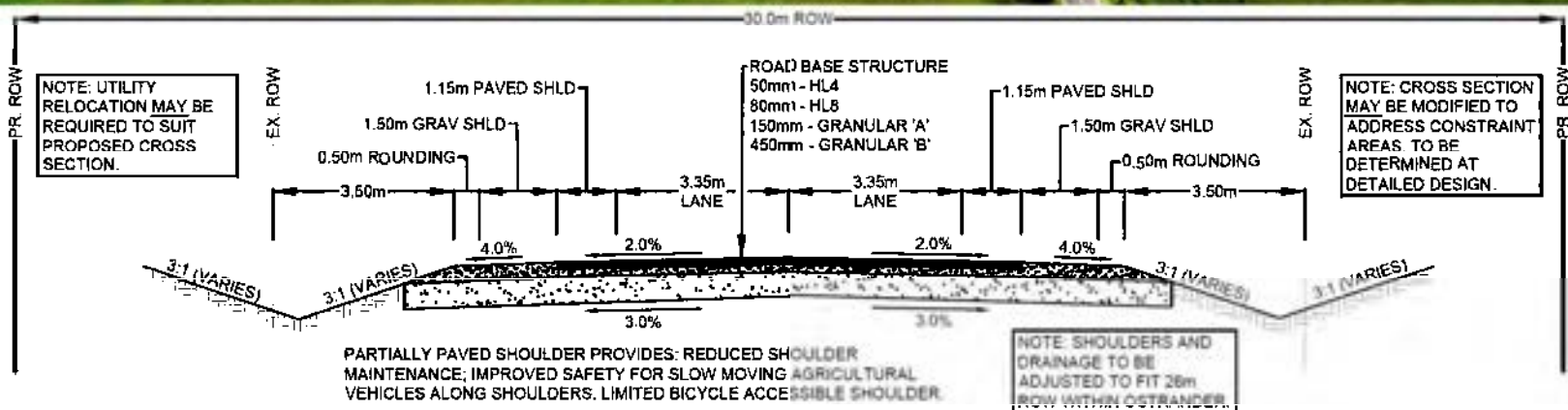
NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 5 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

Alternative Design Concepts

Alternative 1

- 9.0 m paved asphalt
- Each side with 3.35 m travel lane, 1.15 m paved shoulder and 1.5 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)

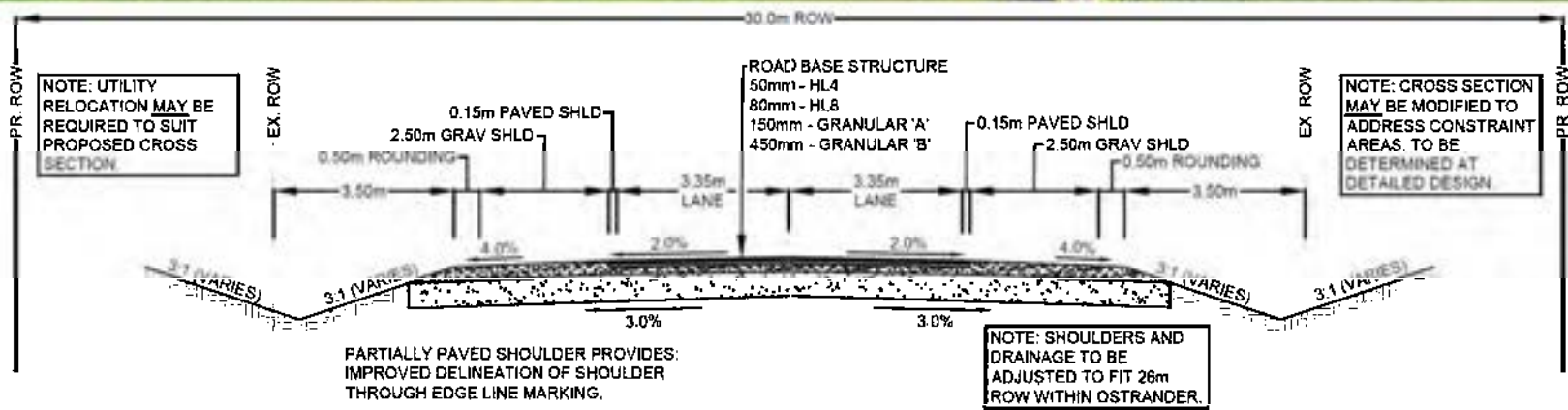


ALTERNATIVE 1 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

Alternative Design Concepts Alternative 2

- 7.0 m paved asphalt
- Each side with 3.35 m travel lane, 0.15 m paved shoulder and 2.5 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)



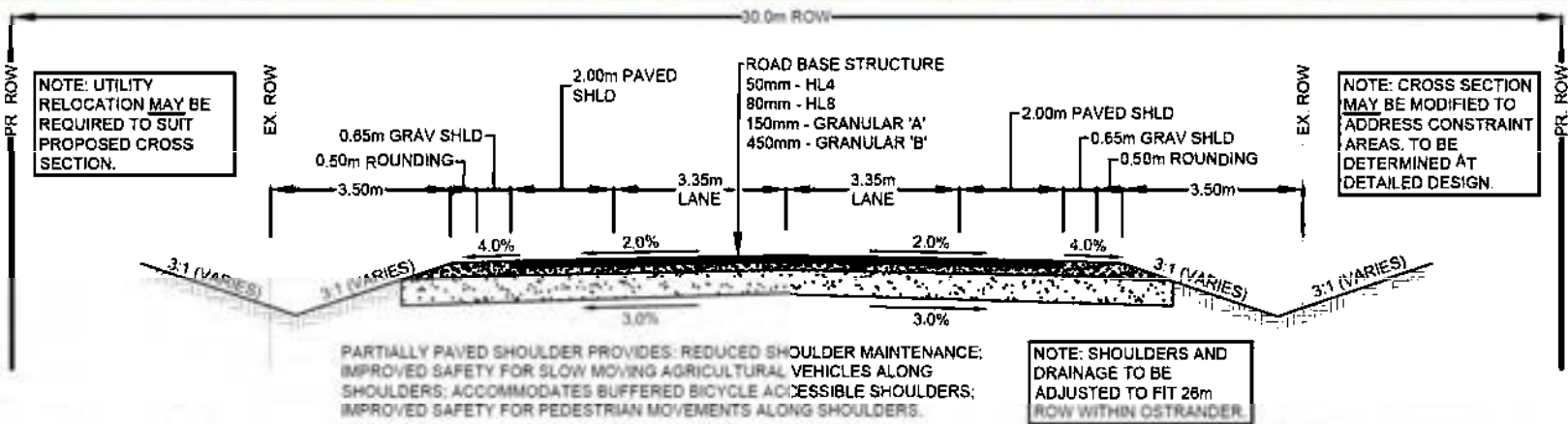
ALTERNATIVE 2 - 0.15m PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

Alternative Design Concepts

Alternative 3

- 10.7 m paved asphalt
- Each side with 3.35 m travel lane, 2.0 m paved shoulder and 0.65 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)



ALTERNATIVE 3 - 2.0m PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

EVALUATION CRITERIA

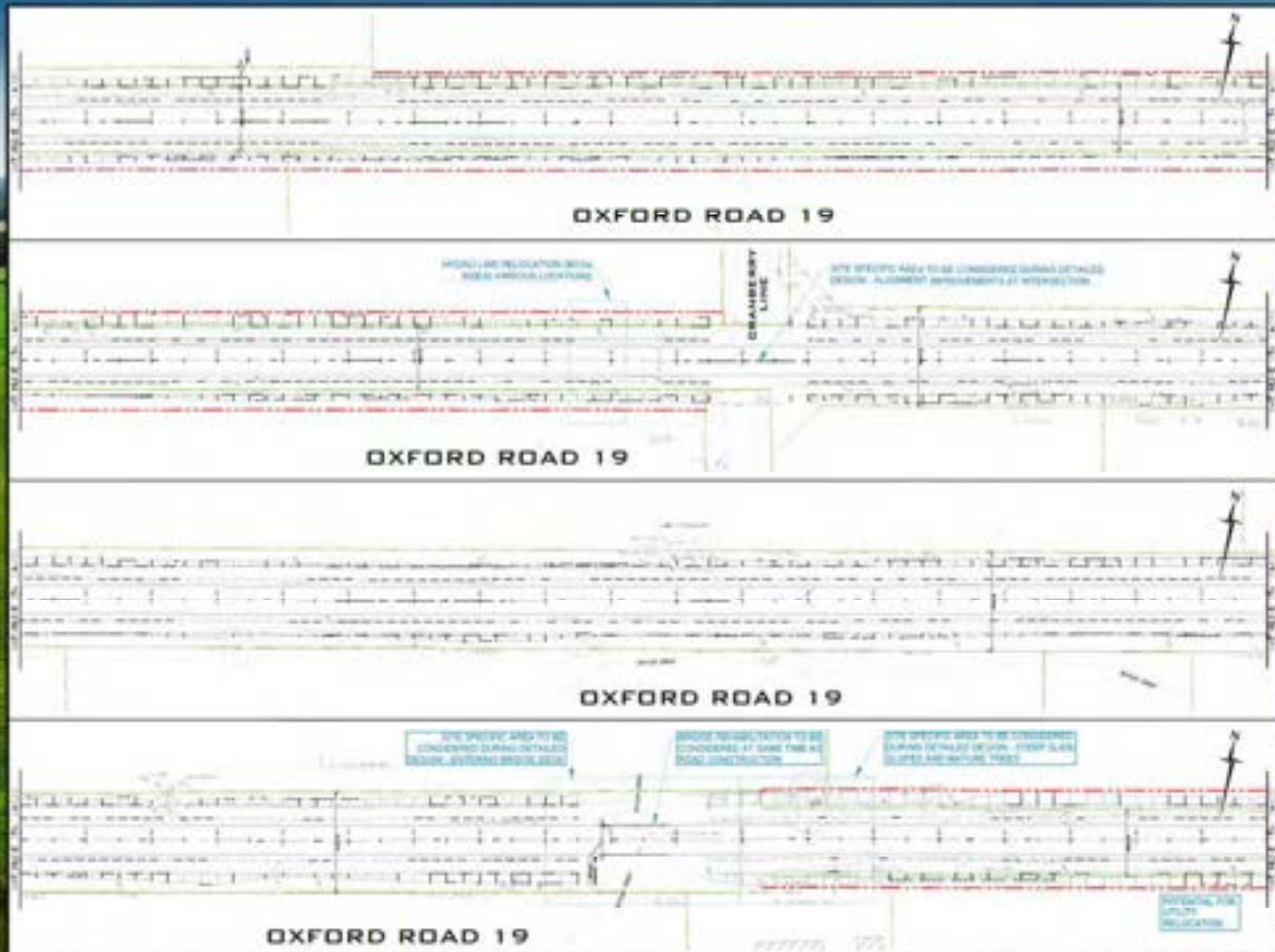
- **Natural Environment**
 - Impacts to vegetation and terrestrial habitat
 - Impacts to fisheries and aquatic habitat
 - Impacts to Hazard Lands (erosion, slope stability, flooding)
 - Impacts to Designated Features/species
 - Impacts to surface water quality and quantity
 - Impacts to groundwater quality and quantity
- **Technical Environment**
 - Accommodation of various modes of transportation and vehicle types
 - Improvement to road safety
 - Road maintenance requirements
 - Design complexity
 - Impacts to utilities and drainage work/structures
- **Socio-Cultural Environment**
 - Impacts to cultural heritage resources
 - Impacts to archaeological resources
 - Private property impacts
 - Nuisance impacts (noise, traffic, visual impact)
- **Financial**
 - Capital and operation/maintenance costs
 - Property acquisition cost

Evaluation Criteria	Each side with 3.35 m travel lane, 1.75 m paved shoulder and 1.5 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road	paved shoulder and 2.5 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road	Each side with 3.35 m travel lane, 2.0 m paved shoulder and 0.65 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road
Natural Environment	Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.	Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.	Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.
Socio-Cultural Environment	Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. Some dust created by agricultural equipment travel along gravel shoulders.	Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. Significant dust created by agricultural equipment travel along gravel shoulders.	Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. May lengthened the temporary construction period impacts if bridges require widening. Very little dust created by agricultural equipment travel along gravel shoulders.
Technical Environment	Wider paved shoulders provides increased spacing between traffic. Limited buffer for cyclists or pedestrians. Moderate improvement of safety for all travel modes using the corridor. Better able to accommodate full range of operating speeds. Moderate improvement to access operations along the corridor (pullover onto shoulder to exit roadway at accesses). Some adjustment of design required to interface the wider asphalt widths with crossings and accesses.	Minimal asphalt width provides for minimum space between traffic. Cyclists and pedestrians use gravel shoulder. Some improvement of safety for all travel modes using the corridor. Lower ability to accommodate vehicles with excessive speeds and / or slow-moving vehicles. No improvement to access operations along the corridor. Maintains existing asphalt interface with crossings (i.e., 3 bridges) and accesses.	Wider paved shoulders provides increased spacing between traffic. Increased buffer for cyclists and pedestrians. Significant improvement of safety for all travel modes using the corridor. Minimal gravel shoulder may direct agricultural equipment further onto paved areas and into travel lane. Better able to accommodate full range of operating speeds. Significant improvement to access operations along the corridor. Design may not be achievable in area of constraints without significant adjustment of such constraints. Potential for longer temporary construction impacts if structure widenings are required.
Economic Environment	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Moderate increase in cost over alternative 2, due to partially paved shoulder. Approximately \$118/m for shoulder. Some additional cost for adjustment of major bridge crossings.	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Lowest cost due to minimal paved shoulder. Approximately \$45/m for shoulder.	All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Higher increase in cost over alternative 2, due to fully paved shoulder. Approximately \$180/m for shoulder. Increased additional cost for adjustment of major bridge crossings.
Overall Summary	Most Preferred	Less Preferred	Least Preferred

Level of Preference: Least Preferred ○ Less Preferred ◐ Moderately Preferred ◑ More Preferred ◒ Most Preferred ◓

Preferred Design Concepts

Sample Plan



CONCEPT DRAWINGS OF THE FULL CORRIDOR ARE AVAILABLE TO VIEW, SHOWING EXISTING AND PROPOSED ROAD AND RIGHT-OF-WAY DETAILS

Pr. Property Line	---
Pr. Top Bank/Matchline	----
Pr. Ditchline	----
Pr. Edge of Shoulder	----
Pr. Edge of Pavement	----
Ex. Property Line	---

Proposed Mitigation Measures

A comprehensive list of proposed mitigation measures will be prepared as part of this EA and included in the Environmental Study Report (ESR).

Some key measures include:

- Health and safety is a priority for Oxford County. Construction will adhere to strict safety guidelines including best practices for vehicle and pedestrian safety.
- Advance notice to local residents of potential traffic impacts from construction.
- Access to residences/farms/pedestrian facilities will be maintained during construction.
- Temporary site fencing and other construction measures will be implemented to minimize noise, vibration, dust, mud and visual impacts.
- The following plans will be prepared by the contractor and implemented during construction: Erosion and Sediment Control Plan; Emergency Response and Communications Plan; Spill Prevention and Contingency Plans; Complaint Response Protocol; Construction Management Plan; Health and Safety Plans; Soil Management Plan; and Traffic Management Plan.

Proposed Mitigation Measures

Natural Heritage Specific Mitigation Measures:

- Plant species loss will be minimized.
- Tree inventory, preservation and protection plan to be prepared at detailed design.
- To reduce the risk of potential impact to wildlife, vegetation clearing will not be completed during appropriate timing to avoid the active period for breeding birds and bat species.
- Installation of temporary fencing as required.
- Any in-water works will occur during appropriate in-water works timing window.
- Refueling and maintenance of construction equipment shall occur within designated areas only. Any hazardous materials used for construction will be handled in accordance to appropriate regulations.
- Spills or depositions into watercourses shall be immediately contained and cleaned up in accordance with provincial regulatory requirements and the contingency plan.

Preferred Design Concepts

Future Detailed Design Considerations:

- Confirm utility relocation requirements (hydro poles, natural gas main, telecommunications)
- Confirm adjustments to Ostrander Municipal Drain
- Widen existing bridge at one location and coordinate with required bridge rehabilitation
- **Adjust cross section and/or drainage measures to address significant localized constraints (e.g., existing buildings, residential areas, topography, natural areas, interface with urban areas)**
- **Features to minimize impact on quality or quantity of stormwater runoff and erosion**
- **Localized improvements to vertical and/or horizontal alignments to improve sight lines**
- Traffic calming near village boundaries
- Finalize right-of-way requirements and property acquisition requirements
- Phasing strategies for construction, considering access and budget constraints



Next Steps

- Confirm Preferred Design Concepts (January 2023)
- Finalize Transportation Study (January 2023)
- Environmental Study Report (February 2023)
- File Environmental Study Report for Schedule C Class EA Study (March 2023)

Invitation for Participation

Public input is an important component of the decision-making process.

You are invited to provide comments by completing the forms provided and submitting forms to the Study Team members below on or before January 13, 2023.

Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com

THANK-YOU FOR ATTENDING



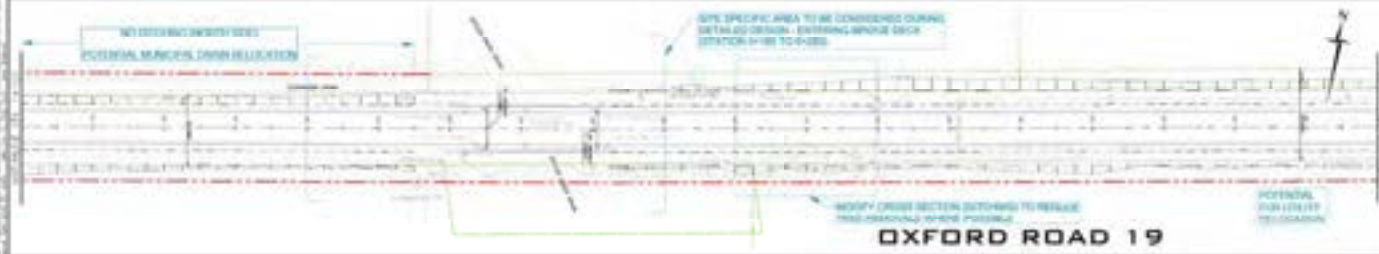
Oxford County
 Planning & Development
 1000 Main Street, Suite 100
 Oxford, VT 05455
 Phone: 802.243.4400
 Fax: 802.243.4401
 Email: info@oxfordcountyny.gov



NO.	DESCRIPTION	DATE	BY	CHECKED BY
1	ISSUED FOR PERMITTING	10/20/20	JL	ML
2	ISSUED FOR CONSTRUCTION	11/10/20	JL	ML
3	ISSUED FOR AS-BUILT	12/15/20	JL	ML
4	ISSUED FOR FINAL	01/15/21	JL	ML



NO.	DESCRIPTION	DATE	BY	CHECKED BY
1	ISSUED FOR PERMITTING	10/20/20	JL	ML
2	ISSUED FOR CONSTRUCTION	11/10/20	JL	ML
3	ISSUED FOR AS-BUILT	12/15/20	JL	ML
4	ISSUED FOR FINAL	01/15/21	JL	ML



OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 0+000 TO 1+480

DATE: 10/20/20

SCALE: 1"=40'

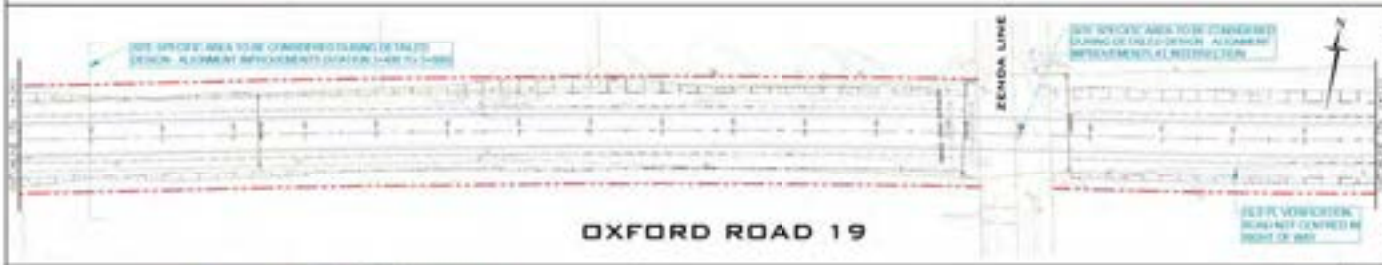
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CONTRACT NO.: 20-000

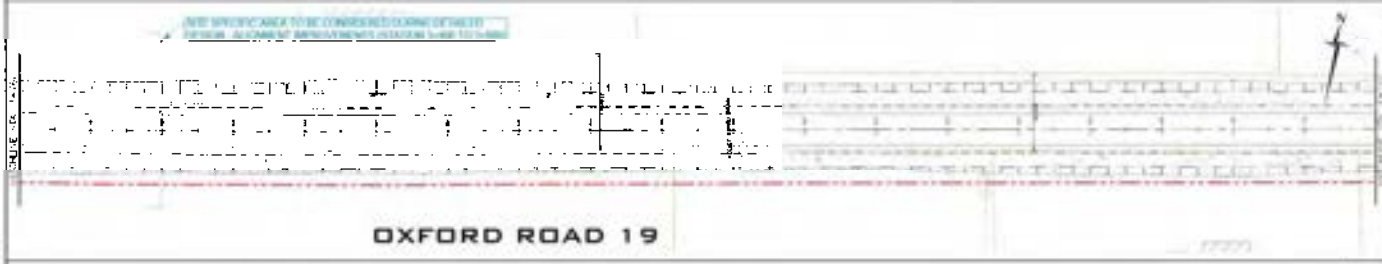
DRAWING NO.: 20-000



Oxford County
 Planning & Design Services
 1000 Highway 100
 Oxford, NC 27565
 Phone: 336.885.1234
 Fax: 336.885.1235
 www.oxfordcountync.gov



NO.	DESCRIPTION	DATE	BY	CHECKED BY
1	DESIGN	10/15/13	J. HARRIS	
2	REVISION	10/15/13	J. HARRIS	
3	REVISION	10/15/13	J. HARRIS	
4	REVISION	10/15/13	J. HARRIS	
5	REVISION	10/15/13	J. HARRIS	
6	REVISION	10/15/13	J. HARRIS	
7	REVISION	10/15/13	J. HARRIS	
8	REVISION	10/15/13	J. HARRIS	
9	REVISION	10/15/13	J. HARRIS	
10	REVISION	10/15/13	J. HARRIS	



DATE: 10/15/13	SCALE: 1/4" = 1'-0"
PROJECT: OXFORD ROAD 19	DATE: 10/15/13
DESIGNER: J. HARRIS	CHECKED: J. HARRIS
DATE: 10/15/13	SCALE: 1/4" = 1'-0"



OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 3+000 TO 4+500

DATE: 10/15/13

SCALE: 1/4" = 1'-0"

PROJECT: OXFORD ROAD 19

DATE: 10/15/13

DESIGNER: J. HARRIS

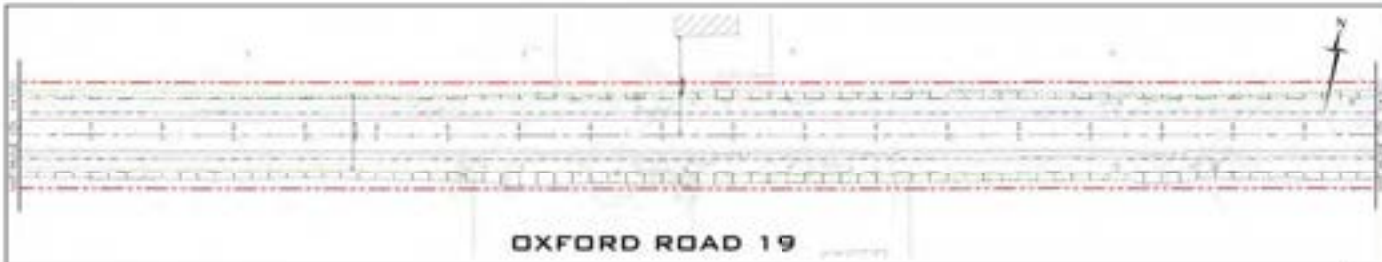
CHECKED: J. HARRIS

DATE: 10/15/13

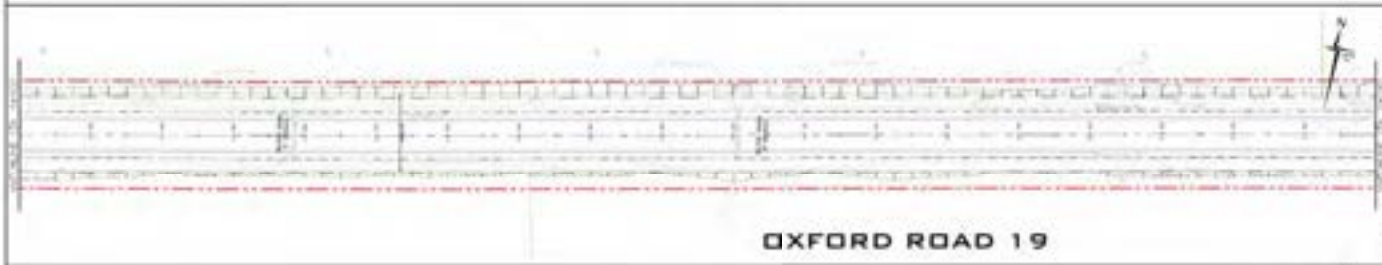
SCALE: 1/4" = 1'-0"

CONTRACT NO. 13-0000

DWG NO. PLAN 3



Oxford County
 Planning & Development
 100 Main Street
 Oxford, Ontario N6A 5R9
 Phone: 519-837-4100
 Fax: 519-837-4101
 Email: info@oxfordcountycanada.ca



Symbol	Description
[Symbol]	Proposed Roadway
[Symbol]	Proposed Pavement
[Symbol]	Proposed Drainage
[Symbol]	Proposed Utilities
[Symbol]	Proposed Landscaping
[Symbol]	Proposed Fencing
[Symbol]	Proposed Signage
[Symbol]	Proposed Lighting
[Symbol]	Proposed Safety Features
[Symbol]	Proposed Traffic Calming
[Symbol]	Proposed Pedestrian Facilities
[Symbol]	Proposed Bicycle Facilities
[Symbol]	Proposed Green Infrastructure
[Symbol]	Proposed Stormwater Management
[Symbol]	Proposed Noise Mitigation
[Symbol]	Proposed Air Quality Mitigation
[Symbol]	Proposed Cultural Heritage Mitigation
[Symbol]	Proposed Archaeological Mitigation
[Symbol]	Proposed Environmental Mitigation



Symbol	Description
[Symbol]	Proposed Roadway
[Symbol]	Proposed Pavement
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[Symbol]	Proposed Utilities
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[Symbol]	Proposed Fencing
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[Symbol]	Proposed Environmental Mitigation



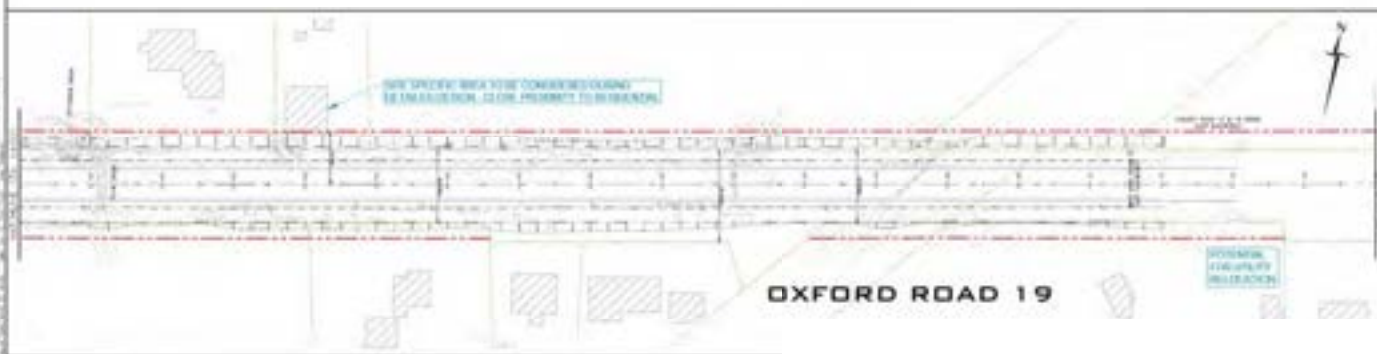
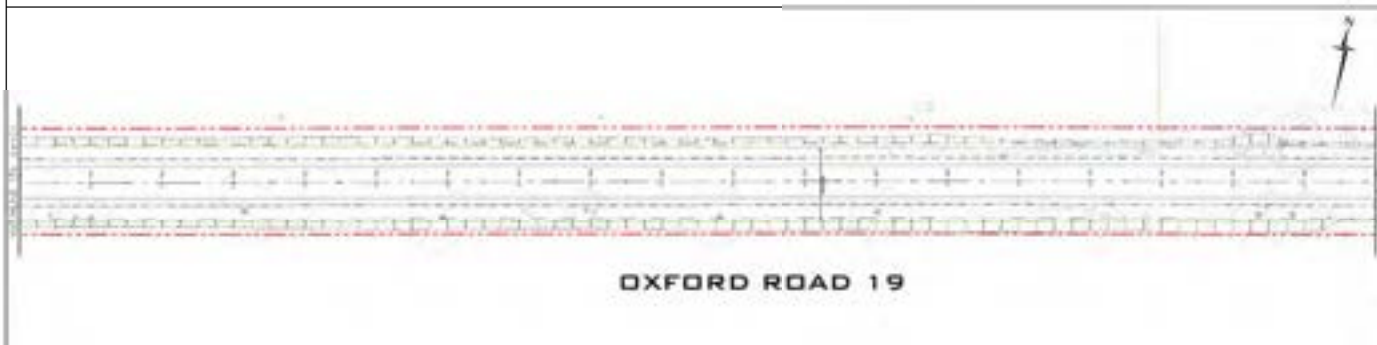
OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
 PLAN 4-020 TO 4-040

DATE: 2023-03-01
 SCALE: 1:500
 CONTRACT NO: 2022-001
 DRAWING NO: 4-020

OXFORD COUNTY
 Planning & Development

SEE SPECIFICATIONS TO STANDARD DRAWINGS FOR MATERIALS AND CONSTRUCTION REQUIREMENTS.

OXFORD COUNTY
 2023-03-01



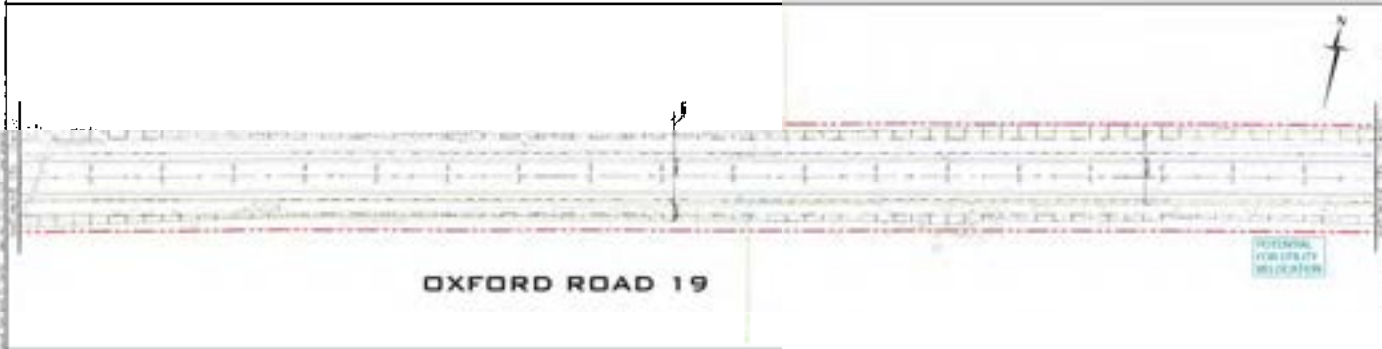
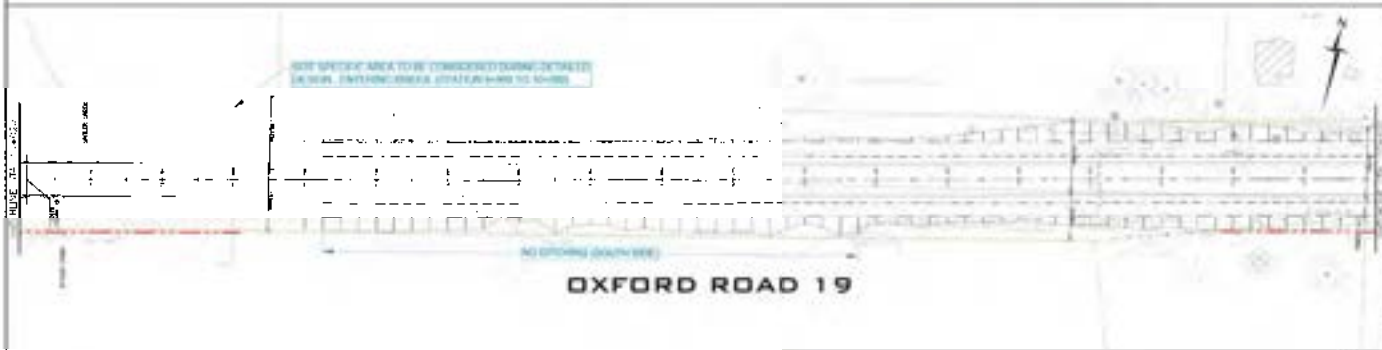
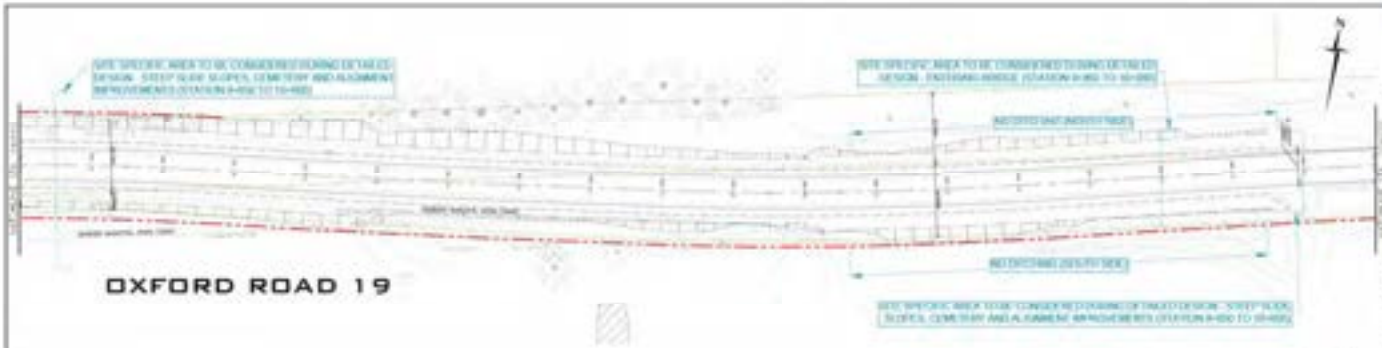
Oxford County
 Planning & Development Department
 1000 Highway 100, Oxford, ON N6A 5R9
 Phone: (519) 835-2200
 Fax: (519) 835-2201
 Email: info@oxfordcountycanada.ca

Legend

Proposed Roadway	Proposed Right-of-Way
Proposed Lane	Proposed Shoulder
Proposed Median	Proposed Gravel
Proposed Concrete	Proposed Asphalt
Proposed Gravel	Proposed Sand
Proposed Sand	Proposed Stone
Proposed Stone	Proposed Earth
Proposed Earth	Proposed Slope
Proposed Slope	Proposed Embankment
Proposed Embankment	Proposed Cut
Proposed Cut	Proposed Retention Wall
Proposed Retention Wall	Proposed Bridge
Proposed Bridge	Proposed Structure
Proposed Structure	Proposed Utility
Proposed Utility	Proposed Easement
Proposed Easement	Proposed Right-of-Way

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
 PLAN 6+00 TO 7+100

DATE: 10/10/2018
 SCALE: 1:500
 CONTRACT NO: 18-001
 DRAWING NO: P-101

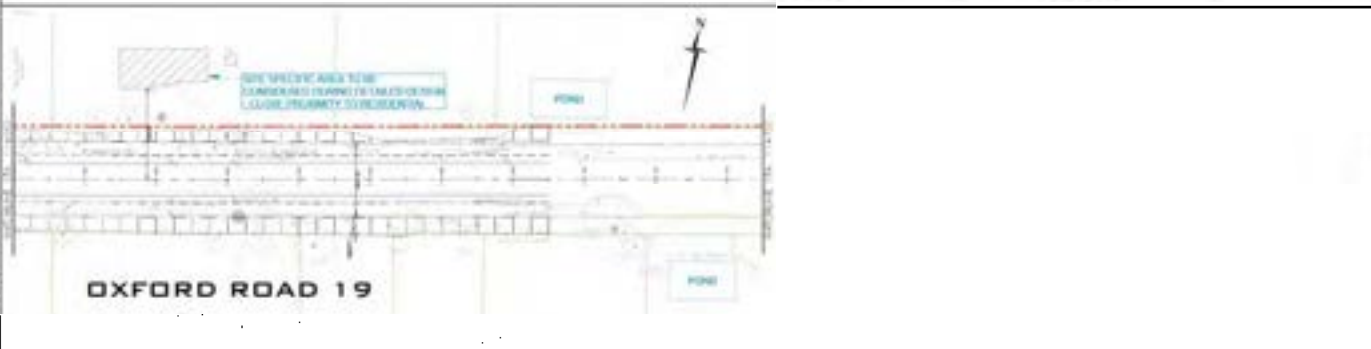


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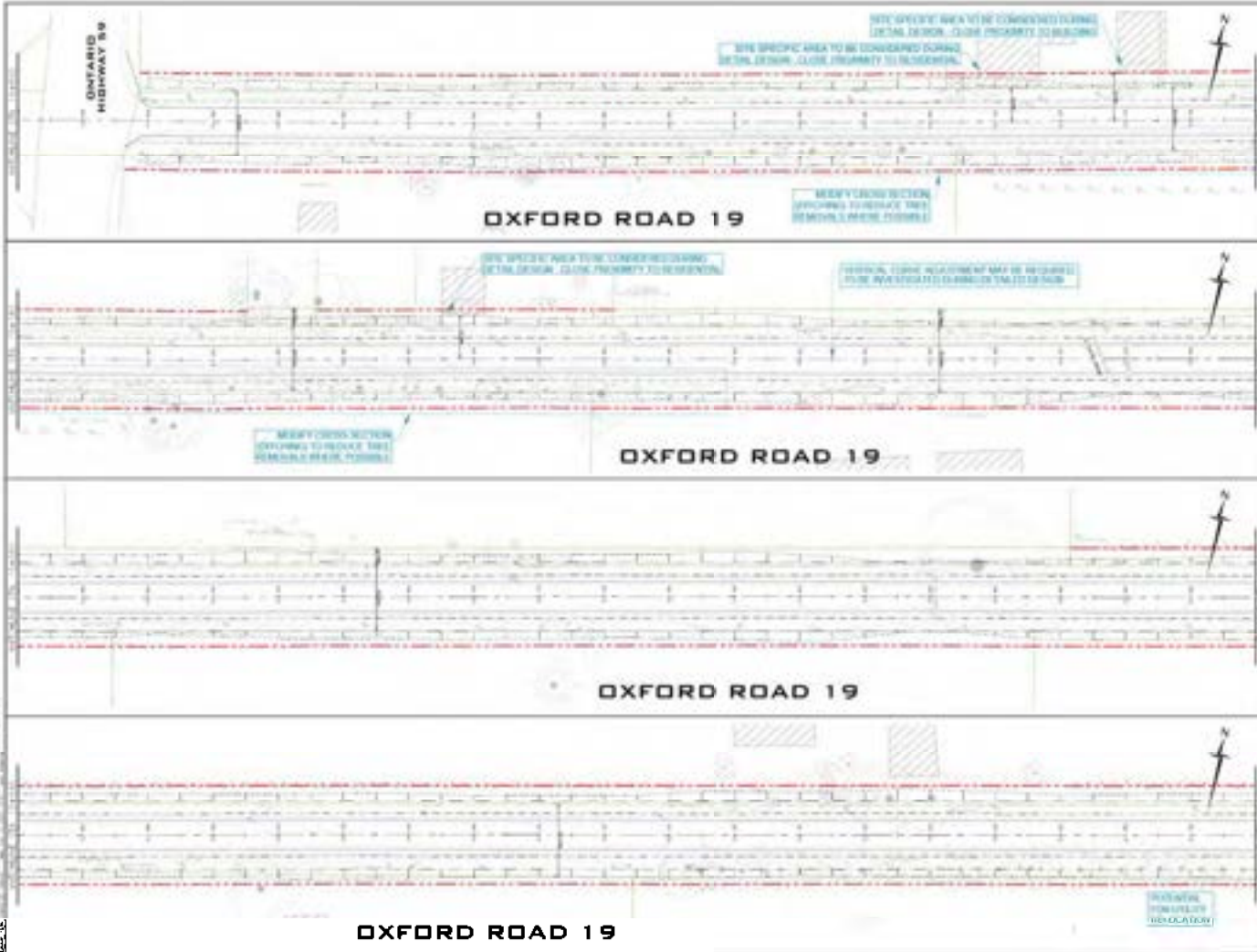
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PROJECT NAME	OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
PROJECT LOCATION	PLAN 0-640 TO 10-700
DATE	04/10/2010
SCALE	1:1000
DRAWN BY	...
CHECKED BY	...
APPROVED BY	...
DATE	04/10/2010
CONTRACT NO.	...
DWG NO./PLAN	...



PROJECT INFORMATION	
Project Name	OXFORD ROAD 19
Client	OXFORD COUNTY
Project No.	10+780 TO 11+750
Scale	1:500
Sheet No.	10+780 TO 11+750
Revision	
Author	
Checked	
Approved	
Date	



DESIGNED BY: IC		DATE:
DRAWN BY: IC		
CHECKED BY: IC		
APPROVED BY: IC		
OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT		
PLAN 10+780 TO 11+750		
SCALE: 1:500		
SHEET NO. 10+780	TO 11+750	
CONTRACT NO.		
DWG NO. PLAN 8		



Oxford County
Building a better future

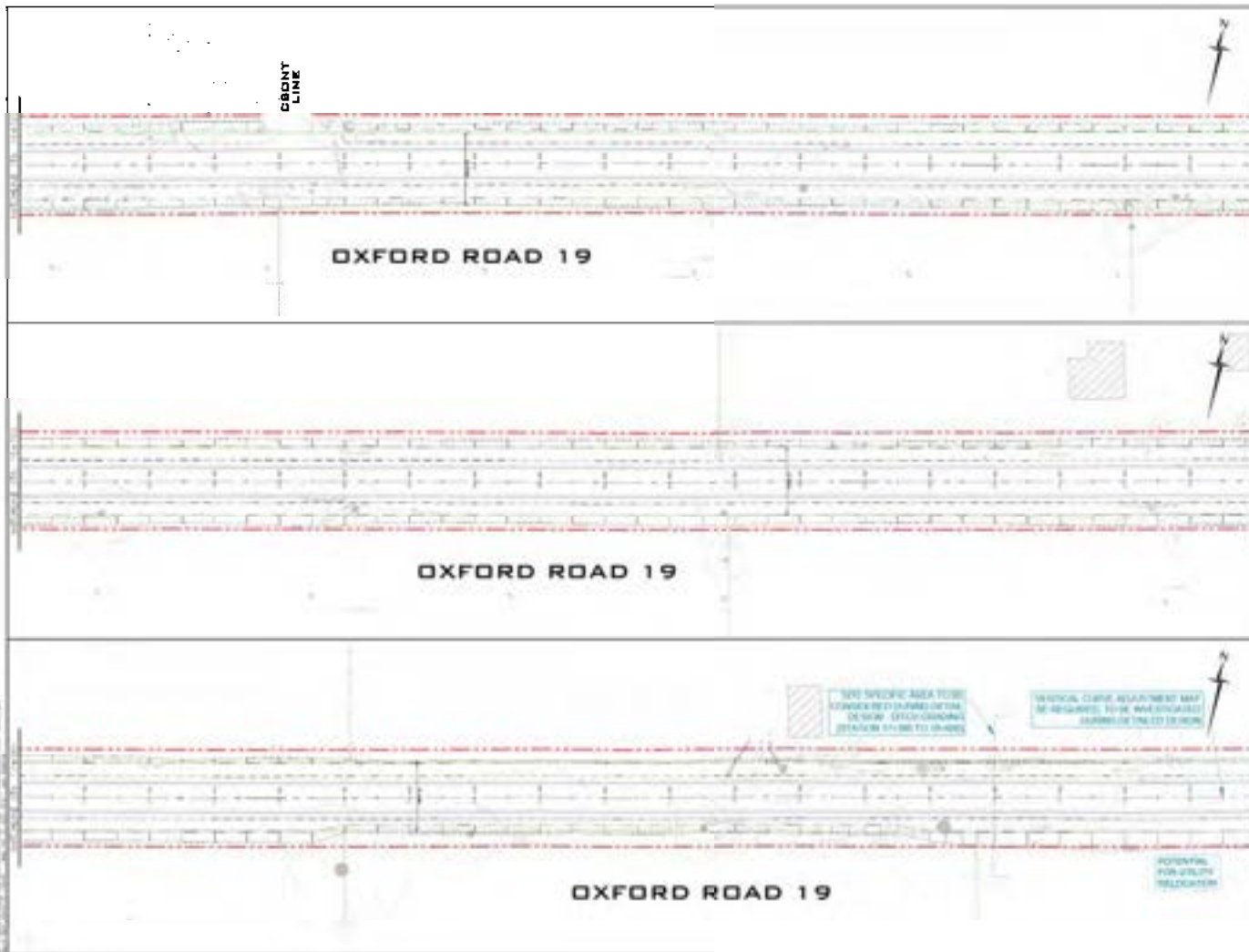
1000 Wellington Street West
 Oxford, ON N6H 2C5
 Phone: 519-835-2222
 Fax: 519-835-2223
 Email: info@oxfordcountycanada.com

Legend

Proposed Roadway	Proposed Roadway
Proposed Right-of-Way	Proposed Right-of-Way
Proposed Utility	Proposed Utility
Proposed Easement	Proposed Easement
Proposed Structure	Proposed Structure
Proposed Fence	Proposed Fence
Proposed Sign	Proposed Sign
Proposed Light	Proposed Light
Proposed Tree	Proposed Tree
Proposed Plant	Proposed Plant
Proposed Water	Proposed Water
Proposed Sewer	Proposed Sewer
Proposed Gas	Proposed Gas
Proposed Electric	Proposed Electric
Proposed Telephone	Proposed Telephone
Proposed Cable	Proposed Cable
Proposed Storm	Proposed Storm
Proposed Drain	Proposed Drain
Proposed Culvert	Proposed Culvert
Proposed Bridge	Proposed Bridge
Proposed Tunnel	Proposed Tunnel
Proposed Structure	Proposed Structure
Proposed Fence	Proposed Fence
Proposed Sign	Proposed Sign
Proposed Light	Proposed Light
Proposed Tree	Proposed Tree
Proposed Plant	Proposed Plant
Proposed Water	Proposed Water
Proposed Sewer	Proposed Sewer
Proposed Gas	Proposed Gas
Proposed Electric	Proposed Electric
Proposed Telephone	Proposed Telephone
Proposed Cable	Proposed Cable
Proposed Storm	Proposed Storm
Proposed Drain	Proposed Drain
Proposed Culvert	Proposed Culvert
Proposed Bridge	Proposed Bridge
Proposed Tunnel	Proposed Tunnel

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
 PLAN 14+880 TO 16+320

DATE: 14+880 TO 16+320
 DATE: 14+880 TO 16+320
 CONTRACT NO.:
 DRAWING NO.:



Oxford County
 Planning & Development
 100 Main Street, Suite 200
 Oxford, VT 05455
 Phone: 802.243.4300
 Fax: 802.243.4301
 www.oxfordcountyvt.gov

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT
 PLAN 16+320 TO 17+460

DATE: 08/14/2018
 SCALE: 1"=40'

CONTRACT NO. 17-146
 SHEET NO. 17-146-01



Minutes of Meeting

Meeting Date: November 17 2022 **Project No.:** 300053425.0000
Project Name: Oxford Rd. 19 Environmental Assessment
Meeting Subject: TAC Meeting #2
Meeting Location: Video Conference
Date Prepared: November 28, 2022

Those in attendance were:

Mark Badali	MECP	Mark.Badali1@ontario.ca
Isabel Johnson	LPRCA	ijohnson@lprca.on.ca
Allan Hodgins	MTO	Allan.Hodgins@ontario.ca
Daniel Leduc	Township of Southwest Oxford (Drainage)	Drainage@swox.ca
Jesse Keith	Oxford County (County)	jkeith@oxfordcounty.ca
Reuben Davis	County	rdavis@oxfordcounty.ca
Frank Gross	County	fgross@oxfordcounty.ca
Heather St. Clair	County	hstclair@oxfordcounty.ca
Henry Centen	R.J. Burnside and Associates Ltd. (Burnside)	Henry.Centen@rjburnside.com
Avid Banihashemi	Burnside	Avid.Banihashemi@rjburnside.com
Jeremy Taylor	Burnside	Jeremy.taylor@rjburnside.com
Amy Boddy	Burnside	Amy.Boddy@rjburnside.com
Michael Siemon	Burnside	Michael.Siemon@rjburnside.com

The following items were discussed

Action by

1. Project Background

- 1.1** The meeting started by welcoming everyone and a round of introduction.
- Henry Centen (HC), R.J. Burnside (Burnside) reviewed the purpose of the TAC meeting and walked through the listed items on the agenda. The agenda included the following items:
- Minutes of ExTAC Meeting #1 (May 10, 2022)
 - Preliminary preferred solution

The following items were discussed	Action by
<ul style="list-style-type: none">• Public Consultation Centre #1 (June 9, 2022) and Draft Summary Report (August 2022)• Status of ongoing studies (Natural Environment, Archaeological, Cultural Heritage)• Identification of conceptual design issues and identification of preliminary preferred design• Review of draft slide deck for Public Consultation Centre #2 (December 6, 2022)• Other matters <p>HC noted that he will review with TAC the slides which would form the basis of the material that will be presented at the upcoming Public Consultation Centre (PCC #2). The presentation will be provided to the TAC members along with the meeting minutes for further review.</p>	
1.2 Review of ExTAC Meeting #1	
<p>1.2.1 HC reviewed the discussions from ExTAC#1:</p> <p>The comments from MTO in the first ExTAC meeting were related to Highway 19, and any intersection improvements at 19 that MTO would have to review and be involved in approving for those improvements.</p> <p>The conservation authority at the ExTAC#1 indicated that they would be involved in reviewing, especially during construction or during the detailed design on erosion, sediment control and maintaining the morphology of the watercourses in the area, and any mitigation work.</p> <p>MECP at the ExTAC#1 indicated that they would be reviewing the ESR and the mitigation, in particular the mitigation measures that are being proposed.</p>	
1.3 Preliminary Preferred Solution	
<p>1.3.1 The preliminary preferred solution was the hybrid solution, which was to remove the half load designation by improving the structural capacity of the road and also widening the shoulders.</p> <p>A 30 m meter right-of-way (ROW) (approximately equivalent of the 100 foot right of way within the Official Plan (OP)) was anticipated. The project team proceeded with that in the rural area where the 30 meter ROW is desirable to accommodate all of the drainage in the widened cross section. A subsequent discussion with the</p>	

The following items were discussed	Action by
<p>County had indicated that in the area of Ostrander, which is an area adjacent to highway 19 there is strip residential development and that a 26 m ROW be applied in that area (consistent with the OP requirements for settlement areas).</p>	
<p>1.4 Public Consultation Centre #1 Input Summary</p>	
<p>1.4.1 Avid Banihashemi (AB) gave a summary of input to date from agencies, Indigenous communities and the public.</p> <p>It was noted that there had been no concerns from <i>agencies</i> thus far, and no comments from any <i>utilities</i>.</p> <p><i>Indigenous communities</i> have not sent any specific comments or concerns at this time. However, correspondence from both Chippewas of the Thames and Mississauga's of the Credit First Nation would like to be involved, with Archaeological Assessment and the Natural Environment work, however, have no capacity at this time.</p> <p>Stage 1 Archaeological Assessment is a desk top exercise and if a Stage 2 Archaeological Assessment is indicated it will be done at detailed design. If a Stage 2 is warranted the Mississauga's of the Credit First Nation have requested to be contacted and involved in the field work.</p> <p><i>Public Comments</i> have been of a general nature, such as concern for repeated issues from previous construction projects and how they impacted local farms and businesses, speed issues, truck movement and whether the improvements will cause additional traffic and noise impact, some road visibility issues i.e., the (road's horizontal alignment and vertical alignment), and some drainage issues along the road.</p>	
<p>1.5 Status of Ongoing Studies</p>	
<p>1.5.1 Natural Environment: desktop and fieldwork is complete: ongoing impact assessment and identification of mitigation measures</p> <p>Archaeological and Cultural Heritage: Existing conditions assessment is complete, ongoing impact assessment and identification of mitigation measures</p> <p>Transportation study (also includes illumination review): Ongoing, generally complete for identifying the preliminary preferred design concept.</p>	

The following items were discussed

Action by

Stormwater: ongoing, review of the corridor and the potential for the impact is complete.

Noise and Air Quality Impact Assessment: The potential impacts assessment along the corridor is complete.

AB noted that as the project moves forward, the team is producing mitigation measures of proposed design concepts on different areas including natural environment and on archaeology and cultural heritage resources, stormwater, air and noise, and other areas. These mitigation measures will be included in a complete table in the environmental study report, which will be available for review.

1.6 Identification of conceptual design issues and identification of preliminary preferred design

1.6.1 HC reviewed the conceptual design considerations:

- No collision hotspots were noted along the corridor; however it is anticipated that the proposed work will improve the safety of traffic operations along the corridor overall.
- There's a need for only two lanes along Oxford Roads 19, given the volumes of traffic up to beyond 2046.
- For the corridor east of Otterville between Otterville and County Road 59, there were no significant improvements identified, since it already has a 30 meter ROW and was previously improved with works in Otterville. This segment also has shoulders that are partly paved, Drainage might be the one item that the municipality may want to consider upgrading.
- On other segments of the corridor, there is a benefit to having some pavement on the shoulders to accommodate cycling and to make it safer for cycling, but also for movement of agricultural equipment along the corridor.
- Vertical and horizontal alignments were considered along the corridor. 20 locations of potential alignment improvements were identified to be reviewed further at the detailed design.
- The 30 m ROW is being recommended to address the Official Plan requirement. but also to accommodate the flexibility and the needs associated with drainage (e.g. open ditch drainage, etc.) along the corridor. 26 m ROW in Ostrander is being recommended since it is a lower speed environment and has the potential for adjusting the cross sections in that area, also in order to accommodate the strip residential development in that area.

The following items were discussed	Action by
1.7 Review of draft slide deck for Public Consultation Centre #2	
1.7.1 HC and AB went through the PCC#2 draft slides (attached to these minutes).	
1.8 ExTAC Members Comments	
1.8.1 <i>MTO Comments</i> Allan Hodgins (AH) noted that as the project moves along any improvements to the intersection of Oxford Road 19 (Plank Line) needs to be reviewed by MTO. He specified further that for anything within 180 meters east or west of highway 19 (MTO permit control for the highway) an early consultation is appreciated to ensure that the permits are issued in a timely manner, if any are required. Also coordination of should ensure that the timing fits with any adjacent MTO works. Once concept plans are available, they can be shared with MTO to identify what's needed from MTO's perspective (illumination, stormwater work, etc.). On illumination, AH noted that MTO will need to verify the locations and how the proposed illumination impacts them. He added that a lot of MTO's provincial highways aren't signalized intersections so it's hard to get a warrant after lighting is already in place. So, it is important to make sure we're all on the same page before it gets too far along. At the detailed design, any works associated with either a turning lane or illumination or even just the connection itself would be subject to MTO approvals and review. HC noted that the ESR will identify the concept designs for illumination, for review and comment by the MTO.	
1.8.2 <i>MECP Comments</i> Mark Badali (MB) asked if the air quality impact assessment that was completed, if it was qualitative or quantitative. HC responded that a quantitative assessment was completed, considering the traffic volumes and the truck volumes etc, and how that would impact the overall air quality in that area. MB noted that the ministry is looking to have some consideration of climate change. In the ESR we'll be looking to have some sort of discussion notes. AB confirmed that a Climate Change section will be included in the ESR, covering both potential impacts of the project on climate change, and the potential impacts of climate change on the project.	

The following items were discussed

Action by

MB asked about the noise study and whether MECP criteria were applied. HC responded that MTO noise guidelines have been applied and further comparisons on MECP noise criteria will be made in the ESR

MB noted that the project has identified potential habitat for Barn Swallow and butternut. MECP Species at Risk branch tends to get more involved at the detailed design stage, when location-specific impacts are clearly identified. AB clarified that continuing the consultation with MECP regarding SAR and SAR habitat and the potential impacts, will be included as a commitment in the ESR, to be part of the detailed design work

MB noted that the locations of vulnerability of the areas groundwater and surface water resources were identified in the PCC slides. The ESR will identify the potential magnitude of impacts to these resources and recommend commitments to mitigate impacts.

MB asked if the Do Nothing alternative was included, as part of the evaluation of alternatives. AB clarified that Do Nothing was included as part of the evaluation of alternative solutions in Phase 2 of the study.

MB noted that since there will be some property acquisition for the project, has there been any concerns raised by the property owners so far? AB clarified that the information presented at the PCC#1 did not yet show the areas of potential property impacts, it is expected that the PCC#2 information will result in additional response from the public.

AB noted that as part of the consultation process, follow up contacts are made (calls or emails) after each notice is sent out, to each of the indigenous communities, to confirm that they received the notice and enquire if they have any questions or concerns. MB noted that as discussed with David Simpson, back in June regarding requests for capacity funding, the proponent is not obligated to provide such funding. However, if there are reasonable requests for funding, MECP strongly encourages the proponent to consider such request, to allow for a reasonable and adequate level of effective engagement by indigenous communities.

2. Adjournment

The meeting was adjourned by HC and the County thanking everyone for attending.

The preceding are the minutes of the meeting as observed by the undersigned. Should there be a need for revision, please advise Burnside within seven days of issuance. In the absence of notification to the contrary, these minutes will be deemed to be an accurate record of the meeting.

Minutes prepared by:

R.J. Burnside & Associates Limited

A handwritten signature in black ink that reads "Henry Centen". The signature is written in a cursive, flowing style.

Henry Centen, P. Eng.

Senior Transportation Engineer

HC:js

Attachments:

Meeting Agenda

Draft PCC#2 Slides

Draft Corridor Improvement Plans

Distribution:

All Attendees

Other than by the addressee, copying or distribution of this document, in whole or in part, is not permitted without the express written consent of R.J. Burnside & Associates Limited.



Meeting Agenda

Date and Time: November 17, 10:30am-12:00pm **Project No.:** 53425.0000

Project Name: Oxford Road 19 Environmental Assessment

Meeting Subject: Ex-TAC Meeting #2

Meeting Location: Teams Teleconference

Items

- Minutes of ExTAC Meeting #1 (May 10, 2022)
- Preliminary preferred solution
- Public Consultation Centre #1 (June 9, 2022) and Draft Summary Report (August 2022)
- Status of ongoing studies (Natural Environment, Archaeological, Cultural Heritage)
- Identification of conceptual design issues and identification of preliminary preferred design
- Review of draft slide deck for Public Consultation Centre #2 (December 6, 2022)
- Other matters



Municipal Class Environmental Assessment Study for Oxford Road 19 Corridor Improvements

PUBLIC CONSULTATION CENTRE 2 / SPRINGFORD HALL
429 MAIN ST W SPRINGFORD, ON
DECEMBER 6, 2022
5:00 P.M. - 7:00 P.M.

WELCOME

to the Public Consultation Centre 2 for the Oxford Road 19 Corridor Improvements

Please Sign In

- Meet with Study Team Members
- Review the display materials and discuss your questions and ideas with the Study Team
- Please fill out a comment sheet and return it to the Study Team in person, by email or fax by **January 13, 2023**

STUDY AREA

The County is undertaking a Class EA study to consider improvement options for the Oxford Road 19 corridor to suit anticipated transportation demands for the 25-year horizon and beyond.

The Study Area includes approximately 16 kilometres of Oxford Road 19 between Highway 19 (Plank Line), and the Norfolk County boundary (Windham Road 19), which excludes the Settlements of Springford and Otterville.



The site location and approximate extent of the Study Area are shown on the map.

PROBLEM/OPPORTUNITY STATEMENT

Following completion of the County of Oxford's Transportation Master Plan, the County of Oxford has identified the need to improve Oxford Road 19 between Highway 19 and the boundary of Norfolk County to support the safe and efficient movement of people and goods to 2046.

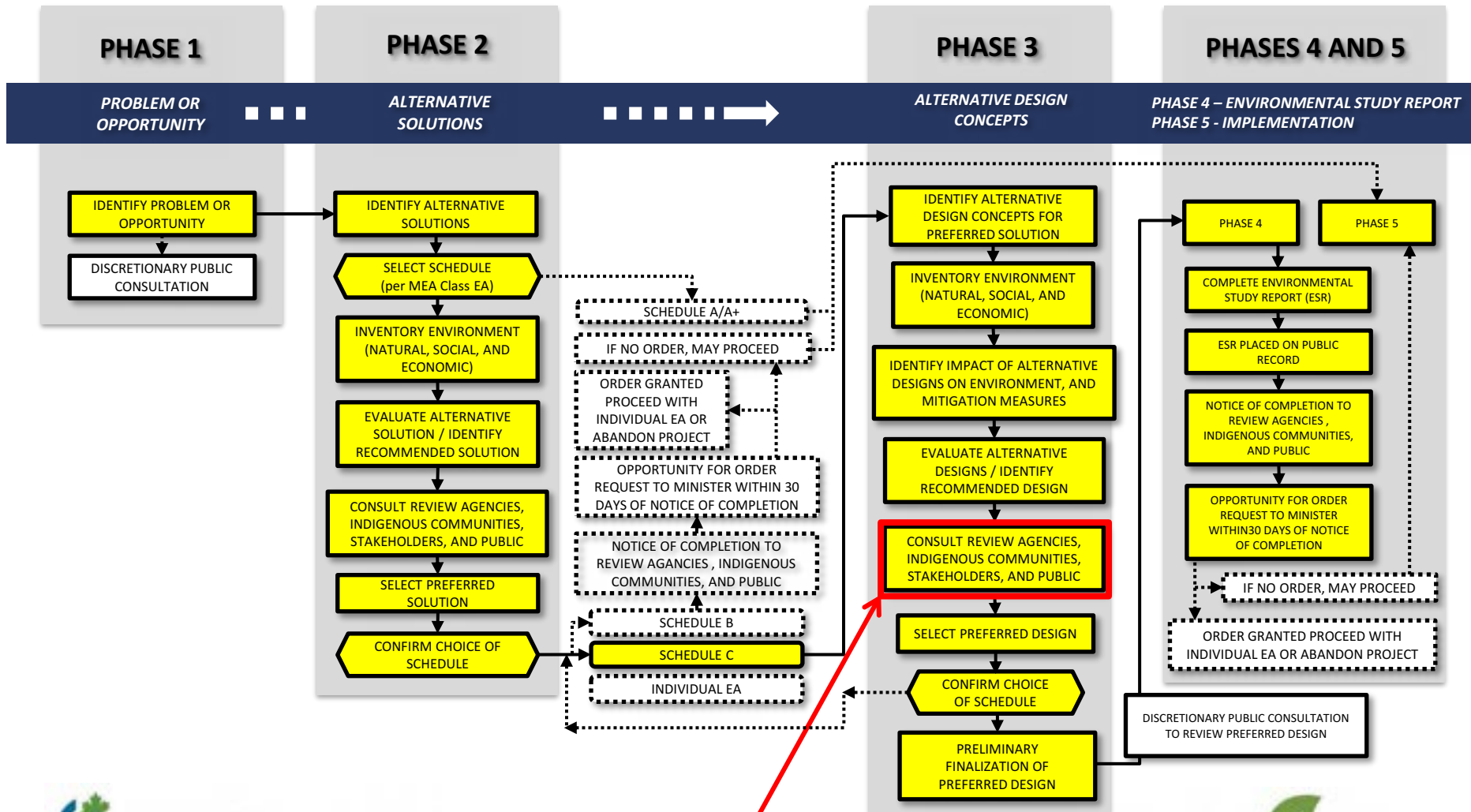


THE EA PROCESS

The Class EA Study is being carried out in accordance with the planning and design process for Schedule C projects as outlined in the Municipal Class Environmental Assessment (October 2000, as amended in 2007, 2011 and 2015), which is approved under the Ontario Environmental Assessment Act.

Nearing completion of the study, an Environmental Study Report (ESR) will be prepared and made available for final public review and comment.

THE EA PROCESS



SUPPORTING STUDIES

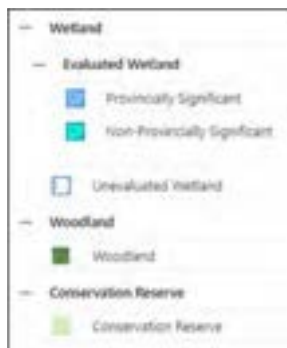
- Transportation Study (including Illumination Assessment)
- Natural Environment Assessment
 - Terrestrial Habitat Assessment
 - Aquatic Habitat Assessment
- Stage 1 Archaeological Assessment
- Cultural Heritage Resource Assessment
- Air and Noise Impact Assessments
- Stormwater Management Assessment



NATURAL HERITAGE RESOURCES

Designated Features within the Study Area

- Significant valleylands and locally significant natural heritage features
- Watercourses: Spittler Creek, Plumb Creek, and Big Otter Creek.
- Spittler Creek and Big Otter Creek are both associated with significant valleylands
- Otterville Provincially Significant Wetland (PSW) Complex and other wetlands across Spittler Creek
- Woodlands



NATURAL HERITAGE RESOURCES

Aquatic habitat

No Species at Risk (SAR) were found through the desktop review.

- Big Otter Creek is a warm water creek with spring-spawning species such as Largemouth Bass and Yellow Perch.
- Spittler Creek is a cool water creek with observed spring-spawning species.
- Plumb Creek is a cool water creek with fall and spring-spawning species such as Brown Trout, Creek Chub, and Blacknose Dace.

Field work was conducted in the field season to assess aquatic habitat in the Study Area:

- Fish habitat was noted at all six crossings. All crossings has adequate, flow / depth, riparian life, and substrate providing suitable fish habitat.
- Large schools of bait fish were noted throughout both the upstream and downstream reaches of the crossings.
- Spawning Hornyhead Chub (*Nocomius biguttatus*) were observed in the downstream reach and underneath the clear span bridge at two of the crossings.



NATURAL HERITAGE RESOURCES

Terrestrial Habitat

The desktop review found several potential Species At Risk (SAR):

Snapping Turtle (Special Concern), Wood Thrush (Special Concern), American Badger (Endangered), American Chestnut (Endangered)

Field work was conducted in the field season to assess ecological communities present in the Study Area:

- Barn Swallows were observed foraging within agricultural fields. Potential habitat for Barn Swallow is restricted to crossing structures within the Study Area. Barn Swallow nests were not present on crossing structures at the time of field investigations. Consideration should be provided for this species during the detailed design phase as this species is known to nest within bridges and culverts.
- A single Butternut was observed on the south side of the ROW. Additional surveys including a Butternut Health Assessment may be required at the detailed design phase if it is anticipated that the specimen will be impacted by road improvements.

Bats

The desktop review found records of 4 bat species: Eastern Small-footed Myotis, Little Brown Myotis, Northern Myotis, and Tricoloured Bat

Impact to potential habitat is not expected. Direct impact is to be avoided with timing vegetation removal to be completed outside of the active season for bats.



SOURCE WATER PROTECTION

The Study Area falls under the Long Point Region Source Water Protection Area and Lake Erie Source Water Protection Area.

Wellhead Protection Areas

Three portions of the study area have been identified to fall under Wellhead Protect Areas (i.e., part of Ostrander, north part of Springford and east of Otterville).



Issue Contributing Area

The area west of Otterville is also considered an Issue Contributing Area (ICA), for its potential to contribute elevated concentrations of particular substances to the drinking water source (e.g., chloride, sodium, nitrate).

Highly Vulnerable Aquifer

A significant part of the Oxford Road corridor (i.e., from west of Otterville to east of Oxford Road 59) is also located in a Highly Vulnerable Aquifer (HVA) area.

Significant Groundwater Recharge Areas

A significant part of the study area is also within Significant Groundwater Recharge Areas (SGRAs)

ARCHAEOLOGICAL RESOURCES

The Ministry of Citizenship and Multiculturalism (MCM) Standards & Guidelines lists criteria that are indicative of archaeological potential. The Stage 1 archaeological assessment showed that:

- 27 Previously identified archaeological sites within 1 km of the Study Area, 2 sites of which are located within the Study Area, and six of which are within 50 m of the Study Area;
- Springford Community Cemetery and Pine Street Burial Ground are located within 20 m of the Study Area. The legal cemetery boundaries shall be avoided by project designs.
- The Pettman Cemetery is approximately 100 m north of the Study Area, however the historic limits are unknown and therefore the lands within the Study Area require Stage 2 archaeological assessment.
- Stage 2 archaeological assessment is recommended in any other areas identified as exhibiting archaeological potential within the impact area of the project.
- Stage 2 archaeological assessment, if required, will be completed during the detailed design.



CULTURAL HERITAGE RESOURCES

Two known heritage properties, designated under Part IV of the Ontario Heritage Act, were identified within the project study area. A total of 5 Potential Built Heritage Resources and 23 Potential Cultural Heritage Landscapes were identified within the project study area.



Built Heritage Resources and Cultural Heritage Landscapes throughout the corridor

TRANSPORTATION STUDY DESIGN NEED ASSESSMENT

- Collision rates along the corridor in the study area (0.71 per 1 million vehicle km's travelled [MVKT] over the last 8 years) as compared to the provincial average collision rate of 1.53 per MVKT (2019), with no significant collision hot spots or patterns.
- No additional travel lanes are required on Oxford Road 19 to beyond year 2046
- A westbound left turn lane may be required at Highway 19 by 2046
- The existing pavement structure should be improved to address the deterioration and to allow for removal of half-load restrictions
- Oxford Road 19 is not part of the County's planned primary or secondary cycling networks, however it does provide connectivity to such cycling networks and trails
- Improvements to vertical or horizontal alignments of the road to be considered at 20 identified locations in the detailed designs, to improve traffic operations
- Right-of-way is recommended to be expanded to a minimum width of 30 metres in the rural area and 26 metres in Ostrander
- Improved illumination is warranted at the intersections with Highway 19 and Oxford Road 59.

AIR AND NOISE ASSESSMENT

Noise

- Traffic noise is forecasted to be 1 dBA over the daytime threshold level of 65 dBA (according to the Ministry of Transportation [MTO] Noise Guide) at two houses along the corridor, which triggers further investigation of noise impacts.
- Further investigation of noise levels were completed at the two houses noted above, however no noise mitigation is warranted based on noise levels estimated within the various living spaces.

Air Quality

- There are no houses on the road that will be exposed to a level of air contaminants that exceed criteria, except for the annual benzene impact.
- Annual benzene impact is unaffected by the local road and people would be impacted by the same level anywhere in the area, regardless of how close they were to this or any other road. Therefore, No significant impacts to air quality are forecasted to result from the proposed road improvements.

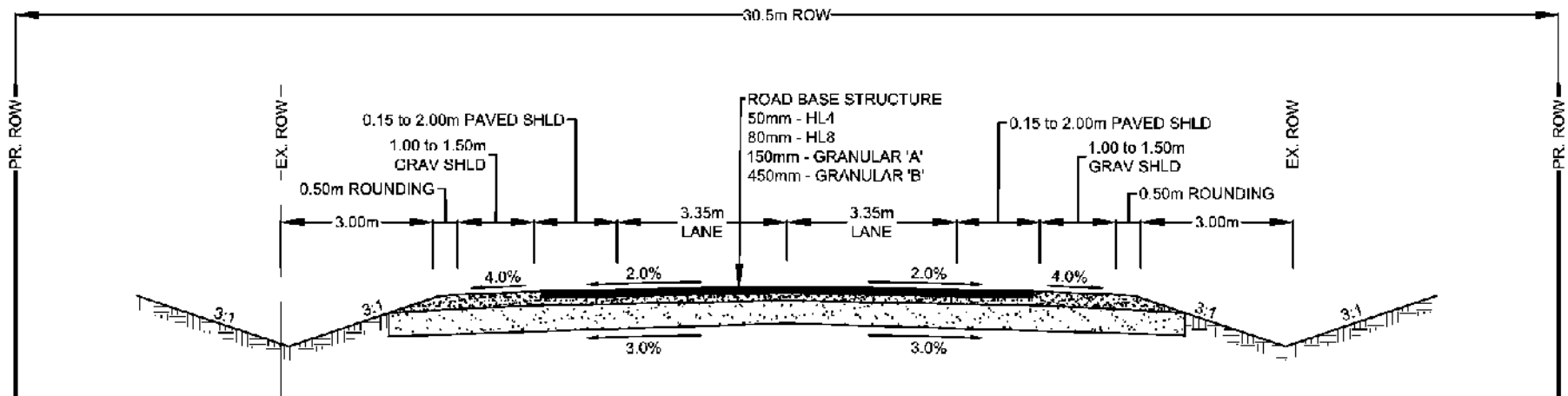
STORMWATER ASSESSMENT

- One structure was identified to have an existing width that will require widening to accommodate wider paved shoulders.
- Vegetated ditches will continue to provide roadside drainage requirements
- Impacts to surface water quantity or quality are expected to be minimal from the proposed project.
- Erosion and sediment control measures will be implemented during the construction phase.

Preferred Alternative Solution

Structural Improvements to the Road, Including Widened Lanes and Shoulders within a Widened ROW

- Provide a two-lane road to County requirements with paved shoulder.
- Widened travel lanes and partially paved shoulders to reduce maintenance and improve safety.
- Improvement of pavement structure to remove half-load restrictions in the Spring.



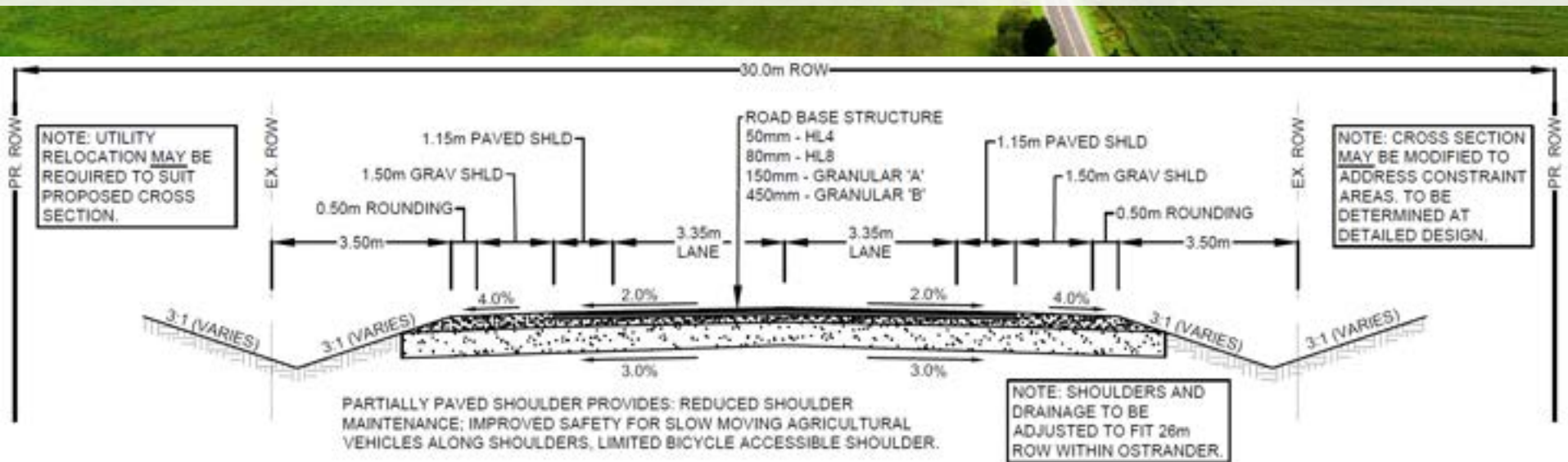
NOTE: WIDTH OF SHOULDER TO BE DETERMINED AS PART OF CONSIDERATION OF DESIGN ALTERNATIVES AND PURPOSE OF SHOULDER (e.g., TO REDUCE MAINTENANCE, TO IMPROVE SAFETY, TO PROVIDE UNBUFFERED BICYCLE ACCESSIBLE SHOULDER OR TO PROVIDE BUFFERED BICYCLE ACCESSIBLE SHOULDER).

ALTERNATIVE 5 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

Alternative Design Concepts

Alternative 1

- 9.0 m paved asphalt
- Each side with 3.35 m travel lane, 1.15 m paved shoulder and 1.5 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)



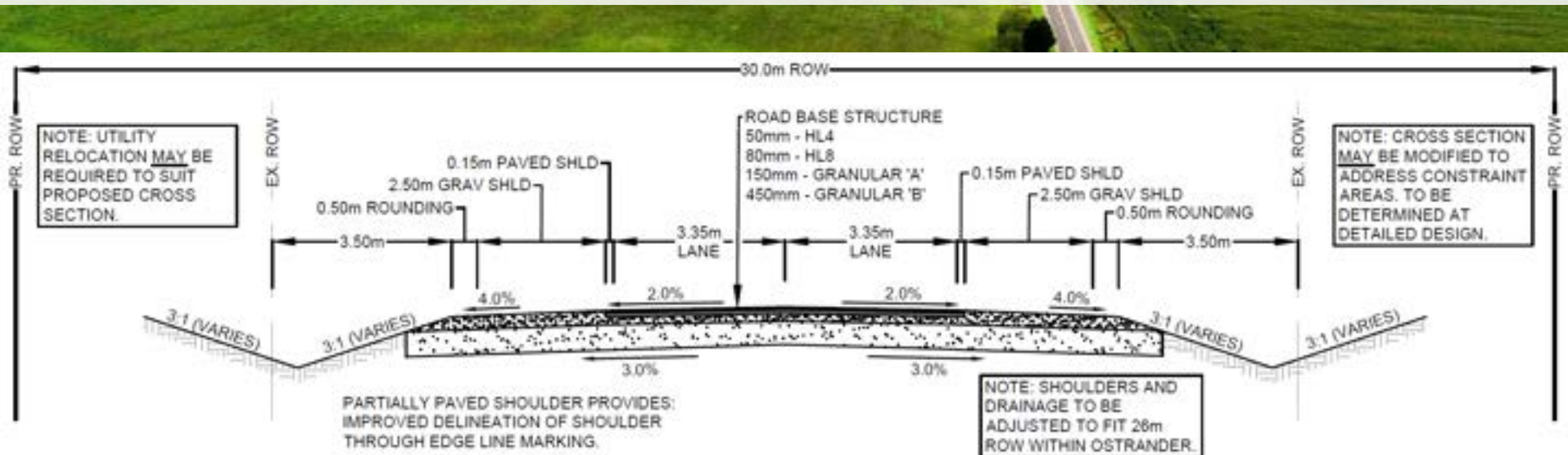
ALTERNATIVE 1 - PARTIALLY PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN ROW TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

Alternative Design Concepts

Alternative 2

- 7.0 m paved asphalt
- Each side with 3.35 m travel lane, 0.15 m paved shoulder and 2.5 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)



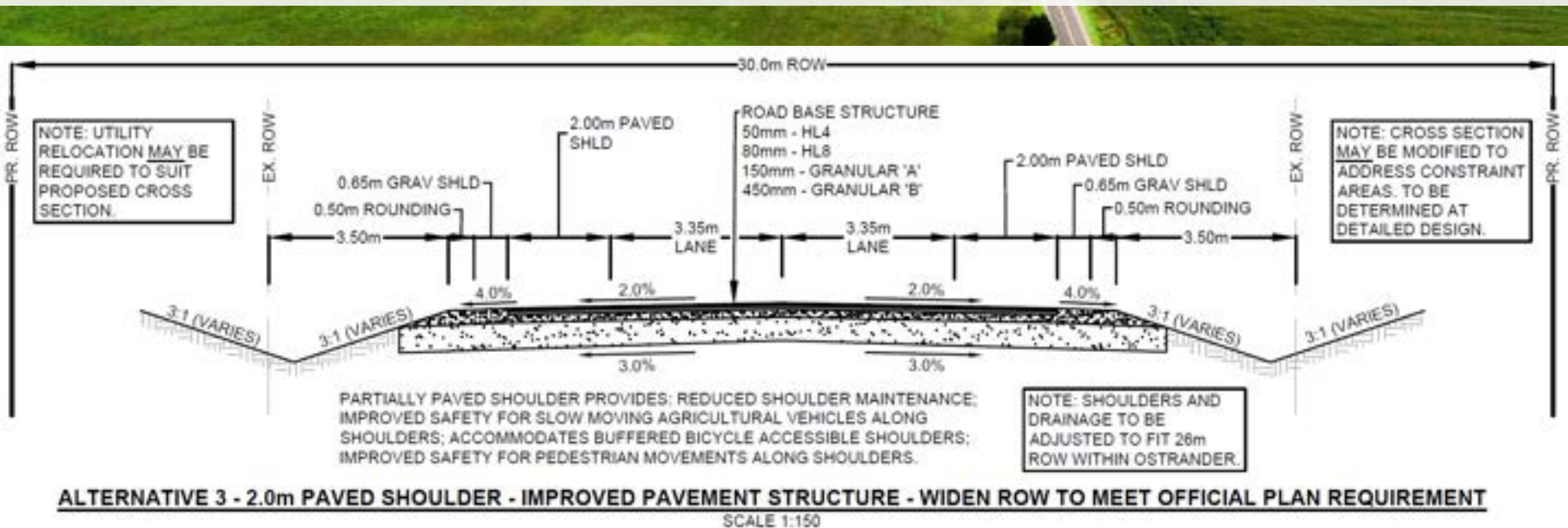
ALTERNATIVE 2 - 0.15m PAVED SHOULDER - IMPROVED PAVEMENT STRUCTURE - WIDEN TO MEET OFFICIAL PLAN REQUIREMENT

SCALE 1:150

Alternative Design Concepts













Alternative 3

- 10.7 m paved asphalt
- Each side with 3.35 m travel lane, 2.0 m paved shoulder and 0.65 m gravel shoulder with 0.5 m rounding
- 30 m ROW - Centered on the existing road centerline (26 m in Ostrander)



EVALUATION CRITERIA

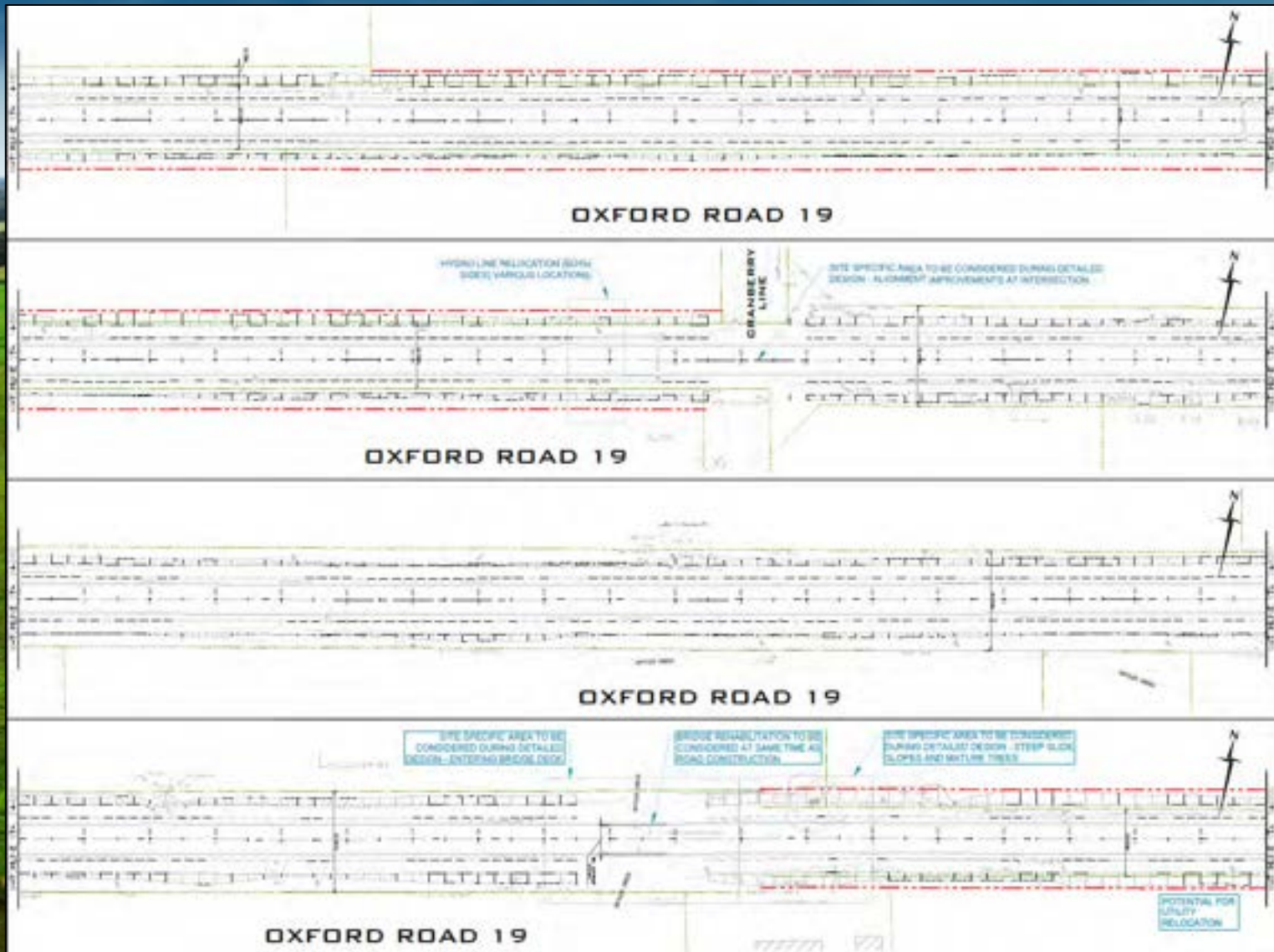
- **Natural Environment**
 - Impacts to vegetation and terrestrial habitat
 - Impacts to fisheries and aquatic habitat
 - Impacts to Hazard Lands (erosion, slope stability, flooding)
 - Impacts to Designated Features/species
 - Impacts to surface water quality and quantity
 - Impacts to groundwater quality and quantity
- **Technical Environment**
 - Accommodation of various modes of transportation and vehicle types
 - Improvement to road safety
 - Road maintenance requirements
 - Design complexity
 - Impacts to utilities and drainage work/structures
- **Socio-Cultural Environment**
 - Impacts to cultural heritage resources
 - Impacts to archaeological resources
 - Private property impacts
 - Nuisance impacts (noise, traffic, visual impact)
- **Financial**
 - Capital and operation/maintenance costs
 - Property acquisition cost

Evaluation Criteria	Each side with 3.35 m travel lane, 1.15 m paved shoulder and 1.5 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road	paved shoulder and 2.5 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road	Each side with 3.35 m travel lane, 2.0 m paved shoulder and 0.65 m gravel shoulder with 0.5 m rounding. 30 m ROW (Rural) or 26 m (Ostrander)- Centered on the existing centreline of the road
Natural Environment	 <p>Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.</p>	 <p>Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.</p>	 <p>Removal of limited vegetation within woodland and riparian communities. The extent of impact to terrestrial Species at Risk (SAR) is anticipated to be low to moderate with suitable mitigation measures. Some potential impacts in the areas of unstable ditch conditions, mitigated through ditch embankment improvement. No aquatic SAR are found within the study area. Minimal impact may occur considering major structures are not being replaced.</p>
Socio-Cultural Environment	 <p>Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. Some dust created by agricultural equipment travel along gravel shoulders.</p>	 <p>Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. Significant dust created by agricultural equipment travel along gravel shoulders.</p>	 <p>Similar amount of property. Some potential impact to archaeological resources (not likely). There may be some impact to cultural heritage resources. Similar noise impacts due to minor increased truck traffic and traffic growth, similar visual impact due to tree and vegetation removal, and similar speeds. May lengthened the temporary construction period impacts if bridges require widening. Very little dust created by agricultural equipment travel along gravel shoulders.</p>
Technical Environment	 <p>Wider paved shoulders provides increased spacing between traffic. Limited buffer for cyclists or pedestrians. Moderate improvement of safety for all travel modes using the corridor. Better able to accommodate full range of operating speeds. Moderate improvement to access operations along the corridor (pullover onto shoulder to exit roadway at accesses). Some adjustment of design required to interface the wider asphalt widths with crossings and accesses.</p>	 <p>Minimal asphalt width provides for minimum space between traffic. Cyclists and pedestrians use gravel shoulder. Some improvement of safety for all travel modes using the corridor. Lower ability to accommodate vehicles with excessive speeds and / or slow-moving vehicles. No improvement to access operations along the corridor. Maintains existing asphalt interface with crossings (i.e., 3 bridges) and accesses.</p>	 <p>Wider paved shoulders provides increased spacing between traffic. Increased buffer for cyclists and pedestrians. Significant improvement of safety for all travel modes using the corridor. Minimal gravel shoulder may direct agricultural equipment further onto paved areas and into travel lane. Better able to accommodate full range of operating speeds. Significant improvement to access operations along the corridor. Design may not be achievable in area of constraints without significant adjustment of such constraints. Potential for longer temporary construction impacts if structure widenings are required.</p>
Economic Environment	 <p>All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Moderate increase in cost over alternative 2, due to partially paved shoulder. Approximately \$118/m for shoulder. Some additional cost for adjustment of major bridge crossings.</p>	 <p>All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Lowest cost due to minimal paved shoulder. Approximately \$45/m for shoulder.</p>	 <p>All alternatives have similar property acquisition cost to widen ROW to meet Official Plan requirements. Higher increase in cost over alternative 2, due to fully paved shoulder. Approximately \$180/m for shoulder. Increased additional cost for adjustment of major bridge crossings.</p>
Overall Summary	Most Preferred	Less Preferred	Least Preferred

Level of Preference: Least Preferred ○ Less Preferred ◐ Moderately Preferred ◑ More Preferred ◒ Most Preferred ●

Preferred Design Concepts

Sample Plan



CONCEPT DRAWINGS OF THE FULL CORRIDOR ARE AVAILABLE TO VIEW, SHOWING EXISTING AND PROPOSED ROAD AND RIGHT-OF-WAY DETAILS

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---

Proposed Mitigation Measures

A comprehensive list of proposed mitigation measures will be prepared as part of this EA and included in the Environmental Study Report (ESR).

Some key measures include:

- Health and safety is a priority for Oxford County. Construction will adhere to strict safety guidelines including best practices for vehicle and pedestrian safety.
- Advance notice to local residents of potential traffic impacts from construction.
- Access to residences/farms/pedestrian facilities will be maintained during construction.
- Temporary site fencing and other construction measures will be implemented to minimize noise, vibration, dust, mud and visual impacts.
- The following plans will be prepared by the contractor and implemented during construction: Erosion and Sediment Control Plan; Emergency Response and Communications Plan; Spill Prevention and Contingency Plans; Complaint Response Protocol; Construction Management Plan; Health and Safety Plans; Soil Management Plan; and Traffic Management Plan.

Proposed Mitigation Measures

Natural Heritage Specific Mitigation Measures:

- Plant species loss will be minimized.
- Tree inventory, preservation and protection plan to be prepared at detailed design.
- To reduce the risk of potential impact to wildlife, vegetation clearing will not be completed during appropriate timing to avoid the active period for breeding birds and bat species.
- Installation of temporary fencing as required.
- Any in-water works will occur during appropriate in-water works timing window.
- Refueling and maintenance of construction equipment shall occur within designated areas only. Any hazardous materials used for construction will be handled in accordance to appropriate regulations.
- Spills or depositions into watercourses shall be immediately contained and cleaned up in accordance with provincial regulatory requirements and the contingency plan.

Preferred Design Concepts

Future Detailed Design Considerations:

- Confirm utility relocation requirements (hydro poles, natural gas main, telecommunications)
- Confirm adjustments to Ostrander Municipal Drain
- Widen existing bridge at one location and coordinate with required bridge rehabilitation
- Adjust cross section and/or drainage measures to address significant localized constraints (e.g., existing buildings, residential areas, topography, natural areas, interface with urban areas)
- Features to minimize impact on quality or quantity of stormwater runoff and erosion
- Localized improvements to vertical and/or horizontal alignments to improve sight lines
- Traffic calming near village boundaries
- Finalize right-of-way requirements and property acquisition requirements
- Phasing strategies for construction, considering access and budget constraints

Next Steps

- Confirm Preferred Design Concepts (January 2023)
- Finalize Transportation Study (January 2023)
- Environmental Study Report (February 2023)
- File Environmental Study Report for Schedule C Class EA Study (March 2023)

Invitation for Participation

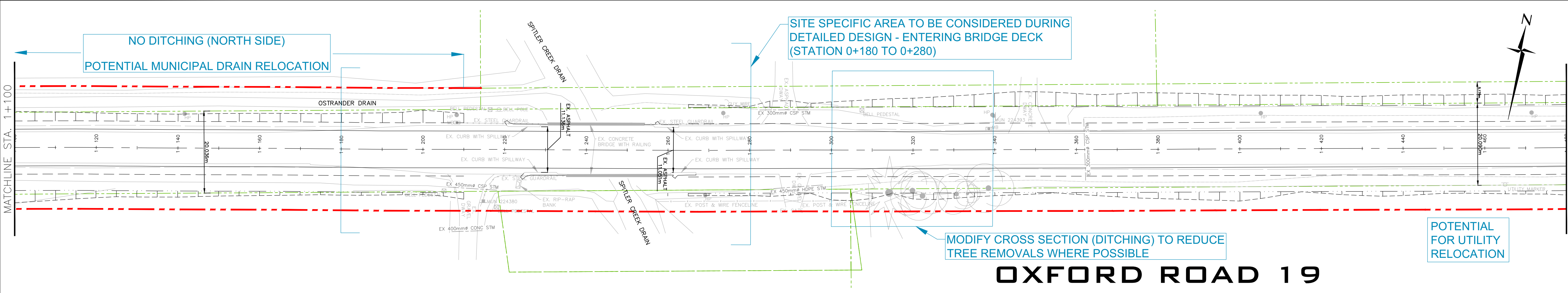
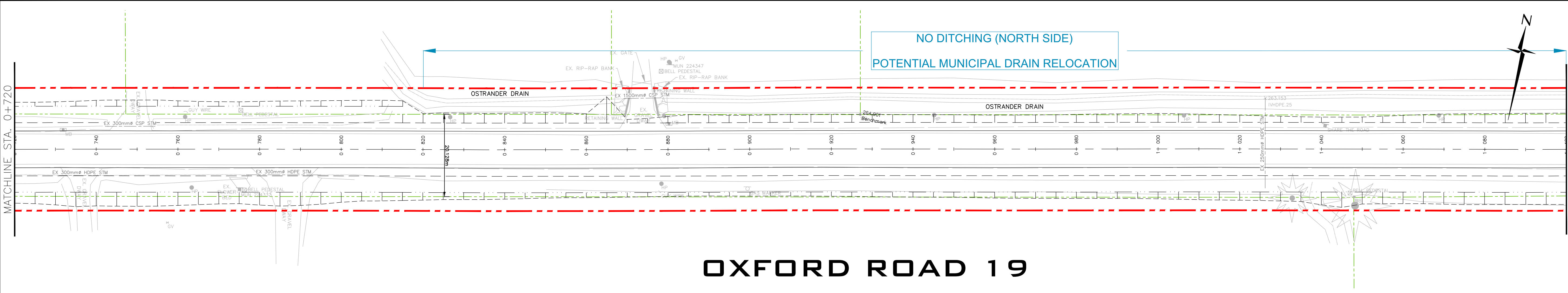
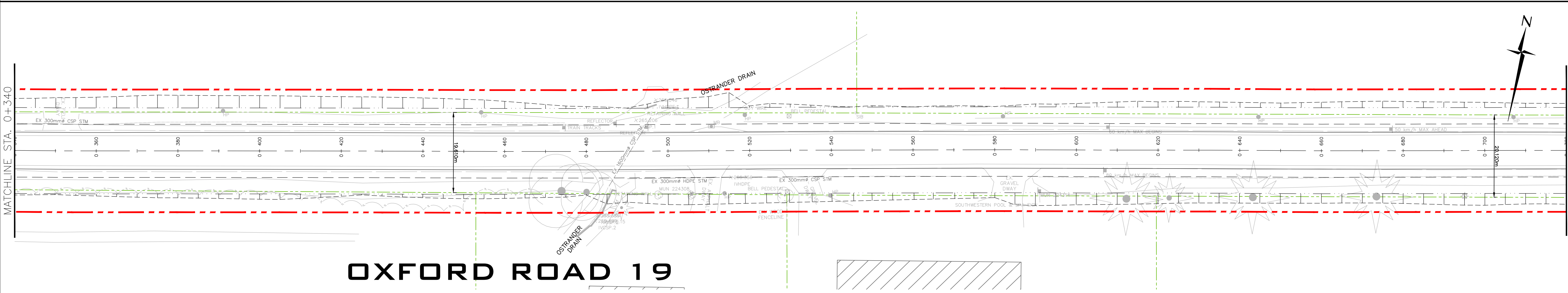
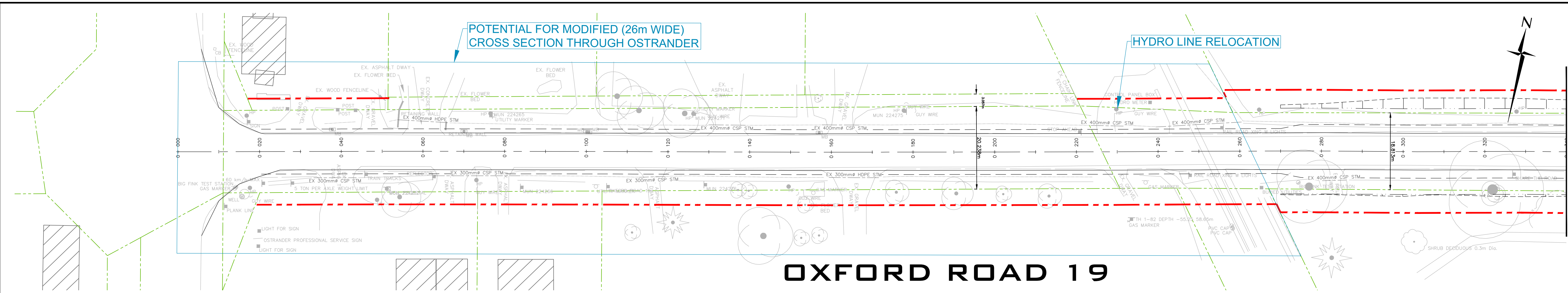
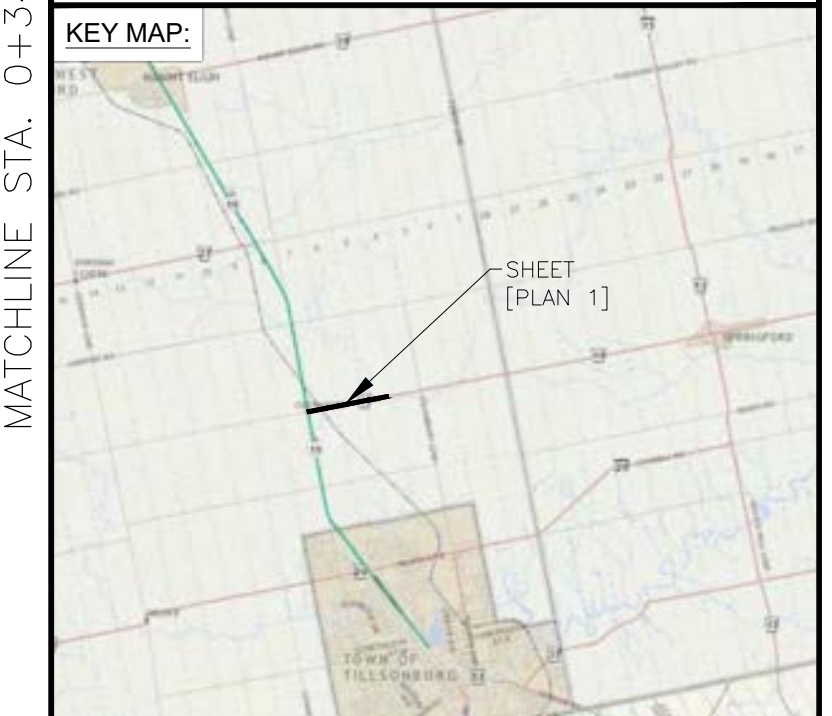
Public input is an important component of the decision-making process.

You are invited to provide comments by completing the forms provided and submitting forms to the Study Team members below on or before January 13, 2023.

Jesse Keith, P.Eng.
Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com

THANK-YOU FOR ATTENDING



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	HW
Ex. Catchbasin	CB
Ex. Clean Out	CO
Ex. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Ex. Fire Hydrant	FH
Ex. Sanitary Manhole	SMH
Ex. Storm Manhole	STMH
Ex. Water Valve	WV
Ex. Cut Cross	CC
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CS
Ex. Deciduous Shrub	DS
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
BENCHMARK #	ELEVATION=m		

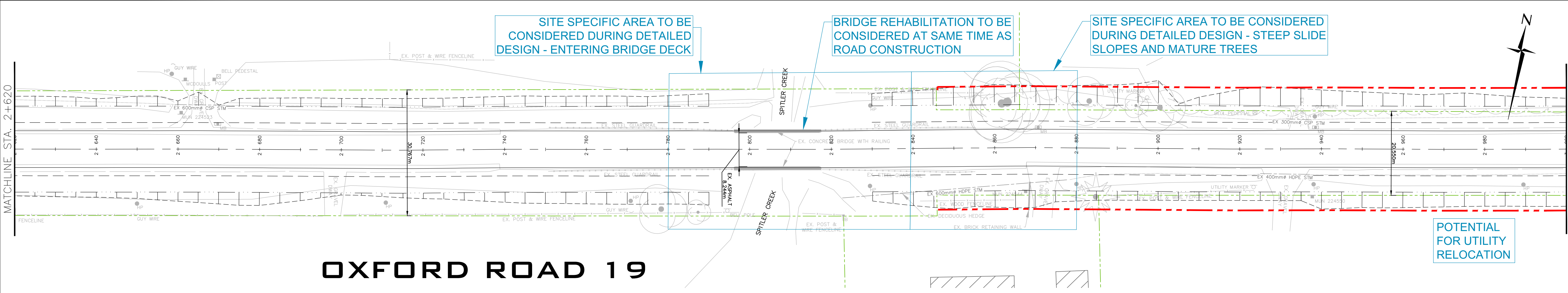
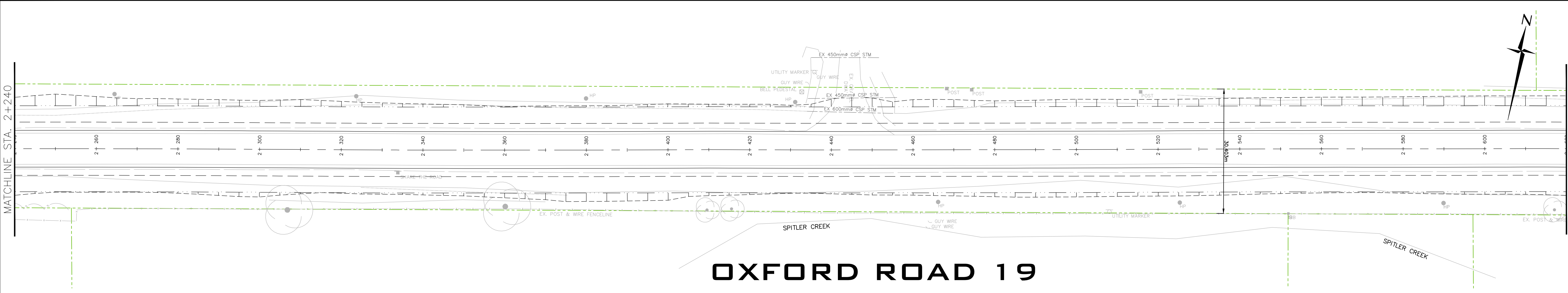
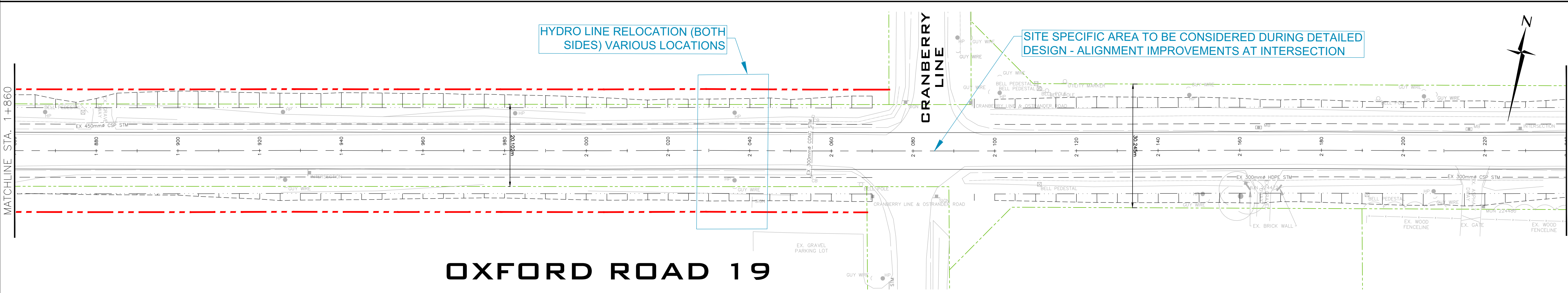
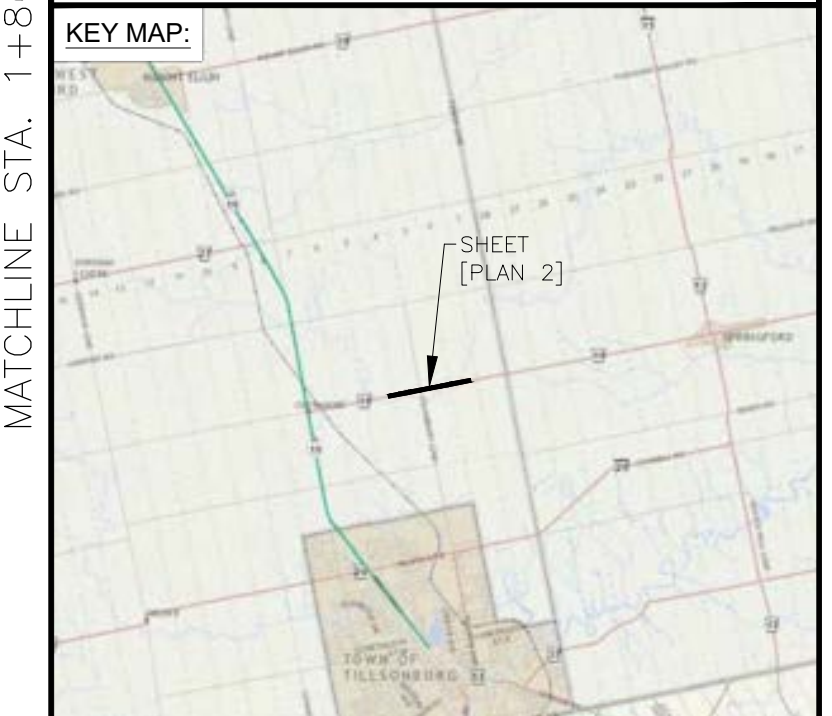
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CONSULTANT:

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CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
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HOR. SCALE - 1:500	
STN: 0+000	TO: 1+480
CONTRACT NO:	
DWG NO: PLAN 1	

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Pr. Top Bank/Matchline	---	Prop. Clean Out	CO
Pr. Ditchline	---	Prop. Curb Stop	CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	DICB
Ex. Property Line	---	Prop. Double CB	DCB
Ex. Centreline of Ditch	---	Prop. Fire Hydrant	FH
Ex. Sanitary Forcemain	---	Prop. Sanitary Manhole	SAMH
Ex. Sanitary Sewer	---	Prop. Storm Manhole	STMH
Ex. Storm Sewer	---	Prop. Water Valve	WV
Ex. U/G Telephone	T	Remove & Dispose	RD
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	XCB		
Ex. Clean Out	CO		
Ex. Curb Stop	CS		
Ex. Ditch Inlet CB	XDICB		
Ex. Double Catchbasin	XDICB		
Ex. Fire Hydrant	FH		
Ex. Sanitary Manhole	SAMH		
Ex. Storm Manhole	STMH		
Ex. Water Valve	WV		
Ex. Cut Cross	---		
Ex. Sign	---		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	---		
Ex. Deciduous Shrub	---		
Ex. Stump	---		
Ex. Coniferous Tree	---		
Ex. Deciduous Tree	---		

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
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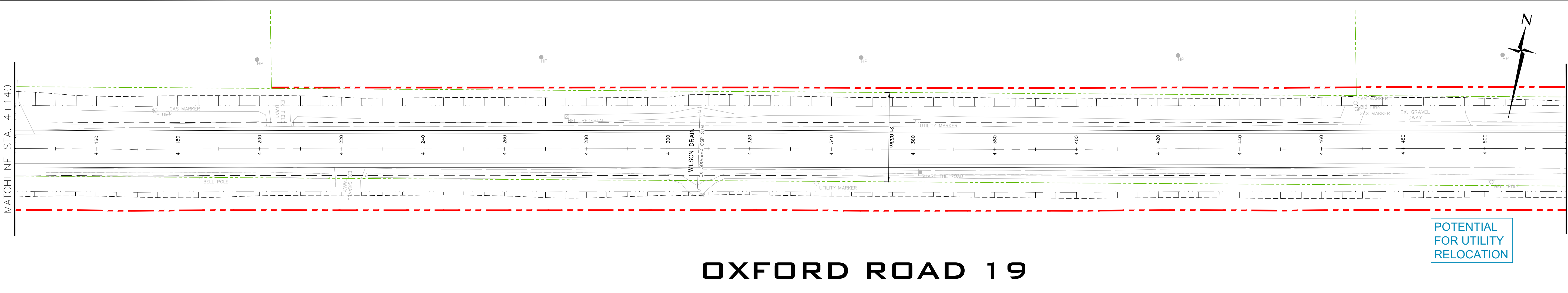
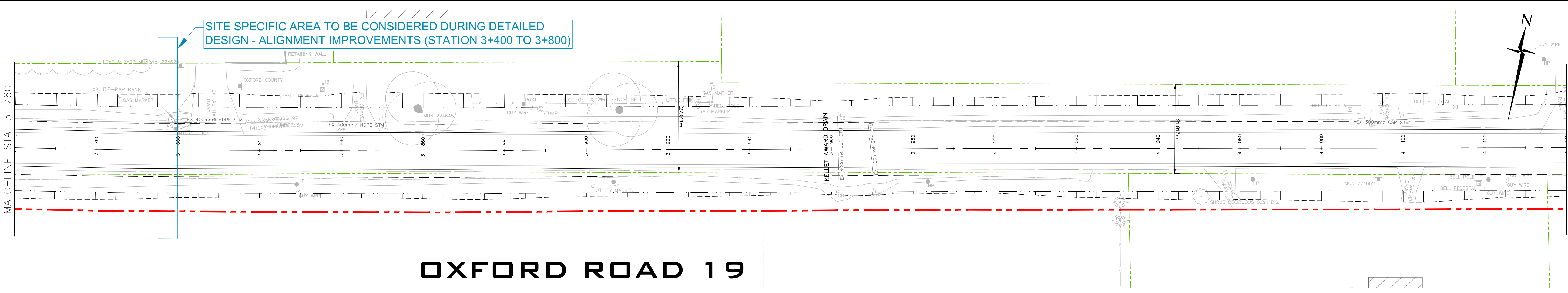
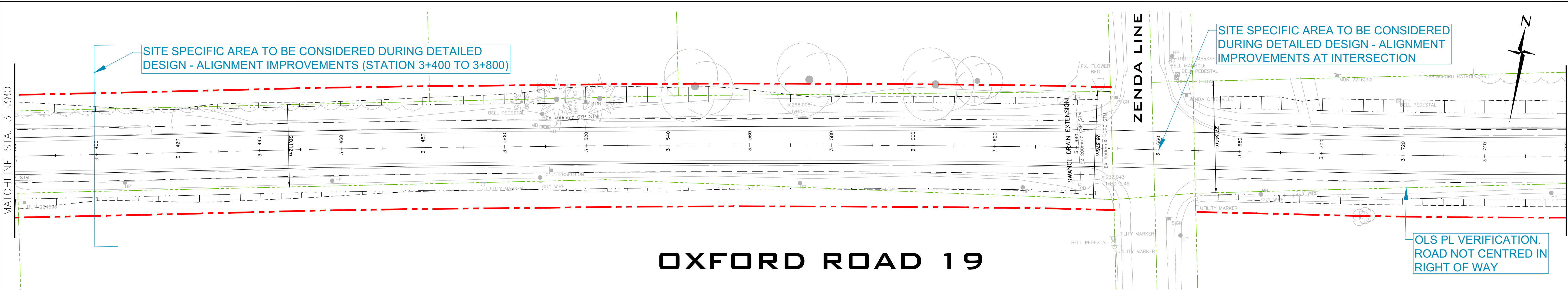
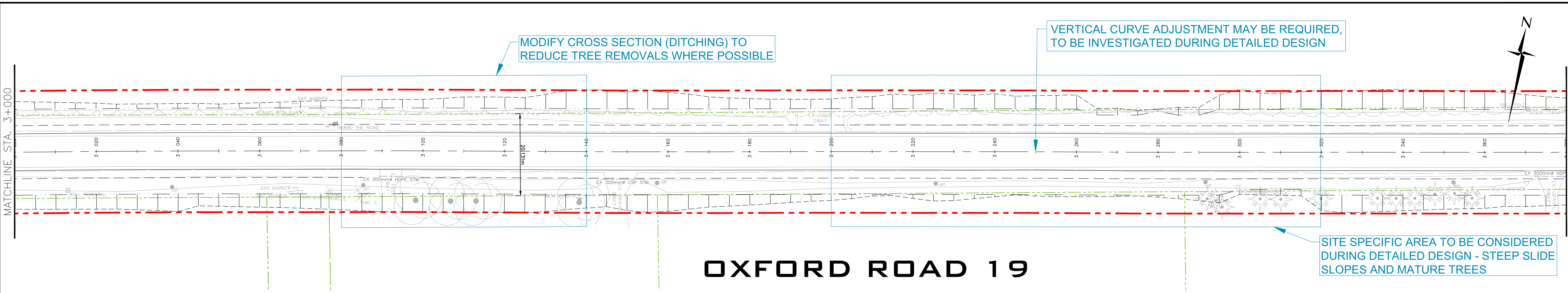
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CONSULTANT:

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APPROVED BY: HC	
DATE: NOV 10, 2022	

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HOR. SCALE - 1:500	
STN: 1+480	TO: 3+000
CONTRACT NO:	
DWG NO: PLAN 2	

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Pr. Ditchline	---	Prop. Curb Stop	●
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	■
Pr. Edge of Pavement	---	Prop. Double CB	■
Ex. Property Line	---	Prop. Fire Hydrant	●
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	●
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	●
Ex. Sanitary Sewer	---	Prop. Water Valve	●
Ex. Storm Sewer	---	Remove & Dispose	⊗
Ex. U/G Telephone	---		
Ex. U/G Cable TV	---		
Ex. U/G Gas	---		
Ex. U/G Hydro	---		
Ex. U/G Traffic Wiring	---		
Ex. Watermain	---		
Ex. Catchbasin	□	Prop. Catchbasin	■
Ex. Clean Out	○	Prop. Clean Out	●
Ex. Curb Stop	○	Prop. Curb Stop	●
Ex. Ditch Inlet CB	□	Prop. Ditch Inlet CB	■
Ex. Double Catchbasin	□	Prop. Double CB	■
Ex. Fire Hydrant	○	Prop. Fire Hydrant	●
Ex. Sanitary Manhole	○	Prop. Sanitary Manhole	●
Ex. Storm Manhole	○	Prop. Storm Manhole	●
Ex. Water Valve	○	Prop. Water Valve	●
Ex. Cut Cross	⊗	Remove & Dispose	⊗
Ex. Sign	■		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊙		
Ex. Deciduous Shrub	⊙		
Ex. Stump	⊙		
Ex. Coniferous Tree	⊙		
Ex. Deciduous Tree	⊙		

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:

BENCHMARK #	ELEVATION=m
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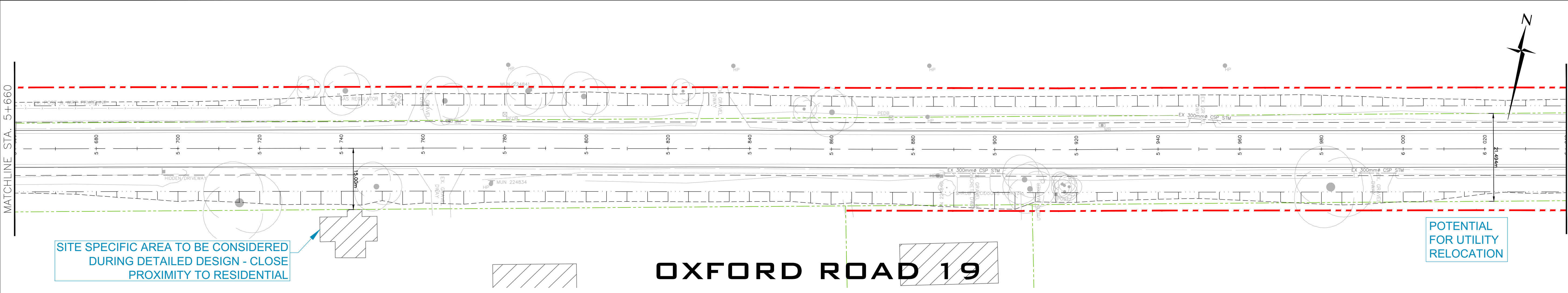
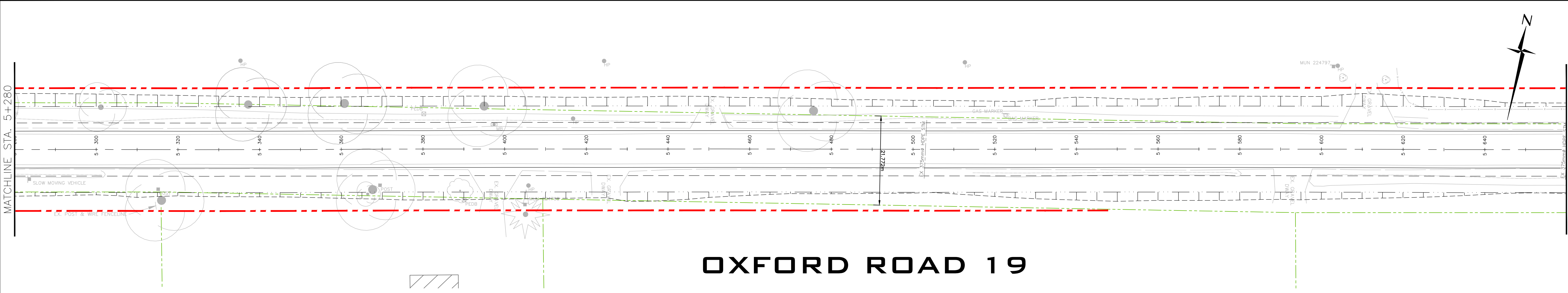
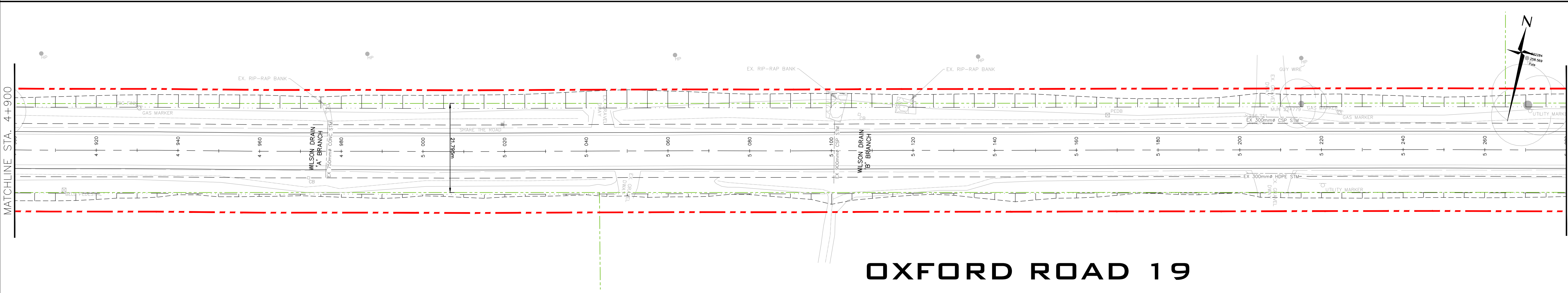
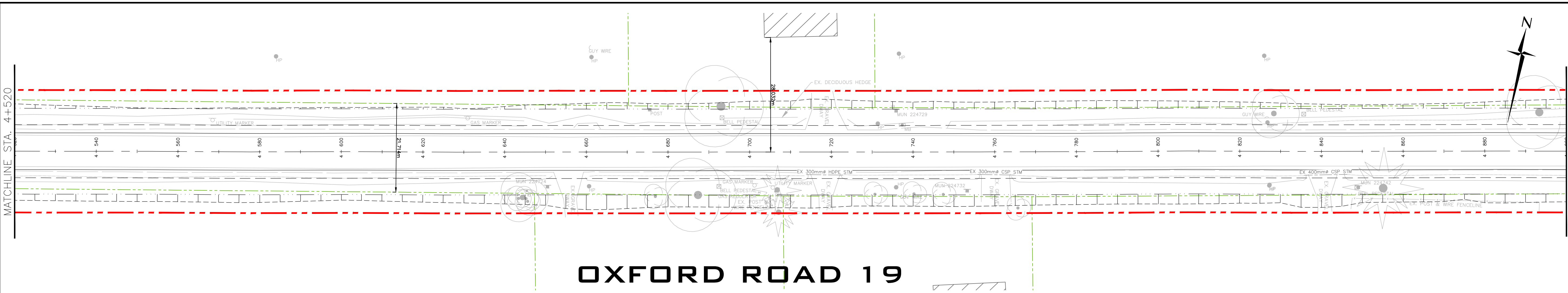
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OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 3+000 TO 4+520

HOR. SCALE - 1:500
STN: 3+000 TO: 4+520
CONTRACT NO:
DWG NO: PLAN 3

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Pr. Ditchline	---	Prop. Curb Stop	● CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	■ DICB
Pr. Edge of Pavement	---	Prop. Double CB	■ DCB
Ex. Property Line	---	Prop. Fire Hydrant	● SAMH
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	● SAMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	● STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	●
Ex. Storm Sewer	---	Remove & Dispose	⊗
Ex. U/G Telephone	T		
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	□ XCB		
Ex. Clean Out	○ CO		
Ex. Curb Stop	○ CSC		
Ex. Ditch Inlet CB	□ XDICB		
Ex. Double Catchbasin	□ XDCB		
Ex. Fire Hydrant	○ XSAFH		
Ex. Sanitary Manhole	○ XSTMH		
Ex. Storm Manhole	○ XSTMH		
Ex. Water Valve	○ XWV		
Ex. Cut Cross	⊗		
Ex. Sign	■		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊙		
Ex. Deciduous Shrub	⊙		
Ex. Stump	⊙		
Ex. Coniferous Tree	⊙		
Ex. Deciduous Tree	⊙		

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
BENCHMARK #	ELEVATION=m		

AREA MUNICIPALITY:

CONSULTANT:

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APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
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HOR. SCALE - 1:500	
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Pr. Edge of Pavement	---	Prop. Double CB	■ DCB
Ex. Property Line	---	Prop. Fire Hydrant	● SAMH
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	● STMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	● STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	●
Ex. Storm Sewer	---	Remove & Dispose	⊗
Ex. U/G Telephone	T		
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	□ XCB		
Ex. Clean Out	○ CO		
Ex. Curb Stop	○ CS		
Ex. Ditch Inlet CB	□ XDICB		
Ex. Double Catchbasin	□ XDCB		
Ex. Fire Hydrant	○ XSAFH		
Ex. Sanitary Manhole	○ XSTMH		
Ex. Storm Manhole	○ XSTMH		
Ex. Water Valve	○ XWV		
Ex. Cut Cross	⊗		
Ex. Sign	■		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		

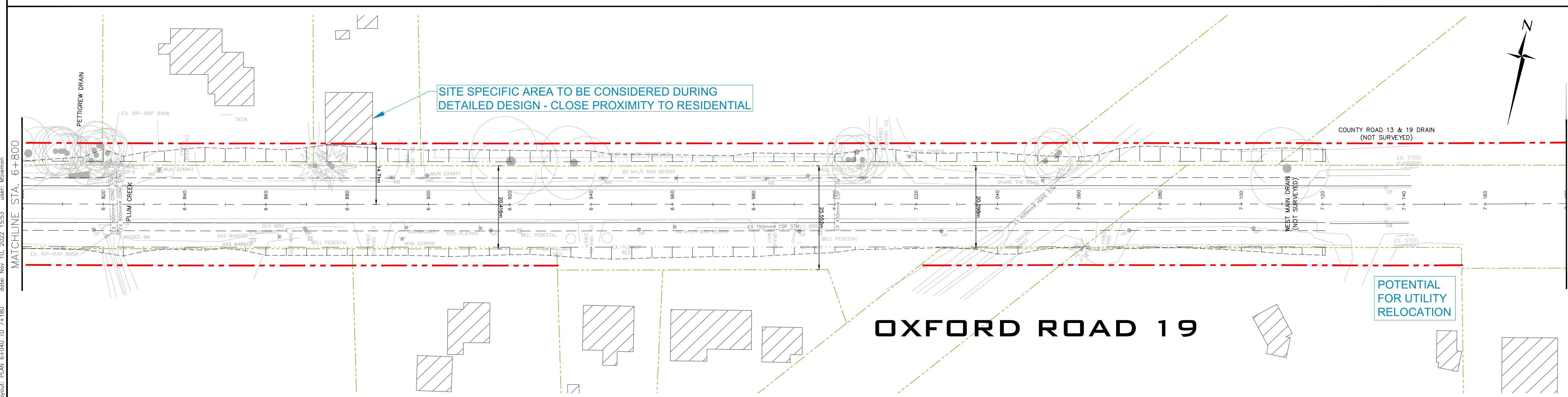
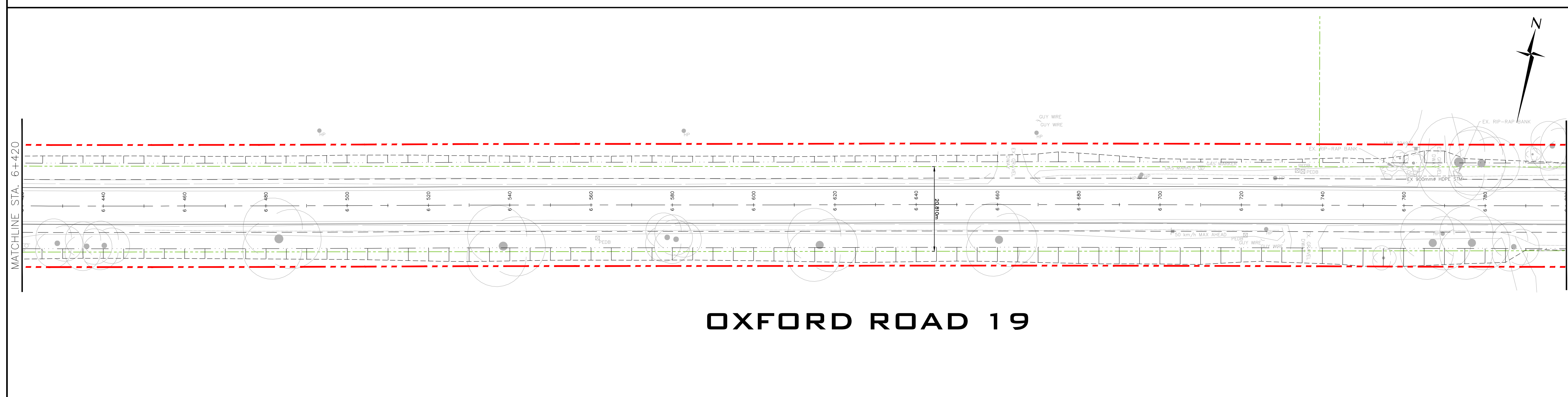
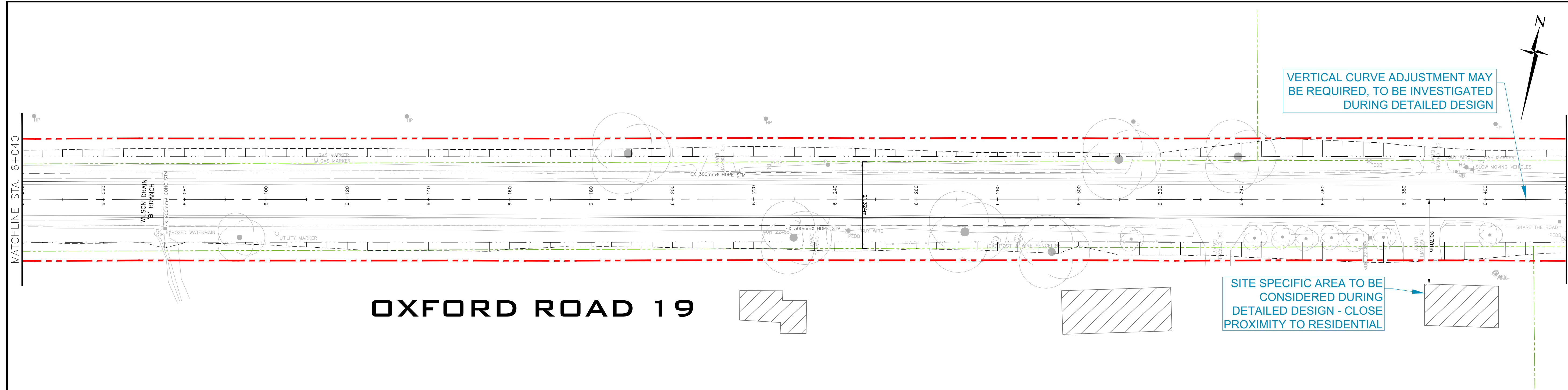
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ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
BENCHMARK #		ELEVATION=	m

AREA MUNICIPALITY:

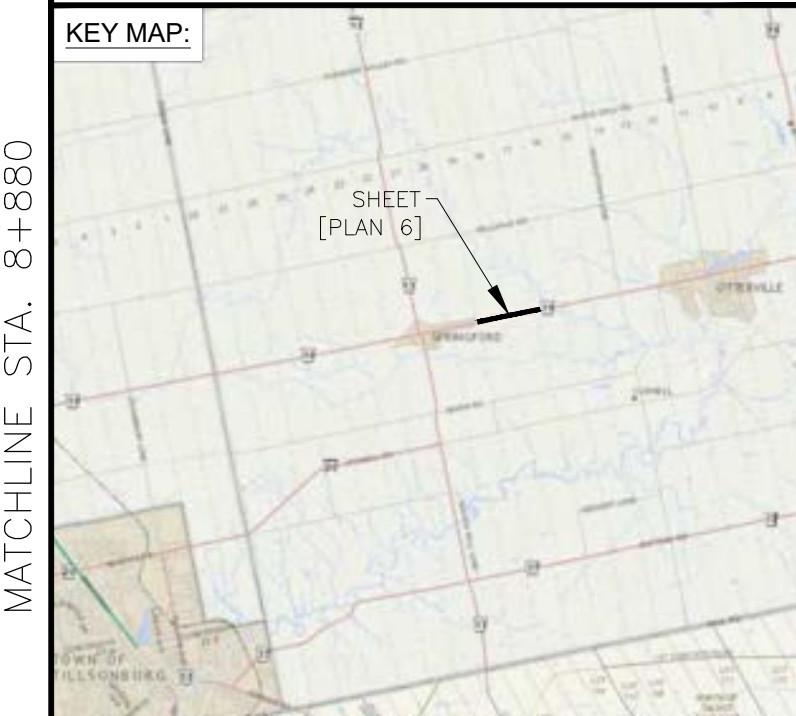
CONSULTANT:

DESIGN BY: HC	STAMP:
DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
PLAN 6+040 TO 7+180	
HOR. SCALE - 1:500	
STN: 6+040	TO: 7+180
CONTRACT NO:	
DWG NO: PLAN 5	

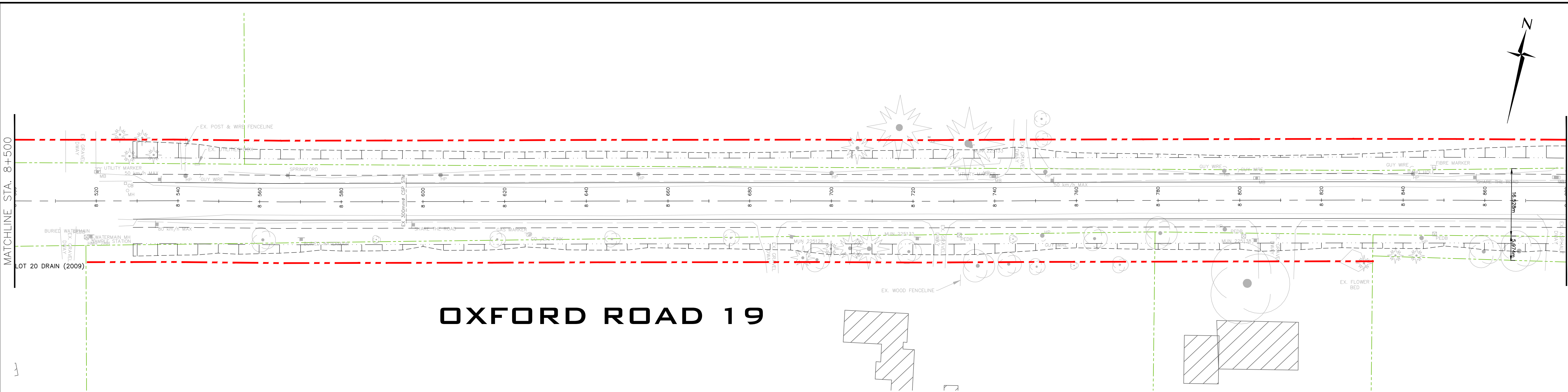


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Layout: PLAN 6+040 TO 7+180 date: Nov 10, 2022 15:53 user: Meehan

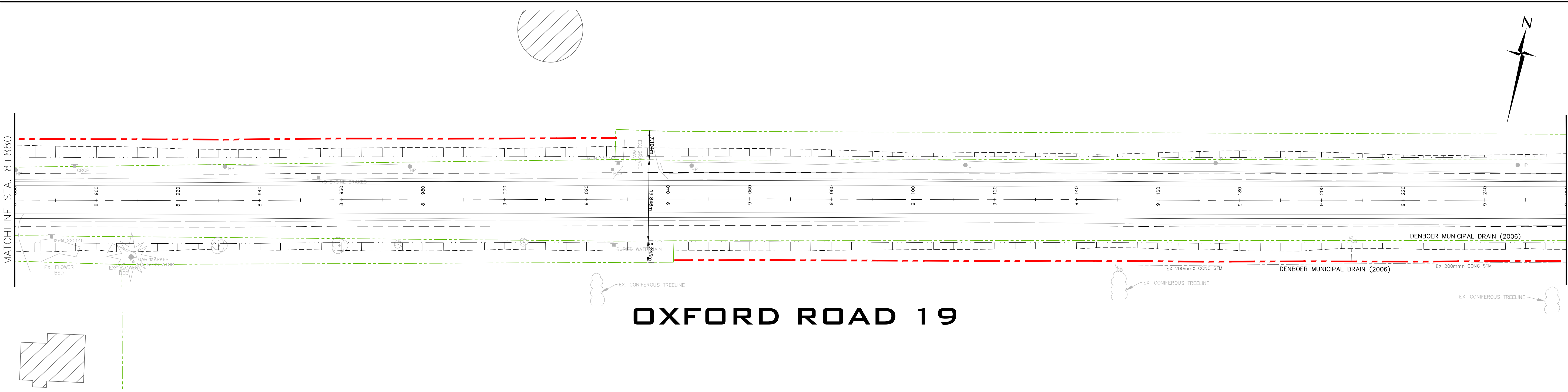


LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forceman	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	XCB
Ex. Clean Out	CO
Ex. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Ex. Fire Hydrant	FH
Ex. Sanitary Manhole	SAMH
Ex. Storm Manhole	STMH
Ex. Water Valve	WV
Ex. Cut Cross	CC
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CSH
Ex. Deciduous Shrub	DSH
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT
Prop. Catchbasin	CB
Prop. Clean Out	CO
Prop. Curb Stop	CS
Prop. Ditch Inlet CB	DICB
Prop. Double CB	DCB
Prop. Fire Hydrant	FH
Prop. Sanitary Manhole	SAMH
Prop. Storm Manhole	STMH
Prop. Water Valve	WV
Remove & Dispose	R&D



OXFORD ROAD 19



OXFORD ROAD 19



OXFORD ROAD 19

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
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ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:	
BENCHMARK #	ELEVATION=mm

AREA MUNICIPALITY:

CONSULTANT:

DESIGN BY: HC

DRAWN BY: MS

CHECKED BY: AB

APPROVED BY: HC

DATE: NOV 10, 2022

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 8+500 TO 9+640

HOR. SCALE - 1:500

STN: 8+500 TO: 9+640

CONTRACT NO:

DWG NO: PLAN 6

file: \\SHERIDAN\Shared Work Area\03425 Oxford Road 19\03_Production\03_053425 PLANS-1.dwg
Layout: PLAN 8+500 TO 9+640 date: Nov 10, 2022 15:55 user: Melemon



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	□ XCB
Prop. Catchbasin	■ CB
Ex. Clean Out	○ CO
Prop. Clean Out	● CO
Ex. Curb Stop	○ CS
Prop. Curb Stop	● CS
Ex. Ditch Inlet CB	□ XDICB
Prop. Ditch Inlet CB	■ DICB
Ex. Double Catchbasin	□ XDCB
Prop. Double CB	■ DCB
Ex. Fire Hydrant	○ XSAFH
Prop. Fire Hydrant	● SFH
Ex. Sanitary Manhole	○ XSTMH
Prop. Sanitary Manhole	● STMH
Ex. Storm Manhole	○ XSMH
Prop. Storm Manhole	● SMH
Ex. Water Valve	○ XWV
Prop. Water Valve	● WV
Ex. Cut Cross	✕
Ex. Sign	■
Ex. Guy Wire	---
Ex. Coniferous Shrub	✪
Ex. Deciduous Shrub	✪
Ex. Stump	✪
Ex. Coniferous Tree	✪
Ex. Deciduous Tree	✪

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:

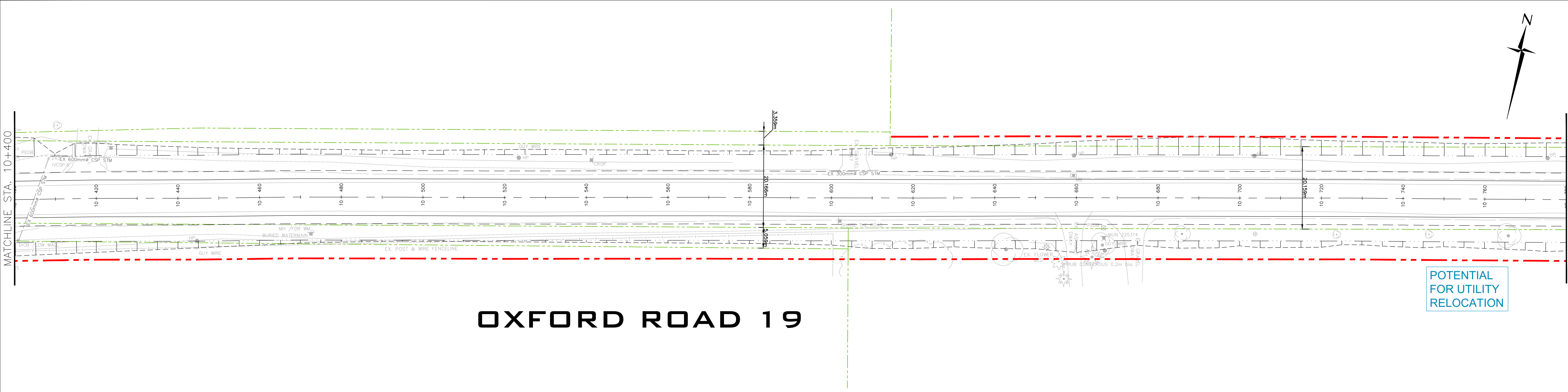
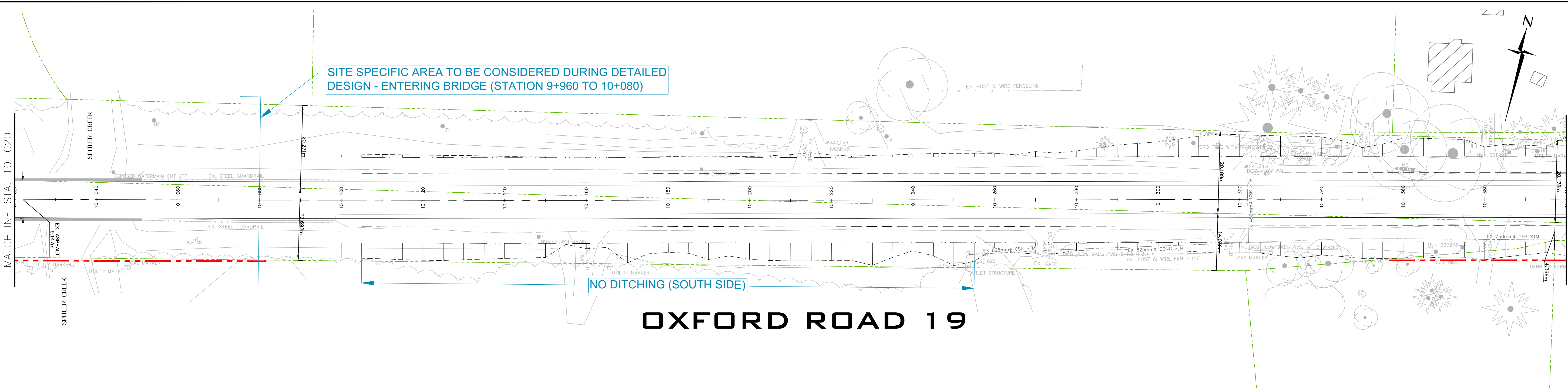
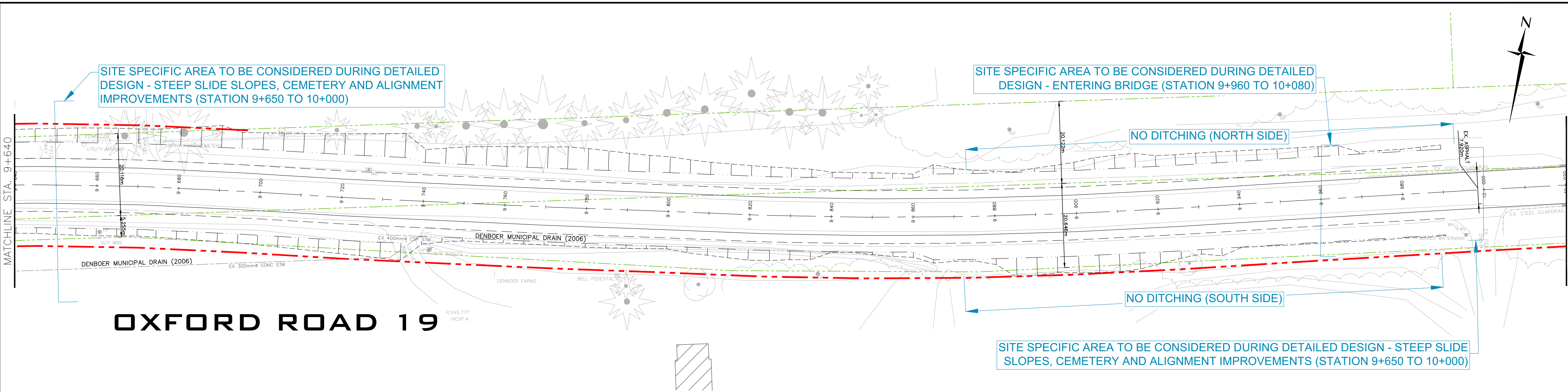
BENCHMARK #	ELEVATION=
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AREA MUNICIPALITY:

CONSULTANT:

DESIGN BY: HC	STAMP:
DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
PLAN 9+640 TO 10+780	
HOR. SCALE - 1:500	
STN: 9+640	TO: 10+780
CONTRACT NO:	
DWG NO: PLAN 7	

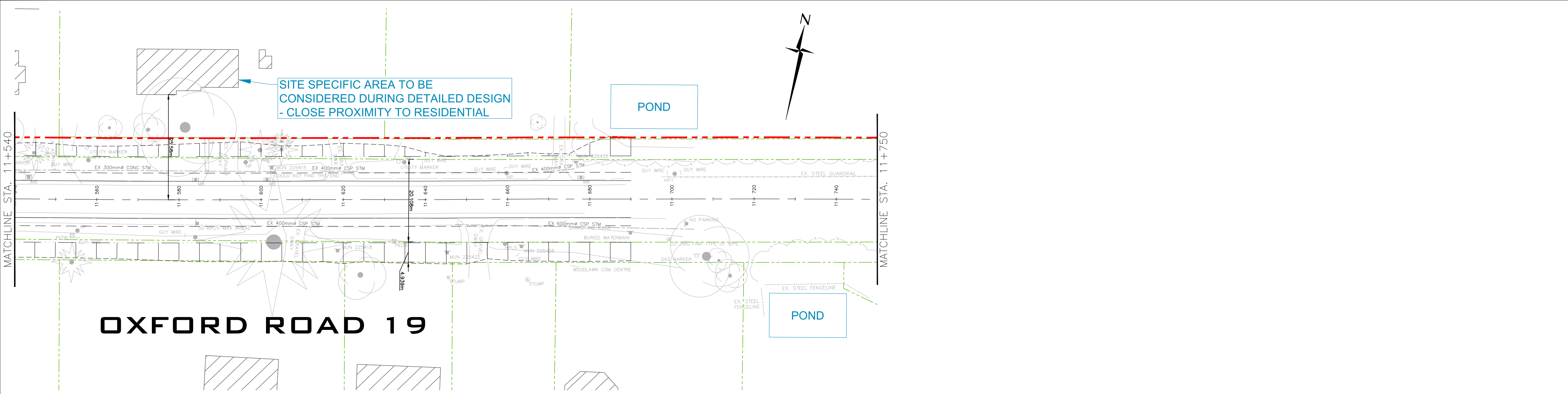
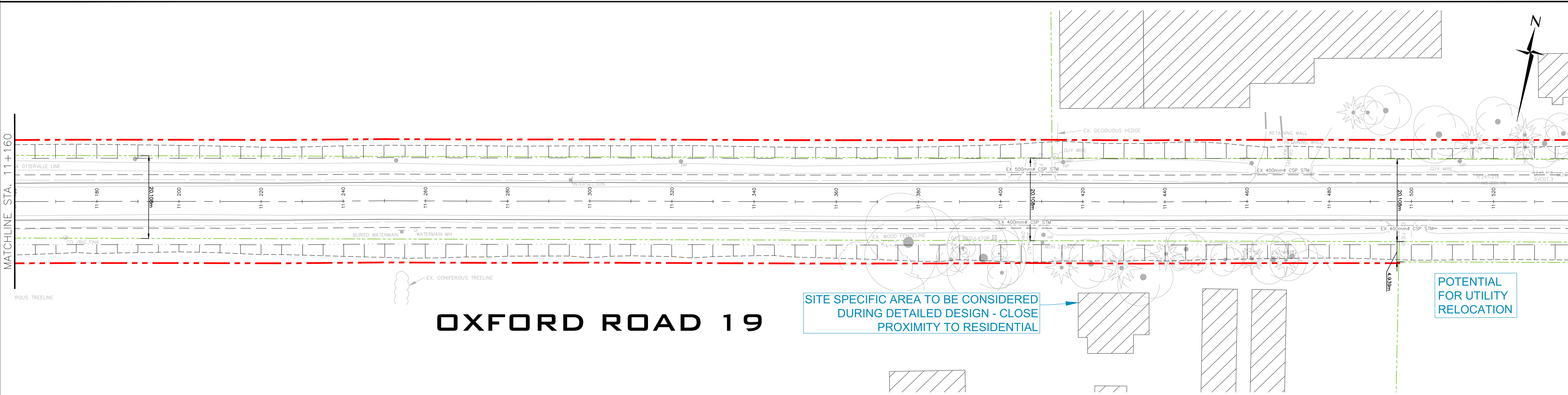
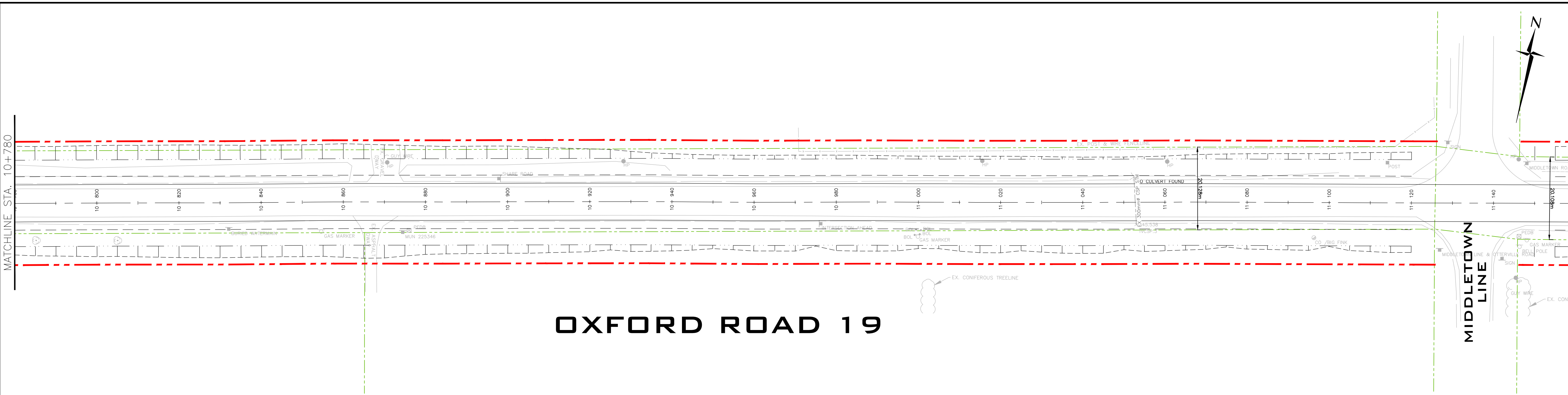


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Layout: PLAN 9+640 TO 10+780 date: Nov 10, 2022 15:56 user: MStrom



LEGEND:

Pr. Property Line	---
Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	CB
Ex. Clean Out	CO
Ex. Curb Stop	CS
Ex. Ditch Inlet CB	DICB
Ex. Double Catchbasin	DCB
Ex. Fire Hydrant	FH
Ex. Sanitary Manhole	SAMH
Ex. Storm Manhole	STMH
Ex. Water Valve	WV
Ex. Cut Cross	CC
Ex. Sign	S
Ex. Guy Wire	GW
Ex. Coniferous Shrub	CSH
Ex. Deciduous Shrub	DSH
Ex. Stump	ST
Ex. Coniferous Tree	CT
Ex. Deciduous Tree	DT
Prop. Catchbasin	CB
Prop. Clean Out	CO
Prop. Curb Stop	CS
Prop. Ditch Inlet CB	DICB
Prop. Double CB	DCB
Prop. Fire Hydrant	FH
Prop. Sanitary Manhole	SAMH
Prop. Storm Manhole	STMH
Prop. Water Valve	WV
Remove & Dispose	R&D



ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:

BENCHMARK #	ELEVATION=
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AREA MUNICIPALITY:

CONSULTANT:

DESIGN BY: HC

DRAWN BY: MS

CHECKED BY: AB

APPROVED BY: HC

DATE: NOV 10, 2022

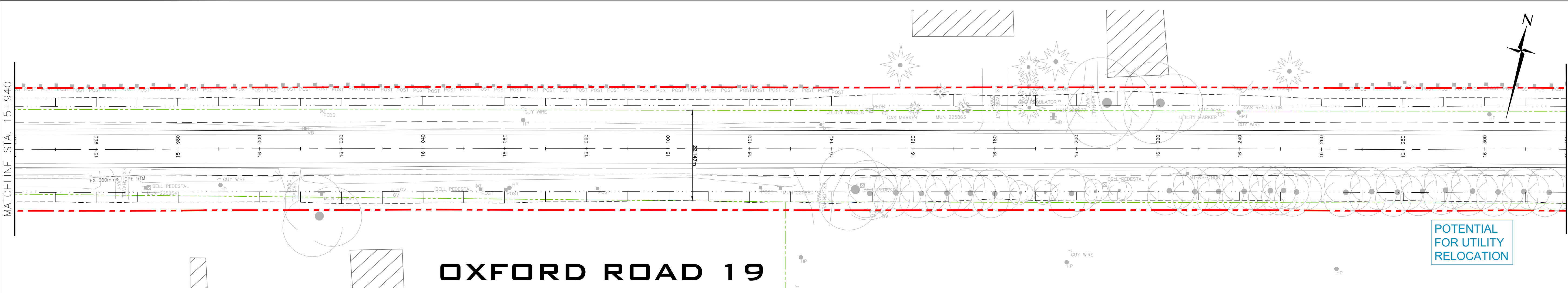
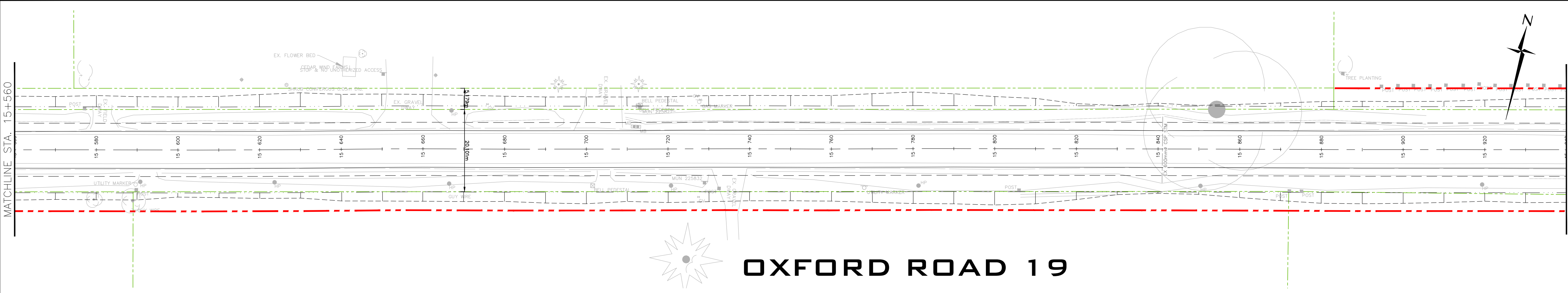
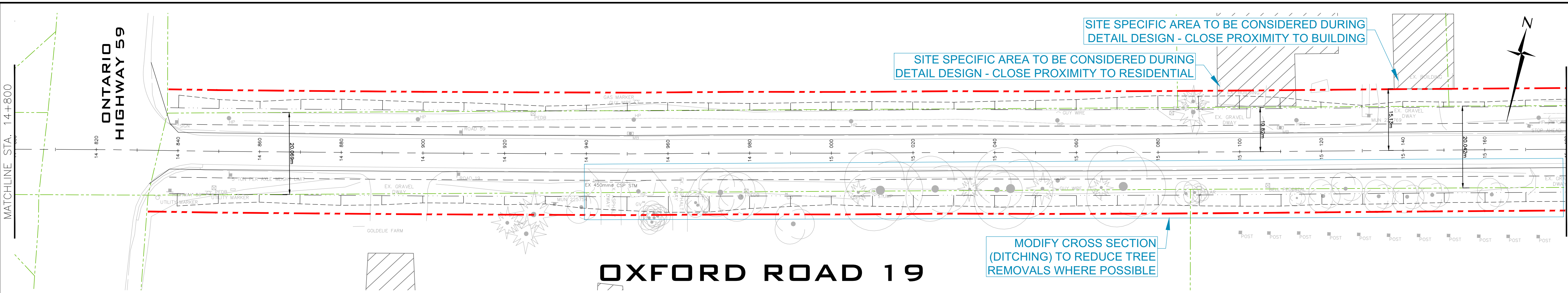
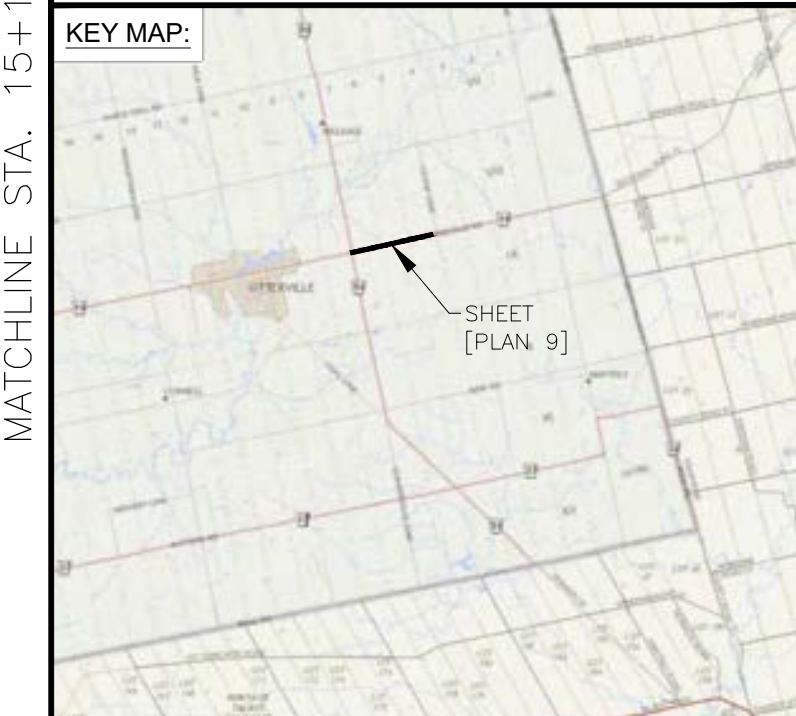
STAMP:

**OXFORD ROAD 19
ENVIRONMENTAL
ASSESSMENT**

PLAN 10+780 TO 11+750

HOR. SCALE - 1:500
STN: 10+780 TO: 11+750
CONTRACT NO:
DWG NO: PLAN 8

file: \\SHERIDAN\Shared Work Areas\053425 Oxford Road 19\03_ProductionDwg\053425 PLANS-1.dwg
Layout: PLAN 10+780 TO 11+750 Date: Nov 10, 2022 15:57 user: MSimon



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■ CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	● CO
Pr. Ditchline	---	Prop. Curb Stop	● CS
Pr. Edge of Pavement	---	Prop. Ditch Inlet CB	■ DICB
Ex. Property Line	---	Prop. Double CB	■ DCB
Ex. Centreline of Ditch	---	Prop. Fire Hydrant	● FHY
Ex. Sanitary Forceman	---	Prop. Sanitary Manhole	● SAMH
Ex. Sanitary Sewer	---	Prop. Storm Manhole	● STMH
Ex. Storm Sewer	---	Prop. Water Valve	● WV
Ex. U/G Telephone	---	Remove & Dispose	⊗
Ex. U/G Cable TV	---		
Ex. U/G Gas	---		
Ex. U/G Hydro	---		
Ex. U/G Traffic Wiring	---		
Ex. Watermain	---		
Ex. Catchbasin	□ XCB		
Ex. Clean Out	○ CO		
Ex. Curb Stop	○ CS		
Ex. Ditch Inlet CB	□ DICB		
Ex. Double Catchbasin	□ DCB		
Ex. Fire Hydrant	○ FHY		
Ex. Sanitary Manhole	○ SAMH		
Ex. Storm Manhole	○ STMH		
Ex. Water Valve	○ WV		
Ex. Cut Cross	⊗		
Ex. Sign	⊗		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊗		
Ex. Deciduous Shrub	⊗		
Ex. Stump	⊗		
Ex. Coniferous Tree	⊗		
Ex. Deciduous Tree	⊗		

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
BENCHMARK #		ELEVATION=m	

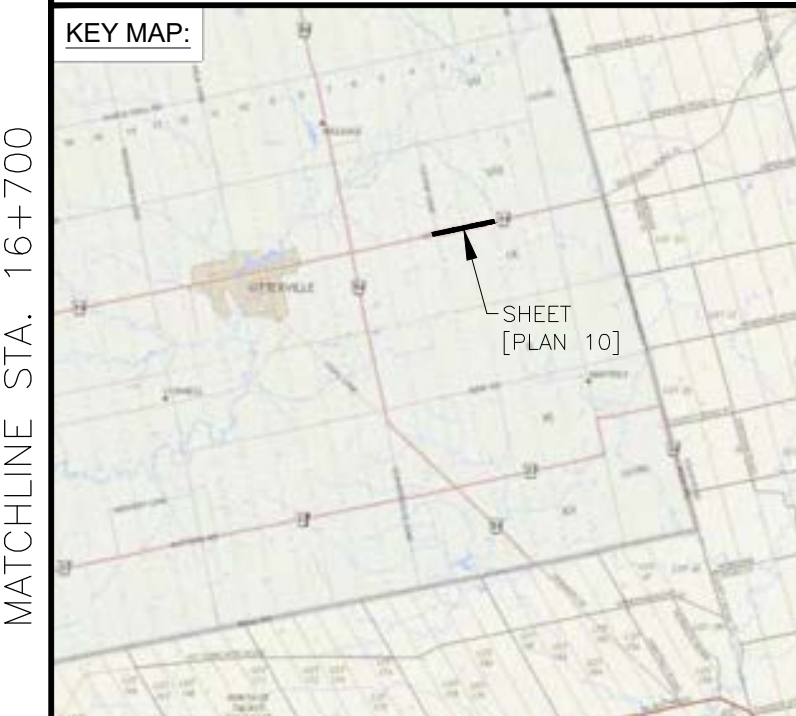
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CONSULTANT:

DESIGN BY: HC	STAMP:
DRAWN BY: MS	
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DATE: NOV 10, 2022	

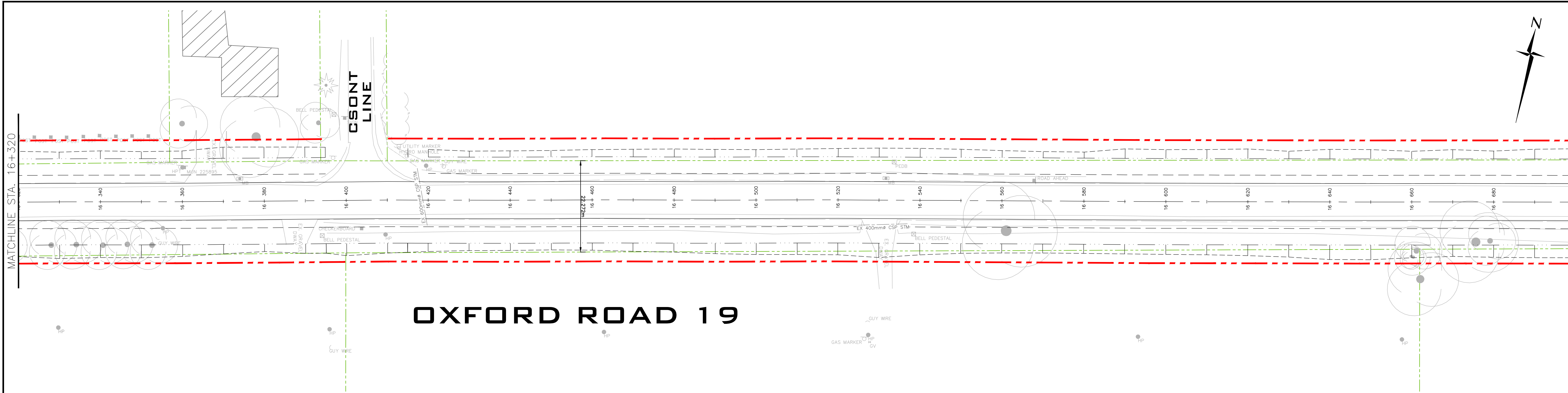
OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
PLAN 14+800 TO 16+320	
HOR. SCALE - 1:500	
STN: 14+800	TO: 16+320
CONTRACT NO:	
DWG NO: PLAN 9	

file: \\SHERIDAN\Shared Work Areas\053425 Oxford Road 19\03_ProductionDwg\053425 PLANS-1.dwg Date: Nov 10, 2022 15:59 user: MSimon

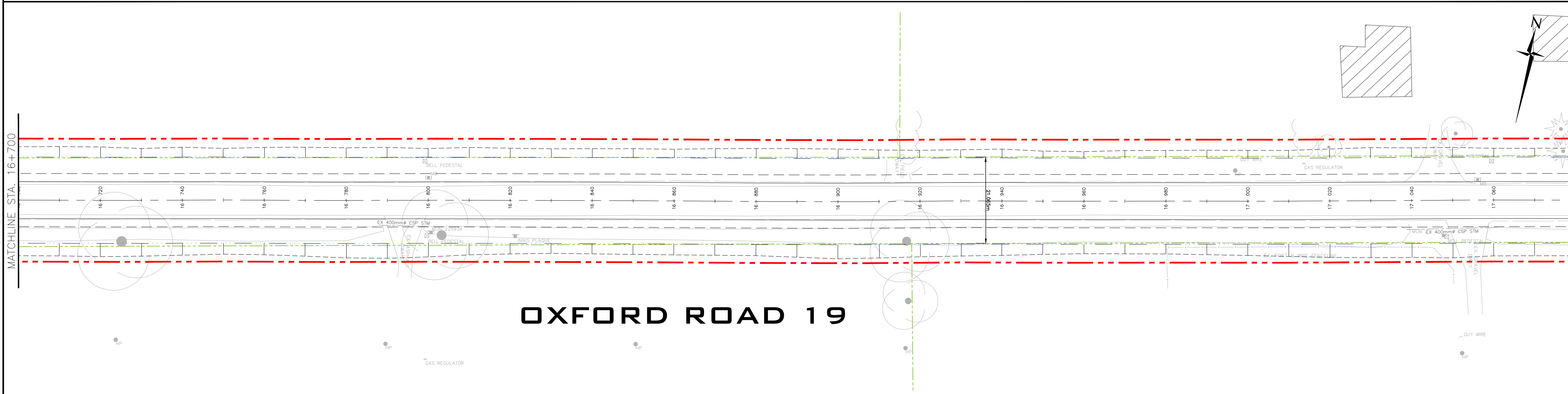


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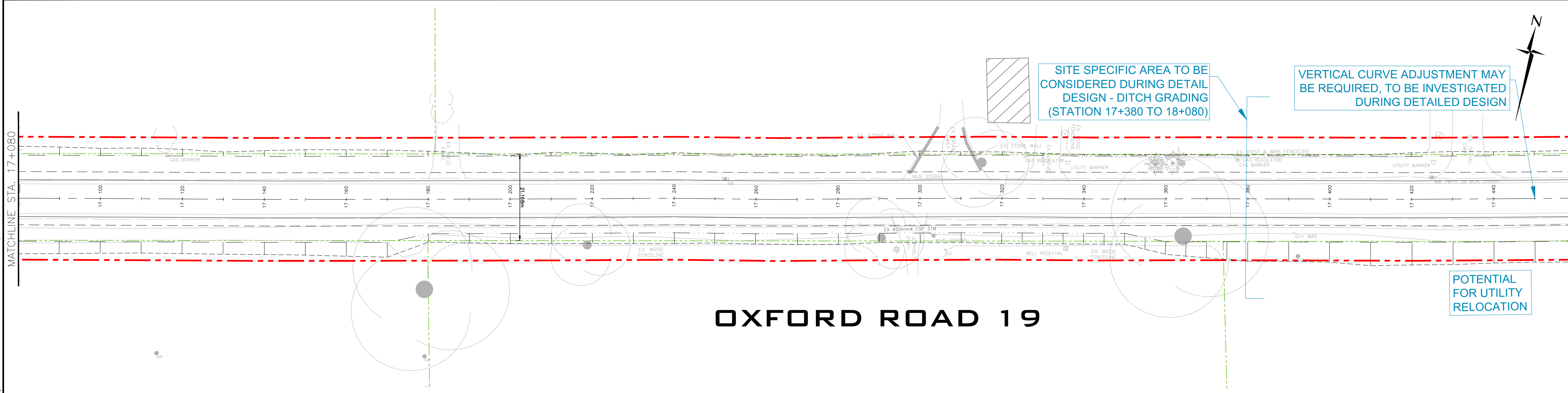
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Pr. Top Bank/Matchline	---
Pr. Ditchline	---
Pr. Edge of Shoulder	---
Pr. Edge of Pavement	---
Ex. Property Line	---
Ex. Centreline of Ditch	---
Ex. Sanitary Forcemain	---
Ex. Sanitary Sewer	---
Ex. Storm Sewer	---
Ex. U/G Telephone	T
Ex. U/G Cable TV	C
Ex. U/G Gas	G
Ex. U/G Hydro	H
Ex. U/G Traffic Wiring	TW
Ex. Watermain	---
Ex. Catchbasin	□ XCB
Ex. Clean Out	○ CO
Ex. Curb Stop	○ CS
Ex. Ditch Inlet CB	□ XDCB
Ex. Double Catchbasin	□ XDCB
Ex. Fire Hydrant	○ XSAFH
Ex. Sanitary Manhole	○ XSTMH
Ex. Storm Manhole	○ XSTMH
Ex. Water Valve	○ XWV
Ex. Cut Cross	✕
Ex. Sign	■
Ex. Guy Wire	---
Ex. Coniferous Shrub	⊗
Ex. Deciduous Shrub	⊗
Ex. Stump	⊗
Ex. Coniferous Tree	⊗
Ex. Deciduous Tree	⊗
Prop. Catchbasin	■ CB
Prop. Clean Out	● CO
Prop. Curb Stop	● CS
Prop. Ditch Inlet CB	■ DCB
Prop. Double CB	■ DCB
Prop. Fire Hydrant	● XSAFH
Prop. Sanitary Manhole	● XSTMH
Prop. Storm Manhole	● XSTMH
Prop. Water Valve	● XWV
Remove & Dispose	⊗



OXFORD ROAD 19



OXFORD ROAD 19



OXFORD ROAD 19

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:

BENCHMARK #	ELEVATION=m
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AREA MUNICIPALITY:

CONSULTANT:

DESIGN BY: HC	STAMP:
DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	

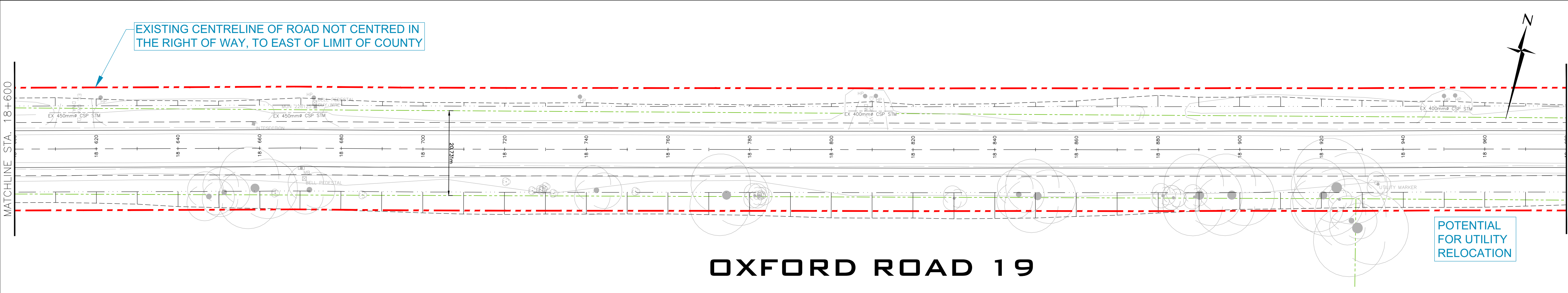
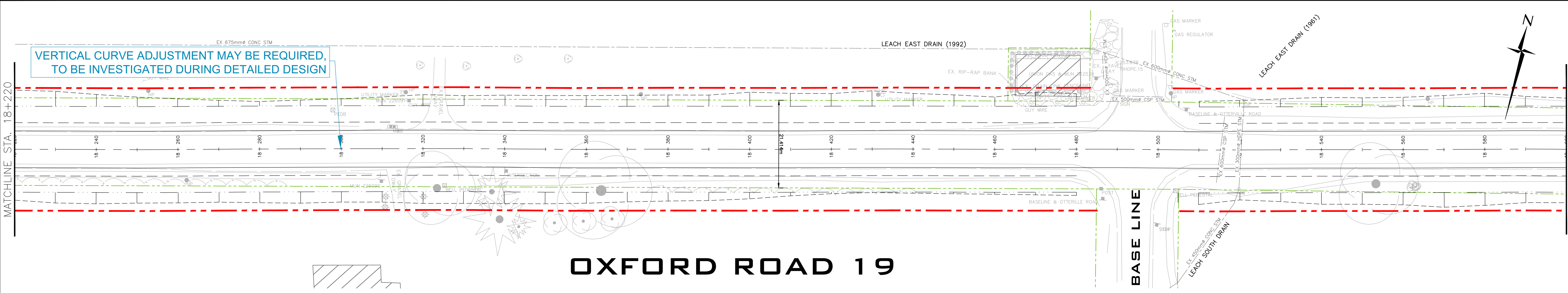
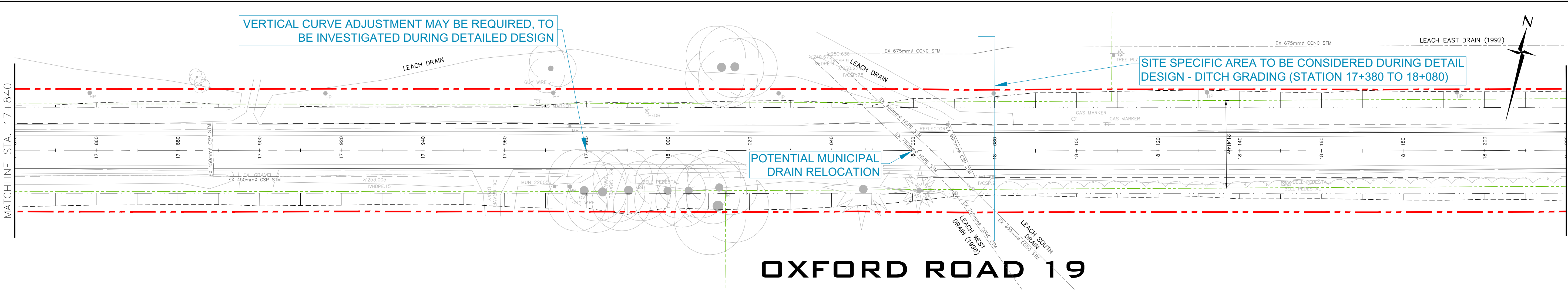
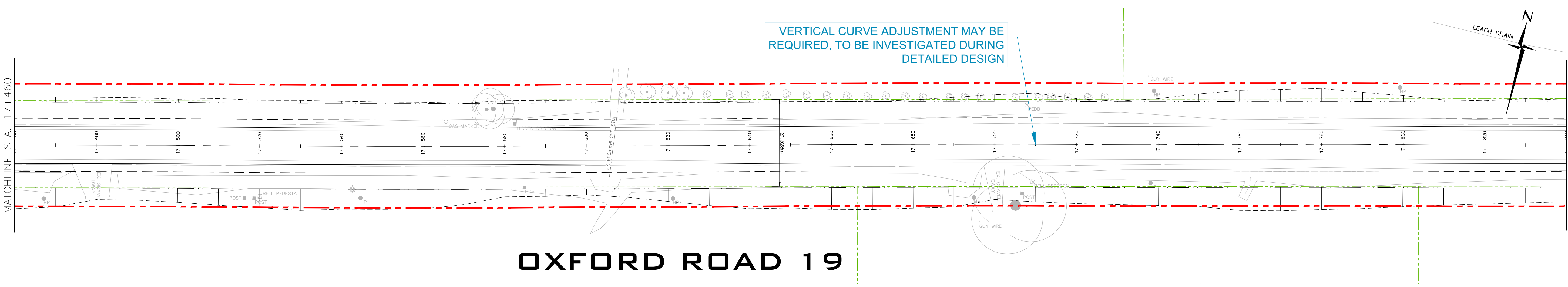
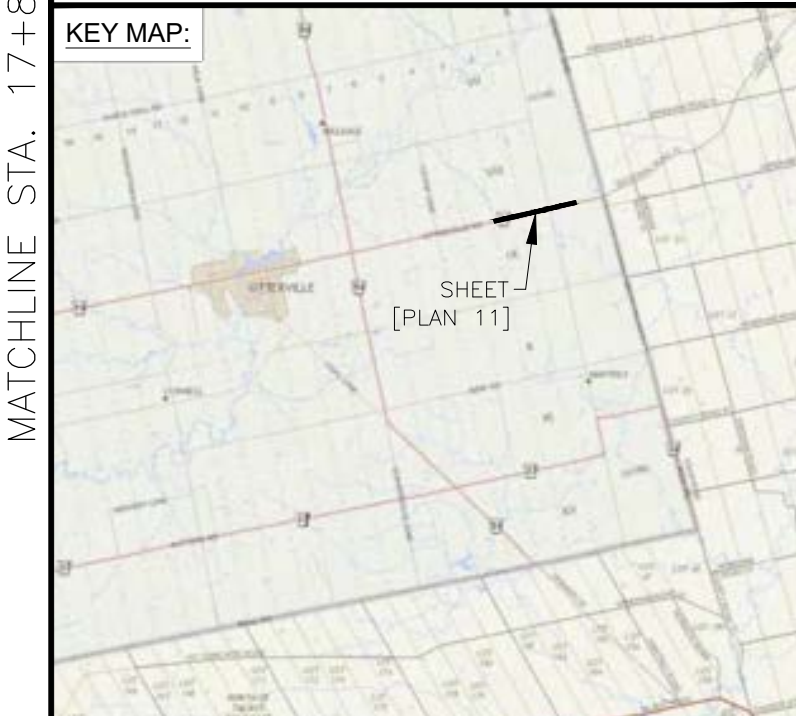
DATE: NOV 10, 2022

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT

PLAN 16+320 TO 17+460

HOR. SCALE - 1:500
STN: 16+320 TO: 17+460
CONTRACT NO:
DWG NO: PLAN 10

file: \\SHERIDAN\Shared Work Areas\053425 - Oxford Road 19\03_Production\Draw\053425 PLANS-1.dwg
Layout: PLAN 16+320 TO 17+460 Date: Nov 10, 2022 16:00 User: MSimon



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	CO
Pr. Ditchline	---	Prop. Curb Stop	CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	DICB
Pr. Edge of Pavement	---	Prop. Double CB	DCB
Ex. Property Line	---	Prop. Fire Hydrant	FH
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	SAMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	WV
Ex. Storm Sewer	---	Remove & Dispose	RD
Ex. U/G Telephone	T		
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	XCB		
Ex. Clean Out	CO		
Ex. Curb Stop	CS		
Ex. Ditch Inlet CB	XDICB		
Ex. Double Catchbasin	XDCB		
Ex. Fire Hydrant	FH		
Ex. Sanitary Manhole	SAMH		
Ex. Storm Manhole	STMH		
Ex. Water Valve	WV		
Ex. Cut Cross	---		
Ex. Sign	---		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	---		
Ex. Deciduous Shrub	---		
Ex. Stump	---		
Ex. Coniferous Tree	---		
Ex. Deciduous Tree	---		

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY
CONTROL POINTS:			
BENCHMARK #		ELEVATION=m	

AREA MUNICIPALITY:

CONSULTANT:

DESIGN BY: HC	STAMP:
DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

OXFORD ROAD 19 ENVIRONMENTAL ASSESSMENT	
PLAN 17+460 TO 18+980	
HOR. SCALE - 1:500	
STN: 17+460	TO: 18+980
CONTRACT NO:	
DWG NO: PLAN 11	

file: \\SHERIDAN\Shared Work Area\053425 Oxford Road 19\03_ProductionDwg\053425 PLANS-1.dwg
Layout: PLAN 17+460 TO 18+980 date: Nov 10, 2022 16:02 user: MSemion



LEGEND:

Pr. Property Line	---	Prop. Catchbasin	■ CB
Pr. Top Bank/Matchline	---	Prop. Clean Out	● CO
Pr. Ditchline	---	Prop. Curb Stop	● CS
Pr. Edge of Shoulder	---	Prop. Ditch Inlet CB	■ DICB
Pr. Edge of Pavement	---	Prop. Double CB	■ DCB
Ex. Property Line	---	Prop. Fire Hydrant	● SAMH
Ex. Centreline of Ditch	---	Prop. Sanitary Manhole	● SAMH
Ex. Sanitary Forcemain	---	Prop. Storm Manhole	● STMH
Ex. Sanitary Sewer	---	Prop. Water Valve	● W
Ex. Storm Sewer	---	Remove & Dispose	⊗
Ex. U/G Telephone	T		
Ex. U/G Cable TV	C		
Ex. U/G Gas	G		
Ex. U/G Gas	G		
Ex. U/G Hydro	H		
Ex. U/G Traffic Wiring	TW		
Ex. Watermain	---		
Ex. Catchbasin	□ XCB	Prop. Catchbasin	■ CB
Ex. Clean Out	⊙ CO	Prop. Clean Out	● CO
Ex. Curb Stop	⊙ CS	Prop. Curb Stop	● CS
Ex. Ditch Inlet CB	□ XDICB	Prop. Ditch Inlet CB	■ DICB
Ex. Double Catchbasin	□ XDCB	Prop. Double CB	■ DCB
Ex. Fire Hydrant	⊙ XSAMH	Prop. Fire Hydrant	● SAMH
Ex. Sanitary Manhole	⊙ XSTMH	Prop. Sanitary Manhole	● SAMH
Ex. Storm Manhole	⊙ XSTMH	Prop. Storm Manhole	● STMH
Ex. Water Valve	⊙ W	Prop. Water Valve	● W
Ex. Cut Cross	⊗	Remove & Dispose	⊗
Ex. Sign	⊗		
Ex. Guy Wire	---		
Ex. Coniferous Shrub	⊙		
Ex. Deciduous Shrub	⊙		
Ex. Stump	⊙		
Ex. Coniferous Tree	⊙		
Ex. Deciduous Tree	⊙		

ISSUED FOR CLIENT REVIEW	2	NOV 10, 2022	HC
ISSUED FOR CLIENT REVIEW	1	OCT 18, 2022	HC
ISSUED FOR REVISIONS	No.	DATE	BY

CONTROL POINTS:

BENCHMARK #	ELEVATION=m
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AREA MUNICIPALITY:

CONSULTANT:

DESIGN BY: HC	STAMP:
DRAWN BY: MS	
CHECKED BY: AB	
APPROVED BY: HC	
DATE: NOV 10, 2022	

**OXFORD ROAD 19
ENVIRONMENTAL
ASSESSMENT**

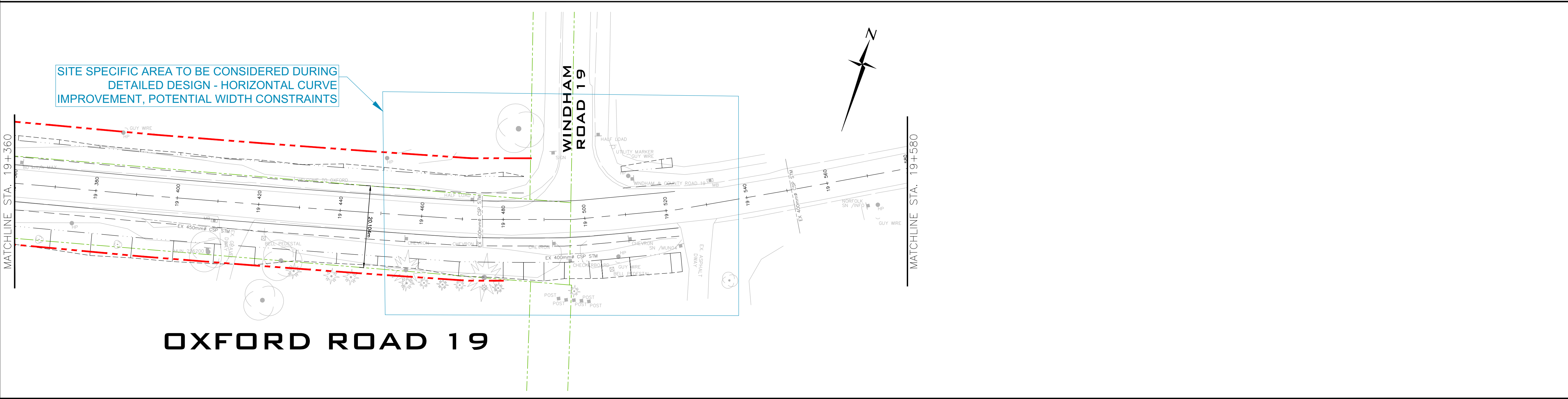
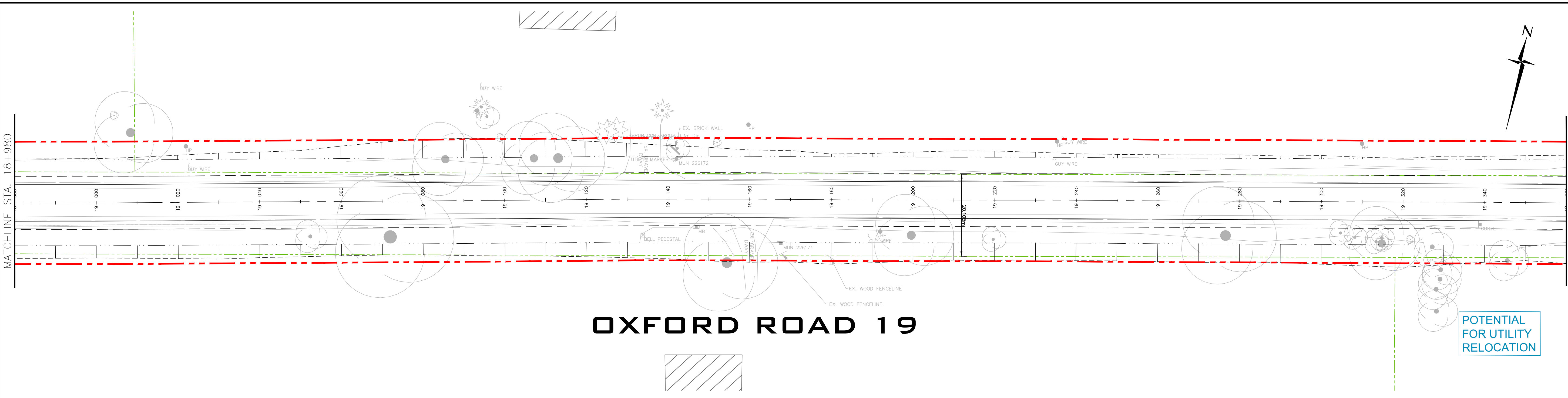
PLAN 18+980 TO 19+580

HOR. SCALE - 1:500

STN: 18+980 TO: 19+580

CONTRACT NO:

DWG NO: PLAN 12



file: \\SHERLOCK\Shared Work Area\053425 Oxford Road 19\03_ProductionDwg\053425 PLANS-1.dwg
Layout: PLAN 18+980 TO 19+580 Date: Nov 10, 2022 16:03 user: MSemion



BURNSIDE

[THE DIFFERENCE IS OUR PEOPLE]

Appendix H6

Consultation with Utilities

Sylvia Waters

From: Jesse Keith <jkeith@oxfordcounty.ca>
Sent: Friday, March 18, 2022 2:51 PM
To: 'Cliff Lee'
Cc: Sylvia Waters; Henry Centen; Tricia Radburn
Subject: RE: 053425-Agency - Notice of Commencement for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

Hi Cliff,

Thanks for confirming. We will remove you from the study contact list as requested.

Best Regards,

JESSE KEITH, P. ENG. (HE / HIM) | Project Engineer, Public Works

OXFORD COUNTY | 21 Reeve St., PO Box 1614, Woodstock, ON, N4S 7Y3

WWW.OXFORDCOUNTY.CA | T 519.539.9800 / 1-800-755-0394, ext 3194



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 Think about our environment. Print only if necessary.

From: Cliff Lee <clee@tnpi.ca>

Sent: March 17, 2022 10:00 AM

To: Sylvia Waters <Sylvia.Waters@rjburnside.com>; FisheriesProtection@dfo-mpo.gc.ca;
neil.mazey@canadapost.postescanada.ca; jennifer.ormsby@opp.ca; drew.crinklaw@ontario.ca;
karla.barboza@ontario.ca; damian.dupuy@ontario.ca; erick.boyd@ontario.ca; jennifer.grahamharkness@ontario.ca;
allan.hodgins@ontario.ca; eanotification.swregion@ontario.ca; mark.badali1@ontario.ca;
Karina.Cerniavskaja@ontario.ca; MNRF Ayl Planners (MNRF) <MNRF.Ayl.Planners@ontario.ca>; Michael Duben
<mduben@oxfordcounty.ca>; Chloe Senior <cseior@oxfordcounty.ca>; Julie Forth <clerk@swox.org>;
al.meneses@norfolkcounty.ca; teresa.olsen@norfolkcounty.ca; Kim Armstrong <karmstrong@norwich.ca>;
CentralFBCPlanning@HydroOne.com; SecondaryLandUse@HydroOne.com; jeff.soetemans@execulink.com;
swo@tdlcanada.ca; nordel@nor-del.com; scott.moon@bell.ca; rowcentre@bell.ca; Rogers.MOC@telecon.ca;
azocco@uniongas.com; Kevin.Schimus@enbridge.com; vince.cina@enbridge.com; notifications@enbridge.com; mark-ups@enbridge.com;
dpresley@mhbcpplan.com; crossingrequesteast@tnpi.ca; azocco@uniongas.com;
Utility.Circulations@zayo.com; stefan.linder@cn.ca; michael.vallins@cn.ca; jeff.willsie@ontsouthland.com;
brad.jolliffe@ontsouthland.com; greg.rankin@ontsouthland.com; Brian_Costigan@cpr.ca

Cc: Jesse Keith <jkeith@oxfordcounty.ca>

Subject: RE: 053425-Agency - Notice of Commencement for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

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Sylvia Waters

From: phil.arbeau@zayo.com on behalf of Utility Circulations <utility.circulations@zayo.com>
Sent: Monday, March 28, 2022 7:07 AM
To: Sylvia Waters
Subject: Re: 053425-Agency - Notice of Commencement for Oxford Road 19 Corridor Improvement in Oxford County Schedule C Municipal Class Environmental Assessment Study

Good morning,

Zayo has no existing plant in the area indicated in your submission. No markup and no objection. Thank you.

Phil Arbeau
Utility Circulations

On Thu, 17 Mar 2022 at 08:54, Sylvia Waters <Sylvia.Waters@rjburnside.com> wrote:

Oxford County (County) is initiating a Schedule C Municipal Class Environmental Assessment Study for **Oxford Road 19 Corridor Improvement** in Oxford County. Following the completion of the Oxford County's 2019 Transportation Master Plan, the County identified the need to improve Oxford Road 19 to support the safe and efficient movement of people and goods. Please see the attached Notice of Commencement.



R.J. Burnside & Associates Limited
128 Wellington Street West, Suite 301, Barrie, Ontario L4N 8J6
Office: +1 800-265-9662 Direct Line: +1 705-797-4379
www.rjburnside.com



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Thank you.

Sylvia Waters

From: mark-ups <Mark-Ups@enbridge.com>
Sent: Monday, November 28, 2022 1:35 PM
To: Sylvia Waters
Subject: RE: 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

For utility requests in legacy Union Gas franchise area, please submit a Design and Planning request through Ontario One Call at www.ontarioonecall.ca or [1-800-400-2255](tel:1-800-400-2255).

More information about submitting a Design and Planning request can be found here:
https://www.ontarioonecall.ca/wp-content/uploads/Design_and_Planning_Best_Practice_2020.pdf



Enbridge Gas Inc. Former Franchise Areas



From: Sylvia Waters <Sylvia.Waters@rjburnside.com>
Sent: Thursday, November 17, 2022 12:13 PM
To: FisheriesProtection@dfo-mpo.gc.ca; neil.mazey@canadapost.postescanada.ca; jennifer.ormsby@opp.ca; dana.kieffer@ontario.ca; omafra.eanotices@ontario.ca; joseph.harvey@ontario.ca; Karla Barboza (karla.barboza@ontario.ca) <karla.barboza@ontario.ca>; damian.dupuy@ontario.ca; erick.boyd@ontario.ca; jennifer.grahamharkness@ontario.ca; Martin.leyten@ontario.ca; Kilgore, Michael (MTO) <Michael.Kilgore@ontario.ca>; eanotification.swregion@ontario.ca; Badali, Mark (MECP) <mark.badali1@ontario.ca>; SAROntario@ontario.ca; brandan.norman2@ontario.ca; Jason.Webb@ontario.ca; MNRF.Ayl.Planners@ontario.ca; Karina.Cerniavskaja@ontario.ca; mdublen@oxfordcounty.ca; csenior@oxfordcounty.ca; planning@oxfordcounty.ca;

hstclair@oxfordcounty.ca; egilbert@oxfordcounty.ca; svanacker@oxfordcounty.ca; rdavis@oxfordcounty.ca; fgross@oxfordcounty.ca; mabercrombie@oxfordcounty.ca; dsimpson@oxfordcounty.ca; ems@oxfordcounty.ca; mayor@swox.org; cao@swox.org; drainage@swox.org; aprouse@swox.org; firechief@swox.org; clerk@swox.org; al.meneses@norfolkcounty.ca; teresa.olsen@norfolkcounty.ca; mike.king@norfolkcounty.ca; lmartin@norwich.ca; kkruger@norwich.ca; karmstrong@norwich.ca; dkramer@norwich.ca; kfarkas@norwich.ca; dvanpagee@norwich.ca; clerks@tillsonburg.ca; lmauthe@lprca.on.ca; ijohnson@lprca.on.ca; CentralFBCPlanning@HydroOne.com; SecondaryLandUse@HydroOne.com; gcsont1@execulink.com; jeff.soetemans@execulink.com; swo@tdlcanada.ca; nordel@nor-del.com; richard.de_bokx@bell.ca; rowcentre@bell.ca; Rogers.MOC@telecon.ca; Amanda Perzia <Amanda.Zocco@enbridge.com>; Amanda Perzia <Amanda.Zocco@enbridge.com>; Adam Collier <Adam.Collier@enbridge.com>; Kevin Schimus <Kevin.Schimus@enbridge.com>; vince.cina@enbridge.com; Notifications <Notifications@enbridge.com>; mark-ups <Mark-Ups@enbridge.com>; dpresley@mhbcplan.com; TCenergy@mhbcplan.com

Cc: jkeith@oxfordcounty.ca; Henry Centen <Henry.Centen@rjburnside.com>; Avid Banihashemi <Avid.Banihashemi@rjburnside.com>

Subject: [External] 053425-Agency - Notice of Public Consultation Centre #2 - Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study

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DO NOT click links or open attachments unless you are 100% sure that the email is safe.

On behalf of Oxford County, please see attached Notice of Public Consultation Centre # 2 (PCC) for Oxford Road 19 Corridor Improvement, Municipal Class Environmental Assessment Study.

The PCC #2 will be a drop-in format to provide residents / interested parties with an opportunity to review and comment on the recommended preferred conceptual design. Representatives from the County and its Consultant (R.J. Burnside & Associates Limited) will be present to answer questions and discuss next steps in the study. The date and location of the PCC #2 are as follows:

Date: Tuesday December 6, 2022
Time: 5:00 p.m. - 7:00 p.m.
Location: Springford Community Hall, 429 Main St. W., Springford, Ontario

If you have questions or comments, please contact either of the following project team members:

Jesse Keith, P.Eng., Project Manager
Oxford County Public Works
519-539-9800 ext.3194
jkeith@oxfordcounty.ca

Henry Centen, P.Eng.,
Project Manager
R.J. Burnside & Associates Limited
519-340-2003
henry.centen@rjburnside.com



Sylvia Waters
Technical Administrator, EPA

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Thank you.

